

Sacramento County Department of Transportation



Americans with Disabilities Act Transition Plan Including Bus Stop Transition Plan Supplement



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Section 1: Executive Summary

Introduction

The Sacramento County Department of Transportation (SacDOT) began working on the Americans with Disabilities Act (ADA) Transition Plan and Pedestrian Master Plan project in April 2002. The main purpose of this project is to develop the County's policies and practices for implementing physical pedestrian improvements within the public right-of-way of the County's unincorporated areas. The goal is to optimize the pedestrian experience, to provide safe and usable pedestrian facilities for all pedestrians, and to assure compliance with all federal, state, and local regulations and standards.

The ADA Transition Plan and the Pedestrian Master Plan are on a parallel schedule, but have separate adoption processes and community advisory committees. The ADA Transition Plan is intended to represent both the legal and functional goals and objectives of the County to make the existing pedestrian facilities within the unincorporated County right-of-way accessible and usable for persons with disabilities. SacDOT is undertaking the Pedestrian Master Plan to enhance walking as a viable transportation choice to help make Sacramento County a better place to live. The Pedestrian Design Guidelines, as part of the Pedestrian Master Plan, will address new design standards to make facilities better for all pedestrians.

The ADA requires all public agencies to develop an ADA Transition Plan for the installation of curb ramps or other sloped areas at all locations where walkways cross curbs. The plan must include a schedule for curb ramp installation and for other improvements necessary to achieve programmatic accessibility for persons with disabilities. The main purpose of this ADA Transition Plan is to describe the curb ramp and other pedestrian facility needs in the unincorporated County, and to outline the recommended procedures for implementing and scheduling remedial work to provide a complying system of curb ramps, pedestrian signals and sidewalks.

The ADA Transition Plan and the Pedestrian Master Plan covers the unincorporated Sacramento County. This area is surrounded by incorporated cities to the north, east and south and by the Sacramento River to the west. Most of the developed areas are located within the middle of the County between the City of Sacramento and the cities of Citrus Heights, Folsom and Rancho Cordova. SacDOT has a wide variety of facilities within the public right-of-way. These facilities include streets and roadways, vehicular and pedestrian bridges, underground and above-ground utilities, vehicular and pedestrian signal systems, signage systems, on-street parking facilities, sidewalks with curb ramps at intersections, planting strips and buffers, pedestrian activity areas and unimproved open spaces.

Public Participation

SacDOT set up the ADA Transition Plan and Pedestrian Master Plan project to encourage and facilitate the maximum degree of public participation. This process included persons with disabilities and those representing disability service organizations. The outreach efforts included the following components:

- **Advisory Groups:** Two advisory committees, the ADA Community Advisory Group (CAG) and the Technical Advisory Committee (TAC) were formed and met regularly over a period of one and a half years to review documents and to provide feedback. Both groups contained members of the local disability community.
- **Outreach to Persons with Visual Impairments:** The ADA Transition Plan will be made available to persons who are visually impaired via text document and Braille master copy. Persons with visual impairments who have access to software that converts text to audio will be provided the document via e-mail, floppy disks or CDs. Sacramento Access News (SAN) has a free telephone reader service for individuals who are blind or with visual impairments that includes information on the ADA Transition Plan.
- **Community Planning Advisory Councils (CPAC):** SacDOT presented the scope of work for the project to the interested CPACs in the unincorporated Sacramento County, which equaled over ten. Attendees of the CPAC meetings were given an opportunity for input into the process.
- **Consumer Survey:** SacDOT developed and distributed a pedestrian and disabled access consumer survey to identify hotspot locations or physical barriers to walking.
- **Press Releases:** The study team created and distributed three press releases to cover the following topics: project kick-off, consumer surveys and transportation fairs.
- **Transportation Fairs:** SacDOT staff and consultants described the project's activities in hands-on settings at four separate venues throughout the unincorporated County.
- **Web Site:** A project web-site was created to disseminate information about the project.
- **Electronic Newsletter:** Electronic newsletters and announcements were distributed to the public via the project list-serve.

The community will be able to submit formal comments about this ADA Transition Plan, either in written form or at a public hearing, per ADA requirements.

Inventory Efforts

SacDOT conducted a five-month survey of pedestrian facilities to document existing conditions within the public rights-of-way. These data were used to recommend improvements to pedestrian facilities and to comply with ADA and State Title 24 requirements and County approved policies. Surveying refers to visiting the particular location by a trained accessibility surveyor, and obtaining measurements, dimensions, gradients or other visual determinations as may be appropriate depending on the particular location. Highlights of the survey process and inventory findings are listed below:

- Approximately 2,200 miles of streets and roadways covering over 15,000 individual segments of roadway boundaries were traveled and surveyed to document physical conditions along the roadways, including conditions that might be barriers to persons with disabilities.
- The inventory focused on more heavily used roadways and intersections and on those roadways and intersections serving governmental, public service and commercial uses.
- For roadways surveyed, approximately 75 percent of County roadways have sidewalks on one or both sides, and 25 percent do not have sidewalks on either side of the street.
- Approximately 11,000 intersections or almost 44,000 street corners were surveyed, and measurements were taken for a variety of dimensions and gradients.
- Approximately 66 percent of all corners surveyed have rolled curbs, approximately 16 percent have vertical curbs, and 18 percent do not have curbs.
- Approximately 41 percent of all developed corners have curb ramps. Of these, approximately 57 percent were older perpendicular curb ramps with flared sides and approximately 40 percent were newer parallel pan-type curb ramps.

All survey findings are contained in a Microsoft Access database titled the ADA Public Rights-of-Way Database. The database is designed to be user-friendly, with interactive screens available to access the summary report, reports for each individual intersection or roadway segment and photographs. The database also includes data entry screens to be used for monitoring construction activity and producing status reports.

ADA Codes and Standards

SacDOT and the study team developed the ADA Codes and Standards for all facilities within the public right-of-way in the unincorporated County. The standards are intended to apply to all construction undertaken within the unincorporated County right-of way after the final approval of the ADA Transition Plan. The ADA CAG and the TAC reviewed three versions of the ADA Codes and Standards. SacDOT submitted the final draft document for review on April 25, 2003. This final draft version was reviewed at ADA CAG and TAC meetings on June 2, 2003.

The ADA Codes and Standards were developed to combine and resolve any conflicts between the *Americans with Disabilities Act Accessibility Guidelines* (ADAAG), published by the U.S. Architectural and Transportation Barriers Compliance Board in July 1991, and the *California State Building Code, Title 24, Part 2*, of the California Code of Regulation, 2001 edition. *Draft Guidelines for Public Rights-of-Way*, published by the U.S. Architectural and Transportation Barriers Compliance Board on June 17, 2002, which are expected to take effect in the future, also were considered in the ADA Codes and Standards. In addition, all County of Sacramento Board of Supervisor approved policies and standards affecting accessibility in the public right-of way were included in the standards.

ADA Capital Implementation Plan

The ADA Capital Implementation Plan was developed to describe the extent of County-operated and other participants' projects necessary to implement the ADA Transition Plan within the unincorporated County public rights-of-way.

Types of projects included in the ADA Capital Implementation Plan can be categorized as follows:

- Curb ramp construction or replacement projects based upon resident requests.
- Curb ramp, sidewalk and intersection retrofit projects included with street overlay or other street or sidewalk construction projects.
- Curb ramp, sidewalk and intersection retrofit projects, in conjunction with construction by private parties.
- Curb ramp, sidewalk and intersection retrofit projects deemed essential for mitigation of barriers based upon the finalized ADA Transition Plan.
- Street and sidewalk construction or retrofit projects planned for the improvement of overall pedestrian facilities.
- Signal retrofit projects.
- Roadway widening projects.

A number of existing and potential programs and funding sources for capital improvement projects were evaluated. These programs included on-going SacDOT capital improvement and maintenance programs, as well as specific projects and funding sources allocated in the seven year Transportation Improvement Plan (TIP). The ADA Capital Implementation Plan uses, to the maximum extent possible, existing and prospective funding programs and sources. The plan recommends specific goals for the construction of accessibility improvements. While specifying locations and the scope of work required at these locations, the plan also is intended to serve as a conceptual plan whereby the extent of future projects can be evaluated prior to preparing detailed cost estimates. Once an overall scope of work and its financial impact is established, annual projects can be finalized and the exact number of specified improvements can be set as project goals.

Curb ramps should be installed at all locations where they are missing and necessary for the full usage of the overall pedestrian path of travel, including at mid-block crosswalks. Older non-conforming curb ramps that pose potential hazards to wheelchair users should be repaired, upgraded or replaced. Some of these curb ramps may be ineffective or even dangerous due to steep slopes, narrow widths, high gutter lips and offset locations that require users to enter and exit streets outside of crosswalks. In addition to curb ramp construction and replacement, crosswalks, pedestrian signals and sidewalks serving each selected intersection should be evaluated for compliance with the ADA Codes and Standards and upgraded where necessary.

The ADA Capital Implementation Plan includes a detailed and prioritized list of approximately 1,800 project locations and items of work, which have been reviewed by SacDOT, the ADA CAG and the TAC. This implementation plan, which targets higher priority uses, anticipates a

15- to 20-year implementation period to achieve compliance with program accessibility requirements. Additional ADA work, such as new construction and additional curb ramps beyond the minimum program access requirements, will continue beyond the timeframe identified above.

Monitoring and Status Reporting

SacDOT currently is engaged in an on-going effort to construct curb ramps, sidewalks and other facilities at numerous locations within the unincorporated County. This construction activity involves several types of projects, including street overlay projects, street beautification projects, utility construction projects and other capital improvement projects in the public right-of-way. In addition, when this ADA Transition Plan is approved and implemented, more curb ramps and related improvements will be constructed.

While it is important to assure that codes and standards used to design and construct curb ramps and related improvements are up-to-date, it is equally important that improvements are constructed properly and in compliance with all applicable codes and standards. Therefore, the monitoring of construction activities and the reporting of the status of improvements is vital in assuring an effective overall compliance program.

The ADA Transition Plan details the methods and procedures for monitoring these construction activities and for tracking the status of compliance with the plan at all construction locations within the unincorporated County.

Section 2: Introduction and Administrative Information

Section 2.1: Introduction to the ADA

The Americans with Disabilities Act (ADA), enacted on July 26, 1990, provides comprehensive civil rights protections to persons with disabilities in the areas of employment, state and local government services, access to public accommodations, transportation and telecommunications. The ADA is companion civil rights legislation with the Civil Rights Act of 1964 and Section 504 of the Rehabilitation Act of 1973. This legislation mandates that qualified disabled individuals shall not be excluded from participation in, denied the benefit of, or be subjected to discrimination under any program or activity. The Act also protects employees with disabilities, with certain protections and requires employers to make reasonable accommodation for applicants and employees with disabilities.

The ADA is divided into five parts, covering the following areas:

Title I: Employment

Under Title I, employers, including governmental agencies, must ensure that their practices do not discriminate against persons with disabilities in the application, hiring, advancement, training, compensation or discharge of an employee, or in other terms, conditions and rights of employment.

Title II: Public Services

Title II prohibits state and local governments from discriminating against persons with disabilities or from excluding participation in or denying benefits of programs, services or activities to persons with disabilities. It is under this Title that this ADA Transition Plan has been prepared. The ADA Transition Plan is intended to outline the methods by which physical or structural changes will be made to effect the non-discrimination policies described in Title II.

Title III: Public Accommodations

Title III requires places of public accommodation to be accessible to and usable by persons with disabilities. The term public accommodation as used in the definition often is misinterpreted as applying to public agencies, but the intent of the term is to refer to any privately funded and operated facility serving the public.

Title IV: Telecommunications

Title IV covers regulations regarding private telephone companies, and requires common carriers offering telephone services to the public to increase the availability of interstate

and intrastate telecommunications relay services to individuals with hearing and speech impairments.

Title V: Miscellaneous Provisions

Title V contains several miscellaneous regulations, including construction standards and practices, provisions for attorney's fees and technical assistance provisions.

Title II of the ADA dictates that a public entity must evaluate its services, programs, policies and practices to determine whether they are in compliance with the nondiscrimination regulations of the ADA. The regulations detailing compliance requirements were issued in July 1991. A self-evaluation also is required. It is intended to examine activities and services, identify problems or barriers that may limit accessibility by persons with disabilities, and describe potential compliance solutions. The entity then must proceed to make the necessary changes resulting from the self-evaluation. The ADA further requires that an ADA transition plan be prepared to describe any structural or physical changes required to make programs accessible.

In the ADA, the term disability means, with respect to an individual:

- (1) a physical or mental impairment that substantially limits one or more of the major life activities of such individual;
- (2) a record of such an impairment; or
- (3) being regarded as having such an impairment.

If an individual meets any one of these three tests, that person is considered to be an individual with a disability for purposes of coverage under the Americans with Disabilities Act. The Final Rules of the ADA describe in greater detail the conditions included and excluded as disabilities under the ADA. These rules are available upon request from the study team, and are incorporated by reference as part of this ADA Transition Plan.

Section 2.2: SacDOT Responsibilities under the ADA

SacDOT has various responsibilities under Title II of the ADA. Title II of the ADA is similar to Section 504 of the Rehabilitation Act of 1973, but differs in that Section 504 applies only to government agencies that receive federal financial assistance. The purpose of Section 504 is to ensure that no otherwise qualified individual with disabilities shall, solely by reason of disability, be discriminated against under any program or activity receiving federal financial assistance. SacDOT has been subject to and operating under the requirements of Section 504 for many years.

The ADA states an intent not to apply lesser standards than are required under other federal, state or local laws; therefore, the law that is the most stringent has precedence. This intent has particular application with respect to SacDOT's obligations under Section 504 or under Title 24 of the California Code of Regulations, which in some cases, exceed ADA requirements with respect to structural and physical changes.

Title II also mandates that County governments may not require eligibility criteria for participation in programs and activities that would screen persons with disabilities, unless it can be proven that such requirements are necessary for the mandatory provision of the service or program. A public entity must reasonably modify its policies and procedures to avoid discrimination toward disabled residents. Nevertheless, if the public entity can demonstrate that a modification fundamentally would alter the nature of its service, it would not be required to make that modification. Title II also discusses the use of auxiliary aids necessary to enable persons who have visual, hearing, mobility or similar impairments to gain access to programs and activities provided by the County so as to make an appropriate reasonable accommodation.

The lone exception to these requirements would be because of undue hardship. Undue hardship is defined in the ADA as an "action requiring significant difficulty or expense" when considering the nature and cost of the accommodation in relation to the size, resources and structure of the specific operation. Undue hardship is determined on a case-by-case basis.

A public entity also is required to designate a person to be responsible for coordinating the implementation of ADA requirements and for investigating complaints of alleged noncompliance. At the time of the ADA Transition Plan preparations, for the intent of this portion of the ADA Transition Plan that relates to streets, sidewalks and public rights-of-way, that designated person is Dave Franke, Program Access Coordinator, Sacramento County Department of Transportation, 906 G Street, Suite 510, Sacramento, CA 95814, Telephone (916) 874-6291, TTY (916) 875-7105.

Section 2.3: ADA Transition Plan Requirements

According to ADA, a public agency is required to prepare an ADA Transition Plan if physical or structural modifications to facilities are required to provide access to programs or services. Title II of the ADA regulates government agencies, with its primary goal being to ensure that all of their programs and services are accessible to individuals with disabilities. The ADA Transition Plan is limited to evaluating physical barriers; however, an analysis of the programs and services rendered by SacDOT also is important to determine what physical changes are necessary. The ADA Transition Plan documents what actions SacDOT will take to alter its facilities. The ADA requires that the ADA Transition Plan be submitted for public review before final approval and adoption by the appropriate regulatory agency.

Generally, the ADA Transition Plan lists existing barriers in the public rights-of-way under SacDOT's jurisdiction, and schedules which barriers to remove to provide access for individuals with disabilities to SacDOT programs. SacDOT is required to provide access to all of its programs, but is not required to remove all architectural barriers in all of its facilities. In addition to making physical improvements, government agencies can choose among various administrative solutions, such as relocating or modifying a particular program, to obtain overall program access.

The ADA Transition Plan is required by Department of Justice rules to address the following aspects of accessibility:

- (1) If a public entity has responsibility or authority over streets, roads or walkways, its ADA Transition Plan shall include a schedule for providing curb ramps or other sloped areas where pedestrian walks cross curbs, giving priority to walkways serving entities covered by the ADA, including State and local government offices and facilities, transportation, places of public accommodation, and employers, followed by walkways serving other areas;
- (2) The ADA Transition Plan shall identify physical obstacles in the public entity's facilities that limit the accessibility of its programs or activities to individuals with disabilities;
- (3) The ADA Transition Plan shall describe the methods that will be used to make the facilities accessible; and
- (4) The ADA Transition Plan shall specify the schedule for taking the steps necessary to achieve compliance with the ADA and, if the time period of the ADA Transition Plan is longer than one year, identify steps that will be taken during each year of the transition period.

The ADA Transition Plan contains detailed physical barrier surveys of SacDOT streets, curb ramps and related facilities. These surveys are contained in a comprehensive computer database, and document barriers present at the time of the survey. The survey does not provide a complete listing of complying architectural or physical features. It also is important to note that improvements made to facilities after the date of the survey are not included as part of this ADA Transition Plan.

The ADA does not designate a specific code or standard for evaluating access to existing facilities. Title II gives government agencies a choice between the Uniform Federal Access Standards (UFAS) and the Americans with Disabilities Act Accessibility Guidelines (ADAAG) as a standard for renovations. Since the ADA states that it does not override requirements of other state and local requirements, the State of California Title 24 access regulations also must be applied. Therefore, for the purpose of this ADA Transition Plan, each facility or site area is evaluated based on the most stringent requirements of the ADA Accessibility Guidelines dated July 26, 1991 or California State Building Code, Title 24, and Part 2, of the California Code of Regulation, 2001 edition.

In creating priorities, it is SacDOT's intent to evaluate all areas of potential deficiency, and to make structural changes where necessary. The assignment of priorities is intended to facilitate public review and to address specific concerns of the local disabled community. It must be emphasized that it is the intent for all individuals with disabilities to be reasonably accommodated by SacDOT.

Section 2.4: Description of Program Accessibility

The final Rules and Regulations of the ADA describe the requirements for program accessibility (Code of Federal Regulations, Title 28, Part 35, Subpart D). A public entity shall operate each service, program or activity, when viewed in its entirety, so that it is accessible to and usable by individuals with disabilities. The ADA does not require the public entity to make all of its existing facilities accessible, nor does it require a public entity to take any action that would fundamentally alter the nature of a service, program or activity. Also, it does not require implementation of the ADA that would result in undue financial and administrative burdens. In such cases where documentation is provided in keeping with strict procedures outlined in the ADA, there are various methods that may be appropriate for providing program accessibility in lieu of making actual physical changes to facilities.

With these facts in mind, the first step in determining what structural changes to existing facilities are necessary is to develop an understanding of the specific public programs and activities occurring at existing facilities within the County. This section attempts to describe the programs and activities in the public right-of-way. It should be noted that this section is not intended to be a self-evaluation, as described in the ADA. A self-evaluation includes an analysis of *all* programs and services offered by a public entity. The self evaluation may include communications, publications, employment and many other factors that are separate from proposed structural or physical modifications to facilities.

The activity of using the public right-of-way may be considered a program in two different ways:

- (1) Streets, sidewalks and curb ramps may be part of a continuous path of travel between activities or programs, at various public and private facilities located on adjacent properties, such as public offices, schools, parks and recreational facilities, public service agencies, hospitals and health clinics, police facilities and public housing uses.
- (2) Streets, sidewalks and curb ramps may themselves represent a program of public pedestrian activities that are essential to the usage and enjoyment of a County's built environment.

The Department of Justice's Title II Technical Assistance Manual points out that a public entity's programs related to streets, sidewalks and curb ramps may be prioritized with respect to relative importance and frequency of usage. It further describes that program accessibility would not require all streets, sidewalks and curb ramps to be fully accessible as required by current codes. A determination of what public rights-of-way are programmatically required to be accessible may vary from jurisdiction to jurisdiction.

Section 3: Public Participation and Input

Section 3.1 Introduction

The ADA states that a public entity is required to make available to applicants, participants, residents and other interested parties information regarding the ADA Transition Plan and its applicability to the services, programs or activities of the public entity, and to apprise the public of the protections against discrimination afforded to them by the ADA. A public entity also is required to provide an opportunity for interested persons, including individuals with disabilities or organizations representing individuals with disabilities, to participate in the development of the ADA Transition Plan by submitting comments and making specific recommendations. The ADA also requires that a copy of the draft ADA Transition Plan shall be made available for public inspection during a formal public review period.

The ADA Transition Plan project was set up to encourage and facilitate the maximum degree of participation from residents of Sacramento County. This process included persons with disabilities and those representing disability service organizations. This section describes the public participation and outreach efforts made by the study team. The main objective of the outreach effort was to ensure that the ADA Transition Plan is one that truly represents the goals and aspirations of the local disability community.

Section 3.2: Community Participation

Introduction

SacDOT set up the ADA Transition Plan and Pedestrian Master Plan project to encourage and facilitate the maximum degree of public participation. This process included persons with disabilities and those representing disability service organizations. The outreach efforts included the following components as shown in bullet points and described in more detail below:

- Advisory Groups
- Outreach to Persons who are Visually Impaired
- Web Site
- Electronic Newsletter and List Serve
- Press Releases
- Transportation Fairs
- Community Planning Advisory Councils
- Public Information Workshops
- Consumer Survey (described in Section 3.3)

The community ultimately will be able to submit formal comments about this ADA Transition Plan, either in written form or at a public hearing, per ADA requirements as explained in Section 3.4.

Advisory Groups

As part of the ADA public participation process, advisory groups were formed to allow for additional input from key stakeholders, planning professionals, policy makers and the general public. The advisory groups acted as a sounding board for the ADA study team and SacDOT staff. Members reviewed and provided feedback on project documents and submittals. In addition, the advisory groups worked toward achieving consensus on project issues.

The following advisory groups were established:

- ADA Community Advisory Group (CAG) for the ADA Transition Plan;
- Technical Advisory Committee (TAC) for the ADA Transition Plan.

Refer to Acknowledgements for a list of the ADA CAG and TAC members.

The study team met with the advisory committees at key milestones throughout the ADA Transition Plan. The first meeting for each group was in April, 2002, and both groups held a total of nine regular meetings, including draft ADA Transition Plan review meetings in January 2004. The CAG and the TAC also met concurrently at different stages to allow the joint groups to discuss key project components.

Outreach to Persons who are Visually Impaired

The ADA Transition Plan will be made available to persons who are visually impaired via text document and Braille master copy. Persons with visual impairments who have access to software that converts text to audio will be provided the document via e-mail, floppy disks or CDs. The ADA Public Rights-of-Way Database will be available for review by appointment at the SacDOT Program Access Coordinator's office at 906 G Street, Suite 510, Sacramento, CA.

Sacramento Access News (SAN) has a free telephone reader service for individuals who are blind or with visual impairments that includes information on the ADA Transition Plan. A demonstration of the service can be reached by calling (916) 732-4000, selecting #1 for local area information, using the demo code 5555, pressing #3 (Special Interest Publications) and then pressing #18 (Sacramento County ADA Transition Plan). One can sign up as a Sacramento Access News subscriber by calling (916) 732-4010. SAN initiated the service for the ADA Transition Plan in August 2002, and received a total of 48 hits to the three project-related sites between August and December 2002. All information was updated on a regular basis to include upcoming events and meetings.

Web Site

The ADA study team used SacDOT's web site as an additional means of disseminating information on the ADA Transition Plan, as well as the Pedestrian Master Plan. The web site address is http://www.sacdot.com/projects/ATP_PMP/.

By using the web site, the public was able to obtain information on the project's purpose, schedule and timeline, pedestrian/ADA consumer survey, archived newsletters and related articles, approved and revised documents and discussion papers, public involvement opportunities and contact information. In addition, the draft and final ADA Transition Plan and Pedestrian Master Plan will be posted on the web site for an expected 12 months after final approval and adoption.

Electronic Newsletter and List Serve

The study team used the e-mail list-serves and electronic newsletters to keep interested parties apprised of the project's progress. The study team found e-newsletters to be a cost-effective way of communicating to a broad audience. The e-newsletters also afforded the study team a convenient method of communicating project updates or upcoming public involvement opportunities on a frequent, real-time basis.

Newsletters also were made available in hard copy, large print, CD, floppy disk or electronic mail. The study team relied on the ADA and pedestrian-oriented organizations and other advocacy groups in the Sacramento area to distribute project information through their existing web sites and e-mail list-serves.

Press Releases

The study team created and distributed three press releases to cover the following topics: project kick-off, consumer surveys and transportation fairs.

Transportation Fairs

The study team conducted a series of four transportation fairs in June and July 2003, for public input in the early stages of both the ADA Transition Plan and the Pedestrian Master Plan. The transportation fairs included both ADA and pedestrian issues at the same venue to allow everyone to understand both planning efforts. The information at the fairs focused on the development process, the results of the inventory and evaluation of existing conditions, draft ADA codes and standards and identification of neighborhood concerns.

Community Planning Advisory Councils

At the beginning of the project, the study team presented the ADA Transition Plan and Pedestrian Master Plan projects to the Community Planning Advisory Councils (CPACs). The presentation focused on each plan's purpose, objectives, scope, schedule and community outreach and involvement opportunities. Attendees of the CPAC meetings were given an

opportunity for input into the plan development process, as well as local problems such as safety issues, needed curb ramps and sidewalk connectivity. The study team distributed the consumer survey, as described in the next section, to CPAC meeting attendees to identify specific problem areas in their neighborhoods. The 14 CPACs located in Sacramento County and included in the public participation process are as follows:

- Antelope;
- Arden/Arcade;
- Carmichael;
- Cosumnes;
- Delta;
- Fair Oaks;
- Franklin / Laguna;
- Natomas;
- North Highlands;
- Orangevale;
- Rio Linda / Elverta;
- Southeast;
- South Sacramento; and
- Vineyard.

Public Information Workshops

SacDOT will present information on the draft ADA Transition Plan to focus groups that specialize in disabled access issues. The ADA Transition Plan also will have a workshop with representatives of the Board of Supervisors on the draft ADA Transition Plan and a Board Hearing on the final draft ADA Transition Plan. These meetings are expected to take place in mid to late 2004.

Section 3.3: Consumer Surveys

Introduction

The study team conducted a pedestrian and disabled access consumer survey to help identify specific community concerns, as well as hotspot locations or physical barriers to individuals with disabilities. The consumer survey also helped with the prioritization of the proposed projects as part of the ADA Capital Implementation Plan. The survey was designed to target all pedestrians, including individuals with disabilities.

The study team distributed questionnaires to public library branches throughout the County and to individuals and organizations on the study team's mailing list. Paratransit, Inc. mailed out about 4,000 questionnaires along with a project newsletter to their consumers in the unincorporated County. WalkSacramento also distributed the surveys to their members. The consumer survey was available on the project web site from September 2002 through April 2003.

For visually-impaired individuals, the questionnaire was made available in the following alternative formats:

- Audio tapes;
- Electronic using text files;
- Floppy disks;
- Large print documents;
- Telephone; and
- Telephone reader service (Sacramento Access News).

Survey Process

The goal was to receive at least 100 completed questionnaires from pedestrians who stated that they were disabled (all respondents were given the opportunity to decline stating that they had a disability) and at least 200 completed questionnaires from the general public. The study team collected and analyzed 197 questionnaires. Out of these surveys, 112 were filled out by individuals who stated that they had a disability.

The survey collected the following information:

- Disability type (optional);
- Reasons why respondent does not walk more;
- Purpose for walking (i.e., work, social/recreational, etc.);
- Time spent walking for each purpose; and
- Major walking constraints in Sacramento County.

Survey Results

Statistically valid results were not necessarily drawn from the survey results, because the pedestrian respondents were not randomly selected. Nevertheless, these pedestrians did provide a broader picture of pedestrian activity in the unincorporated County. About 62 percent of the respondents were female, and the survey showed that the female respondents walk for a longer amount of time compared to the male respondents (Table 1).

More respondents were older, and more respondents stated that they were disabled than the demographics of the unincorporated County. For example, 44 percent of the respondents were 60 years old or greater, and 57 percent of the respondents marked the optional question regarding disability. Furthermore, only 52 percent of the respondents owned a car or truck. According to the 1995 National Personal Transportation Survey (NPTS), one in ten households in the United States do not own an automobile.

The following tables outline key observations on the respondents’ profile and pedestrian habits and constraints. In interpreting these observations, it should be remembered that respondents, since they were self selected, are more likely to travel longer distances and for more purposes than the average resident in the unincorporated County.

Table 1: Daily Times of Pedestrian Activity for Survey Respondents

	Personal / Family (min)	Social / Recreational (min)	School / Church / Civic (min)	Work (min)
Females	29	32	31	16
Males	17	29	11	17
Weighted Average for All	24	31	26	16

Table 2, shown on the next page, shows the total number of survey respondents, as well as the constraints or difficulties reported for specific elements within the various community planning areas.

Table 2: Pedestrian Constraints (or Difficulties) Reported by Community Planning Area

Community Planning Area	Total No. of Respondents	Sidewalk Constraints	Street Crossing Constraints	Disabled Accessibility Constraints
Antelope	12	3 25%	2 17%	3 25%
Arden / Arcade	141	46 33%	48 34%	25 18%
Carmichael	91	38 42%	29 32%	10 11%
Cosumnes	2	1 50%	0 0%	0 0%
Fair Oaks	20	7 35%	8 40%	2 10%
North Highlands / Foothill Farms	49	17 35%	17 35%	12 24%
Orangevale	7	2 29%	4 57%	0 0%
Rio Linda - Elverta	51	23 45%	10 20%	8 16%
South Sacramento	62	19 31%	17 27%	11 18%
Vineyard	9	4 44%	3 33%	1 11%
Total	444	160 43%	138 37%	72 20%

Note: Statistically valid results cannot be drawn from the survey since the pedestrian respondents were not randomly selected.

Section 3.4: ADA Transition Plan Public Review and Comment Period

A public entity that employs 50 or more people is required to seek public input on its ADA Transition Plan. Beyond the legal requirements, such input is vital to assure that those affected by the SacDOT's programs, services and facilities understand the scope and nature of SacDOT's responsibilities for providing equal access to the public. As described in the previous section, the ADA Transition Plan process has spanned several years, and considerable efforts to obtain public input have been undertaken during this period.

The issuance of this public input draft ADA Transition Plan will commence a more formal period of public review and participation. The notice of the issuance of such a draft should be advertised in local publications, and upon issuance, members of the public will be afforded a 45-day comment period to submit written comments. The draft ADA Transition Plan will be available in alternate formats, and written comments will be received in any alternative formats chosen by respondents. Members of the public also will be afforded the opportunity to make public oral comments about the ADA Transition Plan at a public hearing, to be convened before the County of Sacramento Board of Supervisors after the 45-day comment period.

All public comments received should be incorporated in their entirety into a section of the final ADA Transition Plan. All public comments will be reviewed, analyzed and incorporated into the text of the final ADA Transition Plan as deemed to be appropriate.

Requests for copies of the ADA Transition Plan and public comments should be directed in writing (hard copies only, no e-mails accepted) to Sacramento County Department of Transportation, Program Access Coordinator, 906 G Street, Suite 510, Sacramento, CA, 95814, Telephone: (916) 874-6291, TTY (916) 875-7105. The ADA Transition Plan will be provided in various alternative formats upon written request.

Section 3.5: ADA Grievance Procedures

Introduction

The ADA states that a public entity is required to apprise the public of the protections against discrimination afforded to them by Title II of the ADA, including information about how Title II requirements apply to its particular programs, services and activities [28 C.F.R. § 35.106]. A public entity also is required to provide an opportunity for interested persons, including individuals with disabilities or organizations representing individuals with disabilities, to participate in the development of policies and procedures that affect the implementation of an ADA transition plan by submitting comments and making specific recommendations.

A public entity that employs 50 or more persons is required by the ADA to adopt and publish grievance procedures providing for prompt and equitable resolution of complaints or grievances alleging any action that would be prohibited by Title II of the ADA. SacDOT's grievance procedure is described below. Any person with a disability or any parent or guardian who represents a minor person with a disability, who believes that they have been the subject of disability-related discrimination on the basis of the denial of access to facilities, programs or services, may file a grievance or complaint.

Grievance Procedures and Instructions

Step 1: File a Grievance Form

The complainant should fill out the ADA Complaint / Grievance Form shown below, giving all of the information requested. The ADA Complaint / Grievance Form should be filed in writing with the SacDOT Program Access Coordinator within 60 days of the alleged disability-related discrimination. A copy of the ADA Complaint / Grievance Form shall be forwarded by the Program Access Coordinator to the Chief of the Disability Compliance Office. Upon request, reasonable accommodations will be provided in completing the form, or alternative formats of the form will be provided. The ADA Complaint / Grievance Procedure and Form may be obtained from and sent to the Sacramento County Department of Transportation, Program Access Coordinator, 906 G Street, Suite 510, Sacramento, CA., 95814, Telephone: (916) 874-6291, TTY (916) 875-7105.

Step 2: An Investigation is Conducted

A notice of receipt shall be mailed to the complainant by registered mail within five days of the receipt of the complaint or grievance, and the SacDOT Program Access Coordinator or another authorized representative shall begin an investigation into the merits of the complaint within 60 days. If necessary, the SacDOT Program Access Coordinator or another authorized representative may contact the complainant directly to obtain additional facts or documentation relevant to the grievance. If the complainant alleges misconduct on the part of the SacDOT Program Access Coordinator, another authorized representative may be appointed by the

Director of SacDOT to undertake the investigation if the allegations can be substantiated. If the complainant does not wish to be contacted personally, he/she should indicate it on the ADA Complaint / Grievance Form.

After the grievance is received, the complaint shall be brought before the ADA Oversight Committee, co-chaired by the Chief of the Disability Compliance Office and the SacDOT Program Access Coordinator. The co-chairs shall meet on an ad-hoc committee to resolve the grievance.

Step 3: A Written Decision is Prepared and Forwarded to the Complainant

The Chief of the Disability Compliance Office and the SacDOT Program Access Coordinator shall prepare a written decision, after full consideration of the grievance merits, no later than 75 days following the receipt of the grievance. If the complaint alleges misconduct on the part of the SacDOT Program Access Coordinator, another authorized representative may be appointed by the Director of SacDOT to prepare the written decision if the allegations can be substantiated. A copy of the written decision shall be mailed to the complainant by registered mail no later than five days after preparation of the written decision. A copy of the written decision also shall be mailed to the Chief of the Disability Compliance Office.

Step 4: A Complainant May Appeal the Decision

If the complainant is dissatisfied with the written decision, the complainant may file a written appeal with either, at the complainant's option, the Director of SacDOT, or with the Chief of the Disability Compliance Office, no later than 30 days from the date of the mailing of the decision. The appeal must contain a statement of the reasons why the complainant is dissatisfied with the written decision, and must be signed by the complainant, or by someone authorized to sign on the complainant's behalf. A notice of receipt shall be mailed to the complainant by registered mail within five days of the receipt of the appeal. The appeal reviewers, consisting of the Chief of the Disability Compliance Office, and the SacDOT Program Access Coordinator, shall act upon the appeal no later than 60 days after receipt, and a copy of the appeal reviewers' written decision shall be mailed to the complainant by registered mail no later than five days after preparation of the decision. The decision of the appeal reviewer shall be final. A copy of the written decision also shall be mailed to the Chief of the Disability Compliance Office.

The SacDOT Program Access Coordinator, the Director of SacDOT, and the Chief of the Disability Compliance Office shall maintain the confidentiality of all files and records relating to grievances filed, unless disclosure is authorized or required by law. Any retaliation, coercion, intimidation, threat, interference or harassment for the filing of a grievance, or used to restrain a complainant from filing, is prohibited and should be reported immediately to the Chief of the Disability Compliance Office.

Sacramento County Department of Transportation - ADA Complaint / Grievance Form

Complainant: _____

Person Preparing Complaint (if different from Complainant): _____

Relationship to Complainant (if different from Complainant): _____

Street Address & Apt. No.: _____

City: _____ State: _____ Zip: _____

Phone: (____) _____ E-mail: _____

Please provide a complete description of the specific complaint or grievance:

Please specify any location(s) related to the complaint or grievance (if applicable):

Please state what you think should be done to resolve the complaint or grievance:

Please attach additional pages as needed.

Please do not contact me personally.

Signature: _____ Date: _____

Return to: Sacramento County Department of Transportation, Program Access Coordinator, 906 G Street, Suite 510, Sacramento, CA., 95814

Upon request, reasonable accommodation will be provided in completing this form, or copies of the form will be provided in alternative formats. Contact the Program Access Coordinator at the address listed above, via telephone (916) 874-6291 or via TTY (916) 875-7105.

Section 3.6: ADA Transition Plan Oversight Committee

Introduction

A key to ensuring timely and effective implementation of SacDOT's ADA Transition Plan is coordination among the various departments, divisions, offices and committees involved in this effort. To this end, an ADA Transition Plan Oversight Committee, co-chaired by the Chief of the Disability Compliance Office and the SacDOT Program Access Coordinator, shall be established. Its purpose is to assure that a reasonable work schedule is maintained and to monitor any additional work or costs as they are identified. The Committee should meet, at a minimum, quarterly and report annually to the Sacramento County Board of Supervisors on the status of ADA and accessibility improvements, as well as costs incurred to date and projected cost estimates for other components of the ADA Transition Plan. SacDOT will hold an informational public meeting prior to the annual Board of Supervisors report on the status of the ADA Transition Plan. Members of the Disability Advisory Committee, the Physical Access Subcommittee and Chiefs of both the Technical Resources Section and the Construction Management Division also will be briefed prior to the annual report to the Board of Supervisors.

Authority for forming such a committee was adopted by the Sacramento County Board of Supervisors as part of the Interim Policy on Street and Sidewalk Access Improvement Priorities, January 16, 2001. The recommendations contained in this section are consistent with "Part V – Implementation, The Transition Plan," as contained in that document.

The recommended composition of the committee is as follows:

1. Chief of the Sacramento County Disability Compliance Office.
2. Chief of SacDOT, Engineering and Planning.
3. Chief of SacDOT, Maintenance and Operations.
4. Program Access Coordinator of SacDOT.
5. Physical Access Subcommittee representative.

To date, elderly and disabled access funds and other funding sources, such as TEA-21, have been used to make modifications and upgrades to existing streets and sidewalks in the unincorporated County. The oversight committee should evaluate the need for additional funding and look for new funding opportunities, including funding to assist with the tasks performed by the ADA Transition Plan Oversight Committee.

Specific tasks that the ADA Transition Plan Oversight Committee should undertake and oversee would include the following:

(1) Curbs and Curb Ramps Evaluation

The Oversight Committee should monitor the status of curb ramp construction, and should recommend revisions/modifications to the policy to implement the ADA Transition Plan, to

handle public requests, to discuss variances and deviations to the standards and to determine technical infeasibility.

The Committee should evaluate SacDOT's current curb ramp designs on an on-going basis to ensure that they provide the appropriate degree of access, in accordance with the ADA Codes and Standards included in the ADA Transition Plan. Where the public right-of-way allows, alternative curb ramp designs should be investigated to ensure the appropriate complying level of access. Information from the Public Right-of-way Advisory Committee of the U. S. Access Board should be continually evaluated for purposes of determining that current curb ramp designs reflect the latest access trends. Current studies and code changes related to State of California Title 24 also should be closely monitored.

(2) Individual Intersection Curbs, Sidewalks and Pedestrian Islands Evaluation

The Oversight Committee should review the process, and should recommend revisions/modifications to the policy to implement the ADA Transition Plan, to handle public requests, to discuss variances and deviations to the standards, and to determine technical infeasibility. The on-going retrofitting of curbs, sidewalks and pedestrian islands should be in accordance with the ADA Transition Plan and all applicable federal and state laws and regulations, with the highest priority first and the lowest priority last.

Areas around bus stops, transportation, public and medical facilities, shopping areas and other facilities should have the highest priority. When a curb ramp is evaluated for construction or reconstruction, the whole intersection should be evaluated for safety and usability by persons with disabilities to determine usable paths of travel.

(3) Accessible Pedestrian Signals Evaluation

The Oversight Committee should review the process and recommend revisions/modifications to the policy to implement the ADA Transition Plan. The committee should oversee the installation of accessible pedestrian signals throughout the unincorporated County. When accessible pedestrian signals are installed, they should be equipped with all features that are required by the ADA Codes and Standards, and should be in compliance with the outlined policy. Accessible pedestrian signal installations also should be evaluated to reflect any new Federal guidelines contained in the FHWA Manual on Uniform Traffic Control Devices, along with advances in accessible signal technology.

(4) Tactile Guidestrips Evaluation

The Oversight Committee should review the process and recommend revisions/modifications to the policy to implement the ADA Transition Plan. The committee should oversee the needs for the installation of tactile guidestrips where they are necessary in the unincorporated County. In an attempt to make street crossings safer for persons who are blind, the Sacramento County Disability Advisory Committee drafted and approved the Sacramento County Guidestrip Policy on July 22, 1987, which was adopted by the County Board of

Supervisors on August 18, 1987. This policy gave authority to the Physical Access Subcommittee (formerly the Barriers Removal Subcommittee) to establish guidelines for evaluating and prioritizing requests for the installation of the raised tactile guidestrips. Guidestrips are placed at the midpoint between the crosswalk markings and guide a blind pedestrian from one side of a crossing to the other. Also, the installation procedures for these guidestrips were incorporated into the 1989 edition of the Sacramento County Improvement Standards. The County Improvement Standards for installation of guidestrips was updated in October 2001. The responsibility of the Oversight Committee is to incorporate the 1987 Guidestrip policy into these standards with recommendations to evaluate and improve the product.

(5) Private Developers' Improvements Evaluation

The ADA Transition Plan Oversight Committee should obtain from the County's Department of Public Works, Technical Resources Section, proposed plans for private developments, both residential and commercial, and evaluate the scope and impact of such plans on access improvements to streets and sidewalks in unincorporated areas.

The Committee should, through the Technical Resources Section, identify private projects, both residential and commercial, either completed or planned, where SacDOT retains sufficient right-of-way options to provide enhanced access improvements, such as the installation of sidewalks. Distinction should be made between those private developments where the responsibility for access improvements rests with the developer, and those situations where the County has granted variances to developers on access improvements. Where SacDOT has allowed a variance that impacts access for the disabled, the variance should be looked at to determine if SacDOT preserves the option to do future improvements such as installing sidewalks. On an as-needed basis, a representative of the Technical Resources Section should be included in the Oversight Committee to discuss Board approved variances for access improvements.

Section 4: Inventory Methodology and Findings

Section 4.1: Purpose and Summary of the Inventory Effort

The purpose of the inventory effort was to show a baseline of existing pedestrian facilities in unincorporated Sacramento County. These data will be used to improve pedestrian facilities and to comply with ADA and Title 24 requirements and County approved policies.

SacDOT has a wide variety of facilities within the public right-of-way. These facilities include streets and roadways, vehicular and pedestrian bridges, underground and above-ground utilities, vehicular and pedestrian signal systems, signage systems, on-street parking facilities, sidewalks with curb ramps at intersections, improved planting strips, buffers, and pedestrian activity areas, and unimproved open spaces or natural areas. The goal of the overall project is to optimize the pedestrian experience and to provide safe and usable pedestrian facilities for all pedestrians in Sacramento County, and to assure compliance with all federal, state, and local regulations and standards.

A five-month long period of surveying pedestrian facilities was undertaken to document existing conditions within the public rights-of-way under the jurisdiction of SacDOT. Surveying, as used in this section, refers to visiting the particular location by a trained accessibility surveyor, and obtaining measurements, dimensions, gradients or other visual determinations as may be appropriate depending on the particular location. Highlights of the survey process and inventory findings are listed below:

- Approximately 2,200 miles of streets and roadways covering over 15,000 individual segments of roadway boundaries were traveled and surveyed to document physical conditions along the roadways.
- The inventory focused on more heavily used roadways and on those roadways serving governmental, public service, and commercial uses.
- For roadways surveyed, approximately 75 percent of county roadways were two-lane, undivided roadways or collectors, with the remaining 25 percent being higher-use thoroughfares or arterials. (The County Standards define thoroughfares as six-lane roadways with 108-foot wide right-of-way, arterials as four-lane roadways with 84-foot wide right-of-way, and collectors as two-lane roadways with 60-foot wide right-of-way.)
- For roadways surveyed, approximately 75 percent of County roadways had sidewalks on one or both sides, with 25 percent being unimproved.
- Approximately 11,000 intersections or almost 44,000 street corners were surveyed, and measurements were taken for a variety of dimensions and gradients.
- Approximately 66 percent of all corners surveyed were found to have rolled curbs, with approximately 16 percent having vertical curbs and 18 percent being undeveloped or having no curbs at all.
- Approximately 41 percent of all developed corners had curb ramps installed. Of these, approximately 57 percent were older perpendicular curb ramps with flared sides (see

Appendix A for definition) and approximately 40 percent were newer parallel pan-type curb ramps (see Appendix A for definition).

Section 4.2. Inventory Methodology

The field surveying methodology for the ADA Transition Plan and Pedestrian Master Plan was originally described in the *Draft Inventory Methodology*, dated May 6, 2002. An *Errata - Inventory Methodology Report*, dated September 12, 2002, described some revisions to the original methodology. Both reports were reviewed and approved by the ADA CAG and TAC. These reports were followed in the survey process.

Field surveying began on June 3, 2002 and ended on November 8, 2002. During this five-month period, a total of 23 surveyors spent a total of over 9,000 hours collecting detailed measurements and other data within the unincorporated County. Each surveyor underwent at least 80 hours of training on equipment, data collection methods, procedures and ADA principles, including class and in-field instruction. Surveyors followed the procedures outlined in the previously submitted and approved *Surveyor's Manual*. Two full-time supervisors spent an additional 1,700 hours directing the surveyors' efforts, including preparing survey routes, handling assignments and personnel matters, answering questions and spot checking surveyors' completed data.

Prior to beginning all survey work, surveyors were given time goals to complete each type of survey. Time records for all surveyors and their activities were kept by supervisors throughout the survey process, and surveyors as a group met their time allotments to keep the project on schedule. All data for intersections and roadways were collected using personal digital assistants (PDA's) by Handspring. Surveyors typically worked in teams of two persons, with one person being the lead driver and navigator and the other person being the main PDA operator. Both surveyors would take measurements and collect data. Most teams worked either solely on intersections or on roadways, although a few teams worked on both aspects of data collection.

Completed data were downloaded into the master database program at regular intervals, usually at the end of each week, using Pendragon database software. Data were consolidated into tables sorted by data types, stored on compact disks, and transferred into the Microsoft Access database described in Section 4.6.

Other survey efforts were carried out by special methods. Approximately 1,700 digital photographs were taken by the survey team for intersections noted as irregular by the computer print-outs or otherwise noted as having significant implications. Other patterns of pedestrian usage, such as certain pedestrian bridges within the County right-of-way, were surveyed separately using manual checklists (see Section 4.5).

For the survey efforts, the unincorporated County was divided into geographical areas. There were a total of 92 separate and distinct survey areas identified. These areas represented similar land use areas patterns, and boundaries were selected to correspond to either major roadways or natural boundaries such as parks or open spaces. The areas not only aided in the organization of the survey effort, but they were useful in analyzing data for conditions common to similar land uses and geographical boundaries. These survey areas are delineated on the County map shown as Figure 1 on the following page.

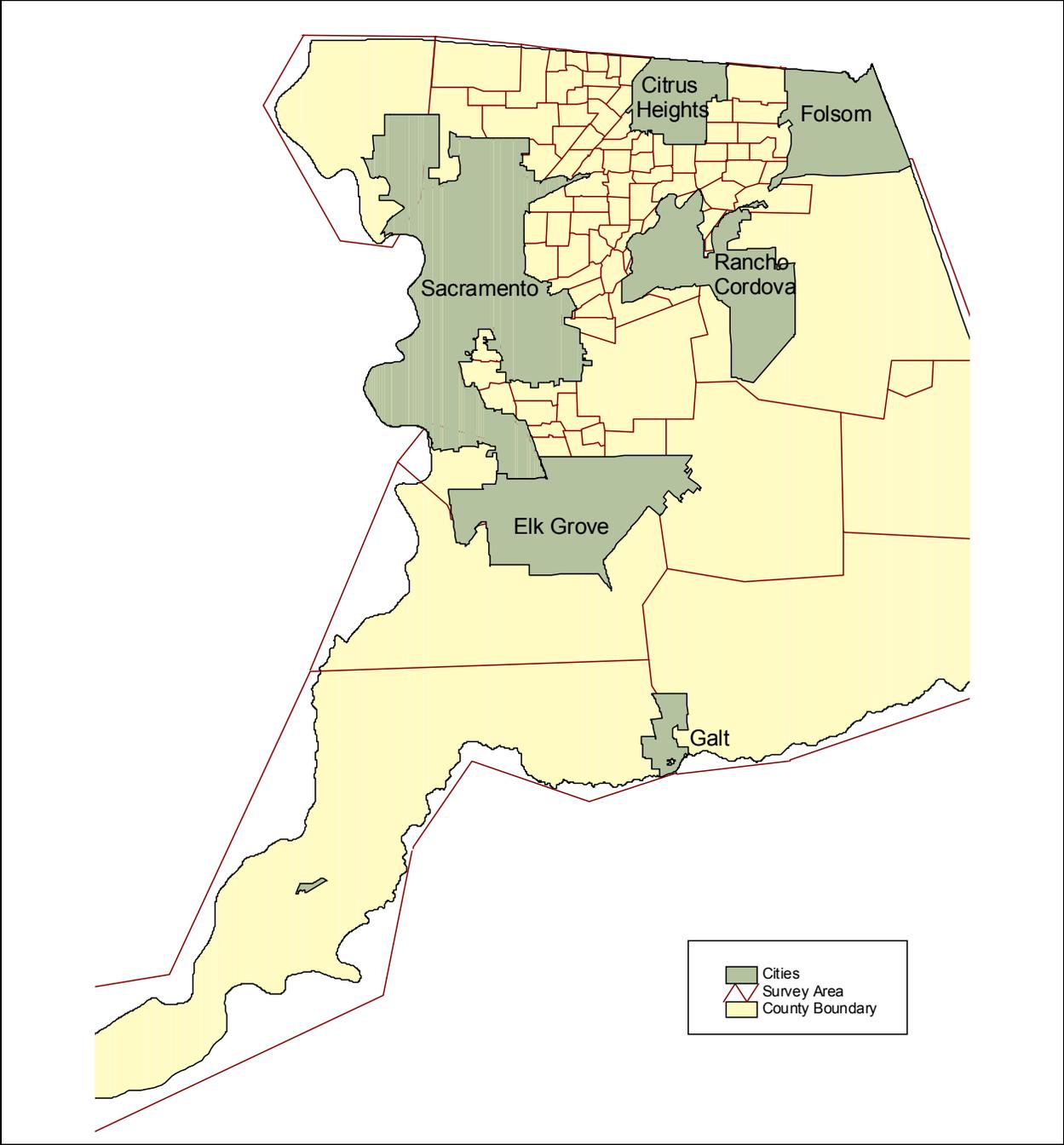


Figure 1: County Map of Survey Areas

(Note: City areas shown in gray are not included in this ADA Transition Plan.)

Section 4.3: Summary of Areas Surveyed and Priorities

Areas within the unincorporated Sacramento County were included in the survey process as described in the previously submitted and approved documents *Draft Inventory Methodology*, dated May 6, 2002, and the *Errata - Inventory Methodology Report*, dated September 12, 2002.

All intersections and roadway segments were classified as Priority Level 1 (High Priority), Priority Level 2 (Medium Priority) or Priority Level 3 (Lower Priority) based upon the criteria contained in these documents. A summary of these priorities and a description of each is as follows:

High Priority Intersections and Roadway Segments (Priority Level 1)

- ◆ Major roadways (Arterials or thoroughfares with a minimum 80-foot wide right-of-way) and intersections along these arterials or thoroughfares;
- ◆ Intersections and roadway segments serving Level 1 facilities, as described in the County's *Interim Policy on Street and Sidewalk Access Improvements - State and Local Government Buildings*, including:
 - County-owned facilities;
 - Public schools (approximately one-quarter mile radius for the main streets);
 - Hospitals, health clinics and health centers (public and private);
 - Public housing and homeless shelters, including senior facilities and rehabilitation facilities;
 - Sheriff's facilities;
 - Transportation hubs (includes bus lines and transit stations);
 - Department of Motor Vehicles offices;
 - County parks; and
 - Prisons.

For these high priority intersections and roadway segments, surveyors measured a variety of detailed accessibility and pedestrian data, as described in Section 4.4.

Medium Priority Intersections and Roadway Segments (Priority Level 2)

- ◆ *Collectors (streets with minimum 60-foot wide right-of-way) and other roadways, and intersections along these highways;*
- ◆ Intersections and roadway segments serving Level 2 facilities, as described in the County's *Interim Policy on Street and Sidewalk Access Improvements - Public Accommodations*, including:
 - Shopping malls, supermarkets and strip retail centers;
 - Major employment sites; and

- Housing complexes, including apartments.

For these medium priority intersections and roadway segments, surveyors also usually measured a detailed variety of accessibility and pedestrian data.

Lower Priority Intersections and Roadway Segments (Priority Level 3)

- Single-family residential areas;
- Industrial areas; and
- Other areas not classified as Priority Level 1 and 2.

For the ADA Transition Plan, lower priority intersections were surveyed using either the detailed survey or a simplified survey, specially designed to gather basic data. For these simplified surveys, surveyors were trained to visually inspect intersection elements to make a basic and objective determination of the overall compliance of the various elements, but did not collect all detailed data collection items if these data were not critical to the determination of overall compliance.

The survey team used the following criteria to determine which intersections in residential areas were surveyed using detailed measurements:

- Construction year of the adjacent land uses. For example, intersections and roadway segments within a specific subdivision are expected to be built with similar standards. These subdivisions were spot-checked to verify original assumptions;
- Geographic equity using zip codes: The survey team covered the unincorporated Sacramento County in an equitable manner;
- Highest pedestrian collision intersections; and
- Disabled person density using Paratransit, Inc.'s dataset of 8,000 active riders.

A County map showing the general extent of routes and areas surveyed as the highest priority roadways, including all intersections along these roadways, is shown in Figure 2 on the following page. This map is intended to be a general, graphic depiction of the extent of high priority surveys, and is not intended to depict or identify specific streets or roadways. For a more detailed map of survey routes, see Section 6.

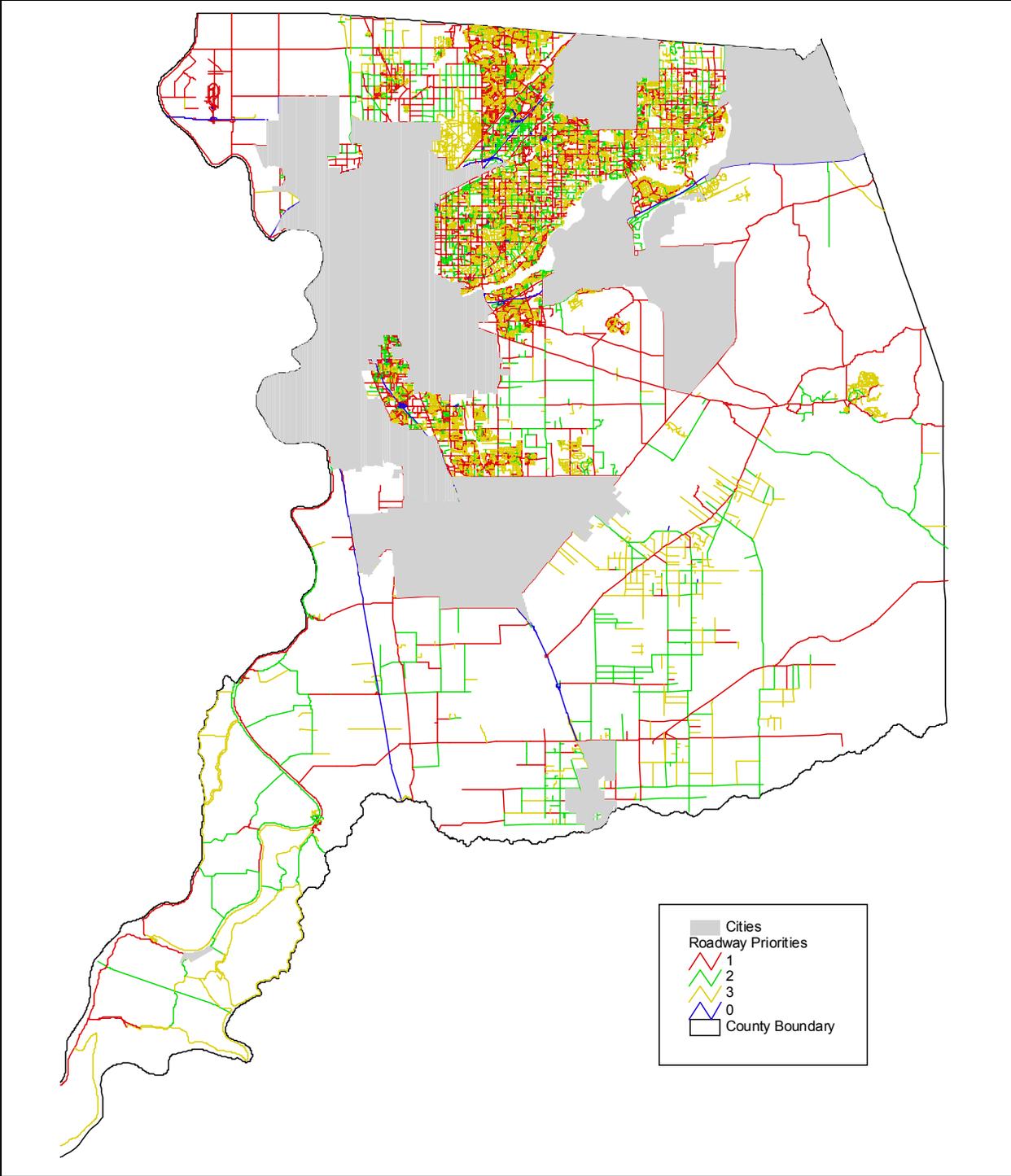


Figure 2: Priority Roadway Segments

(Note: City areas shown in gray are not included in this ADA Transition Plan.)

Section 4.4: ADA Data Collection Items

For detailed measurements at or near intersections, the survey team collected and analyzed the following data:

Crosswalks: Whether crosswalks are present at any or all crossings. If present, the width, type, alignment, presence of tactile guidestrips, presence of islands and disabled access.

Curb Ramps: Whether existing curb ramp(s) are present at any of the corners within the intersection.

Directional Corner of Intersection: NE, SE, SW and NW. (Note: All corners will be referred to by one of these compass points. If the street is not perfectly aligned north and south, the direction will be assigned within the nearest 45 degrees.)

Intersection Geometry: Whether the intersection is standard right angle, T-shaped, Y-shaped, skewed, or any other irregular geometry. Whether there are pedestrian island(s) or right turn lanes.

Islands: If present, then presence of curb ramps and push buttons.

Obstructions and Obstacles: The general presence and nature of abrupt changes in sidewalk level of greater than one-half inch, paving obstructions or accessibility obstacles immediately adjacent to the corner. The following obstacles near a corner will be recorded: utility pole, traffic light pole, drain inlet, fire hydrant, street furniture and newsstand.

Pedestrian Signals: Whether visual or accessible pedestrian signals are present. If present, the type, size, height and location of actuator buttons. The location parameters are “at curb,” “on landing,” “on ramp slope – arm length” and “on ramp slope – not arm length.” Another location question asks if the pedestrian push button is parallel to the crosswalk alignment.

Sidewalk Present: Whether a sidewalk leading to and from the curb is present. If present, the paved sidewalk width at the intersection.

Tactile Guidestrips: Whether tactile guidestrips are present at any crosswalk. If present, the location, height, width and color of the tactile guidestrips.

Traffic Control: Whether traffic signals, stop signs (all way vs. two-way vs. one-way), yield control, roundabout or no control.

Transit Stop Type: The parameters of the transit stop type are none, light rail, bus and other.

If a curb ramp is not present at a particular corner, the following data was collected:

Curb Type: Whether a curb is present, and if present, the type (vertical or rolled).

Flush Corner: If there is no curb, whether a flush transition from the street to the sidewalk is present.

If a curb ramp is present (either one or two at a corner), the following data was collected for each curb ramp:

Car Obstruction: Curb ramp not located so that it could be obstructed by parked vehicle.

Common Landing: Dimensions of any common landing for two curb ramps.

Curb Ramp Type: A general description of the curb ramp: flared, pan, chute, blended corner or built-up.

Detectable Warnings/Truncated Domes: Whether truncated domes are present. If present, the dome location, size, type (e.g., plastic, concrete, concrete tile, brick or other) and color. Truncated domes are placed at level landings to alert visually-impaired individuals of a transition between the sidewalk and the street or railroad tracks.

Grooved Border: Whether a 12 inch grooved border around all sides is present and its width.

Gutter Slope: Slope in percent of the gutter or street transition.

Lip: Whether a lip is present at the bottom of the curb ramp, and if present, the height to the nearest 0.25 inch.

Location in Crosswalk: Curb ramp wholly contained in marked crosswalk, if applicable.

Main Slope: Main slope of the curb ramp or level landing in percent adjacent to and perpendicular to the street.

Main Cross Slope: Cross slope of the main slope of the curb ramp or level landing, parallel to the street. The cross slope is perpendicular to the main slope of a curb ramp.

Side Slope(s): Whether a side slope or parallel slope is present, and if present, the slope of each sloping side or flare parallel to the street in percent.

Slip-resistant Surface: Whether or not the surface is slip-resistant.

Street the curb ramp is facing, or if facing the intersection (diagonal).

Top Landing Depth: Whether a 48 inch deep level landing is provided at the top of the curb ramp, or at the top of each slope of a parallel curb ramp.

Transition Slope: Slope of the transition to the sidewalk, verifying slope of five percent or less for the right and left sides.

Width: Width of the curb ramp or pan. A pan or level landing exists when there is a lack of vertical separation between the sidewalk and the street.

Section 4.5 Inventory Findings

Listed in this section are basic statistics for the survey findings. These statistics include only County-wide statistics. Other breakdowns of findings are available from the ADA Public Rights-of-Way Database, such as separate statistics by Priority Level or by Survey Area, which can be viewed at SacDOT. While statistics for sub-categories of priority, geographical location and land use will be invaluable for future analysis and recommendations, such a listing in this document was deemed to be too voluminous.

The maximum allowable dimensions or gradients are noted for specific elements when they are the proposed standards for new construction. The extent to which other requirements may apply to existing construction has not been determined, although some alternate requirements also are given with the individual statistics.

Intersection Survey Statistics

Total number of intersections:			11,496
Priority Level 1:	4,124	= 35.9 percent	
Priority Level 2:	3,351	= 29.1 percent	
Priority Level 3:	4,021	= 35.0 percent	

Total number of all corners (including all 3 crossings at T-intersections):			43,860
Priority Level 1:	16,096	= 36.7 percent	
Priority Level 2:	12,080	= 27.5 percent	
Priority Level 3:	15,684	= 35.8 percent	

Total number of right-angle corners (only corners at right-angles):			29,892
Priority Level 1:	10,881	= 36.4 percent	
Priority Level 2:	8,489	= 28.4 percent	
Priority Level 3:	10,521	= 35.2 percent	

Corner Statistics

Percentages of types of corners			
Rolled curb:		66.2 percent	
Vertical curb:		15.4 percent	
Flush transition:		0.2 percent	
No curb (undeveloped):		18.2 percent	

Percentages of corners with sidewalks at corners			
With sidewalk:		69.9 percent	
Without sidewalk:		30.1 percent	

Average sidewalk width at corners: 52.5 inches

Percentages of corners with sidewalks with changes in level or gaps greater than ½ inch

With no changes in level or gaps greater than ½ inch:	74.2 percent
With changes in level or gaps greater than ½ inch:	25.8 percent

Percentages of corners with sidewalk obstacles limiting access:

With obstacles:	22.0 percent
With no obstacles:	78.0 percent

Curb Ramp Statistics:

Number of Curb Ramps Surveyed:	12,238
Priority Level 1:	6,485 = 53.0 percent
Priority Level 2:	2,321 = 20.0 percent
Priority Level 3:	3,432 = 27.0 percent

Percentages of right-angle corners with curb ramps: 40.9 percent

Priority Level 1:	60.0 percent
Priority Level 2:	27.3 percent
Priority Level 3:	32.6 percent

Percentages of types of curb ramps

Perpendicular / flared (with side slopes):	56.9 percent
Parallel / pan type (landing level with street):	40.1 percent
Chute (with no side slopes):	1.5 percent
Flush transition (blended corner):	0.7 percent
Other:	0.8 percent

Percentages of gutter slopes at curb ramps (five percent maximum allowed):

Less than or equal to five percent:	25.8 percent
Greater than five percent:	74.2 percent
Less than or equal to seven percent:	61.2 percent
Greater than seven percent:	38.8 percent

Percentages of main slopes on curb ramps (8.33 percent maximum allowed):

Less than or equal to 8.33 percent:	43.0 percent
Greater than 8.33 percent:	57.0 percent
Less than or equal to ten percent:	65.4 percent
Greater than ten percent:	34.6 percent

Percentages of cross slopes on curb ramps (two percent maximum allowed):

Less than or equal to two percent:	61.5 percent
Greater than two percent:	38.5 percent
Less than or equal to three percent:	78.5 percent
Greater than three percent:	21.5 percent

Percentages of side slopes on perpendicular / flared curb ramps (ten percent maximum allowed)

Less than or equal to ten percent:	79.4 percent
Greater than ten percent:	20.6 percent
Less than or equal to 12 percent:	88.7 percent
Greater than 12 percent:	11.3 percent

Percentages of widths of curb ramps (48 inch minimum preferred)

Less than or equal to 36 inches:	0.7 percent
Between 36 inches and 48 inches:	5.4 percent
Greater than 48 inches:	93.9 percent

Percentages of beveled lip height on curb ramps (no lip preferred, ½ inch maximum)

No lip:	56.3 percent
0.25 inch:	16.9 percent
0.50 inch:	14.7 percent
0.75 inch:	3.9 percent
1.00+ inch:	8.2 percent

Percentages of grooved borders on curb ramps (12 inch grooved border preferred)

Curb ramps with grooved border:	68.4 percent
Curb ramps without grooved border:	31.6 percent

Percentages of curb ramps with top landing for perpendicular / flared curb ramps, where top landing is required (48 inch minimum)

Greater than or equal to 48 inches:	88.9 percent
Less than 48 inches:	11.1 percent

Percentages of curb ramps with common landings between two ramps, for parallel / pan type ramps only (48 inch minimum)

Greater than or equal to 48 inches:	65.6 percent
Less than 48 inches:	34.5 percent

Percentages of curb ramps with truncated domes

Without truncated domes:	93.8 percent
With truncated domes:	6.2 percent

Crosswalk Statistics

Percentage of intersections with crosswalks: 15.7 percent

Percentage of types of intersections with crosswalks:

Signalized intersections with crosswalks: 52.3 percent

Sign-controlled intersections with crosswalks: 40.6 percent

Uncontrolled intersections with crosswalks: 7.1 percent

Percentage of types of crosswalks:

White lines, parallel: 71.9 percent

Yellow lines, parallel: 21.9 percent

White lines, ladder pattern: 0.7 percent

Yellow lines, ladder pattern: 5.1 percent

White lines, diagonal stripes: 0.3 percent

Yellow lines, diagonal stripes: 0.1 percent

Percentages of width of crosswalks (96 inch minimum required)

Greater than or equal to 96 inches: 60.8 percent

Less than 96 inches: 39.2 percent

Percentage of number of crosswalks with crooked alignment:

Without crooked alignment: 92.0 percent

With crooked alignment: 8.0 percent

With tactile guidestrips installed (if crooked alignment): 14.0 percent

Pedestrian Signal Statistics

Percentage of all intersections with pedestrian signals: 11.2 percent

Percentage of signalized intersections with accessible pedestrian signals: 11.0 percent
(where pedestrian signals are present)

Percentage of pedestrian signal push button sizes (two inches preferred)

With ½ inch diameter push buttons: 73.0 percent

With two inches diameter push buttons: 27.0 percent

Percentages of push button heights (48 inches preferred, 54 inches maximum)

Less than or equal to 48 inches height: 84.9 percent

Between 48 inches and 54 inches height: 13.8 percent

Greater than 54 inches height: 1.3 percent

Roadway / Sidewalk Survey Statistics

Total miles of roadway: 4,200 approximately

Total number of roadway segments: 7,752

- Priority 1: 5,356
- Priority 2: 2,041
- Priority 3: 315

Total number of sides of roadway (east/west or north/south): 15,461

- Priority 1: 10,689
- Priority 2: 4,142
- Priority 3: 630

Presence of sidewalks

- Roadways segments with sidewalks on 1 or both sides of roadway: 70.1 percent
- Roadways segments without sidewalks on either side of roadway: 29.9 percent

Percentage of sidewalk coverage, when present

- 100 percent along length of roadway segment: 93.7 percent
- 75 percent along length of roadway segment: 1.6 percent
- 50 percent along length of roadway segment: 2.8 percent
- 25 percent along length of roadway segment: 1.9 percent

Average sidewalk width, when present: 4.4 feet

Sidewalk condition, when present:

- Almost new condition: 5.4 percent
- Very good condition: 3.0 percent
- Average condition: 88.5 percent
- Below average condition: 1.5 percent
- Very poor condition: 1.6 percent

Number of fixed obstructions (reducing width to less than 48 inches) along sidewalk

- None 70.5 percent
- 1 16.0 percent
- 2 7.4 percent
- 3 3.1 percent
- 4 1.6 percent
- 5+ 1.4 percent

Number of non-fixed obstructions (reducing width to less than 48 inches) along sidewalk

None	78.5 percent
1	11.4 percent
2	5.1 percent
3	1.9 percent
4	1.6 percent
5+	1.5 percent

Percentage of sidewalks segments with level changes greater than 1/2 inch

No level changes:	45.6 percent
1 or more level changes:	54.4 percent

Types of curb along roadway segments

Rolled curb:	63.5 percent
Vertical curb:	16.5 percent
No curb (open shoulder):	20.0 percent

Percentage of shoulder types, where no curb or sidewalk is present

Flat shoulder:	47.4 percent
Ditch or swale:	52.6 percent

Total number of marked mid-block crossings: 86

(Note: Statistics are included with crosswalks and curb ramps.)

Total number of transit stops: 836

Number of transit stops with loading pad: 821

Number of transit stops with cross-slope pad greater than three percent: 748

Number of bus shelters: 55

Number of bus shelters at least 30 inches by 48 inches in size: 55

Pedestrian Bridges

As part of the overall survey process, the survey team was asked to include pedestrian bridges within the County owned and managed right-of-way. A number of pedestrian bridges adjacent to roadways were surveyed as part of the roadway survey process, and statistics for these bridges are included with the computerized data.

In addition, two specific pedestrian bridges were identified for the survey team, and these bridges were surveyed separately by conventional methods. These include the following:

1. Pedestrian bridge over Arcade Creek along Pasadena Avenue, north of Winding Way:

On the south side, this bridge is reached via a 200-foot long, 12-foot wide asphalt pathway along the east side of Pasadena Avenue, heading north from Winding Way. A 66-foot long level path, composed of crushed gravel and dirt, leads to the south foot of the bridge. There are four bollards allowing a clearance of 15 inches between them at the south entrance to the bridge, and asphalt pavement at this location is broken and cracked, with gaps and changes in level exceeding 1.5 inches. The bridge itself is concrete, level and in good condition, with four-foot high chain link fencing on both sides. On the north side, there are two bollards allowing a clearance of 15 inches between them at the north entrance to the bridge, and asphalt pavement at this location is broken and cracked, with gaps and changes in level exceeding one inch. An asphalt ramp leads down to a cul-de-sac at the end of Pasadena Avenue on the north side. This ramp is 84 feet long and has an average slope of 12 percent, with some portions being as steep as 15 percent. The ramp has no level landings or handrails. There are neither directional signs nor lighting along the route. The portion of Pasadena Avenue north of the bridge does not have sidewalks, as the roadway is undeveloped and the area rural in nature.

2. Pedestrian bridge over Chicken Ranch Slough, north of Clairidge Way and south of Cowan Fundamental School: (Note: According to signage at the site, this bridge is not under the jurisdiction of SacDOT, but rather the San Juan School District.)

This bridge is reached via a 150-foot long, eight-foot wide asphalt pathway located mid-block between private houses, and running north from Clairidge Way. The south entry to the bridge has a concrete abutment, which is in good condition. The bridge itself is made of wood boards, which are in fair to good condition, with a two-inches high curb and four-foot high chain link fencing on both sides. The north abutment is asphalt pavement, which is broken in places and in poor condition. Three bollards limit the clear width to 16 inches. The north end of the bridge connects to asphalt paths that are on the property of Cowan Fundamental School, according to signage at the site.

Photographs

As part of the survey process, approximately 1,700 digital photographs of corners within the unincorporated County were taken. These photographs are in .jpeg format and are stored on a separate compact disk. Criteria for selecting intersections for photographs generally included the following:

1. Major intersections identified to be of critical importance,
2. Specific intersections as requested by various parties,
3. Intersections denoted as irregular during the normal course of surveying, and
4. Intersections deemed to be significant by the survey team during the normal course of surveying.

For information on retrieving and viewing photographs, see the next section.

Section 4.6: ADA Public Rights-of-Way Database Preparation and Contents

All survey findings are contained in a Microsoft Access database titled the ADA Public Rights-of-Way Database. The database is designed to be user-friendly, with interactive screens available to access the summary report, reports for each individual intersection or roadway segment, and photographs. The database includes data entry screens to be used for monitoring and status reports.

When the database is opened, a welcome screen guides the user through a series of buttons, which access the next layer of screens. An Intersections button opens a search screen for finding data for each particular intersection. Data for a specific intersection may be found by either typing a street name or by selecting either of the two intersecting streets from a drop-down list. Once the first street is entered, a drop-down list appears that lists all intersecting streets. Once the second street is selected, a list of the corners with curb ramps appears. Intersections also may be found by using the unique County GIS number.

A series of one-page intersection reports are available through buttons on the screen. The first page of the report gives general data and data for corners. The second page of the report gives data for curb ramps. The third page of the report gives data for crosswalks and pedestrian signals. The fourth page of the report accesses the digital photographs. If no photographs were taken for the specific intersection, a dialog box appears with this information. If photographs are available, the user is instructed to insert the photo CD. Photographs are cataloged on the CD by GIS number and corner direction in numerical order. Photographs may either be viewed from the CD or copied onto the fourth page of the intersection report.

A Roadways button opens a search screen for finding data for each particular roadway segment. Data for a specific roadway segment may be found by either typing the street name or by selecting the street name from a drop-down list. Once entered, a drop-down list appears that lists all intersecting cross streets. Once a second street is selected, a drop-down list appears that lists the other intersecting cross street. Roadway segments also may be found by using the two unique County GIS numbers of the beginning and ending intersections. A one-page roadway and sidewalk report is available through a button on the screen. The report gives general data for the roadway segment and specific data for each side of the roadway.

The ADA Public Rights-of-Way Database is voluminous and is not included as a part of the text of this ADA Transition Plan. Nevertheless, the computerized database is intended to be made available to the general public, either by public access computers made available at the SacDOT office or other methods to be determined by the County.

Section 5: ADA Codes and Standards

Introduction

The ADA Codes and Standards were developed as part of an extensive process to propose applicable guidelines, codes and standards as they relate to the accessibility of all facilities within the public right-of-way in the unincorporated County. The ADA Codes and Standards went through a total of three versions. The TAC and ADA CAG reviewed and provided input to each version.

The first draft of an ADA Codes and Standards Matrix was presented to SacDOT, the ADA CAG and the TAC on May 6, 2002. Based upon comments from all parties received verbally at subsequent meetings, a second draft of the ADA Codes and Standards Matrix was submitted to SacDOT, the ADA CAG and the TAC on November 22, 2002. All parties then were asked to provide written comments on the standards by December 31, 2002. A total of 16 separate response documents were received. All comments from respondents (including some comments received after the December 31, 2002 date) were analyzed by the study team and considered in the preparation of the final draft document. The final draft document was completed and submitted for review on April 25, 2003, and it was reviewed at ADA CAG and TAC meetings on June 2, 2003.

The ADA Codes and Standards were developed to combine and resolve any conflicts between the Americans with Disabilities Act Accessibility Guidelines (ADAAG), published by the U.S. Architectural and Transportation Barriers Compliance Board in July 1991, and the California State Building Code, Title 24, Part 2, of the California Code of Regulation, 2001 edition. Draft Guidelines for Public Rights-of-Way, published by the U.S. Architectural and Transportation Barriers Compliance Board on June 17, 2002, which are expected to take effect in the near future, also were considered, but not necessarily replicated, in the ADA Codes and Standards described in this section. In addition, all County of Sacramento Board of Supervisor approved policies and standards affecting accessibility in the public right-of way were included in the ADA Codes and Standards.

The ADA Codes and Standards described in this section are intended to apply to all construction undertaken with the unincorporated County right-of way after the final approval of the ADA Transition Plan. The codes and standards would include all new development and all construction undertaken as part of the ADA Capital Implementation Plan included in Section 6.

Appendix B illustrates the Standard County Improvement Drawings for curb ramps, sidewalks, driveways, bus stops and other applicable issues.

Section 5.1: Applicability of County ADA Standards

This section describes how the ADA codes and standards impact SacDOT standards and procedures.

1.1 New Development: All areas of newly designed and newly constructed facilities in the County-regulated public right-of-way shall comply with these standards.

1.2 Additions in the Existing Public Right-of-Way: Each addition to an existing County-regulated public right-of-way shall comply with the applicable provisions of these standards. Where the addition connects with existing construction, the connection shall comply with Alterations, as described in the next subsection.

1.3 Alterations in the Existing Public Right-of-Way: Where existing elements or spaces in the County-regulated public right-of-way are altered, each altered element or space shall comply with the applicable provisions of these standards.

1.3.1 Exception: In alterations, where compliance with applicable provisions is technically infeasible, the alteration shall comply to the maximum extent feasible.

1.3.2 Prohibited Reduction in Access. An alteration that decreases or has the effect of decreasing the accessibility of a public right-of-way or site arrival points to buildings or facilities adjacent to the altered portion of the public right-of-way, below the requirements for new construction at the time that the alteration is prohibited.

1.4 Approval Procedures for Exceptions, Equivalent Facilitation and Technically Infeasible Conditions:

SacDOT shall appoint a SacDOT Program Access Coordinator, whose main duties are to review all aspects of compliance with the ADA Codes and Standards contained in this document. The SacDOT Program Access Coordinator shall report directly to the Director of SacDOT, and the Director may delegate such approval authority and responsibility contained in these standards to the SacDOT Program Access Coordinator, as he/she determines to be appropriate.

The SacDOT Program Access Coordinator also shall coordinate all activities with the Chief of the Disability Compliance Office, and shall send all determinations of exceptions, equivalent facilitation and technical infeasibility to the Chief of the Disability Compliance Office. The Chief of the Disability Compliance Office shall corroborate with SacDOT's determinations in all such cases. The Chief of the Disability Compliance Office may delegate specific determinations to the SacDOT Program Access Coordinator as he/she determines to be appropriate.

Upon determinations of exception, equivalent facilitation and technical infeasibility, the Chief of the Disability Compliance Office also shall send all such determinations to the Physical Access Subcommittee of the County's Disability Advisory Committee. The Physical Access

Subcommittee may approve or disapprove of any specific determinations of exception, equivalent facilitation and technical infeasibility. Upon disapproval, the Chief of the Disability Compliance Office may recommend a revision of the SacDOT determination or uphold the original determination. Any member of the Physical Access Subcommittee or of the public may appeal the determination, per the procedures outlined in the SacDOT ADA Grievance Procedures.

1.5 Dimensional Tolerances: All dimensions and numerical requirements contained in these standards are absolute and requirements have been derived taking into account construction practices and constraints, and no dimensional tolerances beyond the maximum or minimum dimensions are allowed, unless otherwise stated.

1.5.1 Advisory: It is advised that designers use numerical criteria in designs and specifications that are below the maximum or are above the minimum requirements stated in these standards, so that the final constructed improvements meet the stated requirements.

1.6 Inclusion and Incorporation into Existing County Improvement Standards:

The intent of the listing of these standards is that all standards will be included and incorporated into SacDOT's Improvement Standards, Section 4 - Streets.

Where parentheses follow a specific standard, the number refers to the specific current County Improvement Standard sections that correspond to the specific requirements and in which the new standards will be included or incorporated.

Standard County Improvement Drawings also may be referenced as part of these standards (Appendix B). Written requirements as included in these standards shall take precedence over any drawings should there be any discrepancies in the requirements.

1.7 Future Applicable Federal and State Code Revisions: All future enactments and revisions to legally applicable Federal or State accessibility codes, standards or guidelines, such as the ADA Accessibility Guidelines or Title 24 of the California Code of regulation, shall be incorporated into these ADA Codes and Standards to the extent that such enactments or revisions exceed the requirements contained herein. Nevertheless, such enactments or revisions shall not decrease any requirement as contained herein.

Section 5.2: Applicable Reference Codes and Standards

The following codes and standards are referenced as applicable by law or statute. Nothing in these County standards shall have the effect of reducing any specific requirements of the referenced standards (1) or (3), or any other codes or standards required by applicable law or statute. Should other new codes or standards become applicable law or statute after the adoption of these County standards, such new codes or standards shall supercede these County standards, but only to the extent that new codes or standards are more restrictive or exceed these County standards.

(1) **The Americans with Disabilities Act Accessibility Guidelines (ADAAG)**, published by the U.S. Architectural and Transportation Barriers Compliance Board in July 1991, binding regulatory law in 1992, with several revisions through July 1998. (Note: Some jurisdictions mistakenly use a revised edition of these standards dated September 1994; this edition was never approved and should NOT be used.) The ADAAG guidelines were written to apply to newly constructed places of public accommodation. The ADAAG is an appendix to Title III of the ADA. The technical standards of the ADAAG also provide a technical definition for accessible elements. These guidelines were not written to specifically apply to public facilities, which must provide equal access to people with disabilities to all programs and services of local and state governments. Therefore, while meeting the technical requirements of the ADAAG assures owners of places of public accommodation of full compliance with the ADA, such technical compliance may not be sufficient to provide full access to programs and services for government entities.

(2) **Draft Guidelines for Public Rights-of-Way**, published by the U.S. Architectural and Transportation Barriers Compliance Board on June 17, 2002. These guidelines are currently out for public review and are intended to replace the current ADAAG guidelines listed in (1) in the future. The guidelines have not been approved, but are represented to be the most current state-of-the-art with respect to accessibility in the public right-of-way. The guidelines also were written to apply to new construction. The extent to which they should be applied to major alterations and retrofits is still under review by the Access Board, and is scheduled to be the subject of a technical assistance manual due for release in 2004.

(3) **California State Building Code, Title 24, Part 2**, of the California Code of Regulation, 2001 edition. These code requirements apply to any actual construction work within the public right-of-way at the time that the work is constructed, but the requirements of Title 24 are limited to the actual work being constructed and do not apply to adjacent areas beyond the construction limits.

(4) **Current SacDOT Improvement Standards**, including (a) County Design Improvement Standards for Streets, Sections 4-1 through 4-33, June 11, 2003, (b) Policy on Street and Sidewalk Access Improvement Priorities, December, 2000, (c) Policy for Audible Pedestrian Signals, adopted by the Board of Supervisors on July 20, 1999, (d) Improvement Standards for Crosswalks and Tactile Guidestrips, May, 2001, (e) Sacramento County Guidestrip Policy, approved by the Board of Supervisors on August 18, 1987.

Section 5.3: Sidewalk and Pedestrian Access Standards

3.1 Scope: Where sidewalks, corners or pedestrian access paths are provided adjacent to streets or roadways within the public right-of-way, they shall meet the requirements of this section.

3.2 Clear Width: Where a sidewalk is provided adjacent to a street or roadway, each part shall provide a minimum clear width of 48 inches, not including the width of any curb that may be present between the sidewalk and the street or gutter. This standard already is required in the County Design Improvement Standards for Streets, Section 4-21, June 11, 2003.

3.2.1 Exception: All frontages directly in front of all school properties shall have a clear width of 96 inches, except frontages in front of fenced play areas with no access may have a clear width of 72 inches.

3.2.2 Exception: Where existing conditions or obstructions or reduced right-of-way widths preclude providing a 48 inch clear width, the sidewalk width may be reduced to less than 48 inches for a distance not exceeding 24 inches, but in no case shall the clear width be less than 36 inches.

3.2.3 Advisory: For streets or roadways with a right-of-way width of 84 feet or greater, a minimum clear width of 72 inches is preferred.

3.3 Passing Space: If a sidewalk has less than 60 inches clear width, a passing space of at least 60 inches by 60 inches shall be located at reasonable intervals not to exceed 200 feet.

3.3.1 Exception: Where existing conditions or reduced right-of-way width preclude providing a 60 inch passing space, such space shall not be required.

3.4 Cross Slope: The cross slope of the sidewalk shall be 1:67 (1.5 percent), with allowances for a construction variance of 1:200 (0.5 percent) in either direction.

3.5 Running Slope: The running slope of the sidewalk shall not exceed the grade of the adjacent roadway or 1:20 (five percent), whichever is greater.

3.6 Level Areas on Continuous Slopes: For sidewalks with a running slope exceeding five percent for at least 400 feet, a 60-inch long landing with a maximum slope of two percent shall be provided for every 400 feet of the sidewalk length, except for roadway overpasses.

3.7 Meandering Sidewalks: Sidewalks may be separated from the curb by approved landscaping, forming a meandering sidewalk. The distance between the back of the curb and the edge of the sidewalk shall not be less than five feet nor more than 25 feet, except at transitions. If trees are planted between the back of the curb and the edge of the sidewalk, the distance between the back of the curb and the edge of the sidewalk shall not be less than five feet.

Meandering sidewalks shall comply with the requirements of either Case I or Case II, as described below. This standard already is required in the County Design Improvement Standards for Streets, Section 4-21, June 11, 2003.

For Case I, the sidewalk shall have a 24-inch wide minimum straight path along the sidewalk. For Case II, the sidewalk shall have no abrupt changes of direction and shall be constructed using only tangents of any length and inside radii of at least 150 feet. Refer to County Standard Drawing 4-29 (Appendix B).

3.8 Curbs at Streets Adjacent to Sidewalks: Curbs on the street side of sidewalks and corners shall be approximately vertical, with a height of at least five inches but no greater than eight inches. This standard already is required in the County Design Improvement Standards for Streets, Section 4-18, June 11, 2003.

3.8.1 Exception: Where a new portion of curb is constructed within an existing system of rolled curbs and existing drainage patterns must be maintained, a rolled curb matching the existing curb may be constructed. This exception shall not apply to a transit stop location, where the curb must be provided per Section 8.6

3.9 Surfaces: The surface shall be either Portland cement concrete or asphalt concrete, and it shall be firm, stable and slip-resistant.

3.9.1 Exception: A material other than concrete or asphalt may be used when it can be adequately demonstrated to the SacDOT Program Access Coordinator that it provides an equal firm, stable and slip-resistant surface.

3.10 Changes in Level: Changes in level up to 1/4 inch may be vertical and without edge treatment. Changes in level between 1/4 inch and 1/2 inch shall be beveled with a slope no greater than one horizontal to two vertical. Changes in level greater than 1/2 inch (13 mm) shall be accomplished by means of a ramp. Multiple changes in level shall be separated horizontally by at least 30 inches.

3.11 Gratings: If gratings are located in the sidewalk surface along a pedestrian access route or in the accessible portion of a curb ramp, they shall have spaces no greater than 1/2 inch wide in the direction of travel. If gratings have elongated openings, they shall be placed so that the long dimension is perpendicular to the direction of travel. Whenever possible, drainage inlets should be located outside of the crosswalk area, particularly the portion of the crosswalks that adjoin the accessible portion of curb ramps.

3.12 Protruding Objects:

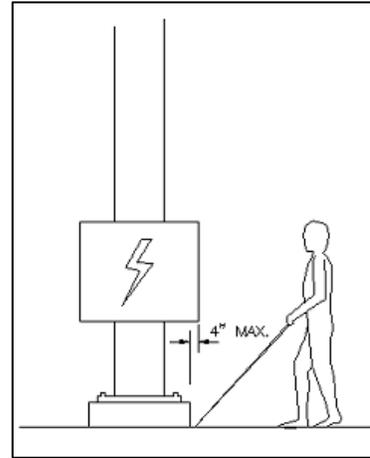
Protruding objects shall not reduce the clear width required for sidewalks.

Objects with leading edges located between 27 inches above and 80 inches below the finish surface shall protrude no more than four inches horizontally into the pedestrian access route.

Free-standing objects mounted on posts or pylons shall overhang pedestrian access routes no more than four inches when located between 27 inches above and 80 inches below the finish surface (Figure 3).

Where a sign or other obstruction is mounted between posts or pylons and the clear distance between post or pylons is greater than 12 inches, the lowest edge of such sign or obstruction shall be located between 27 inches above and 80 inches below above the surface, and there shall be a bar or similarly detectable element 15 inches above the surface connecting the two posts or pylons.

Figure 3: Barrier for Vertical Clearance Less than 80 Inches



Source: Public Rights-of-Way Access Advisory Committee, *Building A True Community*, January 2001.
Note: For alternative format, refer to the corresponding text

3.13 Barrier Curbs at Drop-offs:

Warning or barrier curbs shall be provided at the locations described below:

Abrupt changes in level at the edge of sidewalks, except between a sidewalk and an adjacent street, exceeding four inches in a vertical dimension, such as at planters or fountains located in or adjacent to sidewalks, shall be identified by curbs projecting at least six inches in height above the surface.

At bus stops, where a slope behind a sidewalk slopes toward the sidewalk, a barrier curb projecting at least six inches in height above the surface shall be provided to prevent water flow across the sidewalk.

Where the slope behind a sidewalk is greater than six (horizontal) to one (vertical) and the slope is away from the sidewalk, a barrier curb projecting at least six inches in height above the surface shall be provided for pedestrian safety. A retaining wall or fence may be provided in lieu of the required barrier curb.

These standards already are required in the County Design Improvement Standards for Streets, Section 4-20, June 11, 2003.

3.14 Driveway Crossings:

Where a sidewalk crosses a driveway, the minimum width of 48 inches and the cross slope of 1:67 (1.5 percent), with allowances for a construction variance of 1:200 (0.5 percent) in either direction, shall be provided for the entire width of the driveway.

Each driveway shall have a ½-inch to one-inch lip, beveled at 45 degrees, at the street or gutter.

Driveway entries shall not be designed or used as curb ramps.

This standard already is required in the County Design Improvement Standards for Streets, Section 4-10, June 11, 2003.

3.15 Rail Crossings:

Where a sidewalk crosses rail systems at grade, the surface of the sidewalk shall be level and flush with the top of the rail at the outer edge and between the rails.

Where a sidewalk crosses rail systems at grade, the horizontal gap at the inner edge of each rail shall be constructed to the minimum dimension necessary to allow passage of railroad car wheel flanges and shall not exceed 2½ inches (three inches for freight rails).

Where a sidewalk crosses rail systems at grade, detectable warning surfaces complying with Section 5.5 “Detectable Warning Standards” shall extend the full width of the sidewalk and 36 inches deep in the direction of pedestrian travel and shall be provided on each side of the rails.

3.16 Stairs: To the maximum extent feasible, stairs shall not be constructed within the public right-of way.

3.16.1 Exception: If provided, steps or stairs shall provide 1.5 inch diameter handrails 34 inches to 38 inches above each nosing on both sides, with extensions at the top and bottom meeting all applicable portions of the California State Building Code, Chapter 11B. If provided, steps or stairs shall provide a two inch contrasting yellow color stripe at each tread and the upper approach of each staircase. The contrasting color stripe shall be yellow conforming to Federal Color No. 33538, as shown in Table IV of Standard No. 595B.

Section 5.4: Curb Ramp and Blended Transition Standards

4.1 Scope: Each corner of an intersection shall be provided with two curb ramps, each oriented in the direction of pedestrian crossing to the adjacent corner, except that only one curb ramp with a six foot pan may be provided if two curb ramps are technically infeasible or excepted as described below. Curb ramps shall comply with the requirements of this section for flared sides, detectable warning devices, landings and ramps (Figure 4).

4.1.1 Exception: Where pedestrian crossing in a specific direction is prohibited by a continuous raised median, barricade or sign, no curb ramp shall be provided. Where only one curb ramp is provided at a corner to serve only one direction of travel to an adjacent corner, the curb ramp shall be aligned and oriented parallel to the intended direction of travel.

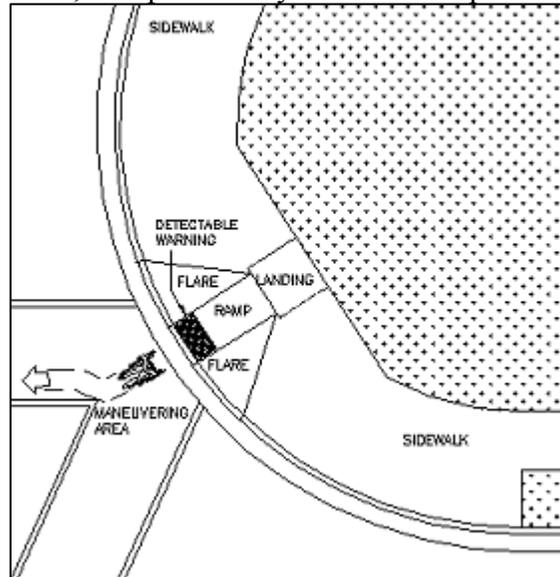


Figure 4: Curb Ramp Components

Source: Public Rights-of-Way Access Advisory Committee, *Building A True Community*, January 2001.

Note: The illustration shows the location of the ramp, flares, landing and other curb ramp features. For alternative format, refer to the corresponding text.

4.1.2 Exception: One curb ramp located at the center of the curb return at each corner or directional to the path of travel may be provided if technically infeasible to construct two ramps. For major streets with right-of-way width of 80' or larger, one curb ramp with a six foot pan shall be provided. For collector and minor residential streets with right-of-way width less than 80', one curb ramp with a four foot pan shall be provided where the sidewalk is located adjacent to the curb and gutter. Technical Infeasibility is based on the following intersection conditions:

1. A corner with a curb return radius that is so large that the crosswalks meet at the midpoint of the curve.
2. A corner where placing two curb ramps or flush landings would result in them being located outside the crosswalk markings, or would result in stop bars and/or stop signs or ramps placed too far back on the side street for driver safety or pedestrian safety.
3. An intersection that is skewed, such that two curb ramps or flush landings will not fit in the acute angle corners

4. An alteration, where the corner has retaining walls, buildings, signal poles and/or controller cabinets, utility poles or other barriers that are technically infeasible to relocate.
5. An intersection in which one street has an unavoidably steep grade, and a shared curb ramp or flush landing at the midpoint of the curb return may have less severe warp than a curb ramp or flush landing closer to the tangent of the steep street.
6. An intersection in an area of steep terrain, where both streets are flattened to allow for acceptable crosswalk slopes. It may be feasible to flatten a small intersection area and provide acceptable crosswalks leading to a shared ramp. Placement of a pair of curb ramps would necessitate a larger flattened area, resulting in steeper sidewalks between intersections.

Where the above conditions exist, the designers are encouraged to try to reduce the curb radius or take other measures to eliminate the need for shared curb ramps or flush landings.”

In cases where curb ramps are installed on collector and minor residential streets (less than 80' in right-of-way width) intersections where the sidewalk is adjacent to the curb and gutter, a single ramp is normally installed. The reasoning is that on those street intersections, two ramps with flush landings would locate the crossing too far from the intersection for pedestrian safety (see item #2 above). In cases where a landscaped buffer is provided between the curb, gutter and sidewalk, two ramps can be constructed provided modified perpendicular ramps are installed..

4.2 Curb Ramp Types: Curb ramps shall be primarily perpendicular curb ramps, as shown in County Standard Drawing 4-23B (Appendix B), if there is sufficient right-of-way or sidewalk depth to construct the perpendicular curb ramp in full compliance with subsection 5.3. If there is not sufficient right-of-way or sidewalk depth to construct a perpendicular curb ramp, a parallel curb ramp, as shown in County Standard Drawing 4-23A (Appendix B), may be constructed. Blended transitions shall not be constructed, unless specifically approved by the Director of SacDOT and the Chief of the Disability Compliance Office.

4.3 Perpendicular Curb Ramps: Perpendicular curb ramps shall comply with the details described in this subsection, and shall have a running slope that cuts through the curb at right angles or meets the gutter grade break at right angles (Figure 5). This standard already is required in the County Design Improvement Standards for Streets, Section 4-17, June 11, 2003.

4.3.1 Running Slope: The running slope of the main portion of the curb ramp shall be 1:12 (8.33 percent) maximum.

4.3.1.1 Advisory: Where feasible, the minimum running slope of the main portion of the curb ramp is preferred to be 1:15 (6.67 percent).

4.3.2 Cross Slope: The cross slope of the main portion of the curb ramp shall be 1:67 (1.5 percent), with allowances for a construction variance of 1:200 (0.5 percent) in either direction.

4.3.3 Landing: A landing measuring 48 inches minimum by 48 inches minimum shall be provided at the top of the curb ramp, and shall be permitted to overlap other landings and clear spaces. Running and cross slopes of the landing shall be 1:67 (1.5 percent) maximum, with allowances for a construction variance of 1:200 (0.5 percent) in either direction.

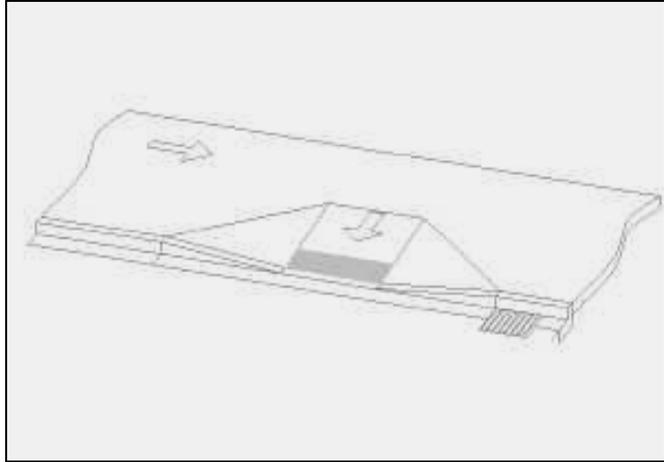


Figure 5: Perpendicular Curb Ramp

Note: For alternative format, refer to the corresponding text.

Source: www.access-board.gov/rowdraft.htm

4.3.4 Flared sides: Flared sides with a slope of 1:10 (ten percent) maximum, measured along the curb line, shall be provided where a circulation path crosses the curb ramp.

4.3.5 Clear Width: The clear width of the main portion of the curb ramp, excluding flared sides, shall be 48 inches minimum.

4.3.6 Detectable Warnings: Detectable warning surfaces complying with Section 5.5 shall be provided for the full width of the main portion of the curb ramp or blended transition, with the front edge located approximately six inches behind the curb line.

4.3.7 Grooved Border: A 12-inch wide grooved border with 1/4 inch grooves approximately 3/4 inch on center shall be provided at the top of the main slope and at the side of each side slope.

4.3.8 Surfaces: Surfaces of curb ramps and landings shall comply with Section 3.9. Gratings, access covers, and other appurtenances shall not be located on curb ramps, landings, and gutter areas directly in front of curb ramps.

4.3.8.1 Exception: Where existing conditions, obstructions or reduced right-of-way widths preclude the relocation of drop inlets outside of the gutter areas directly in front of curb ramps, then the gratings for the drop inlets shall conform to Section 3.11 “Gratings”.

4.3.9 Changes in Level: Vertical changes in level greater than those described in Section 3.10 shall not be permitted on curb ramps, landings or gutter areas directly in front of curb ramps.

4.3.10 Gutter Slope: The counter slope of the gutter area or street at the foot of a curb ramp or landing shall be 1:20 (five percent) maximum.

4.3.11 Clear Space: Beyond the curb line toward the street, a clear space measuring 48 inches minimum by 48 inches minimum shall be provided within any marked crosswalk that may be present and located wholly outside of the parallel vehicle travel lane.

4.3.12 Obstructions: Curb ramps shall be located or protected to prevent their obstruction by parked cars.

4.4 Parallel Curb Ramps: Parallel curb ramps shall comply with the details described in this subsection, and shall have running slopes that are in-line with the direction of sidewalk travel (Figure 6). This standard already is required in the County Design Improvement Standards for Streets, Section 4-17, June 11, 2003.

4.4.1 Running Slope: The running slope of each side slope shall be 1:12 (8.33 percent) maximum.

4.4.1.1 Advisory: Where feasible, the minimum running slope of each side slope is preferred to be 1:15 (6.67 percent).

4.4.2 Cross Slope: The cross slope of each side shall be 1:67 (1.5 percent), with allowances for a construction variance of 1:200 (0.5 percent) in either direction.

4.4.3 Clear Width: The clear width of each side slope shall be 48 inches minimum.

4.4.4 Landing: A landing measuring 48 inches minimum by 48 inches minimum shall be provided at the bottom of each ramp slope. Landing slopes shall be 1:100 (one percent) minimum and 1:67 (1.5 percent) maximum, with allowances for a construction variance of 1:200 (0.5 percent) in either direction.

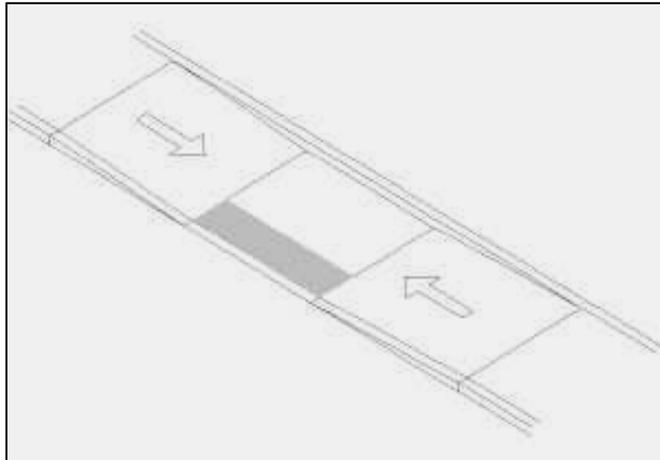


Figure 6: Parallel Curb Ramp

Note: For alternative format, refer to the corresponding text.

Source: www.access-board.gov/rowdraft.htm

4.4.5 Diverging Sidewalks: Where a parallel curb ramp does not occupy the entire width of a sidewalk, drop-offs at diverging segments shall be protected with a six inch curb or similar barrier.

4.4.6 Common Landing Width: Where two parallel curb ramps are located at a corner, the landing between the top of each side slope shall be 48 inches minimum.

4.4.7 Detectable Warnings: Detectable warning surfaces complying with Section 5.5 shall be provided for the full width of the lower landing between the side slopes of the curb ramp, with the front edge located approximately six inches behind the curb line.

4.4.8 Grooved Border: A 12-inch wide grooved border with 1/4 inch grooves approximately 3/4 inch on center shall be provided at the top of each side slope.

4.4.9 Surfaces: Surfaces of curb ramps and landings shall comply with Section 3.9. Gratings, access covers and other appurtenances shall not be located on curb ramps, landings and gutter areas directly in front of curb ramps.

4.4.9.1 Exception: Where existing conditions or obstructions or reduced right-of-way widths preclude the relocation of drop inlets outside of the gutter areas directly in front of curb ramps, then the gratings for the drop inlets shall conform to Section 3.11 "Gratings".

4.4.10 Changes in Level: Vertical changes in level greater than those described in Section 3.10 shall not be permitted on curb ramps, landings, or gutter areas directly in front of curb ramps.

4.4.11 Gutter Slope: The counter slope of the gutter area or street at the foot of the lower landing shall be 1:20 (five percent) maximum.

4.4.12 Clear Space: Beyond the curb line toward the street, a clear space of 48 inches minimum by 48 inches minimum shall be provided within any marked crosswalk that may be present and located wholly outside the parallel vehicle travel lane.

4.4.13 Obstructions: Curb ramps shall be located or protected to prevent their obstruction by parked cars.

Section 5.5: Detectable Warning Standards

5.1 Scope: Where detectable warnings (truncated domes) are required by other sections of these standards, they shall comply with the requirements of this section.

5.2 Size and Location: Detectable warnings shall be 36 inches in depth and span the full width of the area where they are required.

5.3 Specifications: The detectable warning surface shall be prefabricated durable slip resistant surface and shall have in-line square grid pattern truncated domes with a diameter of nominal 0.9 in. (22.9 mm) at the base tapering to 0.45 in. (11.4 mm) at the top, a height of nominal 0.2 in. (5.08 mm), and a center-to-center spacing of 1.66 in. (59.7 mm) (2.35 in. center-to-center spacing on the diagonal.) “Nominal” as used here shall be in accordance with California State Referenced Standards Code Sections 12-11A and B-102. Detectable warnings shall be safety yellow conforming to Federal Color No. 33538, Table IV of Standard No. 595B. The 0.2 inch height of domes shall be measured from the top of the highest point on the field slip resistant surface to the highest point on the top of the truncated dome. Detectable warning surfaces shall differ from adjoining walking surfaces in resiliency or sound on cane contact.

Section 5.6: Pedestrian Crossing Standards

6.1 Scope: All signalized intersections shall be provided with marked crosswalks as described in this section. Crosswalk markings should be designed and installed per the County's Traffic Signal and Striping Design Guidelines, the Manual of Uniform Traffic Control Devices (MUTCD) and the MUTCD California Supplement as referenced in Section 5.5 "Crosswalks: General" of the Sacramento County Pedestrian Design Guidelines. If provided, all marked crosswalks shall comply with the requirements of this section.

6.2 Width: Marked crosswalks shall be 96 inches wide minimum, as measured between the striped lines.

6.2.1 Advisory: Where feasible, marked crosswalks shall be 120 inches wide, as measured between the striped lines.

6.3 Color and Size: Crosswalk stripes shall be 12 inches wide, and white in color.

6.3.1 Exception: Crosswalks serving schools shall be yellow in color.

6.4 Advisory Cross Slope: The cross slope of the pavement within a marked crosswalk shall be 1:67 (1.5 percent) maximum, measured perpendicular to the direction of pedestrian travel, with allowances for a construction variance of 1:200 (0.5 percent) in either direction.

6.5 Running Slope: The running slope of the pavement within a marked crosswalk shall be 1:20 (five percent) maximum measured parallel to the direction of pedestrian travel in the crosswalk.

6.6 Pedestrian Signal Phase Timing: The Sacramento County, Department of Transportation, shall time its pedestrian intervals at signalized intersections following the most current Manual on Uniform Traffic Control Devices (MUTCD) Standard. This policy allows for timing at a reduced walking rate at locations where those who walk (or use wheelchairs) slower than the four feet per second standard routinely use the intersection. The County policy will change in accordance with changes to the MUTCD standard, which currently reads:

MUTCD's Section 4E.10 Pedestrian Intervals and Signal Phases

When pedestrian signal heads are used, a WALKING PERSON (symbolizing WALK) signal indication shall be displayed only when pedestrians are permitted to leave the curb or shoulder. A pedestrian clearance time shall begin immediately following the WALKING PERSON (symbolizing WALK) signal indication. The first portion of the pedestrian clearance time shall consist of a pedestrian change interval during which a flashing UPRAISED HAND (symbolizing DONT WALK) signal indication shall be displayed. The remaining portions shall consist of the yellow change interval and any red clearance interval (prior to a conflicting green being displayed), during which a flashing or steady UPRAISED HAND (symbolizing DONT WALK) signal indication shall be displayed. If countdown pedestrian signals are used, a steady UPRAISED HAND (symbolizing DONT WALK) signal indication shall be displayed during the yellow change interval and any red clearance interval (prior to a conflicting green being displayed) (see Section 4E.07). At intersections

equipped with pedestrian signal heads, the pedestrian signal indications shall be displayed except when the vehicular traffic control signal is being operated in the flashing mode. At those times, the pedestrian signal lenses shall not be illuminated.

Guidance:

Except as noted in the Option, the walk interval should be at least seven seconds in length so that pedestrians will have adequate opportunity to leave the curb or shoulder before the pedestrian clearance time begins.

Option:

If pedestrian volumes and characteristics do not require a seven-second walk interval, walk intervals as short as four seconds may be used.

Support:

The walk interval itself need not equal or exceed the pedestrian clearance time calculated for the roadway width, because many pedestrians will complete their crossing during the pedestrian clearance time.

Guidance:

The pedestrian clearance time should be sufficient to allow a pedestrian crossing in the crosswalk who left the curb or shoulder during the WALKING PERSON (symbolizing WALK) signal indication to travel at a walking speed of 1.2 m (4 ft) per second, to at least the far side of the traveled way or to a median of sufficient width for pedestrians to wait. Where pedestrians routinely walk (or use wheelchairs) slower than 1.2 m (4 ft) per second, a walking speed of less than 1.2 m (4 ft) per second should be considered in determining the pedestrian clearance time.

Option:

Passive pedestrian detection equipment, which can detect pedestrians who need more time to complete their crossing and can extend the length of the pedestrian clearance time for that particular cycle, may be used to avoid using a lower walking speed to determine the pedestrian clearance time.

Guidance:

Where the pedestrian clearance time is sufficient only for crossing from the curb or shoulder to a median of sufficient width for pedestrians to wait, additional measures should be considered, such as median-mounted pedestrian signals or additional signing.

Option:

The pedestrian clearance time may be entirely contained within the vehicular green interval, or may be entirely contained within the vehicular green and yellow change intervals. On a street with a median of sufficient width for pedestrians to wait, a pedestrian clearance time that allows the pedestrian to cross only from the curb or shoulder to the median may be

provided. During the transition into preemption, the walk interval and the pedestrian change interval may be shortened or omitted as described in Section 4D.13..

6.7 Medians and Pedestrian Refuge Islands: Medians and pedestrian refuge islands in crosswalks shall be cut through level with the street or have curb ramps complying with Section 5.4. Where the cut-through connects to the street, edges of the cut-through shall be aligned with the direction of the crosswalk for a length of 24 inches minimum.

6.7.1 Width: The width of all cut-throughs shall be 48 inches minimum.

6.7.1.1 Advisory: Where feasible, the width of all cut-throughs shall be 60 inches.

6.7.2 Length: Where signal timing is not designed or intended for full crossing of all traffic lanes or where the crossing is not signalized, cut-through medians and pedestrian refuge islands shall be 96 inches minimum in length in the direction of pedestrian travel.

6.7.3 Detectable Warnings: Medians and refuge islands shall have detectable warnings complying with the section titled Detectable Warnings (Section 5.5). Detectable warnings at cut-through islands shall span the full width of the cut-through and shall be separated by a 24-inch minimum length of walkway without detectable warnings.

6.8 Crosswalk Alignment: (per Sacramento County Guidestrip Policy, approved by the Board of Supervisors on August 18, 1987, see County Standard Drawing 4.29): Marked crosswalks shall have straight alignment, with no change of direction between the terminal ends of the crosswalk.

6.8.1 Exception: Where a straight crosswalk is not feasible at existing intersections due to the particular geometry of the intersection, or where blind pedestrians using a white cane have become confused and disoriented while making street crossings as a result of an intersection with unusual or nonstandard geometry, as determined by the ADA Transition Plan Oversight Committee, tactile guidestrips shall be installed within the crosswalk. Examples of unusual geometry include right turn lanes, unusual crosswalks, diagonal crosswalks, exceptionally large corner radii, T-intersections, intersections with exceptionally wide streets, and two one-way streets that intersect. Where required, a tactile guidestrip shall be located in the center of the crosswalk for the entire length of the crosswalk. The color of the tactile guidestrip shall match the crosswalk color. The tactile guidestrip material shall be vitrified polymer composite consisting of epoxy polymer composition employing aluminum oxide particles in the raised surface of the guidestrip. The nominal dimensions of the tactile guidestrip shall be 4 inches wide by 24 inches long by 5/16 inch thick. Tactile guidestrips shall be formed with structural flanges, which shall extend below the surface a minimum of 1-1/4 inch.

Section 5.7: Accessible Pedestrian Signal Standards

7.1 Scope: Each crosswalk with pedestrian signal indication shall have a signal device that includes accessible indications of the walk interval. Where a pedestrian pushbutton is provided, it shall be integrated into the signal device and shall comply with the requirements of this section.

7.2 Types and Location of Accessible Pedestrian Signals:

Accessible pedestrian signals (APS) may be either of the following types: (1) Overhead - the APS is mounted to the Pedestrian Head, or (2) Pedestrian Activated Signal Control (PASC) - the APS accessibility features is incorporated into the PASC.

All overhead and PASC accessible pedestrian signal devices shall serve the nearest crosswalk in relationship to their installation site. The speakers of all overhead and PASC APS devices shall be oriented toward the center of the crosswalk or the direction of travel to the maximum extent feasible. If possible due to intersection configuration, all overhead and PASC APS devices shall be separated a minimum of 120 inches from any other APS device, unless on an island or median, where space will not permit.

7.3 Audible Walk Indication:

The audible pedestrian signals shall emit two distinct audible signals that resemble birdcalls; "cuckoo" for the north-south walk phase and "peep-peep" for the east-west walk phase, (per the California Department of Transportation Traffic Manual, Revised, July 1996, Section 9-04.8) or the closest proximity to these compass directions.

Volume measured at 36 inches from the pedestrian signal device shall be between two and five decibel (dB) above ambient noise level and responsive to ambient noise level change. Automatic volume adjustment in response to ambient traffic sound level should be provided up to a maximum volume of 89 dB.

When accessible pedestrian signals have an audible tone, they shall have a tone for the walk interval. The audible tone shall be audible from the beginning of the associated walk interval.

Activation of the pedestrian-activated signal control shall simultaneously activate the accessible pedestrian signal. There shall be no extended button press required to activate the auditory tone feature that announces the onset of the walk interval. An extended button press shall be permitted to activate additional features (e.g. auditory announcement, "wait," "street name," etc). Buttons that provide additional features shall be marked with three Braille dots forming an equilateral triangle in the center of the pushbutton.

7.4 Pedestrian Pushbuttons: Pedestrian pushbuttons shall comply with the details described in this section.

7.4.1 Location: Pedestrian pushbuttons shall be located 60 inches maximum from the crosswalk line extended, and if possible due to intersection configuration, 120 inches maximum and 30 inches minimum from the curb line, and 120 inches minimum from any other pedestrian pushbutton at a crossing. The control face of the pushbutton shall be installed to face the intersection and be parallel to the direction of the crosswalk it serves.

7.4.2 Reach and Clear Space: A clear space measuring 30 inches wide by 48 inches deep shall be provided at each pushbutton and shall connect to or overlap the pedestrian path of travel.

7.4.3 Mounting Height: Pedestrian pushbuttons shall be mounted at a height from 34 inches minimum to 46 inches maximum to the centerline above the lowest adjacent walking surface.

7.4.4 Operation: Pedestrian pushbuttons shall require no more than five pounds of pressure to operate.

7.4.5 Size and Contrast: Pedestrian pushbuttons shall be a minimum of two inches across in one dimension and shall contrast visually with their housing or mounting.

7.4.6 Locator Tone: If used by the particular accessible signal manufacturer, pedestrian pushbuttons shall incorporate a locator tone (one per pole) at the pushbutton. Locator tone volume measured at 36 inches from the pushbutton shall be two dB minimum and five dB maximum above ambient noise level and shall be responsive to ambient noise level changes. Automatic volume adjustment in response to ambient traffic sound level should be provided up to a maximum volume of 89 dB. The duration of the locator tone shall be 0.15 seconds maximum and shall repeat at intervals of one second. The locator tone shall operate during the "don't walk" and flashing "don't walk" intervals only and shall be deactivated when the pedestrian signal system is not operative.

7.4.6.1 Exception: At existing pedestrian pushbuttons without locator tones, pole-supported pedestrian pushbuttons shall be identified with color coding consisting of a textured horizontal yellow band two inches in width encircling the pole, and a one-inch wide dark border band above and below the yellow band. Color-coding should be placed immediately above the pushbutton.

7.4.7 Vibrotactile Indicator: If used by the particular accessible signal manufacturer, pedestrian pushbuttons shall incorporate a vibrotactile indicator at the pushbutton. The vibrotactile indicator shall indicate that the walk interval is in effect, and for which direction it applies, through the use of a vibrating directional arrow. The vibrotactile indicator shall be part of the pedestrian pushbutton and adjacent to the intended crosswalk.

7.5 Directional Information and Signs: If used by the particular accessible signal manufacturer, pedestrian pushbuttons shall provide tactile and visual signs on the face of the

device or its housing or mounting indicating crosswalk direction and the name of the street containing the crosswalk served by the pedestrian signal.

7.5.1 Arrow: Signs shall include a tactile arrow aligned parallel to the crosswalk direction. The arrow shall be raised 1/32 inch minimum and shall be 1-1/2 inches minimum in length. The arrowhead shall be open at 45 degrees to the shaft and shall be 33 percent of the length of the shaft. Stroke width shall be 10 percent minimum and 15 percent maximum of arrow length. The arrow shall contrast with the background, white on black or black on white.

7.5.2 Street Name: Signs shall include street name information aligned parallel to the crosswalk direction. The street name shall be printed in non-serif raised white letters a minimum of 5/8 inches high, accompanied by contracted Grade 2 Braille below, on a black background.

Section 5.8: Transportation and Vehicle Access Standards

8.1 Scope: Where new public transportation facilities are constructed or substantially altered within the County-regulated right-of-way, they shall comply with the requirements of this section.

8.2 Location: Bus stops or other transit stops serving fixed transit routes shall be located on at least one pedestrian access route complying with the requirements of Section 5.3 for the pedestrian path of travel from the transit stop to the nearest four-way street intersection. Curb ramps located at the nearest four-way intersection or other locations along the pedestrian access route shall comply with the requirements of Section 5.4.

8.3 Clear Width: The sidewalk adjacent to each transit stop shall provide a minimum clear width of 72 inches, not including the width of any curb that may be present between the sidewalk and the street or gutter, for a minimum length of 28 feet. This standard already is required in the County Design Improvement Standards for Streets, Section 4-16, June 11, 2003.

8.3.1 Exception: All transit stops directly in front of all school properties shall have a clear width of 96 inches, except frontages in front of fenced play areas with no access may have a clear width of 72 inches.

8.4 Cross Slope: The cross slope of the sidewalk along the required length of the transit stop shall be 1:67 (1.5 percent), with allowances for construction a variance of 1:200 (0.5 percent) in either direction.

8.5 Running Slope: The running slope of the sidewalk along the required length of the transit stop shall not exceed the grade of the adjacent roadway or 1:20 (five percent), whichever is greater.

8.5.1 Advisory: A running slope of no greater than 1:48 (two percent) is preferred.

8.6 Curbs at Streets Adjacent to Sidewalks: Curbs on the street side of the sidewalk along the required length of the transit stop shall be approximately vertical, with a height of at least five inches but no greater than eight inches. This standard already is required in the County Design Improvement Standards for Streets, Section 4-16, June 11, 2003.

8.7 Surface: The surface of the sidewalk along the required length of the transit stop shall be either Portland cement concrete or asphalt concrete, and it shall be firm, stable and slip-resistant.

8.7.1 Exception: A material other than concrete or asphalt may be used when it can be adequately demonstrated to the SacDOT Program Access Coordinator that it provides an equal firm, stable, and slip-resistant surface.

8.8 Barrier Curbs at Drop-offs: At transit stops, where a slope behind a sidewalk slopes toward the sidewalk, a barrier curb projecting at least six inches in height above the surface shall be provided to prevent water flow across the sidewalk. This standard already is required in the County Design Improvement Standards for Streets, Section 4-20, June 11, 2003.

8.9 Bus Shelters: Where Regional Transit provides a shelter at a transit stop, their shelter shall be located along a pedestrian access route complying with Section 5.3, and it shall provide a minimum 42 inches by 60 inches clear space, not including benches that are located completely beneath the shelter.

8.10 Bus Stop Signage: Where provided by Regional Transit, all new bus stop signage shall comply with ADAAG 4.30.2,3,&.5 (1991).

8.10.1 Exception: Bus schedules, timetables or maps do not have to comply with these standards.

Section 5.9: Street and Sidewalk Furnishings and Appurtenances Standards

9.1 Clear Space: Street and sidewalk furnishings shall have a 30 inch wide (measured parallel to the pedestrian travel direction) by 48 inch deep (measured perpendicular to the pedestrian travel direction) clear space in front of each portion used by a pedestrian and shall be connected to the sidewalk or pedestrian access route.

9.2 Facilities and Elements: Where drinking fountains, telephones, concession stands, kiosks, information counters or public toilet facilities are provided, they shall comply with all applicable portions of the California State Building Code, Chapter 11B.

9.3 Benches: The leading edge of benches and all similar sidewalk furnishings shall be set back 12 inches minimum from the required minimum width of the pedestrian access route. Bench seats shall be 17 inches to 19 inches vertical from the adjacent walkway surface to the seat.

Section 5.10: Temporary Construction Standards

10.1 Scope: Where construction or other temporary conditions prohibit full access to pedestrian facilities with the County-regulated right-of-way, an alternate pedestrian route shall be provided in compliance with the requirements of this section.

10.2 Location: To the maximum extent feasible, the alternate pedestrian route shall parallel the disrupted pedestrian route, on the same side of the street. Where access is not available on the same side of the street, the alternate pedestrian route may be located on the opposite side of the street as long as the distance in excess of the disrupted pedestrian route does not exceed 300 feet, and as long as all requirements of these standards are met.

10.3 Elements: The alternate pedestrian route shall include sidewalks and pedestrian access routes, curb ramps, pedestrian crossings and all other elements included in these standards.

10.4 Width: The alternate pedestrian route shall have a width of 48 inches minimum.

10.4.1 Exception: Where technical infeasibility exists, the alternate pedestrian route may have a width of 36 inches minimum.

10.5 Barricade Protection: The alternate pedestrian route shall be protected with a solid barricade to separate alternate pedestrian route from any adjacent construction, drop-offs, openings or other hazards. Barricades shall be continuous, stable and non-flexible, and shall consist of a solid wall or fence with the bottom or lower rail 1-1/2 inches maximum above the walking surface, and the top of the fence, wall or upper rail 36 inches minimum above the walking surface. Barricade support members shall not protrude beyond the barricade face into the alternate pedestrian route. Barricades shall be of a contrasting color, with yellow or orange preferred. This standard already is required in the County Design Improvement Standards for Streets, Section 4-24, June 11, 2003.

10.6 Signs: Signs complying with California Building Code Section 1117B.5 shall be provided at both the near side and the far side of the intersection preceding a disrupted pedestrian route, with appropriate wording to guide pedestrians to the alternate pedestrian route. When raised characters or symbols are used, they shall be raised 1/32 inch (0.794 mm) minimum and shall be sans-serif uppercase characters accompanied by Grade 2 Braille. Dots shall be 1/10 inch (2.54 mm) on centers in each cell with 2/10 inch (5.08 mm) space between cells. Dots shall be raised a minimum of 1/40 inch (0.635 mm) above the background.

Section 6: ADA Capital Implementation Plan

Section 6.1: Introduction

The ADA Capital Implementation Plan is a final step in determining the extent of SacDOT-operated and other participants' projects necessary to implement the ADA Transition Plan within the unincorporated Sacramento County public right-of-way.

Types of projects included can be categorized as follows:

- Curb ramp construction or replacement projects based upon resident request.
- Curb ramp, sidewalk and intersection retrofit projects, included with street overlay or other street or sidewalk construction projects.
- Curb ramp, sidewalk and intersection retrofit projects, in conjunction with construction by private parties.
- Curb ramp, sidewalk and intersection retrofit projects deemed essential for mitigation of barriers based upon the finalized ADA Transition Plan.
- Street and sidewalk construction or retrofit projects planned for the improvement of overall pedestrian facilities.
- Signal retrofit projects.
- Roadway widening projects.

All proposed capital improvement projects were prioritized based upon SacDOT policy, as described in "Interim Policy on Streets and Sidewalk Access Improvement Priorities," dated and approved January 16, 2001.

A number of existing and potential programs and funding sources for capital improvement projects are described in this section. These programs include on-going SacDOT capital improvement and maintenance programs, as well as specific projects and funding sources allocated in the seven year Transportation Improvement Plan (TIP). The ADA Capital Implementation Plan uses, to the maximum extent possible, existing and prospective funding programs and sources. The plan recommends specific goals for the construction of accessibility improvements. While specifying locations and the scope of work, the plan also is intended to serve as a conceptual plan whereby the extent and goals of future projects will be evaluated prior to preparing detailed cost estimates. Once an overall scope of work and its financial impact is established, annual projects can be finalized and the exact number of specified improvements can be set as project goals.

The ADA Capital Implementation Plan includes a detailed and prioritized list of approximately 1,800 potential project locations and items of work, which have been reviewed by SacDOT, the ADA CAG and the TAC. This implementation plan, which targets higher priority uses, anticipates a 15- to 20-year implementation period to achieve compliance with program accessibility requirements. Additional ADA work, such as new construction and additional curb

ramps beyond the minimum program access requirements will continue beyond the timeframe identified above.

Section 6.2: Extent of Required ADA Work

The extent of work included in the ADA Transition Plan includes the types of capital improvements that should be made to intersections, streets and sidewalks. The extent of work included in the plan has been the result of an extensive process that has included review and recommendations of all basic elements of the ADA Transition Plan by SacDOT, the ADA CAG and the TAC. These basic elements include the ADA Codes and Standards, the ADA Monitoring Program and the ADA Prioritized Capital Implementation Plan. The general types and extent of ADA work that is required for SacDOT to transition into compliance with the programmatic access requirements of Title II of the ADA are included in this section.

Most recommended capital improvements will be comprehensive in their approach. A comprehensive approach refers to making a series of related improvements at each particular location of work in an effort to bring the entire location and any public uses (as described in the Use Priority 1 list on a subsequent page), if they occur, into compliance with the applicable ADA Codes and Standards. For example, at a typical four-way signalized intersection, the extent of work would include not only the construction of curb ramps at each corner, but it also would include removing accessibility barriers along the pedestrian route from any Priority 1 use leading to the curb ramps, and installing accessible pedestrian signals, crosswalk striping; accessible islands, if required; and appropriate signage. It is probable that some capital improvement projects may, to a lesser degree, include only specific elements that represent physical barriers that need to be removed at a particular location, or that are specifically funded by an existing program.

The typical extent and scope of work for the most common types of capital improvements, listed from most to least comprehensive, is shown below:

- (1) Complete ADA retrofit of signalized four-way intersection: eight new curb ramps, two per corner (unless infeasible due to existing conditions such as utility conflicts or geometry or an exception as described in Section 5.4); new complying sidewalk paving to meet existing sidewalks and other sidewalk improvements to provide access to Priority 1 uses along the path of travel; new accessible pedestrian signals with push buttons; and crosswalk striping (if not existing, including removal and replacement of crosswalk striping where in poor condition) for all crossing directions where crosswalks are required by the ADA Codes & Standards. Scope may include new islands with cut-throughs or curb ramps, if required by the standards or at the design engineer's discretion.
- (2) Complete ADA retrofit of controlled intersection: eight new curb ramps, two per corner (unless infeasible due to existing conditions such as utility conflicts or geometry or an exception as described in Section 5.4); and crosswalk striping (if not existing, including removal and replacement of crosswalk striping where in poor condition) for all crossing directions where crosswalks are required by the ADA Standards; new complying sidewalk paving to meet existing sidewalks and other sidewalk improvements to provide access to

- Priority 1 uses along the path of travel. Scope may include providing new islands with cut-throughs or curb ramps, if required by the standards or at the design engineer's discretion.
- (3) Complete ADA retrofit of signalized T-intersection: six new curb ramps with two per corner, except only one on either side of the top of the T (unless infeasible due to existing conditions such as utility conflicts or geometry or an exception as described in Section 5.4); new accessible pedestrian signals with push buttons; and crosswalk striping (if not existing, including removal and replacement of crosswalk striping where in poor condition) for all crossing directions where crosswalks are required by the ADA Standards; new complying sidewalk paving to meet existing sidewalks and other sidewalk improvements to provide access to Priority 1 uses along the path of travel. Scope may include providing new islands with cut-throughs or curb ramps, if required by the standards or at the design engineer's discretion.
 - (4) At signalized intersections, installation of new accessible pedestrian signals with push buttons and crosswalk striping (if not existing, including removal and replacement of crosswalk striping where in poor condition) for all crossing directions where crosswalks are required by the ADA Codes and Standards.
 - (5) Partial ADA retrofit at four-way intersection, single-family residential area: four new curb ramps (one per corner); crosswalk striping at all signalized or stop-controlled intersections, for crossing directions where a crossing is not prohibited.
 - (6) Partial ADA retrofit at T-intersection, single-family residential area: two new curb ramps to cross main street at one location of T-intersection, and at least one and preferably two new curb ramps to cross secondary street.
 - (7) One or more new single curb ramps where other curb ramps at the intersection are complying.
 - (8) Renovation of existing curb ramp to remove hazardous conditions.
 - (9) Installation of new curb, gutter and concrete sidewalk (Installation of sidewalk should be on a case-by-case basis).
 - (10) Partial curb, gutter and sidewalk installation to provide programmatic access (Installation of sidewalk should be on a case-by-case basis).
 - (11) Miscellaneous sidewalk or walkway repair or replacement.
 - (12) Selected sidewalk and bus pad pavement as required for transit access (Installation of sidewalk should be on a case-by-case basis).
 - (13) Selected sidewalk and bus pad pavement as required for new mid-block crosswalk with pedestrian signals (Installation of sidewalk should be on a case-by-case basis).
 - (14) Roadway widening or installation of required asphalt conforms for accessible pedestrian access routes.
 - (15) Removal of sidewalk barriers (either moving or removing the barrier or reconstructing the pedestrian walkway around the barrier, or the reconstruction of driveways).

Again, the above list is for project planning purposes only, and represents an attempt to categorize the general extent of work at each location. The exact extent of all ADA work is described in the ADA Codes and Standards.

Section 6.3: Prioritization Criteria for ADA Work

Capital improvement projects forming the ADA Capital Implementation Plan have been prioritized to determine which projects should be undertaken first. The major determinants for prioritizing and ranking projects is based upon current SacDOT policy, as described in "Interim Policy on Streets and Sidewalk Access Improvement Priorities," dated and approved January 16, 2001. It should be noted that current SacDOT policy regarding prioritization also follows ADA guidelines, as contained in 28 CFR Part 35 section 35.150 (c), (d) and 35.151 (e), and in the Accessibility Policy Statement of the U. S. Department of Transportation, dated July 1999.

Use Priority A: Public Input Requests

SacDOT has operated a program of public input requests for constructing curb ramps, installing accessible pedestrian signals and for providing other accessibility improvements on an annual basis for a number of years. The program is called the "Elderly and Disabled Accessibility Project," and is operated by SacDOT in conjunction with the Disability Compliance Office. Generally, requests for improvements come from community members with disabilities who wish to access shopping areas, medical facilities, bus stops, transportation and other facilities or areas to accommodate their activities of daily living. These requests should continue to be handled as the first line of priority.

When the requests come into SacDOT's Program Access Coordinator or to the Chief of the Disability Compliance Office, an evaluation for construction or reconstruction is undertaken. If a curb ramp is requested, the evaluation consists of the requested curb ramp and the entire intersection at which the curb ramp is located. Any existing curb ramp is evaluated for usability and safety to determine the usable path of travel through that intersection.

The Elderly and Disabled Accessibility Project primarily replaces curb, gutter and sidewalk sections with curb ramps. These ramps are installed at intersection corners, near schools and hospitals, at driveways or any other curb, gutter and sidewalk where accessibility needs to be improved. The curb ramp need is determined by constituent request, but if insufficient requests are made within the annual funding period, SacDOT, the Chief of the Disability Compliance Office and the Physical Access Subcommittee recommend additional curb ramps or other improvements necessary to fully use the available funds.

Use Priority 1: State and Local Governmental and Public Use

Priority 1 areas are those within the public right-of-way that abut or serve public and governmental agencies and offices, and include the following in the recommended order of priority:

1. State, county and local government buildings located within the unincorporated County,
2. Public hospitals, health clinics, medical clinics, mental health clinics and therapy centers,
3. Public housing projects and public homeless shelters,
4. Sheriff neighborhood service centers,
5. CalWorks offices, and Sacramento Employment Training Agency facilities,
6. County parks,
7. Public schools, including in the following order, but not limited to: community colleges; high school, junior high and elementary school programs with magnet programs for children with disabilities; and all other schools,
8. State and local district offices with high public traffic, beginning with, but not limited to: transportation hubs and major corridors and routes; Department of Motor Vehicles offices; state parks, and prisons.

Use Priority 2: Public Accommodations

Priority 2 areas are those within the public right-of-way that abut or serve places of public accommodations that are privately owned, including, but not limited to, the following in the recommended order of priority:

1. Private hospitals, doctors' offices, and medical and mental health offices,
2. Senior facilities,
3. Major shopping malls,
4. Large housing complexes,
5. Major employment sites,
6. Supermarkets,
7. Retail strip centers,
8. Small apartment facilities,
9. Service sites of disability organizations,
10. Rehabilitation facilities.

Use Priority 3: Low-Density Residential and Other Uses

Priority 3 areas are those within the public right-of-way that abut or serve:

1. Single-family residential areas,
2. Industrial areas,
3. Areas that have not fallen into any of the above groups.

Current SacDOT policy also uses basic considerations and evaluation factors when determining whether a curb is suitable for construction or reconstruction. These factors are called condition priorities because they are based on the physical condition of an existing intersection, corner or curb ramp. These factors also are used for consideration when determining the priority within a category/group list. The following list describes these factors, in order of importance (The descriptions below are verbatim from Board-approved SacDOT policy, with any additional descriptions not included in the SacDOT policy shown in parentheses):

Condition Priority 1:

The highest priority is to reconstruct curb ramps at locations where existing curb ramps have an unsafe condition that may cause a trip and fall. Examples are vertical displacement of the curb ramp, steep side slopes, deteriorated conditions, etc. (See also discussion of unsafe conditions in Condition Priority 5 below.)

Condition Priority 2:

A new curb ramp will be installed at locations where there is no curb ramp to provide accessibility.

Condition Priority 3:

When a corner has one existing curb ramp and conditions allow for the construction of an additional curb ramp at the same corner, and provided that traffic controls allow for a safe path of travel, an additional curb ramp will be installed. (SacDOT's policy is that this condition applies only to corners at intersections on arterial and thoroughfare streets, and that it would not apply to signalized locations on major streets for which the geometry of the intersection makes it impossible to install an eight-phase signal operation.)

Condition Priority 4:

A curb ramp is constructed or reconstructed at a location with difficult physical conditions such as major utility conflicts, physical barriers, or other constraints, which would create a hardship situation on the entity. (At rare locations, existing conflicting facilities or intersection geometry may make installation of a curb ramp technically infeasible.)

Condition Priority 5:

An existing curb ramp will be reconstructed when it does not meet current federal and state accessibility standards (i.e. steep slopes, improper landings, lack of detectable warnings, etc.).

In reviewing the priorities for reconstructing existing curb ramps, SacDOT should establish criteria that separate existing curb ramps that are so bad that they pose a real barrier or safety hazard from those that are just a bit out of specifications. Such determinations should be made on a case-by-case basis as described in Section 3.6. All of these non-complying curb ramps should be on the list of ADA-required work, but those curb ramps that for most persons work fairly well should be shifted to the end of the list. It is recommended that such criteria for those curb ramps and related facilities

that are out of compliance, but not posing a great need for quick reconstruction, are those with one or more of the following:

- Main slopes greater than 8.3 percent, but less than 11 percent.
- Side flared slopes greater than 10 percent, but less than 12 percent.
- Pan or landing cross-slopes greater than two percent, but less than four percent.
- Gutter slopes greater than five percent, but less than ten percent.
- Detectable warning surfaces missing.
- Curb ramp lips greater than one-half inch, but less than one inch.

Current SacDOT policy also includes priorities and evaluation criteria based upon pedestrian usage. A points system evaluates criteria such as proximity to facilities for the disabled, proximity to key facilities, density and access to public transit. The policy also contains mobility evaluation criteria. Since these policies are quite lengthy, they are not detailed in this report

All of these prioritization criteria were used, although not at the level of filling out evaluation forms for each curb ramp or intersection, in the preparation of the ADA Capital Implementation Plan.

Section 6.4: Types of Projects and Funding Sources

There are a number of existing and potential programs and funding sources for capital improvement projects included in the ADA Capital Implementation Plan. These programs are described in this section.

On-Going Capital Improvement Programs

These programs are operated by or coordinated with SacDOT on an on-going, annual basis. The extent of funding levels may be fixed or may vary yearly. These programs include the following:

1) Curb, Gutter and Sidewalk Maintenance Program

SacDOT's Curb, Gutter and Sidewalk Maintenance Program identifies curbs, gutters and sidewalks that are in need of repair or replacement and develops a priority list for their inclusion into the maintenance program. The Curb, Gutter and Sidewalk Maintenance Program has two facets: permanent replacement and temporary repair. Priority is based upon such factors as severity of damage, the amount of pedestrian traffic, and the proximity to schools, parks, bus stops and hospitals.

Defective residential curb, gutter and sidewalks are prioritized by a computerized rating system. Areas with sidewalks that have the highest numerical rating are inspected for any additional work that may not have been reported. Defective sidewalks in the immediate neighborhood then are included in the contract for replacement.

Damaged curb, gutter and sidewalk must meet certain criteria to be added to the scheduled priority list. Criteria includes: uplift or sag of 3/4 inch or more, misalignment of 1-1/4 inches or more, or standing water of more than one inch deep for a distance of more than ten feet. These criteria are being revised to comply with current ADA standards. A revised policy is expected to be approved by the Sacramento County Board of Supervisors in 2004.

2) Elderly and Disabled Accessibility Project

The Elderly and Disabled Accessibility Project primarily replaces curb, gutter and sidewalk with new curb ramps. These curb ramps and sidewalks are installed near schools and hospitals, at driveways, or at other curb, gutter and sidewalk locations where accessibility for persons with disabilities could be improved. Accessible pedestrian signals and tactile guidestrips also are installed as part of this program. Improvement needs are determined by constituents' requests. The Elderly and Disabled Accessibility Project is funded by Measure A funds at a current funding level of \$300,000 per year.

3) Pavement Maintenance Program (Street Overlay Projects)

SacDOT operates an annual pavement maintenance program for overlaying streets with new asphalt. Recent federal court judgements (most notably *Kenney v. Yerusalem, PA.*) have required that curb ramps be installed along sidewalks adjacent to street overlays, and the SacDOT has and continues to construct numerous curb ramps as a part of these projects.

4) Caltrans Construction Projects

Caltrans construction and renovation of roadways and facilities along State highways within the unincorporated County typically includes new curb ramps and other accessibility-related improvements. While SacDOT does not directly manage these projects, it coordinates locations and details of the work with Caltrans.

5) Private Developer Construction Projects adjacent to the County Right-of Way

There is typically private construction throughout the unincorporated County that has direct impact on improvements within the SacDOT right-of-way. As a condition of the approval of a building permit, Contractors are typically required to construct or improve the sidewalk, including curb ramps, directly adjacent to the subject property. For larger projects, developers also may be required to construct intersections complete with traffic signals.

A renewed training effort for plan checkers and inspectors is recommended to assure that the full potential of the ADA Transition Plan is realized.

Specific Funding Programs and Projects

The ADA Capital Implementation Program is envisioned as one that will use, to the maximum extent possible, existing and prospective funding programs and sources. The ADA improvements will be funded by a variety of funding sources either as stand alone projects or as a minor component of a transportation improvement project. These programs and sources include the following:

1) Funding Programs

Measure A Sales Tax Program: The Measure A Sales Tax Program utilizes Measure A revenues and expenditures for specific projects and uses from FY (Fiscal Year, from July 1 - June 30) 2001/2002 through FY 2008/2009.

Development Fee Program: The Development Fee Program utilizes fees charged to developers of large housing and commercial projects and has allocations for FY 2001/2002 through FY 2008/2009.

Road Fund: Road Fund is used for capital improvement projects and maintenance overlay projects.

Financing Districts: Expenditures and funding allocations are planned for the various Sacramento County Financing Districts, including:

Antelope Public Facilities Financing Plan

Elk Grove Specific Plan

Vineyard Public Facilities Financing Plan

Laguna Community Facilities District

Laguna Creek Ranch/Elliot Ranch

North Vineyards Station Public Facilities Financing Plan (not yet approved by the Board of Supervisors)

Fulton Avenue Property and Business Improvement District

Florin Road Partnership

Vineyards Springs Comprehensive Plan Public Facilities Funding Plan

State Funding Programs: projects funded by the various State funding programs:

STIP- State Transportation Improvement Program

SR2S- Safe Routes to School Program

TCRP- Traffic Congestion Relief Program

Federal Funding (IS TEA): Funding approved under the Intermodal Surface Transportation Efficiency Act of 1991 (ISTEA) programs:

CMAQ- Congestion Mitigation and Air Quality Program

HBRR- Highway Bridge Replacement & Rehabilitation Program

HES- Hazard Elimination & Safety Program

RSTP- Regional Surface Transportation Program

TEA- Transportation Enhancement Activities

Federal Funding (TEA-21): Funding approved under the 1998 Transportation Equity Act for the 21st Century (TEA-21). The funding programs included in TEA-21 are the same as for ISTEA, as indicated above. The CIP also includes funding allocations from the TEA-21 Intelligent Transportation Systems (ITS) Program for the Watt Avenue Transit Priority and Mobility Enhancement project.

2) Specific SacDOT Construction Projects

The following specific projects are funded through FY 2008-2009. The list does not include all SacDOT projects, but those that provide for a significant degree of funding for necessary accessibility improvements. Other projects also may be included in the detailed project list that follows in Appendix C.

Audible Pedestrian Signal Projects.

Sidewalk Continuity Projects.

Disabled Access to Transit.

Pedestrian Guide Strips Projects.

Traffic Signal Projects.

Major Intersections Design Projects.

Coloma Rd. Enhancements, Sunrise Blvd. to West of Truckee Rd.

Del Paso Manor Elementary Sidewalk Project.

Elkhorn Blvd. Widening, Rio Linda Blvd. to State Route 99.

Hickory Ave Bikeway Project, Greenback Ln. to Oak Ave.

Watt Ave Transit Priority & Mobility Enhancements.

North Vineyard Station Public Facilities Project.

Fulton Avenue Business Improvement Project.

Florin Road Beautification Project.

Folsom Blvd. Enhancements Project.

3) ADA Transition Plan Funding

As part of the ADA Transition Plan, it is foreseen that an ADA Transition Plan Retrofit Project will be recommended to provide funding for required ADA improvements. Funding may come from one or more of either Measure A sales tax funds, developer fees, Sacramento Area Council of Government programs, and currently unspecified County-wide sources, and not necessarily from SacDOT funds.

Section 6.5: Description of ADA Capital Implementation Plan

The ADA Capital Implementation Plan of the ADA Transition Plan includes specified goals for the construction of accessibility improvements. The exact goals should take into account all of the various items of work required under the plan, including curb ramps, accessible pedestrian signals, sidewalk barrier removal and sidewalk installation, crosswalk markings and other work necessary to comply with the ADA Codes and Standards. The ADA Capital Implementation Plan lays the groundwork for concepts concerning the extent of ADA work required, prioritization, locations and potential funding sources. Until exact funding sources are finalized, the annual work and expenditures proposed must be of a preliminary nature.

It is recommended that SacDOT commit to an aggressive schedule to bring the unincorporated County into ADA compliance. This work should include installation, repair and replacement of curb ramps, together with other specified improvements, on an annual basis. An evaluation of existing annual ADA work is presented on the following page as Table 3.

If new construction associated with development is removed from the totals shown in Table 3, it is estimated that SacDOT's estimated annual budget for the ADA Capitol Implementation Plan is roughly \$4,593,000. SacDOT proposes to increase the Measure A funding to the Elderly and Disabled Annual Program, which has a current annual budget of \$300,000 from Measure A Sales Tax, as follows:

Fiscal Year	Elderly & Disabled Program Funding
2004/2005	\$ 750,000
2005/2006	\$1,500,000
2006/2007	\$2,250,000
2007/2008	\$3,000,000
2008/2009	\$3,000,000
2009/2010	Dependent on Measure A renewal

The Elderly and Disabled Program annual funding will be used to implement the highest priority locations of the ADA Capital Implementation Plan. In addition, the existing funding sources will continue to fund locations within the plan. The above table assumes that sidewalk repair will include the items described in Section 6.4 as part of the Curb, Gutter and Sidewalk Maintenance Program, plus the removal of other barriers that may be subsequently determined as part of the ADA. Likewise, sidewalk repair work would include the retrofit of existing driveways that have excessive cross slope, by using methods presented in the Pedestrian Design Guidelines and other criteria to be subsequently determined as part of the ADA. The pedestrian bridges identified in Section 4.5, as well as other similar examples, also are included in the recommended list of improvement projects that should be upgraded as part of this ADA Transition Plan.

Table 3: Estimated Existing Scenario for Annual ADA Capital Implementation Program

Type of Project	No. of Curb Ramps	No. of Complete ADA Intersections	No. of Crosswalks	Sidewalk Repair & Construction Segments
Estimated Cost	\$6,200 /EA	\$70,000 /EA	\$800 /Xwalk	\$5,000 / Intersection
1 Improvements by public input request (Elderly & Disabled Accessibility Project)	= 50 \$310,000 <i>1</i>	= 3 \$210,000 <i>2</i>	= 0 \$0	= 0 \$0
2 Caltrans construction projects	= 30 \$186,000 <i>3</i>	= 4 \$280,000 <i>3</i>	= 10 \$8,000 <i>3</i>	= 0 \$0
3 County street overlay projects	= 270 \$1,674,000 <i>4</i>	= 0 \$0	= 10 \$8,000	= 60 \$300,000 <i>5</i>
4 Private developer projects	= 700 \$4,340,000 <i>6</i>	= 2 \$140,000 <i>7</i>	= 0 \$0	= 0 \$0
5 Major intersection projects	= 30 \$186,000 <i>8</i>	= 8 \$560,000 <i>9</i>	= 0 \$0	= 5 \$25,000
6 County improvement projects	= 30 \$186,000 <i>8</i>	= 8 \$560,000 <i>10</i>	= 0 \$0	= 20 \$100,000 <i>11</i>
Totals	= 1110 \$6,882,000	= 25 \$1,750,000	= 20 \$16,000	= 85 \$425,000

Comments

- 1 Sidewalk Ramp Improvement Project Phase 1 - 47 ramps, estimated total project cost = \$180,000
- 2 Accessible Signal Project - 3 intersections, estimated total project cost = \$199,000
- 3 Contract #3526, I80/Greenback Lane / Elkhorn Blvd. Interchange Modification Project
- 4 Per Maintenance Engineering, 3 year average of ramps installed with overlay projects = 270
- 5 Per Maint. Eng., Curb, Gutter & Sidewalk Repl. Project avg. is \$300,000 annually for 100 locations. Exception is Curb Gutter & Sidewalk Repl. Project 02/03, funded by Tobacco Tax, at roughly \$1,000,000
- 6 3-year average from 1998 to 2000 per Signal Design inventory
- 7 Estimate two signals per year designed by County forces for Developer projects
- 8 3-year average from 1998 to 2000 per Signal Design inventory, split equally between County improvement and major intersection projects
- 9 Traffic Signal Project 02/03, 8 Intersections, \$2.3 Million, assume 1/4 of cost is for disabled access improvements
- 10 Assumes roughly eight signalized intersections constructed with County improvement projects, see comment 9.
- 11 Does not include County Sidewalk Continuity Project-Phase 2, estimated total project cost = \$2.4 Million Covers 18 roadway segments.

Section 6.6: Detailed Reports of Proposed Work

Detailed descriptions of proposed disabled access improvement projects are included in the ADA Public Rights-of-Way Database, as described in Section 4.6. A table of CIP projects is included in Appendix C. The table contains almost 2,000 entries. The CIP table also is summarized in Tables 4 and 5 below.

The ADA improvement projects are subdivided by fiscal year as part of a 15 to 20 year implementation plan. It is estimated that this period would yield a degree of compliance that could be described as compliance with Use Priorities 1 and 2 and Condition Priorities 1 and 2, as described in Section 6.3. It is felt that a detailed breakdown of projects past these periods would be inappropriate, since conditions would be subject to numerous changes over such a time span. It is anticipated that overall compliance with all Use and Condition Priorities could be achieved in 20 to 25 years.

The locations of work are subject to review and recommendations by SacDOT, the ADA Transition Plan Oversight Committee and the public. Likewise, it is probable that specific locations and project groupings will need to be adjusted among the various years of the plan, after a more detailed review by SacDOT program managers. Other breakdowns of proposed work locations and extent are available, including by County areas, types of work or funding sources. It should be noted that the detailed field surveys undertaken, as described in Section 4, form the basis of existing conditions requiring correction under the proposed projects. It should also be noted that the detailed reports include projects that may be part of the ADA Transition Plan work implemented and funded by other jurisdictions or municipalities.

Construction and soft costs given in both the detailed and summary tables of improvement projects should be considered schematic, order of magnitude costs, based upon the unit costs and estimating parameters developed specifically for this ADA Transition Plan. These costs originally were prepared by Burrell Engineering in July 2002, and were revised by SacDOT in October 2003. The costs include all incidental soft costs, such as engineering, bidding and permitting costs, utilities and other appurtenances and contingencies. The cost basis and preliminary cost estimates are not included as part of this ADA Transition Plan, but they are available from SacDOT upon request.

The detailed table of improvement projects does not necessarily depict the complete and exact locations of all sidewalk and driveway repair work to be undertaken as part of the ADA Transition Plan, since much of this work will be determined by public input requests and will be evaluated in conjunction with intersection work or other construction projects. It should be noted that the Pedestrian Master Plan Draft Capital Improvement Program also contains sidewalk and related pedestrian access work.

Maps shown as Figures 7-10 represent graphic depictions of the various locations of improvements included in the ADA Capital Implementation Plan. It should be noted that due to incompatibilities of the various programs used to map the tables, not all intersections are necessarily shown. Also, not all areas of the County are necessarily mapped.

Table 4: Summary of ADA Implementation Plan Projects by Fiscal Year

Fiscal Year	Total No. of Intersections Included	Est. No. of Curb Ramps Compliant	Est. Total Curb Ramp Costs	Est. Total Accessible Signal Costs	Est. Total Sidewalk Costs	Est. Total Cap. Improvement Costs*
04/05	160	525	\$2,856,245	\$584,800	\$940,800	\$4,707,805
05/06	160	570	3,212,050	546,320	992,650	5,118,780
06/07	155	610	3,535,685	546,320	764,440	5,246,125
07/08	200	650	3,126,970	525,600	1,022,490	5,035,220
08/09	160	580	3,152,495	336,160	803,600	4,675,975
09/10	130	560	3,406,715	371,680	860,670	5,017,465
10/11	140	540	3,011,585	419,040	902,350	4,693,135
11/12	200	590	3,250,810	200,000	1,094,650	4,904,860
12/13	220	450	2,716,920	223,680	1,359,210	4,499,610
13/14	140	540	3,296,960	279,920	910,660	4,877,340
14/15	150	575	3,541,360	259,200	932,890	5,194,530
15/16	210	470	2,812,800	220,720	1,403,480	4,668,720
16/17	140	465	2,805,600	471,040	886,510	4,498,910
17/18	155	470	2,766,280	474,000	845,920	4,423,480
Totals	2,320	7,595	\$43,492,475	\$5,458,480	\$13,720,320	\$67,561,955

* Note: Costs included in this table do not include Elderly and Disabled Project costs. Total costs also include other items such as crosswalks.

Table 5: Summary of ADA Implementation Plan Projects by Community Planning Area

Community Planning Area	Total No. of Intersections Included	Est. No. of Curb Ramps Compliant	Est. Total Curb Ramp Costs	Est. Total Accessible Signal Costs	Est. Total Sidewalk Costs	Est. Total Cap. Improvement Costs*
Antelope	115	345	\$1,852,055	\$318,400	\$671,440	\$2,937,895
Arden/Arcade	423	1475	8,676,820	800,820	2,691,260	13,606,360
Carmichael	117	300	1,669,455	271,040	654,890	3,070,865
Cosumnes	48	125	675,100	235,520	242,260	1,204,060
Delta	20	60	420,000	80,000	115,400	665,000
Fair Oaks	114	405	2,637,385	404,240	954,790	4,265,205
Franklin/Laguna	41	105	610,000	200,200	325,500	1,300,500
Natomas	40	100	580,000	195,800	350,000	1,255,080
North Highlands	399	1360	7,273,100	575,920	2,156,870	10,102,530
Orangevale	165	500	2,859,210	488,800	922,210	4,295,780
Rio Linda/Elverta	89	275	1,510,000	223,680	492,040	2,284,850
Southeast	24	80	450,000	80,000	110,000	678,000
S. Sacramento	274	935	5,539,615	578,540	1,843,630	8,826,405
Vineyard	185	560	3,274,150	416,080	911,530	4,990,560
Unspecified			5,465,585	589,440	1,278,500	8,078,865
Totals	2,320	7,595	\$43,492,475	\$5,458,480	\$13,720,320	\$67,561,955

* Note: Costs included in this table do not include Elderly and Disabled Project costs. Total costs also include other items such as crosswalks.

Figure 7: ADA Capital Implementation Plan Project Locations, Northwest County

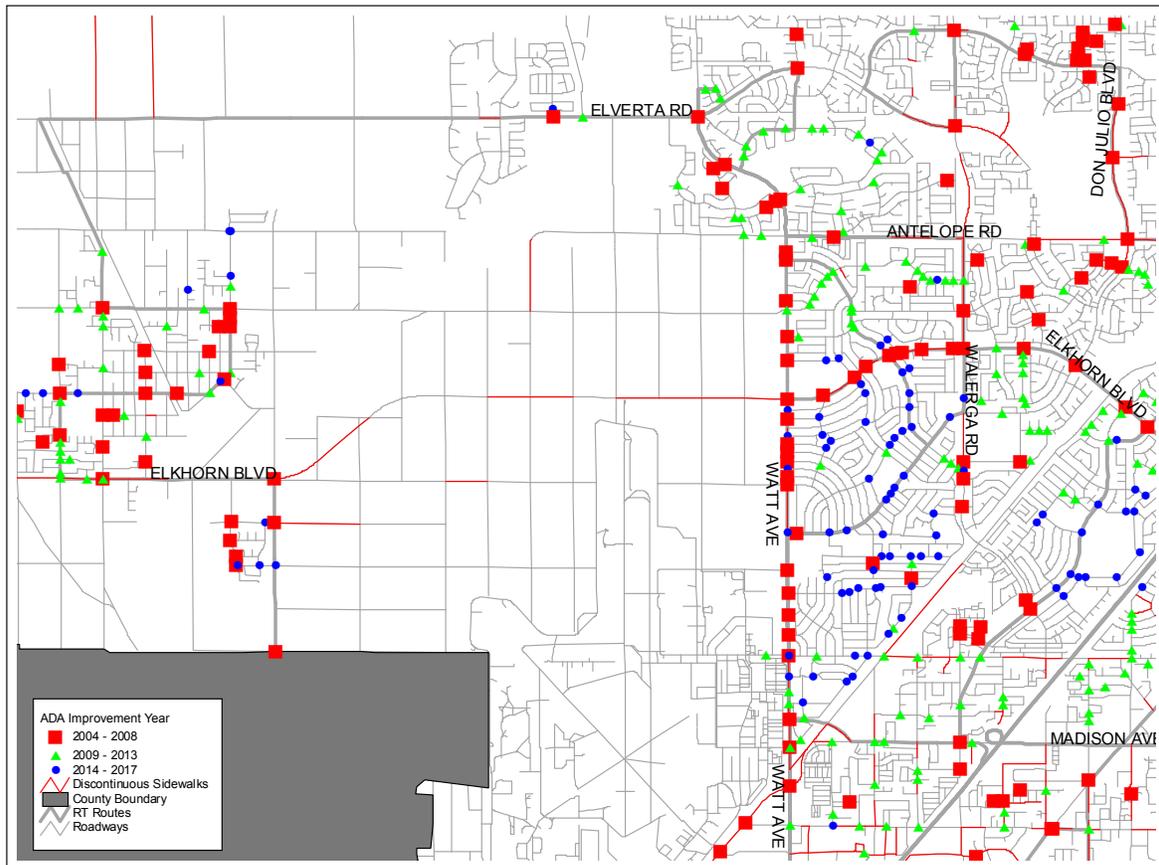


Figure 8: ADA Capital Implementation Plan Project Locations, Northeast County

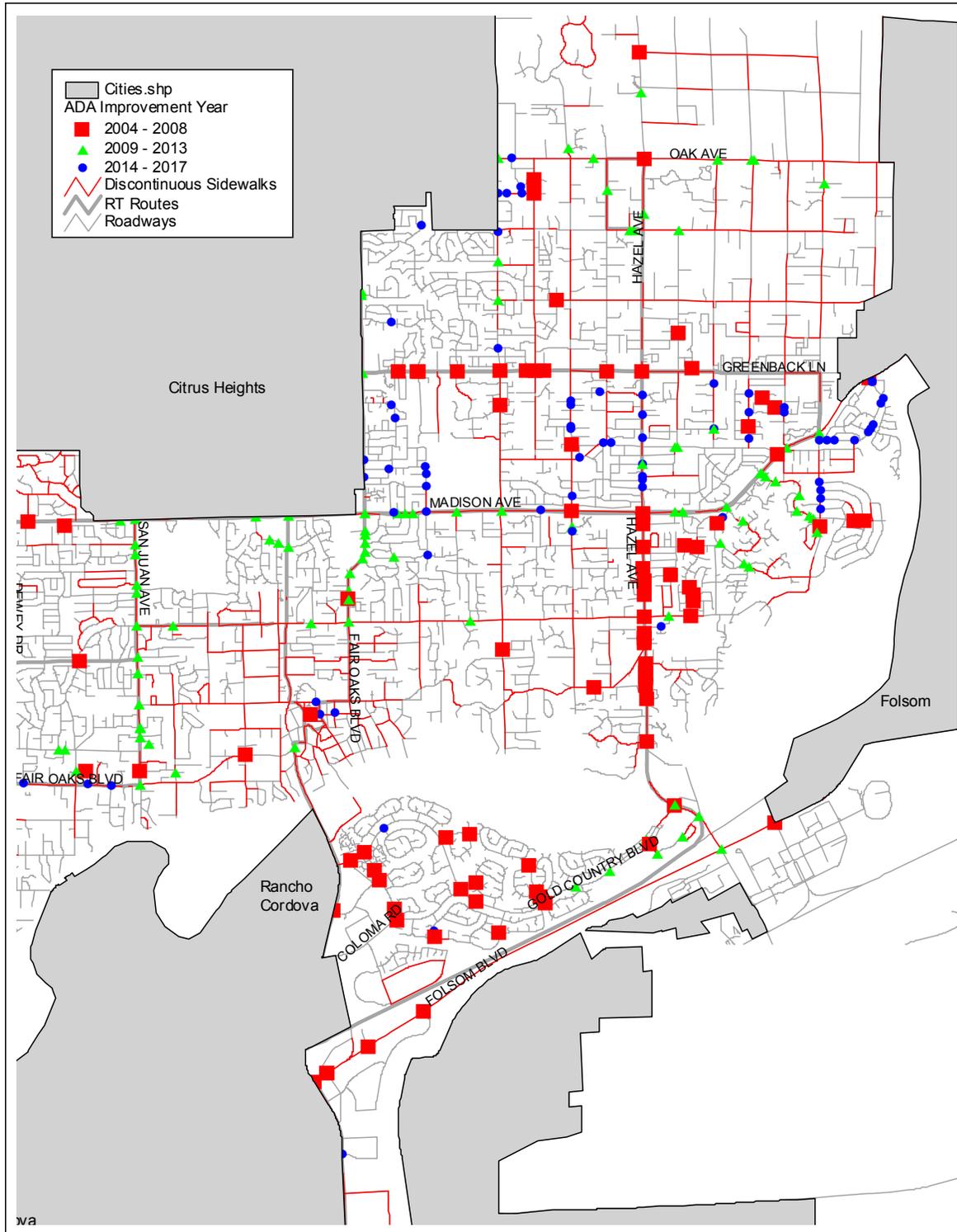


Figure 9: ADA Capital Implementation Plan Project Locations, East County

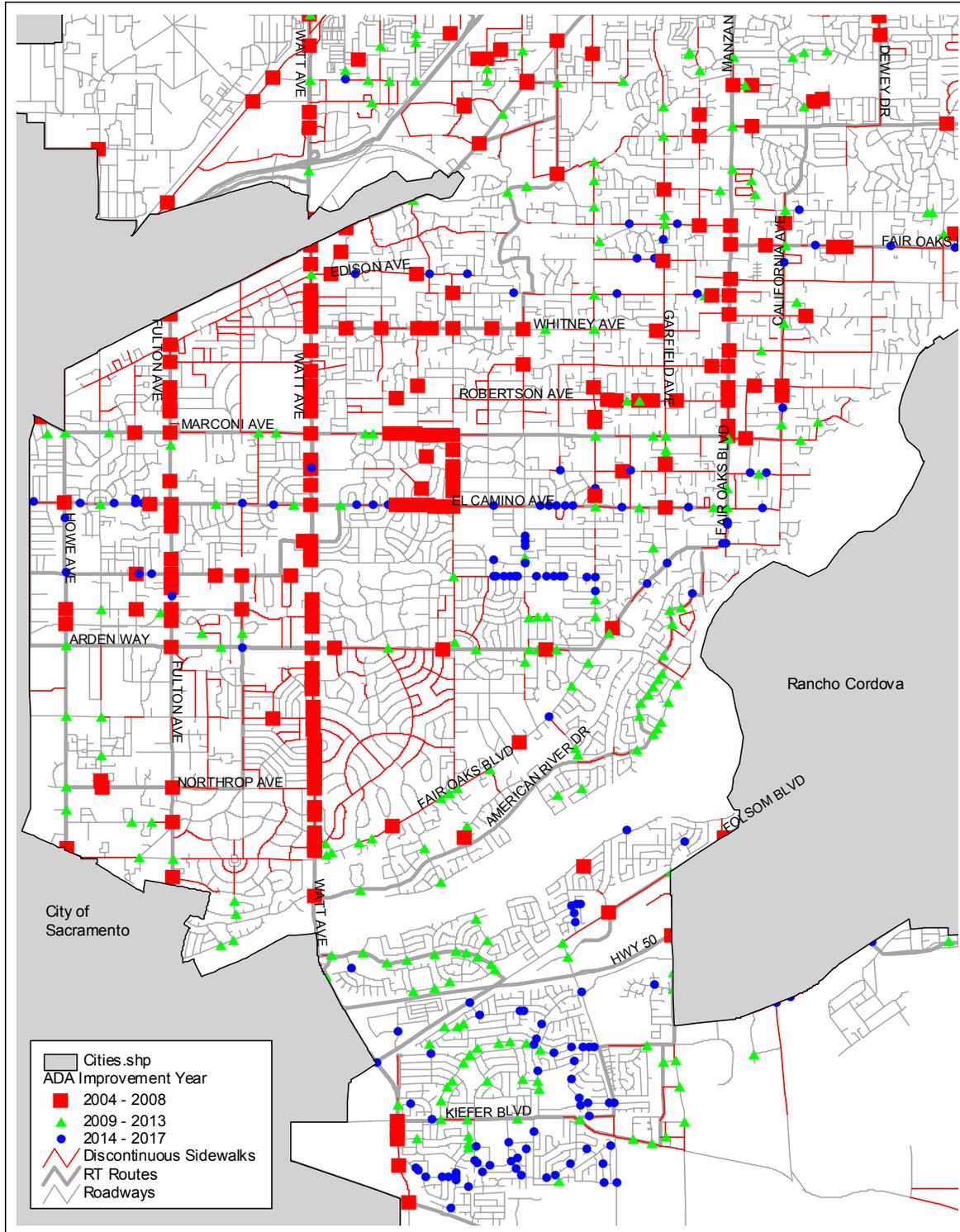
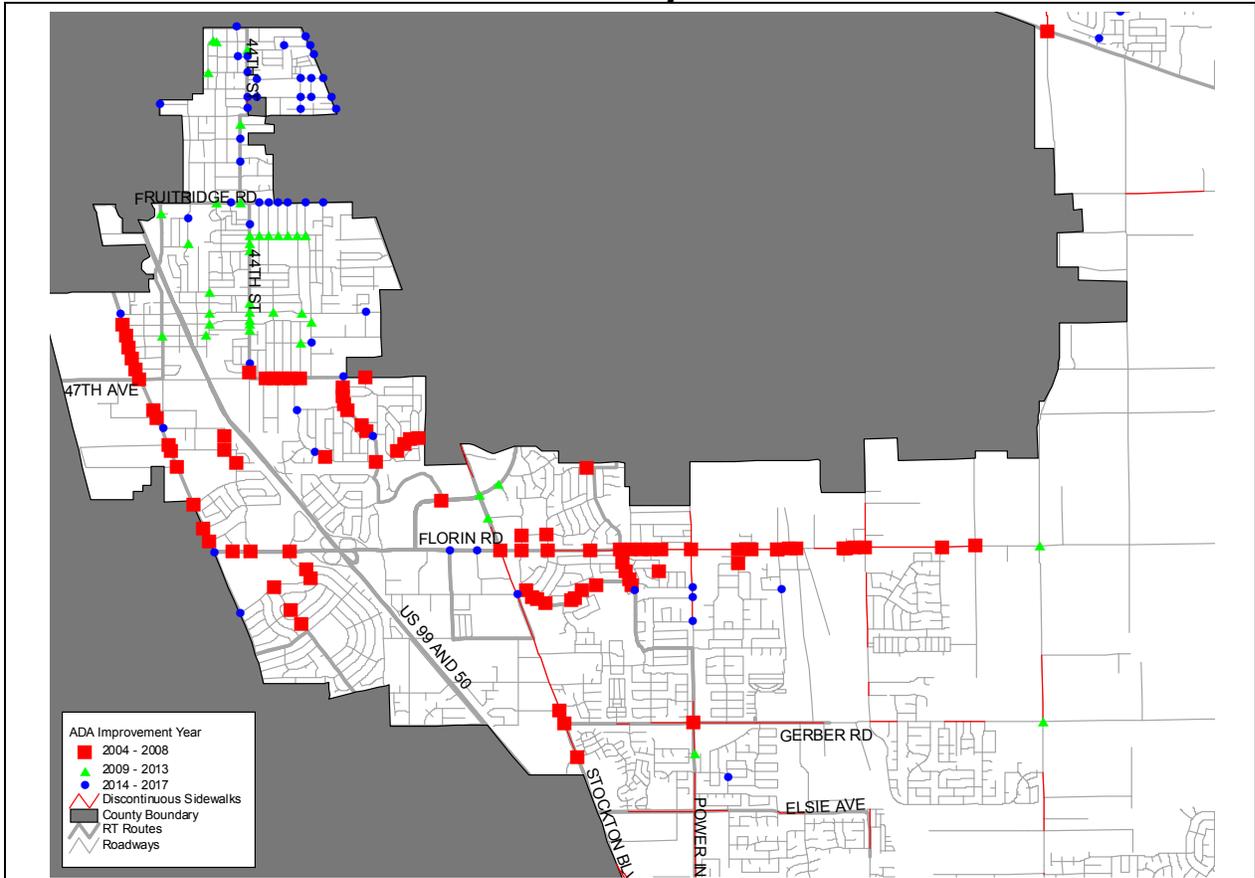


Figure 10: ADA Capital Implementation Plan Project Locations, Central South County



Section 7: Monitoring and Status Reporting

Section 7.1: Introduction

SacDOT currently is engaged in an on-going effort to construct curb ramps, sidewalks, and other pedestrian facilities at numerous locations within the unincorporated County. This construction activity involves several types of projects, including street overlay projects, street beautification projects, utility construction projects and other capital improvement projects in the public right-of-way. In addition, when this ADA Transition Plan is approved and implemented, more curb ramps and related improvements will be constructed.

While it is important to assure that codes and standards used to design and construct curb ramps and related improvements are up-to-date, it is equally important that ADA improvements are constructed properly and in compliance with all applicable codes and standards. Therefore, the monitoring of construction activities and the reporting of the status of improvements is vital in assuring an effective overall compliance program.

This section details the methods and procedures for monitoring these construction activities and for tracking the status of compliance with the ADA Transition Plan for the public right-of-way at all construction locations within the unincorporated County.

Section 7.2: Field Inspections and Monitoring

All curb ramps and sidewalks currently being constructed under the jurisdiction of SacDOT are required to be personally inspected by a trained inspector either employed by or under contract with SacDOT. The types of projects under which curbs ramps and other improvements are or will be constructed and inspected are the following:

1. Curb ramp or sidewalk construction or rehabilitation undertaken under the jurisdiction of SacDOT or its contractors as part of capital improvement projects or other specialized construction projects,
2. Curb ramp or sidewalk construction or rehabilitation undertaken by other agencies or private parties within the unincorporated County, over which SacDOT has jurisdiction, and
3. Curb ramp or, sidewalk construction or rehabilitation undertaken as part of the ADA Transition Plan.

Specific procedures for all field inspections are recommended to be as follows:

1. Every curb ramp constructed under the jurisdiction of SacDOT is to be personally inspected by a Department-approved inspector within ten days after the completion of the curb ramp construction. All inspectors are to be appropriately trained to know and understand the ADA Codes and Standards, as described in 5, and the measurements necessary to inspect curb ramps and other improvements as they are constructed.
2. The inspector is to inspect and obtain all dimensions using a tape measure to verify that all dimensions meet or exceed the ADA Codes and Standards.
3. The inspector is to inspect and obtain all slopes and gradients using a two-foot to four-foot long Smart-level or equal slope-measurement tool to verify that all slopes and gradients meet or exceed the ADA Codes and Standards. Three measurements are to be taken and recorded for each access-related slope.
4. The inspector is to inspect all other physical conditions relating to the curb ramp and related construction to verify that all construction meets or exceeds the ADA Codes and Standards.
5. The inspector is to inspect all physical conditions relating to the installation of all accessible pedestrian signals to verify that all installations meet or exceed the ADA Codes and Standards.
6. The inspector is to inspect all physical conditions relating to the installation of all sidewalks to verify that all installations meet or exceed the ADA Codes and Standards.
7. All dimensions, slopes, and other conditions verified in 2, 3, 4, 5, and 6 above are to be entered on approved SacDOT Curb Ramp Inspection Forms, Pedestrian Signal Inspection Forms, and Sidewalk Inspection Forms, and such forms are to be certified as correct and truthful by the Inspector's signature.
8. Any exceptions to full compliance with the ADA Codes and Standards are to be described on the Curb Ramp Inspection Form, Pedestrian Signal Inspection Form or Sidewalk Inspection Form, certified as a "Finding for Non-compliance Element(s)" by the inspector, reviewed and approved by the Program Manager, and finally approved by

Sacramento County Department of Transportation ADA Transition Plan

the Director of SacDOT or his/her authorized representative and the SacDOT Program Access Coordinator or his/her authorized representative.

9. The Curb Ramp Inspection Form, Pedestrian Signal Inspection Form and Sidewalk Inspection Form are to be submitted to the Program Access Coordinator within ten days after the completion of the curb ramp construction.

Copies of the Curb Ramp Inspection Form, the Pedestrian Signal Inspection Form and the Sidewalk Inspection Form are included in a following section.

Section 7.3: Computerized Tracking and Status Reporting

Once the Curb Ramp Inspection Form, Pedestrian Signal Inspection Form, and Sidewalk Inspection Form have been completed and approved for each constructed curb ramp, installed pedestrian signal or constructed sidewalk, the data are to be entered into the master ADA Public Rights-of-Way Database. This database has been developed as part of the overall ADA Transition Plan project, and it contains detailed data for intersections, sidewalks and roadways throughout the unincorporated County.

The database also contains all intersections included in the ADA Transition Plan Capital Implementation Plan. Several reports are available to show the details of these intersections and adjacent pedestrian facilities. Also, when As Constructed data are entered into the database, as described below, locations included in the ADA Transition Plan Capital Implementation Plan can be tracked automatically to include the recommended changes. The status of the overall compliance activities also can be tracked and monitored.

All data from the Curb Ramp Inspection Forms, Pedestrian Signal Inspection Forms and Sidewalk Inspection Forms are to be entered into a special section of the database reserved for As Constructed data. The database will continue to maintain records both for pre-existing conditions and for newly constructed conditions.

The data entry for the Curb Ramp Inspection Forms, Pedestrian Signal Inspection Forms and Sidewalk Inspection Forms could be performed either by the field inspectors themselves or by separate clerical personnel, at the discretion of the Program Manager.

Once the As Constructed data have been entered into the computerized database, several reports are available to track the progress of the work and to document the current status of either a specific project or a specific geographical area. One report has been developed specifically for As Constructed data. This report includes a printout of each specific curb ramp and pedestrian signal control constructed and installed as part of the project. A sample of this report is included in the next section.

The computerized database is intended to be made available to the general public, either by public access computers made available at the SacDOT office or other methods to be determined by the County. In addition, SacDOT staff will be available to the general public to provide updated As Constructed monitoring and status reports upon request.

Section 7.4: Sample Curb Ramp Survey / Status Report

Date of inspection 1/2/03
 Project CRP-2003-2
 Contractor Curb Ramp Const. Co.
 County Inspector Joe Curbramp

Intersection (GIS) number: 1800
 North/south street: Opal St
 East/west street: Johnson Ave.
 Corner position: NE
 Curb ramp type: Pan
 Orientation: Center
 Street facing curb ramp: Intersection
 Curb type: Vertical
 Adjoining sidewalk present: Yes
 Marked crosswalk present: Yes
 Intersection priority: 1

<u>Curb Ramp Conditions</u>	<u>Requirement</u> <u>(Acceptable Range)</u>	<u>Measurement</u>	<u>Complies</u>
Gutter/street slope:	2.0% to 5.0%	4.7%	Yes
Pan slope:	1.5% to 2.0%	1.8%	Yes
Left side slope:	2.0% to 8.33%	8.2%	Yes
Left transition slope:	2.0% to 5.0%	4.2%	Yes
Right side slope:	2.0% to 8.33%	4.7%	Yes
Right transition slope:	2.0% to 5.0%	6.2%	No
Width of pan:	48 inches or greater	51 inches	Yes
Width of curb ramps:	48 inches or greater	50 inches	Yes
Lip at street:	None (0 inches)	None	Yes
Curb installed behind pan:	Yes	Yes	Yes
Common landing between ramps:	---	None	N.A.
Common landing length:	N.A.	N.A.	N.A.
Grooved border installed:	Yes	Yes	Yes
Detectable warnings installed:	Yes	Yes	Yes
Curb ramp within marked crosswalk:	Yes	Yes	Yes

<u>Pedestrian Signal Conditions</u>	<u>Requirement</u> <u>(Acceptable Range)</u>	<u>Measurement</u>	<u>Complies</u>
Push button within 60 inches of crosswalk:	Yes	Yes	Yes
Push button height:	36 to 46 inches	42 inches	Yes
Push button diameter:	2 inches	2 inches	Yes
Clear space 30 inches by 48 inches minimum:	Yes	Yes	Yes
Push button parallel to crosswalk:	Yes	Yes	Yes
Accessible pedestrian signal operational:	Yes	Yes	Yes
Locator tone operational:	Yes	Yes	Yes

Other Comments: None

Section 7.5: Inspection Forms

Copies of the Curb Ramp Inspection Form, the Pedestrian Signal Inspection Form and the Sidewalk Inspection Form are included on the following pages.



Curb Ramp Inspection Form

Must be filled out for every ramp constructed in County right-of-way in Sacramento County. This form must be filled out and submitted within ten days of ramp construction to the SacDOT Program Access Coordinator.

Curb Ramp Location:

N/E
 N/W
 S/E
 S/W

Project Name: _____

N/S STREET _____

E/W STREET _____

Curb Ramps per County Standard Drawing 4-23 & 4-24:

Vertical Curb
 Rolled curb

One Ramp

Check One:

Two Ramps

STANDARDS:

- NO POLES OR PULLBOXES SHALL BE LOCATED IN RAMPS
- ALL ELEMENTS, EXCEPT GUTTERS AND RETAINING CURBS, MUST BE CONSTRUCTED PLANAR IN NATURE, WITH WEAKENED PLANED JOINTS SCORED BETWEEN EACH ELEMENT.
- PAN MUST BE WITHIN STRIPED CROSSWALKS

A1: _____ B1: _____ C1: _____ D1: _____

A2: _____ B2: _____ C2: _____ D2: _____

A3: _____ B3: _____ C3: _____ D3: _____

RIGHT SIDE LENGTH: _____ FT. _____ IN.

PAN WIDTH: _____ FT. _____ IN.

LEFT SIDE LENGTH: _____ FT. _____ IN.

IF TWO RAMPS ON CORNER (DATA FOR 2ND RAMP):

E1: _____ F1: _____ G1: _____ H1: _____

E2: _____ F2: _____ G2: _____ H2: _____

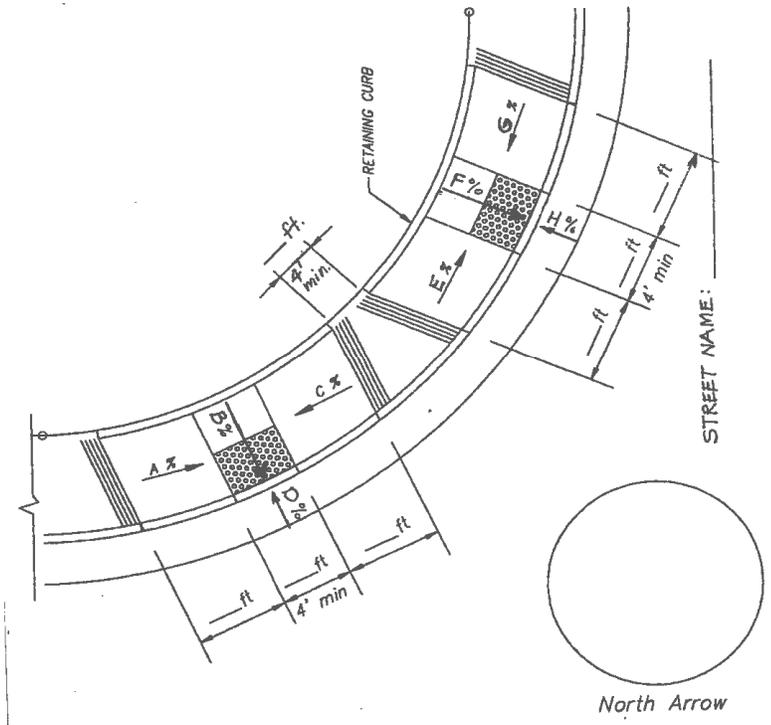
E3: _____ F3: _____ G3: _____ H3: _____

RIGHT SIDE LENGTH: _____ FT. _____ IN.

PAN WIDTH: _____ FT. _____ IN.

LEFT SIDE LENGTH: _____ FT. _____ IN.

DISTANCE BETW. RAMPS: _____ FT. _____ IN.



STREET NAME: _____

Inspected/measured by (print): _____

Date Field Measured: _____

Directions:

Three measurements for all data shall be taken at the ramps and pans, one measurement shall be taken at the centerline of the element and the other two shall be taken 18 inches on either side of the first measurement for the element. None of the three recorded measurements of any element may exceed the limits indicated above.

See reverse side for Compliance with Standards.



Pedestrian Signal Inspection Form

Must be filled out for all new pedestrian signals installed in County right-of-way in Sacramento County. This form must be filled out and submitted within ten days of signal installation to the SacDOT Program Access Coordinator.

Pedestrian Signal Location:

Project Name: _____

↑ N/E ↑ N/W ↑ S/E ↑ S/W

N/S STREET _____

E/W STREET _____

Pedestrian Signals per County Standard Drawings:

Check One:

↑ Vertical Curb ↑ Rolled curb ↑ One Ramp ↑ Two Ramps

STANDARDS:

- EACH PUSH BUTTON LOCATED WITHIN 60" OF EXTENDED CROSSWALK LINE, AND 120" FROM OTHER BUTTONS.
- EACH PUSH BUTTON LOCATED 120" MAX. AND 30" MIN. FROM CURB LINE.
- PUSH BUTTON PARALLEL TO DIRECTION OF CROSSING.
- CLEAR SURFACE SPACE OF 30" X 48" AT PUSH BUTTON.
- HEIGHT TO C.L. OF PUSH BUTTON FROM 42" TO 46" MAX.
- MAX. 5 LBS. OF PRESSURE TO OPERATE.
- LOCATOR TONE FROM 2 DB TO 5 DB, & ABOVE AMBIENT.

IF ONE PUSH BUTTON IS PRESENT:

A:

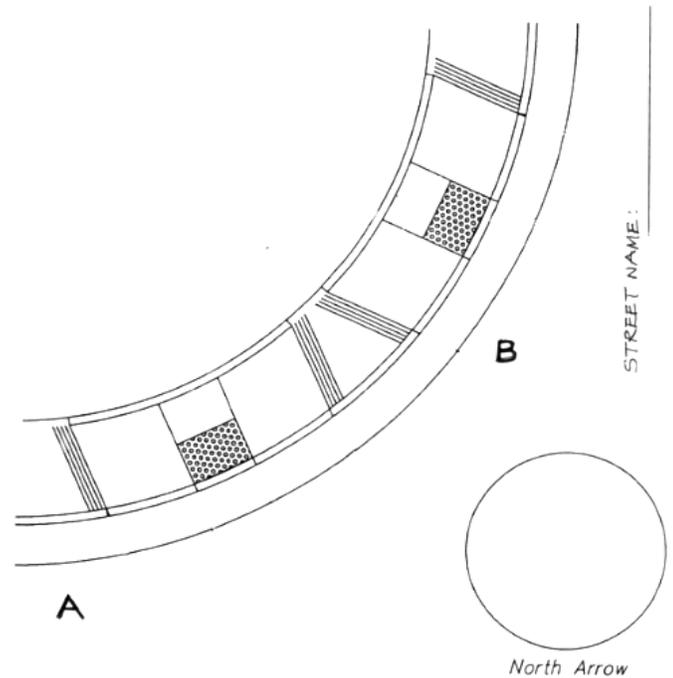
HEIGHT OF PUSH BUTTON (IN.): _____
 DIAMETER OF PUSH BUTTON (IN.): _____
 ACCESSIBLE SIGNAL OPERATIONAL (Y/N): _____
 LOCATOR TONE OPERATIONAL (Y/N): _____
 PUSH BUTTON PARALLEL TO DIRECTION (Y/N): _____

IF TWO PUSH BUTTONS ARE PRESENT:

B:

HEIGHT OF PUSH BUTTON (IN.): _____
 DIAMETER OF PUSH BUTTON (IN.): _____
 ACCESSIBLE SIGNAL OPERATIONAL (Y/N): _____
 LOCATOR TONE OPERATIONAL (Y/N): _____
 PUSH BUTTON PARALLEL TO DIRECTION (Y/N): _____

ANY CONDITIONS NOT IN COMPLIANCE WITH THE STANDARDS LISTED ABOVE _____



STREET NAME: _____

DRAW LOCATION OF PUSH BUTTON(S) AT THE PROPER LOCATION (IF ONLY ONE RAMP, DRAW BUTTON LOCATION ON RAMP A)

Inspected/measured by (print): _____

Date Field Measured: _____

See reverse side for Compliance with Standards.



Sidewalk Inspection Form

Must be filled out for every sidewalk constructed in County right-of-way in Sacramento County. This form must be filled out and submitted within ten days of construction to the SacDOT Program Access Coordinator.

Street: _____ **Project Name:** _____

Sides of Street:

↑ N ↑ E ↑ S ↑ W

Nearest Cross-Streets:

↑ STREET _____ ↑ STREET _____

Sidewalks per County Standard Drawing 4-25:

Check all that apply:

↑ Vertical Curb ↑ Rolled curb ↑ No Curb ↑ Planter strip entire length ↑ Partial length planter strip

STANDARDS:

- CLEAR WIDTH AT LEAST 48" (NOT INCLUDING CURB) OR AT LEAST 36" AT AN OBSTRUCTION
- NO OVERHEAD OBSTRUCTIONS LOWER THAN 84".
- CROSS-SLOPE 1.5%. RUNNING SLOPE NOT GREATER THAN STREET OR 5%, WHICHEVER IS GREATER.
- NO GAPS WIDER THAN 1/2".
- SURFACE CONCRETE OR ASPHALT, BROOM FINISH OR EQUAL SLIP-RESISTANCE.
- NO DROP-OFF GREATER THAN 4" AT BACK OR AT PLANTER STRIP.

NORTH OR EAST CROSS-STREET _____

NORTH OR WEST SIDE OF STREET:

SIDEWALK CLEAR WIDTH: A1: _____ FT. _____ IN.
 A2: _____ FT. _____ IN.
 A3: _____ FT. _____ IN.

PLANTER STRIP WIDTH: B: _____ FT. _____ IN.
 (PUT "0" IF NO PLANTER STRIP)

RUNNING SLOPE: C1: _____ % C2: _____ % C3: _____ %

CROSS-SLOPE: D1: _____ % D2: _____ % D3: _____ %

OTHER SPECIFIC CONDITIONS: _____

SOUTH OR EAST SIDE OF STREET:

SIDEWALK CLEAR WIDTH: E1: _____ FT. _____ IN.
 E2: _____ FT. _____ IN.
 E3: _____ FT. _____ IN.

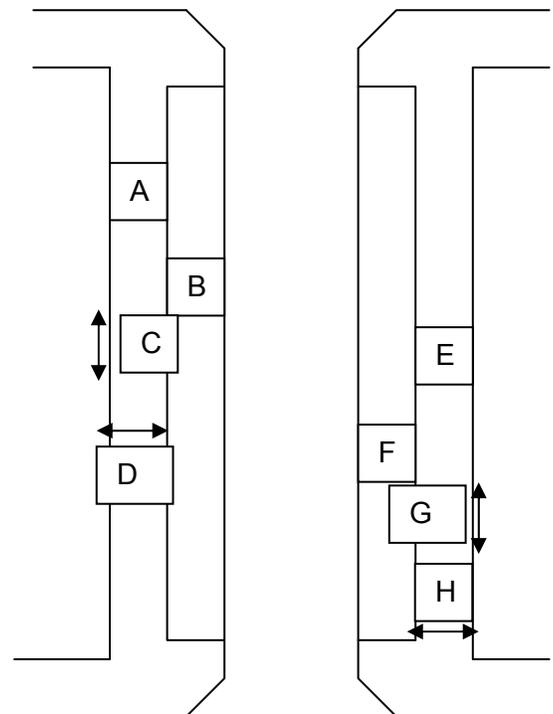
PLANTER STRIP WIDTH: F: _____ FT. _____ IN.
 (PUT "0" IF NO PLANTER STRIP)

RUNNING SLOPE: G1: _____ % G2: _____ % G3: _____ %

CROSS-SLOPE: H1: _____ % H2: _____ % H3: _____ %

OTHER SPECIFIC CONDITIONS: _____

DRAW ALL OBSTRUCTIONS OR HAZARDS ON THE PLAN:



SOUTH OR EAST CROSS-STREET _____

Inspected/measured by (print): _____ **Date Field Measured:** _____

Directions: Three measurements shall be taken at various locations along the sidewalk. None of the three recorded measurements may exceed the limits indicated above. See reverse side for Compliance with Standards.

Appendix A: Glossary

Accessible Pedestrian Signal. A device that communicates information about the pedestrian walk phase in non-visual formats such as audible tones, vibrotactile features or auditory announcements.

Island. Curbed or painted area outside of the vehicular path that is provided to separate and direct traffic movement, and which also may serve as a refuge for pedestrians.

Blended Curb or Transition. A curb ramp shallower than 1:20 (5 percent), where the sidewalk is blended into or flush with the street.

Cross Slope. The slope that is perpendicular to the intended direction of travel.

Crosswalk. That part of a roadway at an intersection that is included within the extensions of the lateral lines of the sidewalks on opposite sides of the roadway, measured from the curb line or, in the absence of curbs, from the edges of the roadway or, in the absence of a sidewalk on one side of the roadway, the part of the roadway included within the extension of the lateral lines of the sidewalk at right angles to the centerline.

Marked Crosswalk. Any portion of a roadway at an intersection or elsewhere that is distinctly indicated for pedestrian crossing by lines or other markings on the surface.

Curb. A vertical or rolled transition from the roadway or gutter to the sidewalk or planting strip.

Curb Line. A line at the face of the curb that marks the transition from the roadway or gutter to a sidewalk or planting strip.

Curb Ramp. A ramp cutting through a curb.

Detectable Warning. A surface feature built in or applied to walking surfaces or other elements to warn of hazards on a pedestrian access path.

Driveway. A vehicular path serving a single parcel of private property.

Element. An architectural or mechanical component of a facility, space, site or public right-of-way.

Equivalent Facilitation: A departure from a particular technical or scoping requirement of these standards by the use of other designs and technologies, where the alternative designs and technologies used provide substantially equivalent or greater access to and usability of the element.

Facility. All or any portion of structures, improvements, elements, and pedestrian or vehicular routes located on a site or in a public right-of-way.

Flush Transition. See Blended Transition.

Grade. See Running Slope.

Grade Break. The meeting line of two adjacent surfaces of different slope (grade).

Land Use Zone. The land use of a particular property location, as defined by Title II of the Sacramento County Zoning Code.

Locator Tone. A repeating sound that identifies the location of the pedestrian push button.

Parallel Curb Ramp. A system of two sloped ramps that run parallel to the curb line from a common lower landing that is approximately level with the street.

Pedestrian Access Route (Path). Any walk or path intended for pedestrian movement or activity.

Perpendicular Curb Ramp. A curb ramp with a main slope running perpendicular to the curb line, and which may include one or more flared side slopes.

Program Access Requirements. Requirements in the ADA Transition Plan for making the public right-of-way accessible to persons with disabilities.

Public Right-of-Way. Land or property owned by a public entity and usually is acquired for or devoted to transportation or pedestrian purposes.

Ramp. A sloping portion of a walkway with a running slope exceeding five percent.

Running Slope. The slope that is parallel to the direction of travel expressed as a ratio of rise to run, usually expressed in percent.

Sidewalk. That portion of a public right-of-way between the curb line or lateral line of a roadway and the adjacent property line that is improved for use by pedestrians.

Sidewalk Ramp: See Curb Ramp.

Street Furniture. Elements in the public right-of-way that are intended for use by pedestrians.

Tactile Guidestrip. A horizontal strip applied to the walking surface along an accessible pedestrian access route that provides directional cues for persons with low vision or persons who are blind and use a cane.

Technical Infeasibility. With respect to an alteration of an existing element, that it has little likelihood of being accomplished because existing physical or site constraints prohibit modification or addition of elements, spaces or features that are in full and strict compliance with the minimum requirements for new construction and that are necessary to provide accessibility.

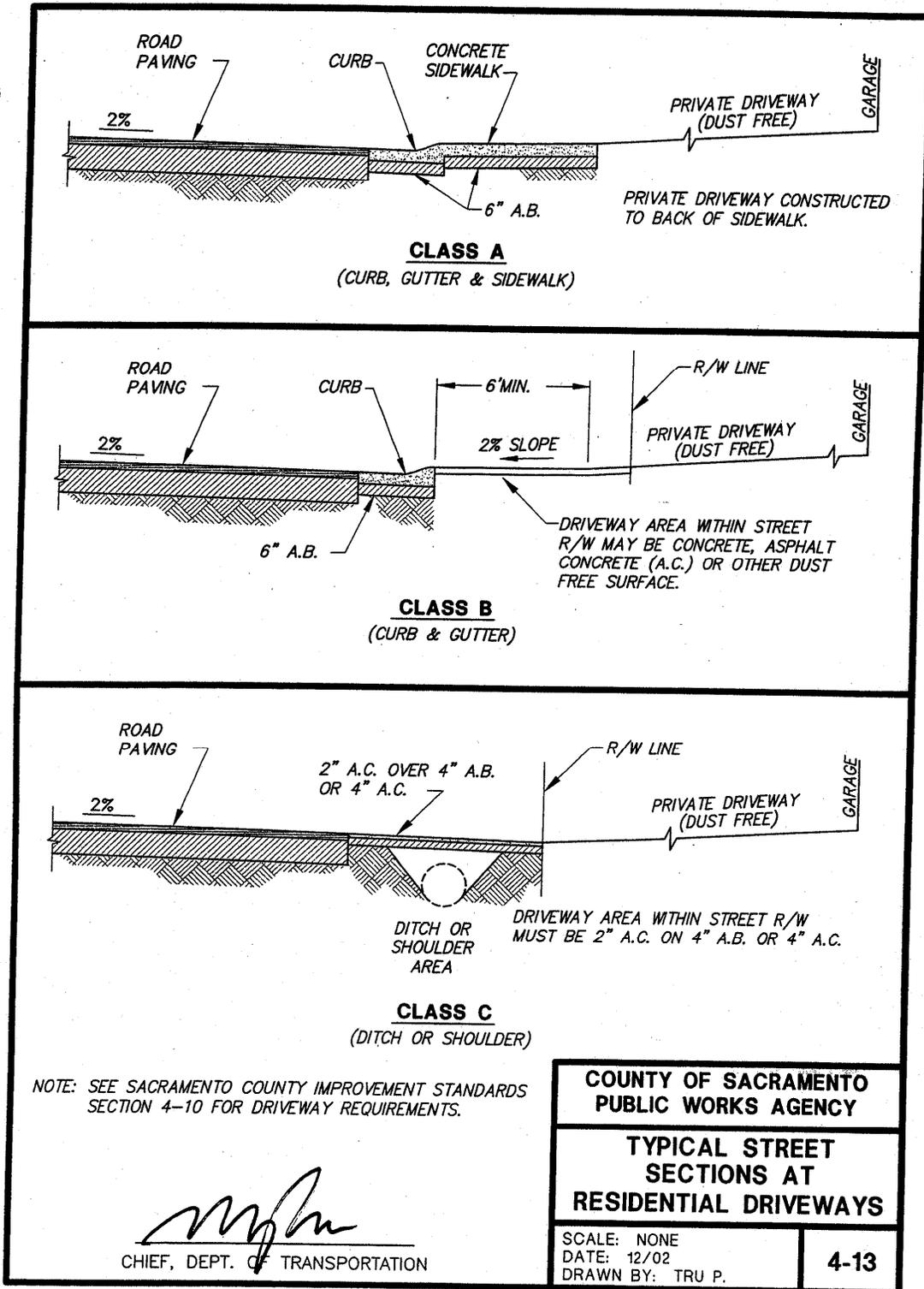
Walk Interval. The phase of a traffic signal cycle during which the pedestrian is to begin crossing, typically indicated by a walk message or the walking person symbol and its audible equivalent.

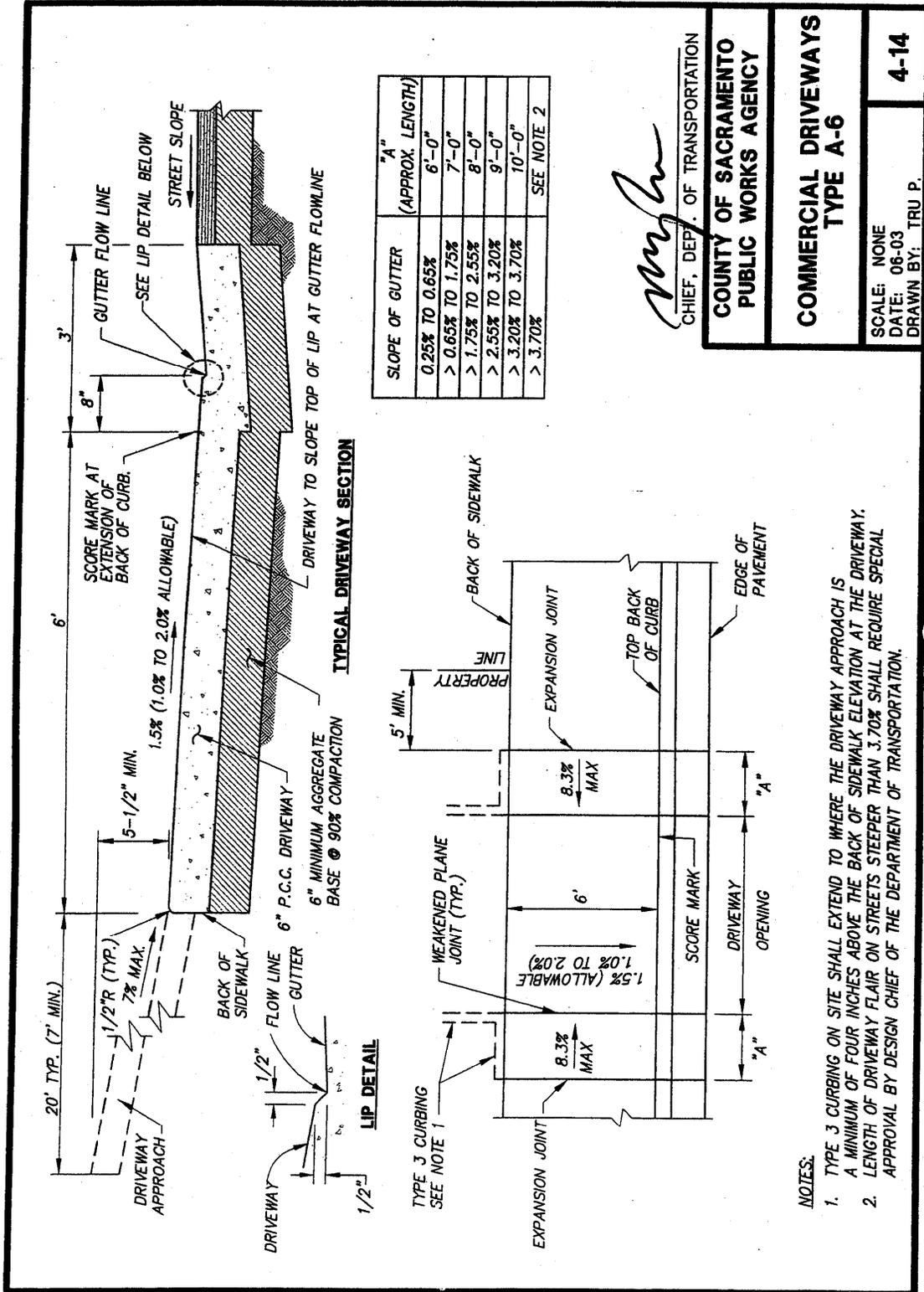
Appendix B: Standard County Improvement Drawings

(Note: Drawing numbers shown refer to numbers assigned by the County Improvement Standards, not by the ADA Transition Plan)

4-1	Alley Details and Driveway Transitions
4-14	Commercial Driveways Type A-6
4-15A	Special Commercial Frontage Entrance Type A-7
4-15B	Sidewalk Ramp for Type A-7 Driveways without Additional R/W
4-21	Bus Turnout
4-22	Bus Stop
4-23A	Sidewalk Ramp Detail
4-23B	Sidewalk Ramp Detail for Plaza Areas
4-23C	A.C. Conforms to New Sidewalk Ramp Construction
4-24	Curb Ramp Placement
4-25	Curbs and Gutter
4-26	Cross Gutter
4-27	Barrier Curb Detail
4-28	Under Sidewalk Drain
4-29	Meandering Sidewalk Standards
4-32	Pavement Widening Detail
4-33	Street Sign Fully Reflectorized
4-34	Street Name Sign Placement Details
4-35	Street Name Sign on Street Light Pole Placement Detail
4-36	Street Name Sign Installation on Street Light Pole
4-37	Street Name Sign Placement Details
4-39	Signs and Barricades at Abrupt Change of Pavement Width
4-40	Sidewalk Barricade
4-41	Utility Pole Placement Locations
4-42	A.C. Sidewalk Conform
4-43	Sidewalk Concrete Joint Details

For an alternate format, refer to the corresponding text in Section 5.





[Signature]
 CHIEF, DEPT. OF TRANSPORTATION

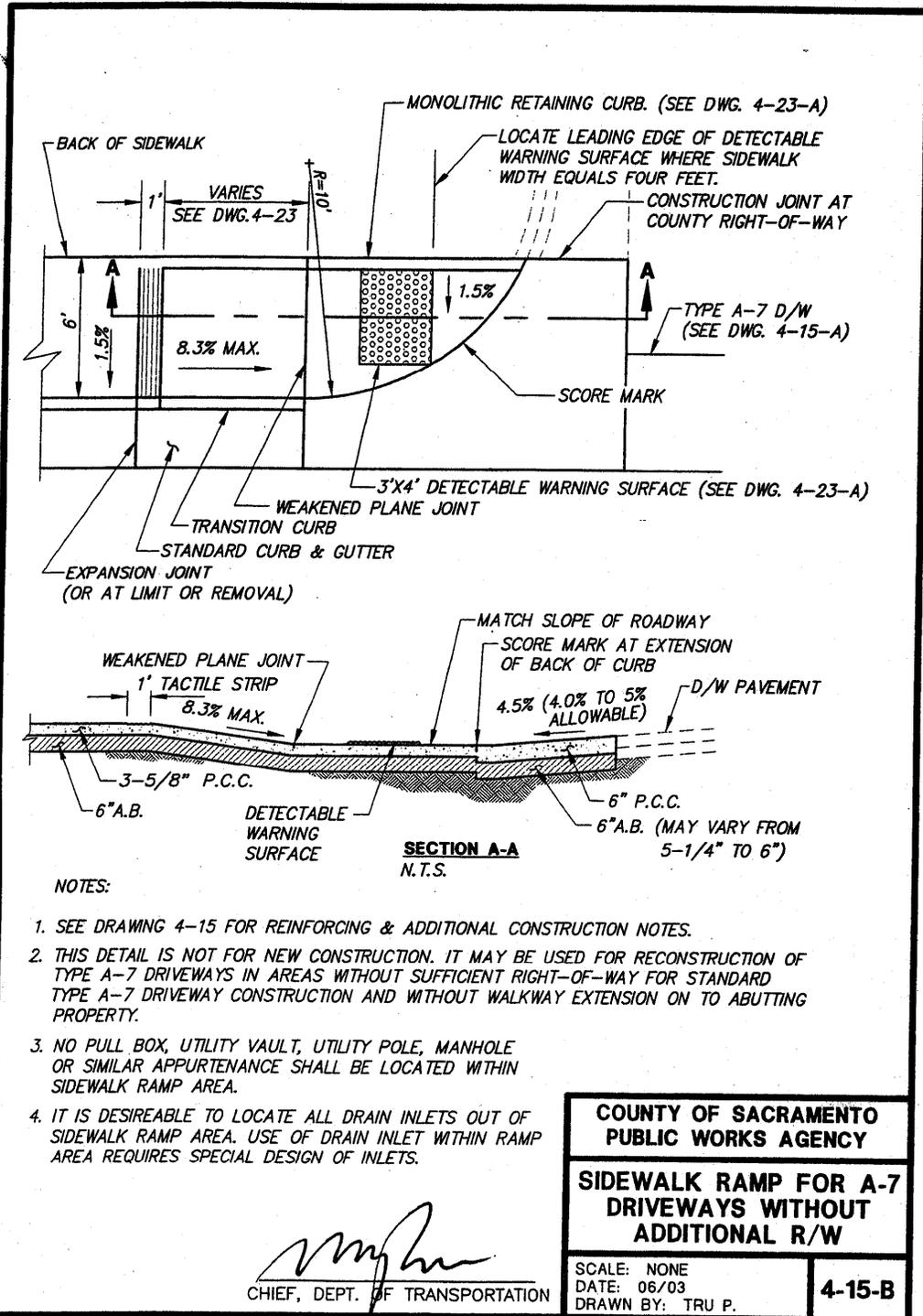
**COUNTY OF SACRAMENTO
 PUBLIC WORKS AGENCY**

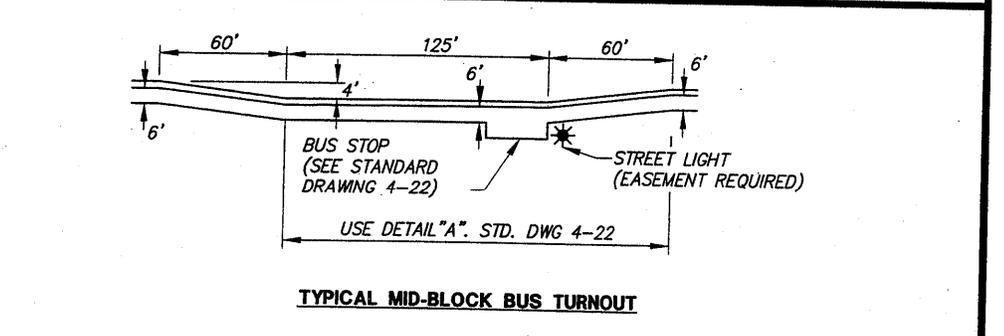
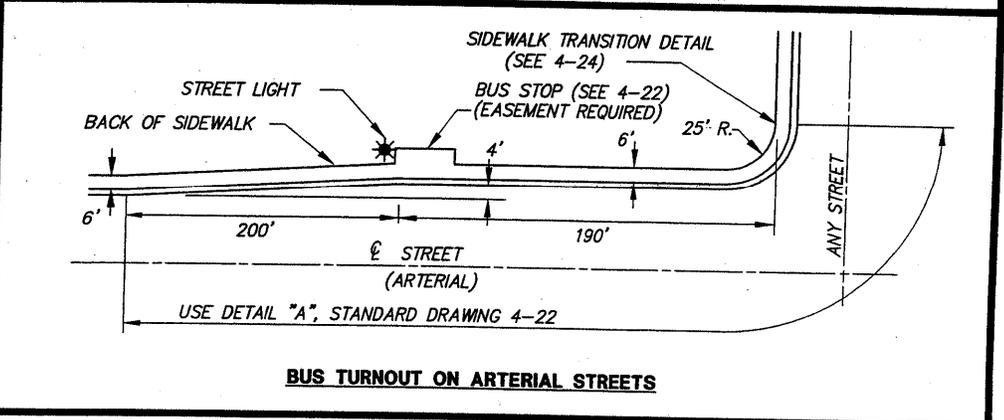
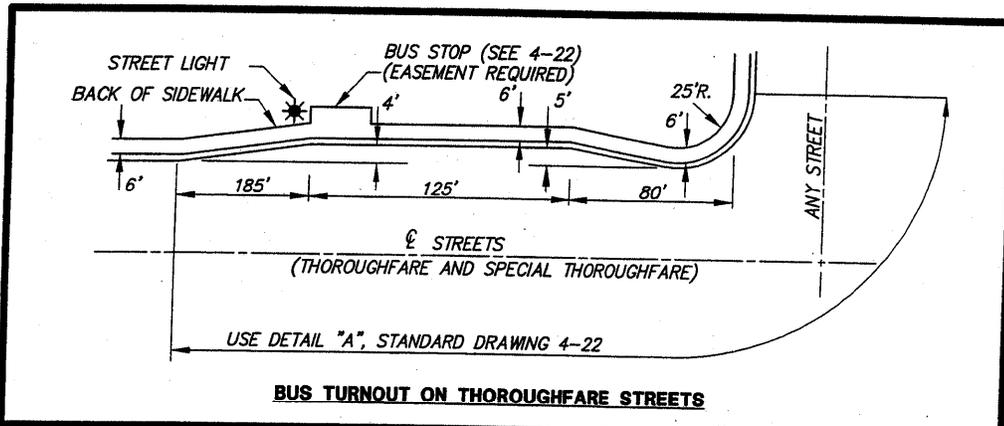
**COMMERCIAL DRIVEWAYS
 TYPE A-6**

SCALE: NONE
 DATE: 08-03
 DRAWN BY: TRU P.

4-14

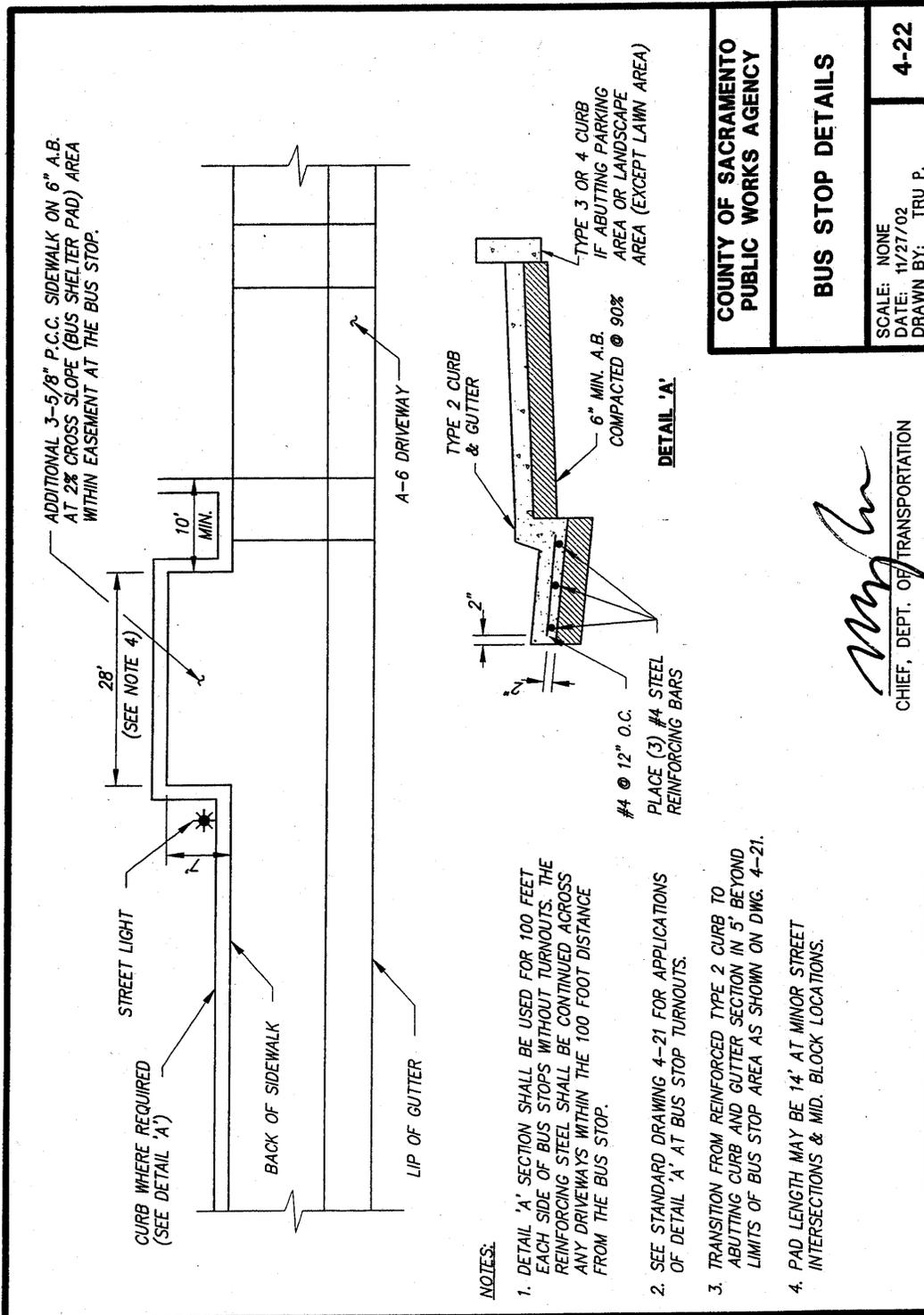
- NOTES:**
- TYPE 3 CURBING ON SITE SHALL EXTEND TO WHERE THE DRIVEWAY APPROACH IS A MINIMUM OF FOUR INCHES ABOVE THE BACK OF SIDEWALK ELEVATION AT THE DRIVEWAY.
 - LENGTH OF DRIVEWAY FLAIR ON STREETS STEEPER THAN 3.70% SHALL REQUIRE SPECIAL APPROVAL BY DESIGN CHIEF OF THE DEPARTMENT OF TRANSPORTATION.





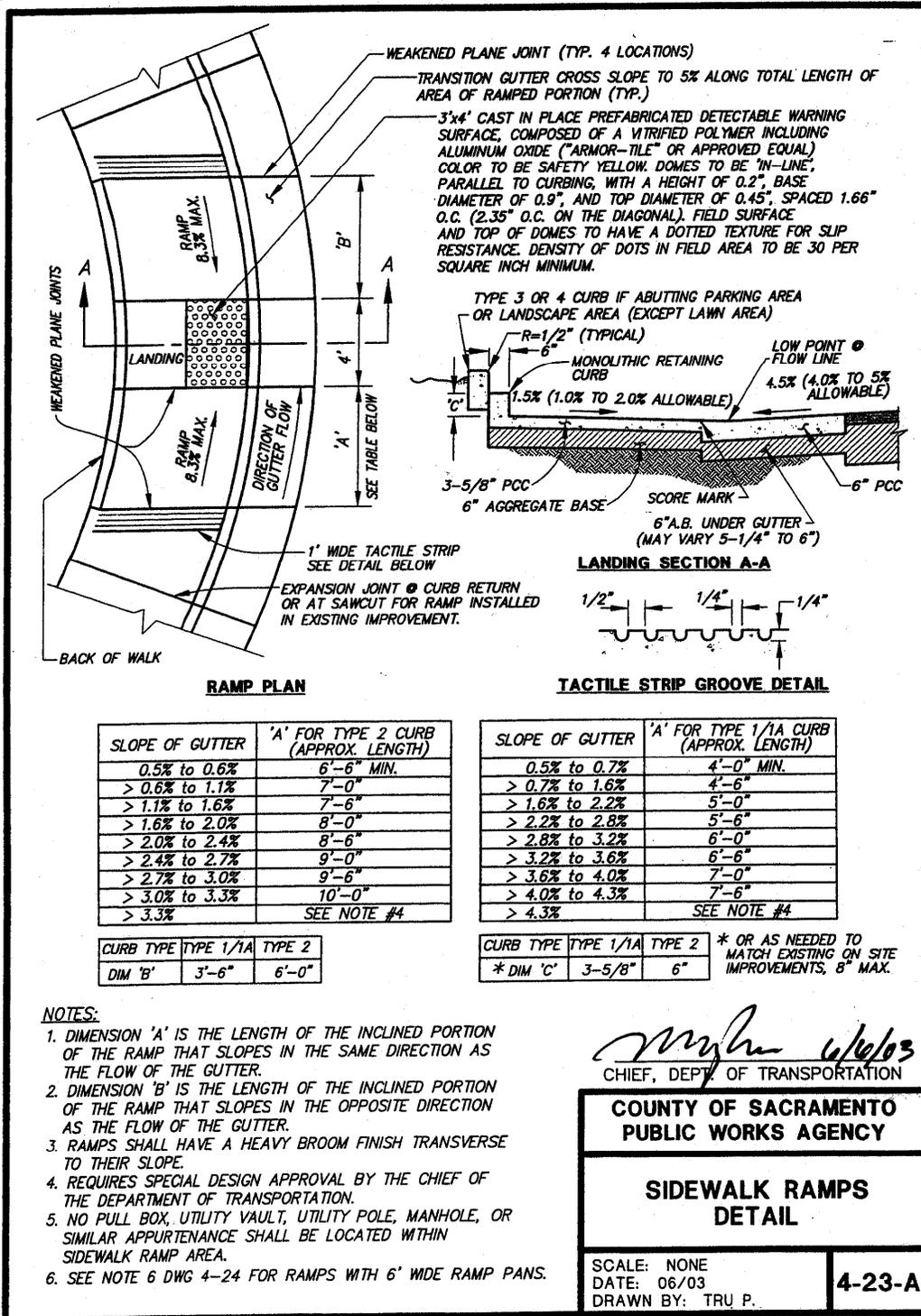
[Signature]
 CHIEF, DEPT. OF TRANSPORTATION

COUNTY OF SACRAMENTO PUBLIC WORKS AGENCY	
BUS TURNOUTS	
SCALE: NONE DATE: 11/27/02 DRAWN BY: TRU P.	4-21



COUNTY OF SACRAMENTO PUBLIC WORKS AGENCY	
BUS STOP DETAILS	
SCALE: NONE DATE: 11/27/02 DRAWN BY: TRU P.	4-22

M. J. ...
CHIEF, DEPT. OF TRANSPORTATION



SLOPE OF GUTTER	'A' FOR TYPE 2 CURB (APPROX. LENGTH)
0.5% to 0.6%	6'-6" MIN.
> 0.6% to 1.1%	7'-0"
> 1.1% to 1.6%	7'-6"
> 1.6% to 2.0%	8'-0"
> 2.0% to 2.4%	8'-6"
> 2.4% to 2.7%	9'-0"
> 2.7% to 3.0%	9'-6"
> 3.0% to 3.3%	10'-0"
> 3.3%	SEE NOTE #4

SLOPE OF GUTTER	'A' FOR TYPE 1/1A CURB (APPROX. LENGTH)
0.5% to 0.7%	4'-0" MIN.
> 0.7% to 1.6%	4'-6"
> 1.6% to 2.2%	5'-0"
> 2.2% to 2.8%	5'-6"
> 2.8% to 3.2%	6'-0"
> 3.2% to 3.6%	6'-6"
> 3.6% to 4.0%	7'-0"
> 4.0% to 4.3%	7'-6"
> 4.3%	SEE NOTE #4

CURB TYPE	TYPE 1/1A	TYPE 2
DIM 'B'	3'-6"	6'-0"

CURB TYPE	TYPE 1/1A	TYPE 2
* DIM 'C'	3-5/8"	6"

* OR AS NEEDED TO MATCH EXISTING ON SITE IMPROVEMENTS, 8" MAX.

NOTES:

1. DIMENSION 'A' IS THE LENGTH OF THE INCLINED PORTION OF THE RAMP THAT SLOPES IN THE SAME DIRECTION AS THE FLOW OF THE GUTTER.
2. DIMENSION 'B' IS THE LENGTH OF THE INCLINED PORTION OF THE RAMP THAT SLOPES IN THE OPPOSITE DIRECTION AS THE FLOW OF THE GUTTER.
3. RAMPS SHALL HAVE A HEAVY BROOM FINISH TRANSVERSE TO THEIR SLOPE.
4. REQUIRES SPECIAL DESIGN APPROVAL BY THE CHIEF OF THE DEPARTMENT OF TRANSPORTATION.
5. NO PULL BOX, UTILITY VAULT, UTILITY POLE, MANHOLE, OR SIMILAR APPURTENANCE SHALL BE LOCATED WITHIN SIDEWALK RAMP AREA.
6. SEE NOTE 6 DWG 4-24 FOR RAMPS WITH 6' WIDE RAMP PANS.

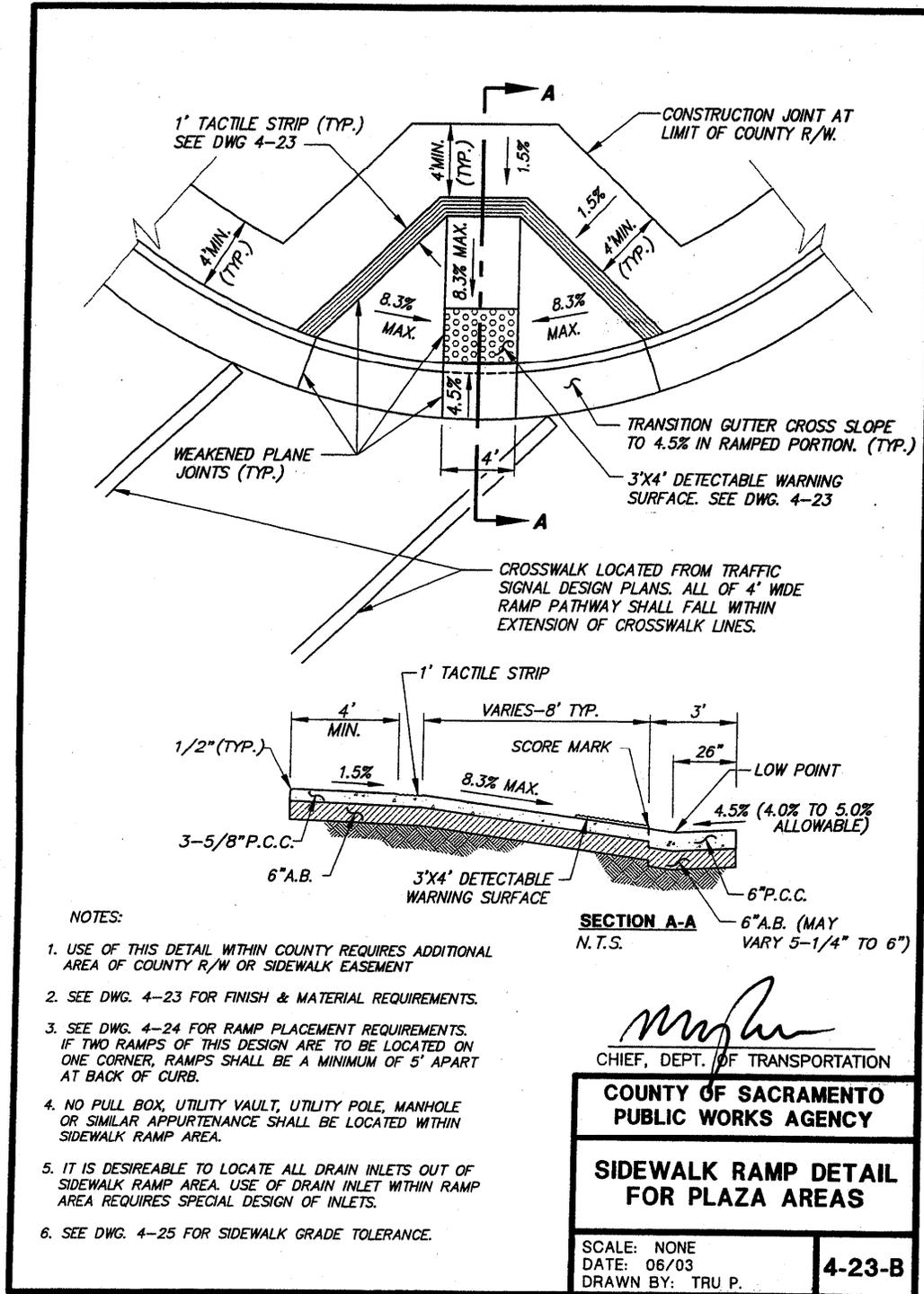
[Signature]
CHIEF, DEPT. OF TRANSPORTATION

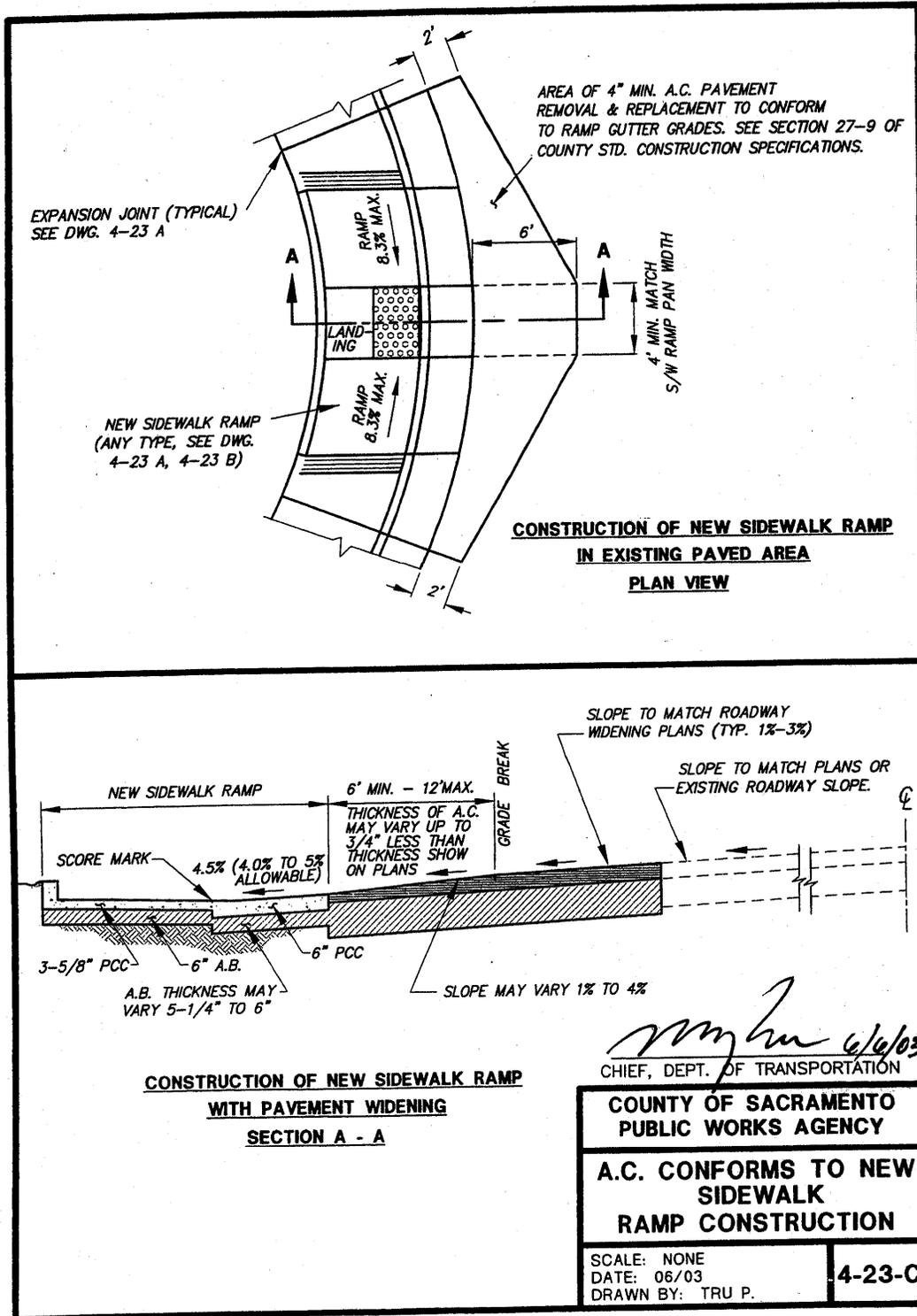
**COUNTY OF SACRAMENTO
PUBLIC WORKS AGENCY**

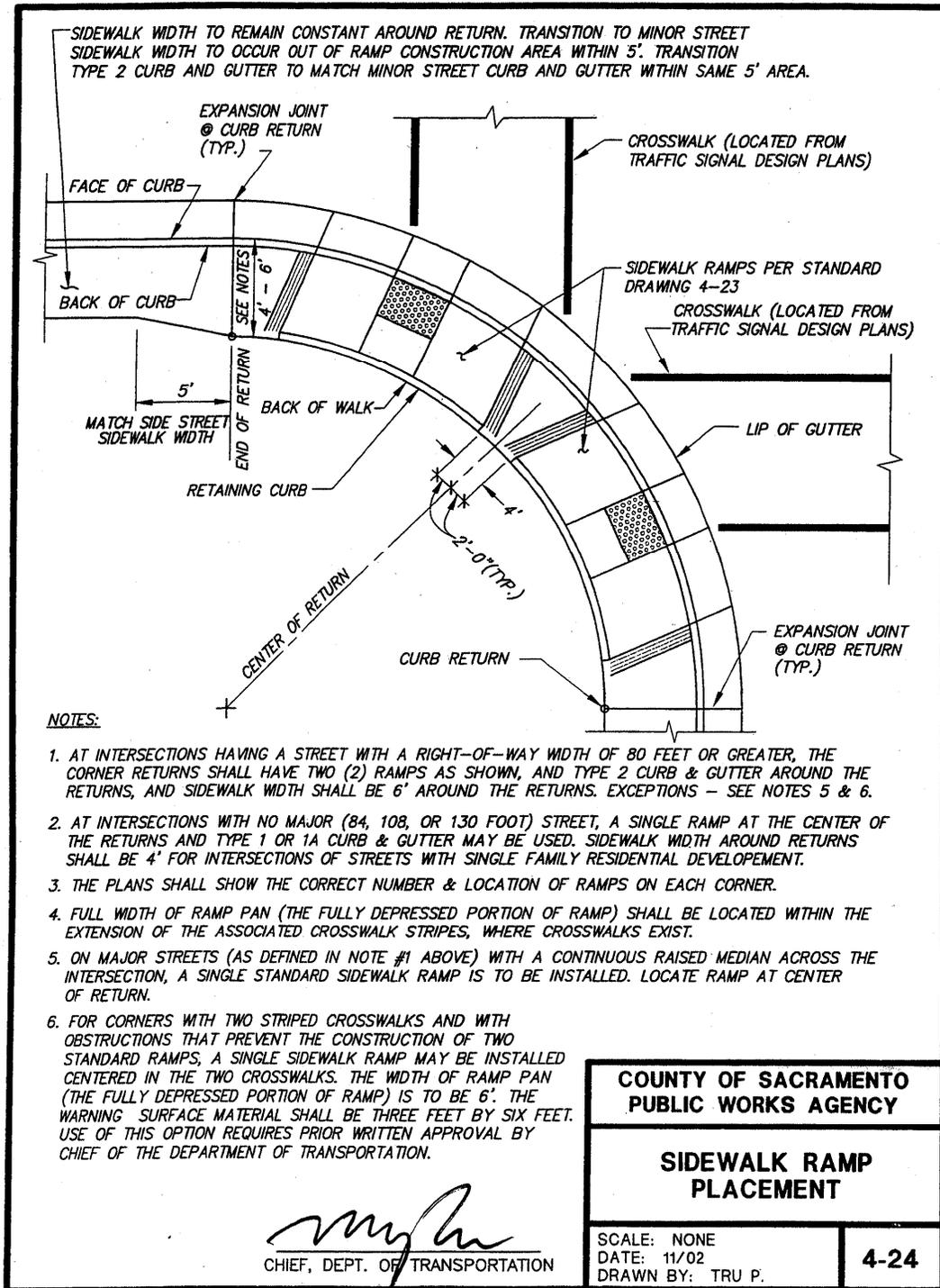
**SIDEWALK RAMPS
DETAIL**

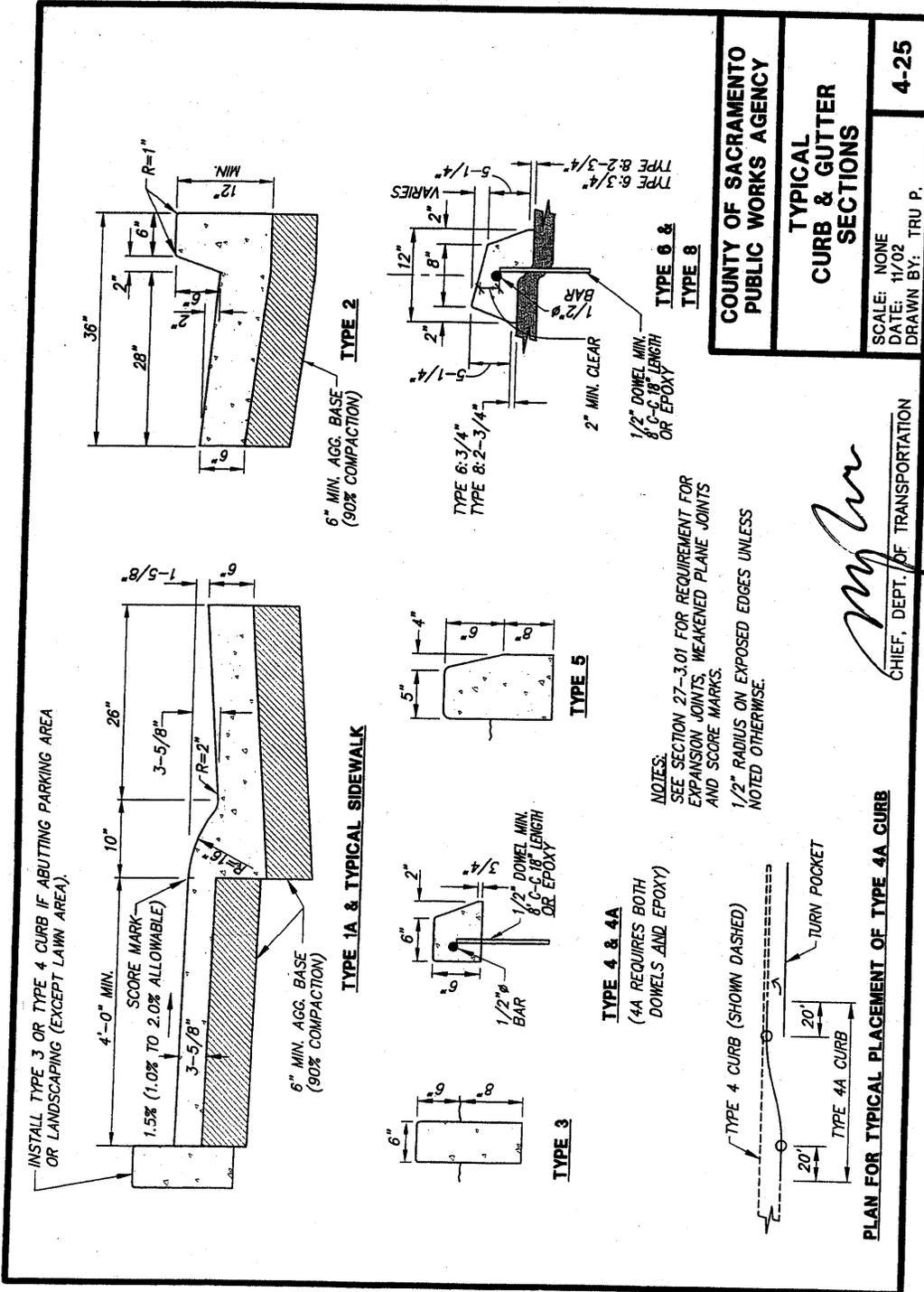
SCALE: NONE
DATE: 06/03
DRAWN BY: TRU P.

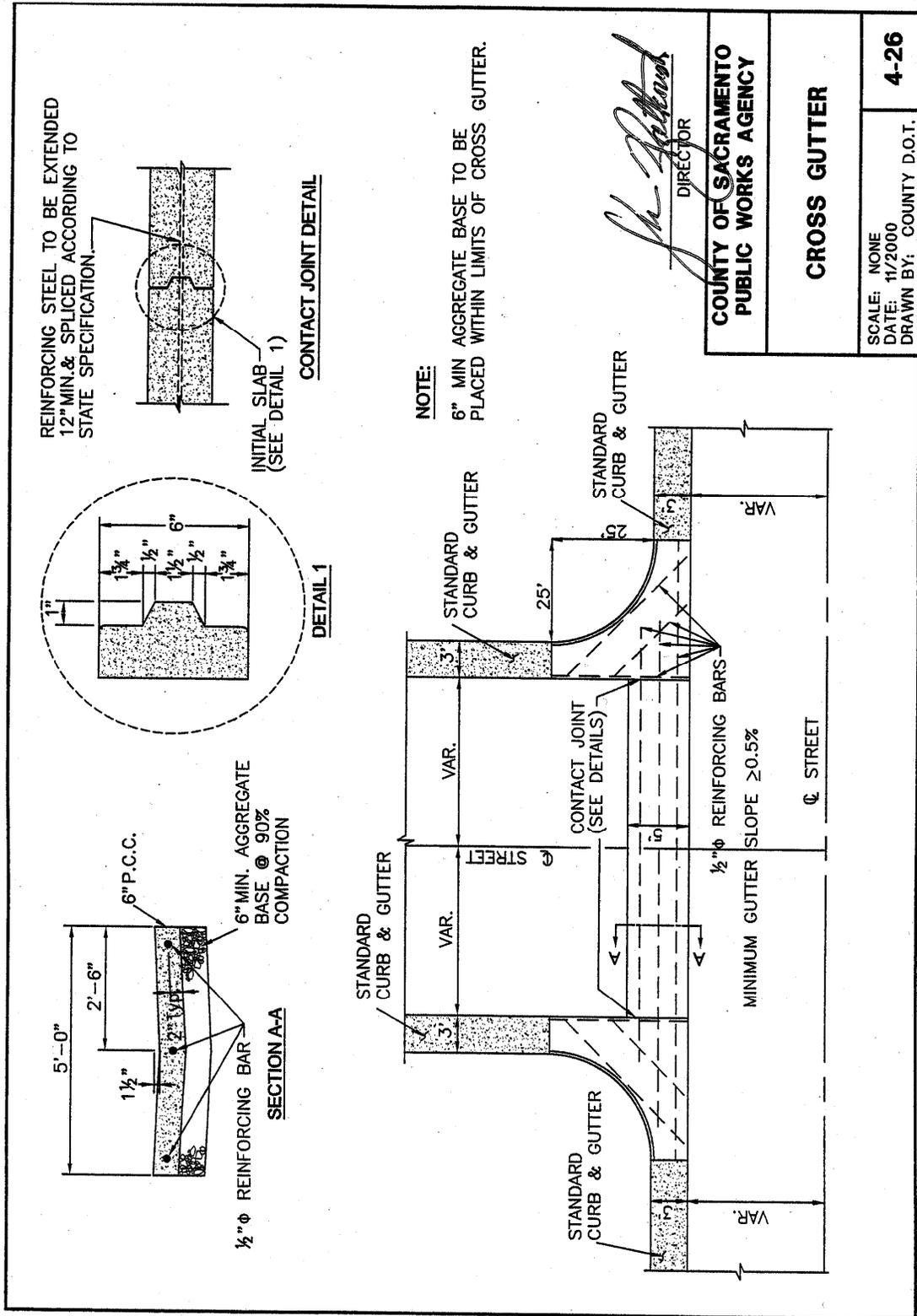
4-23-A

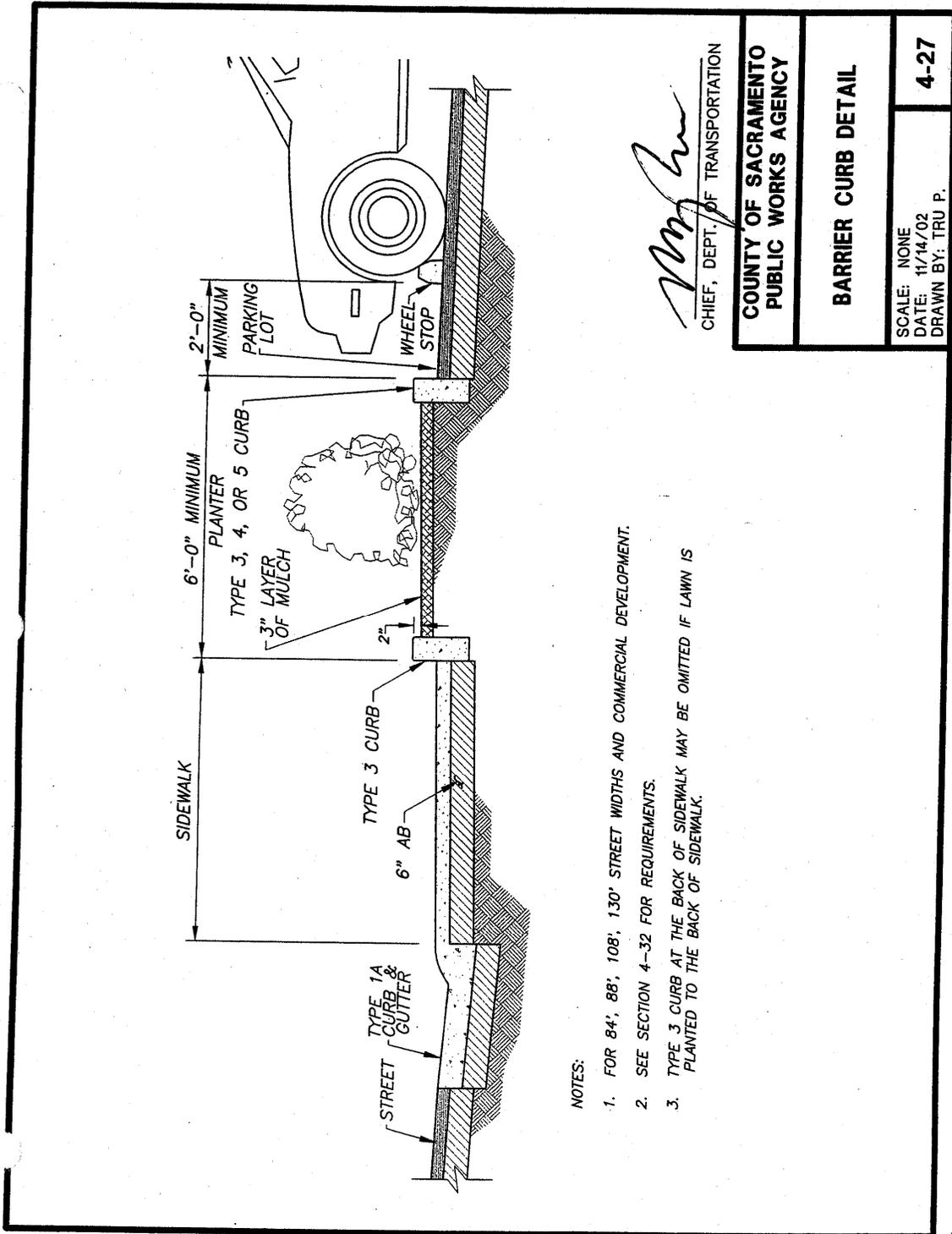






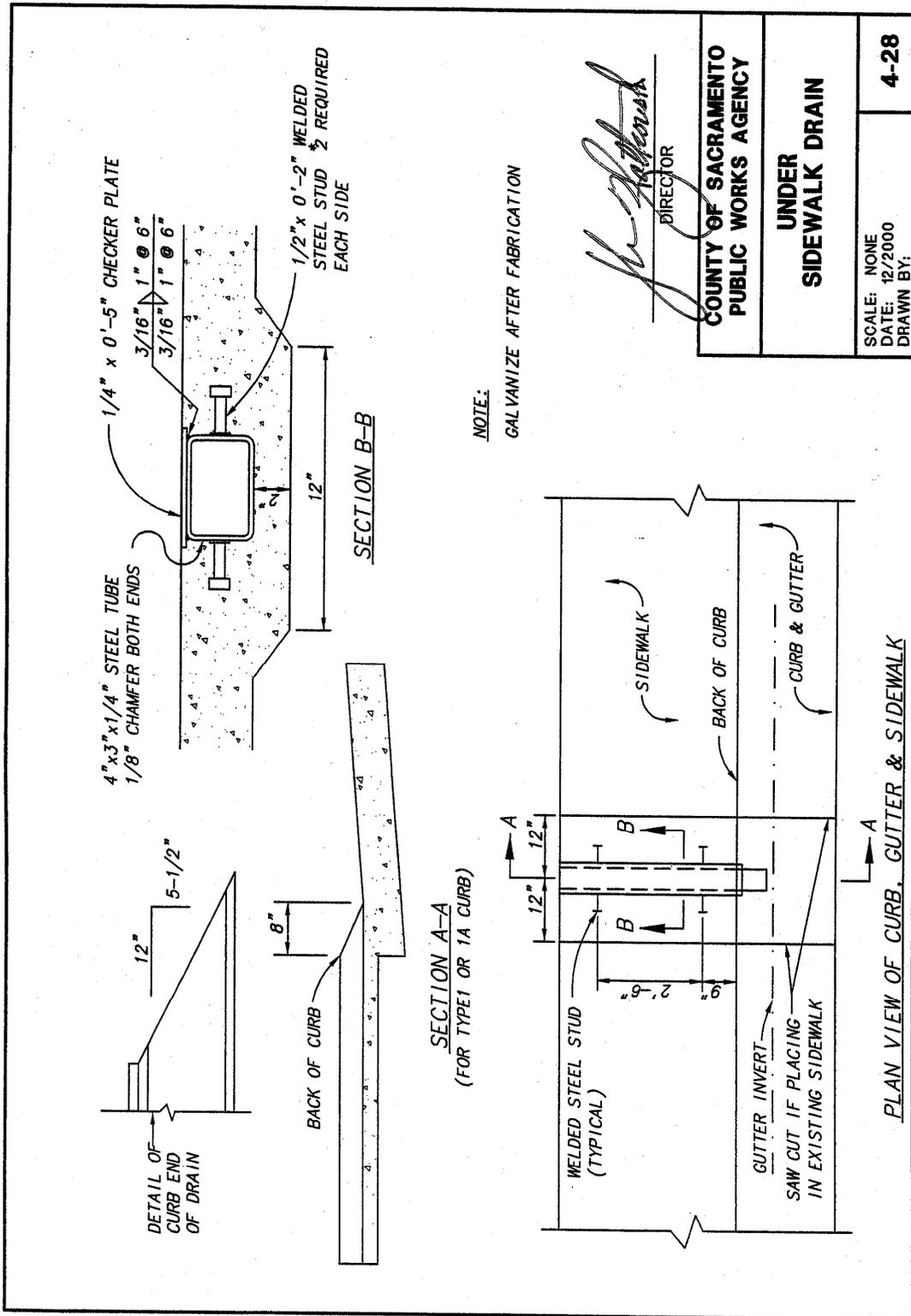


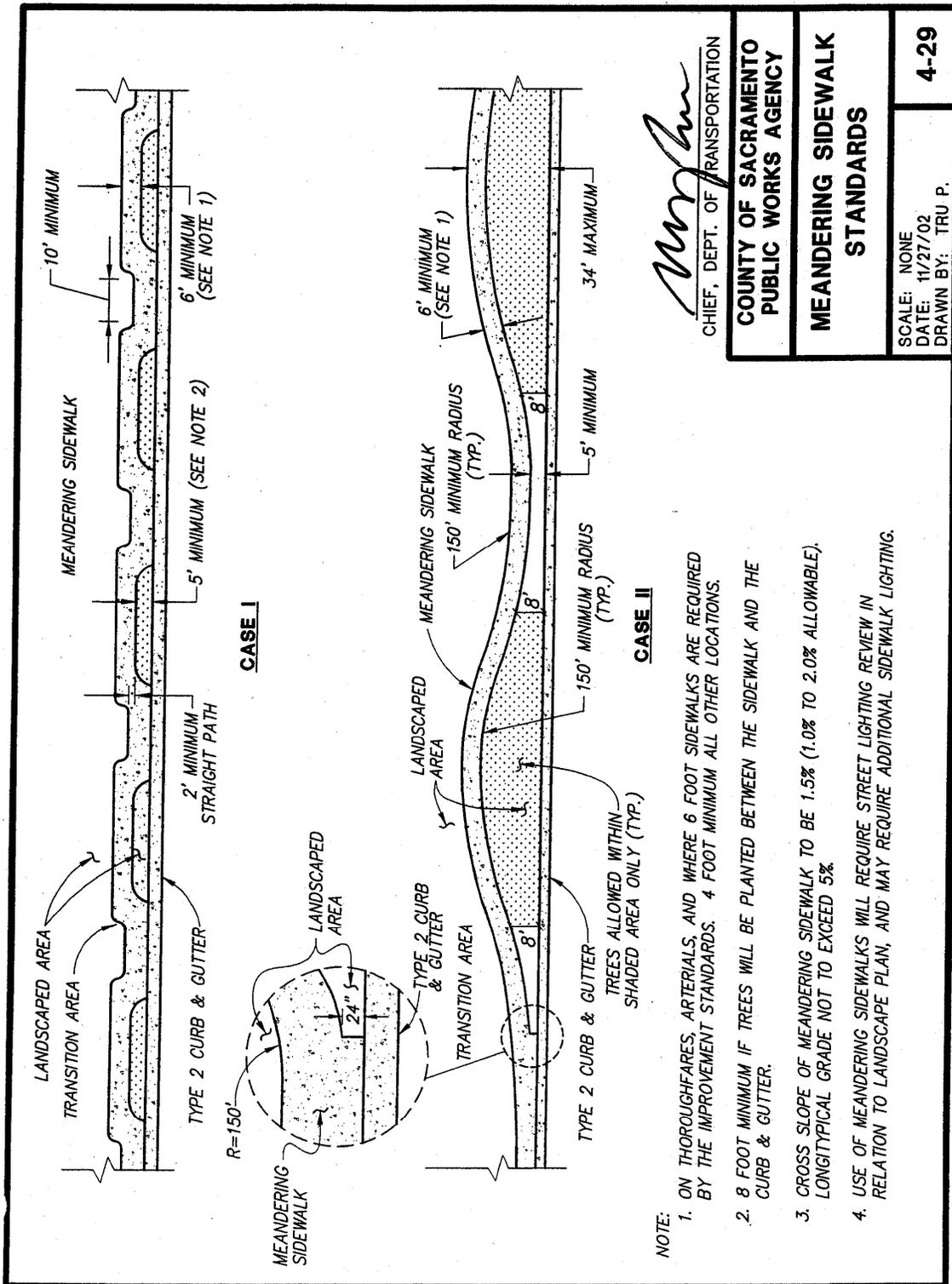




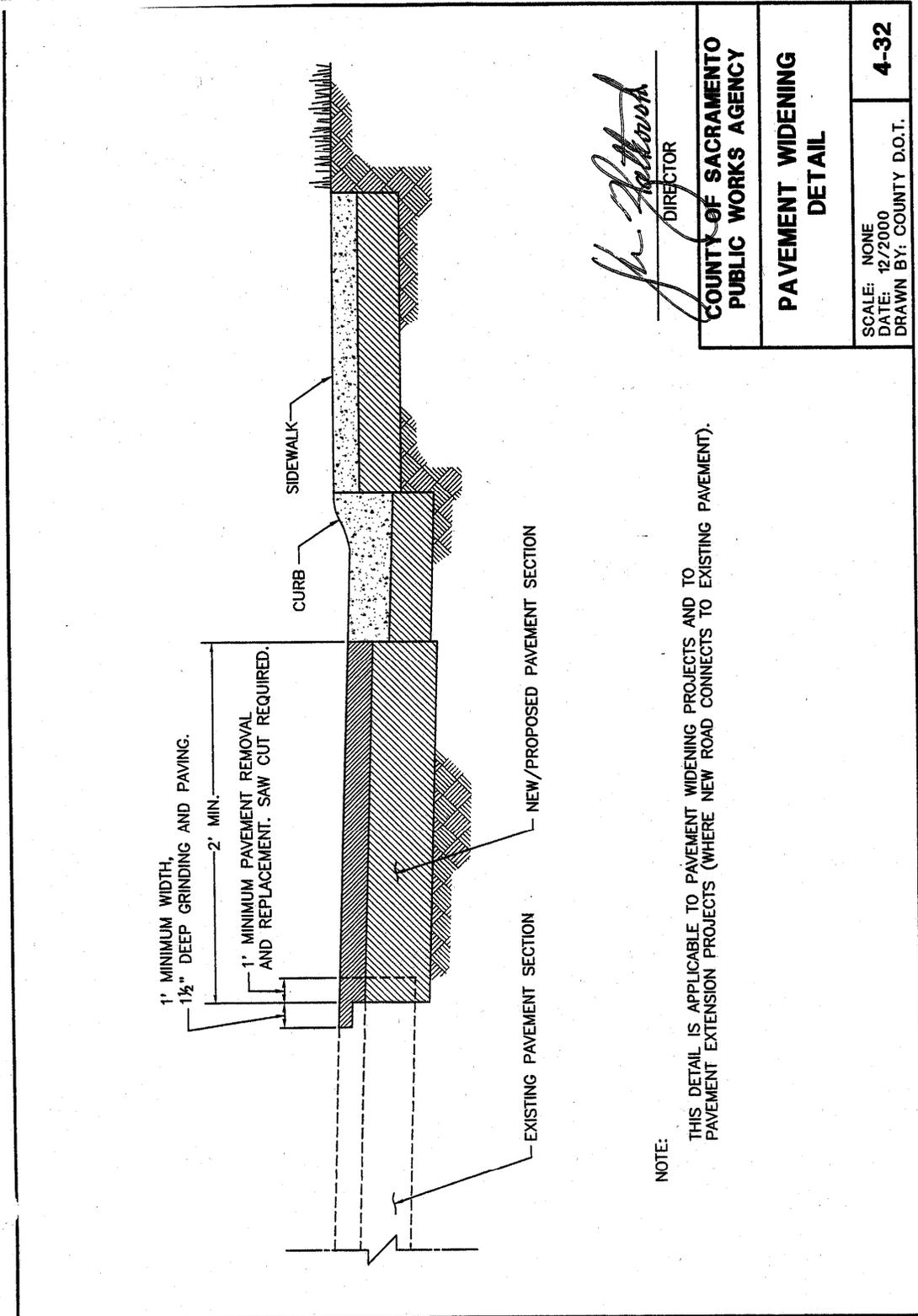
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 CHIEF, DEPT. OF TRANSPORTATION

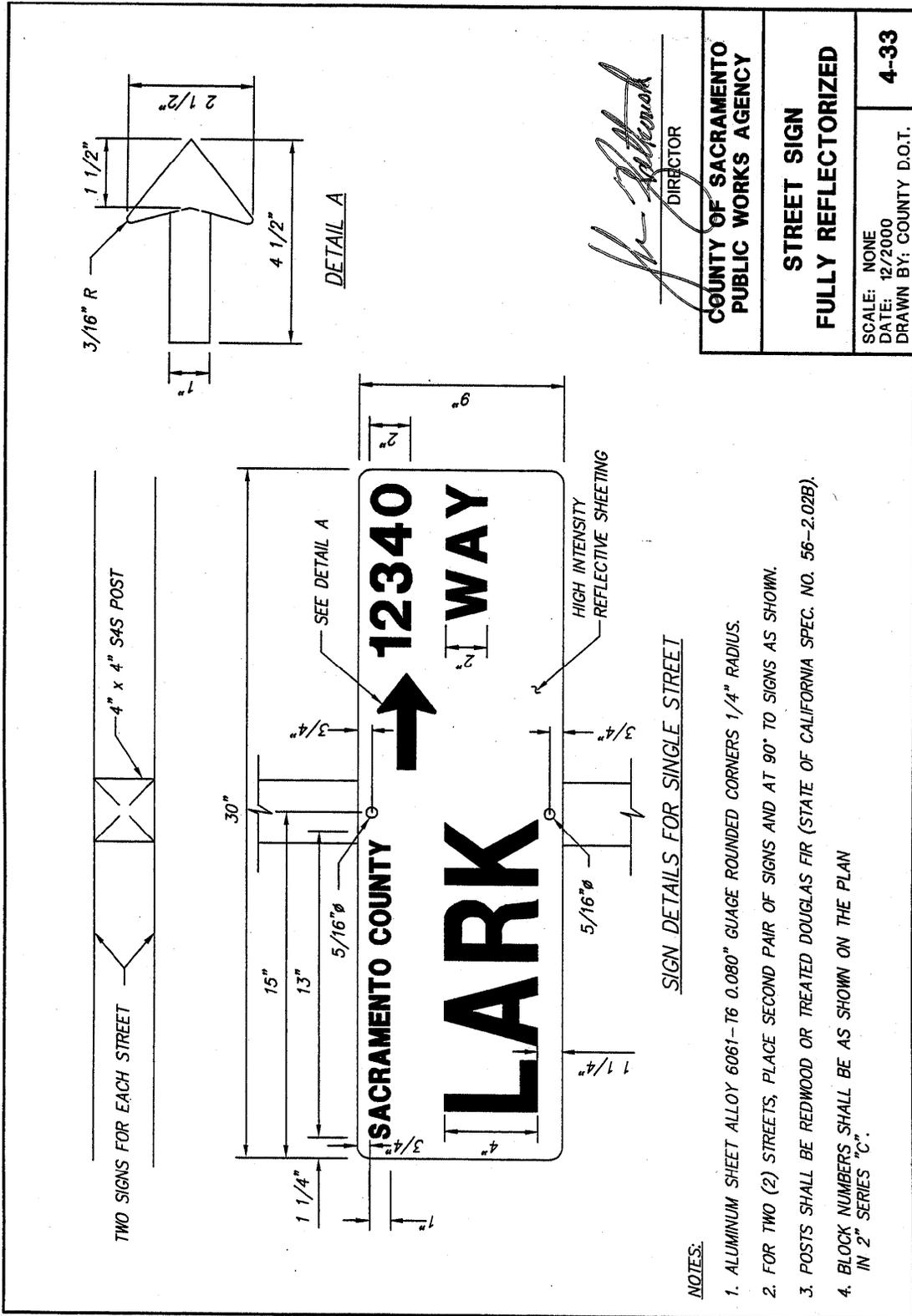
COUNTY OF SACRAMENTO PUBLIC WORKS AGENCY	
BARRIER CURB DETAIL	
SCALE: NONE DATE: 11/14/02 DRAWN BY: TRU P.	4-27

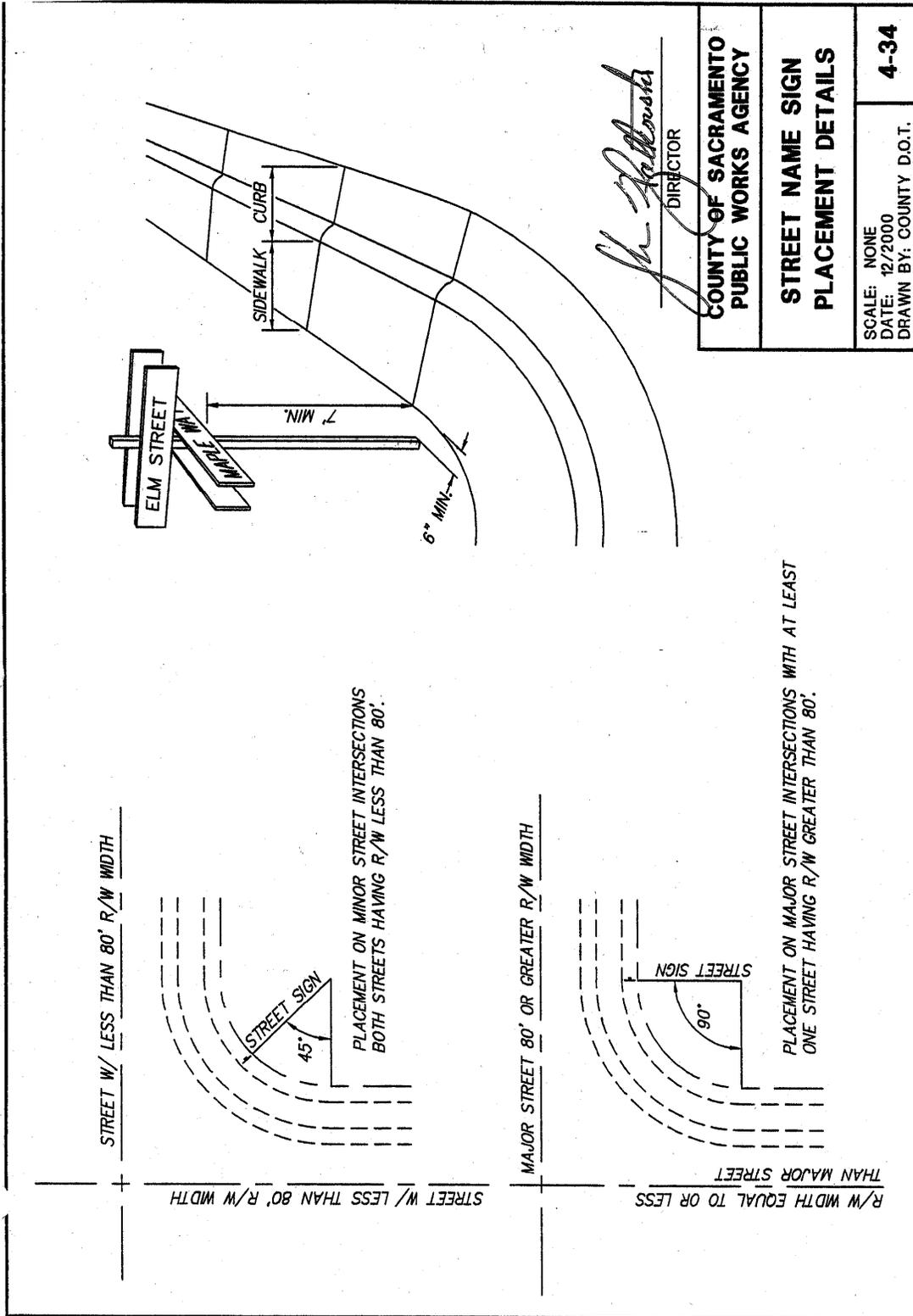


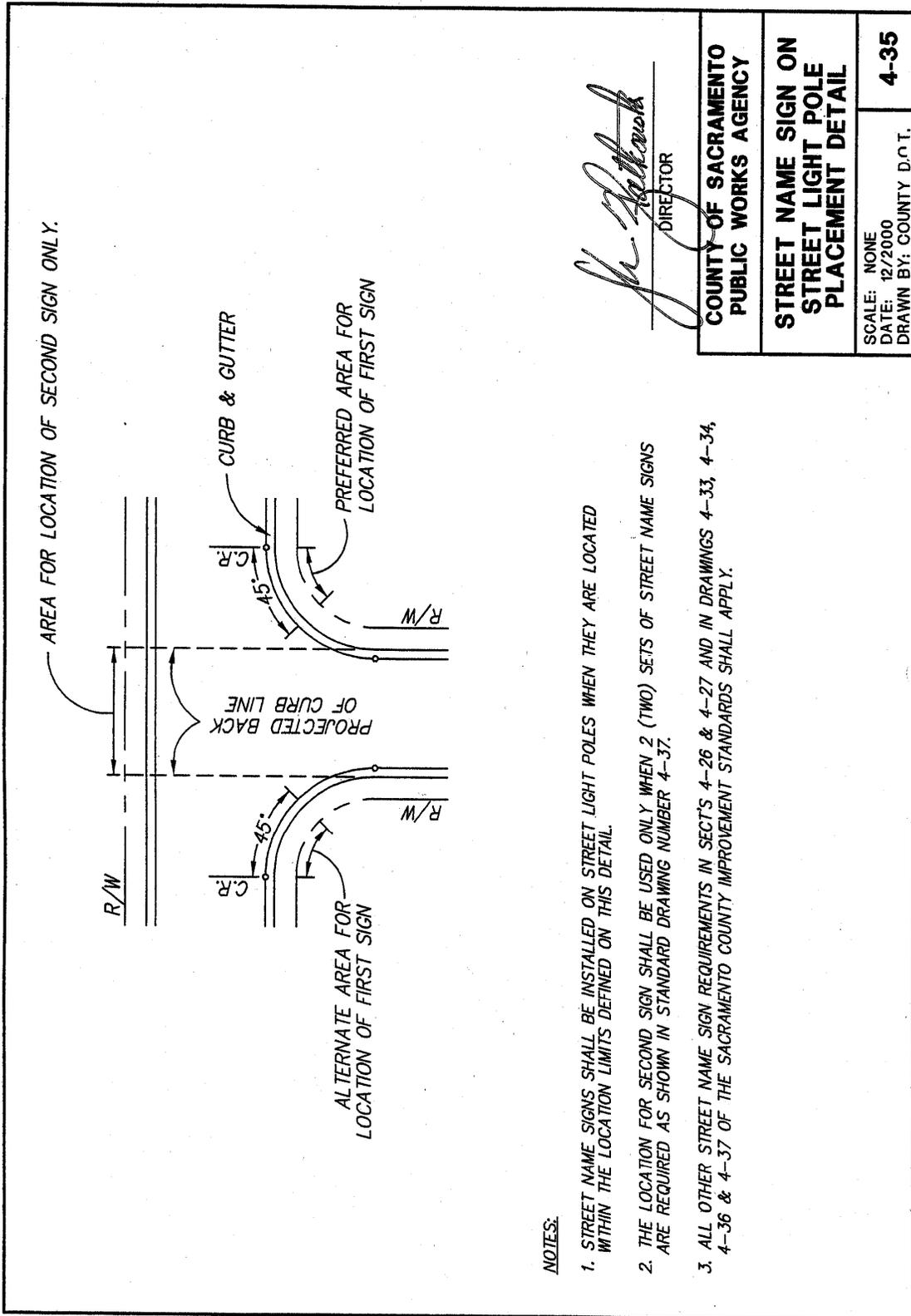


Tru P.
CHIEF, DEPT. OF TRANSPORTATION
**COUNTY OF SACRAMENTO
PUBLIC WORKS AGENCY**
**MEANDERING SIDEWALK
STANDARDS**
SCALE: NONE
DATE: 11/27/02
DRAWN BY: TRU P.
4-29







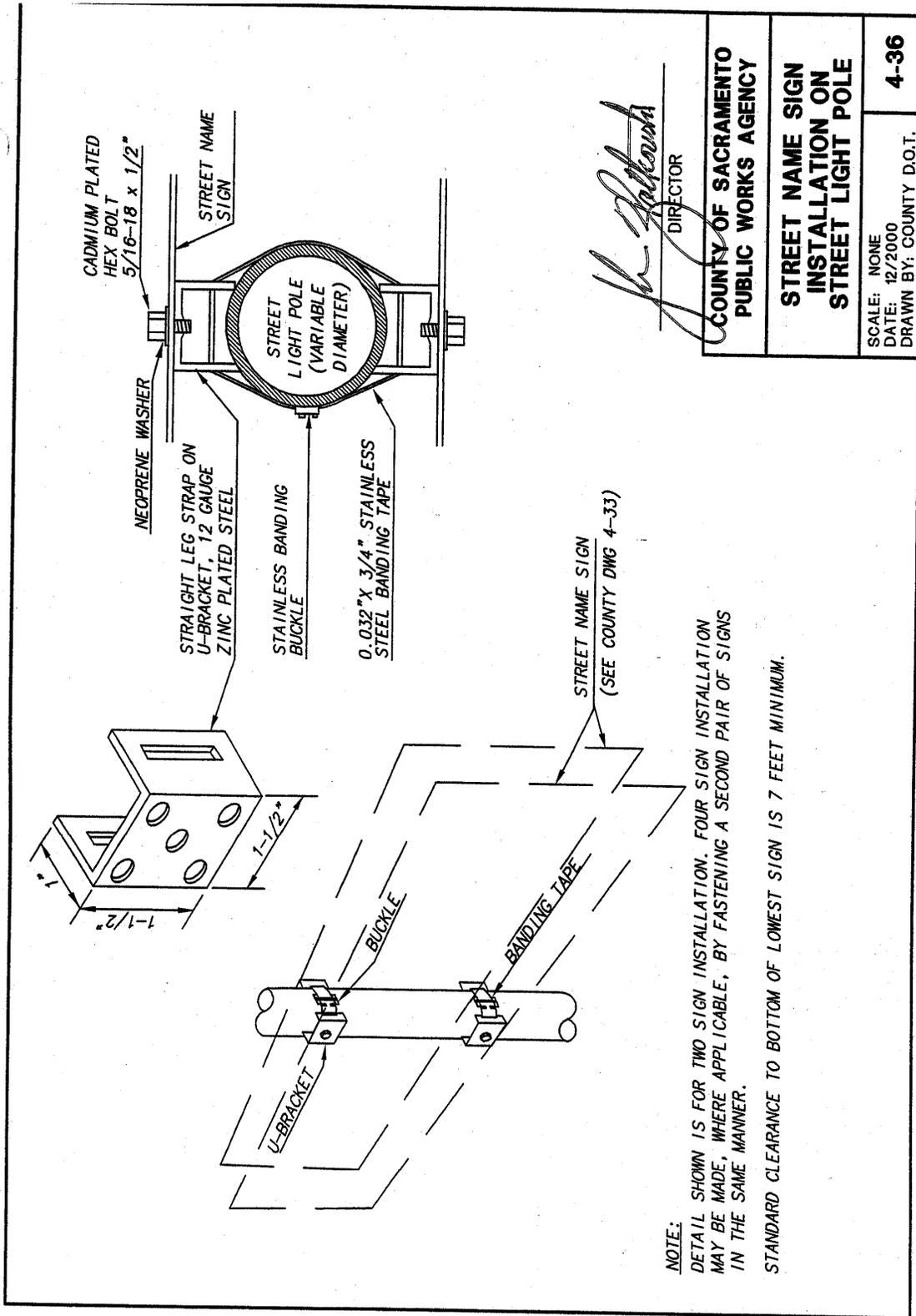


NOTES:

1. STREET NAME SIGNS SHALL BE INSTALLED ON STREET LIGHT POLES WHEN THEY ARE LOCATED WITHIN THE LOCATION LIMITS DEFINED ON THIS DETAIL.
2. THE LOCATION FOR SECOND SIGN SHALL BE USED ONLY WHEN 2 (TWO) SETS OF STREET NAME SIGNS ARE REQUIRED AS SHOWN IN STANDARD DRAWING NUMBER 4-37.
3. ALL OTHER STREET NAME SIGN REQUIREMENTS IN SECTS 4-26 & 4-27 AND IN DRAWINGS 4-33, 4-34, 4-36 & 4-37 OF THE SACRAMENTO COUNTY IMPROVEMENT STANDARDS SHALL APPLY.

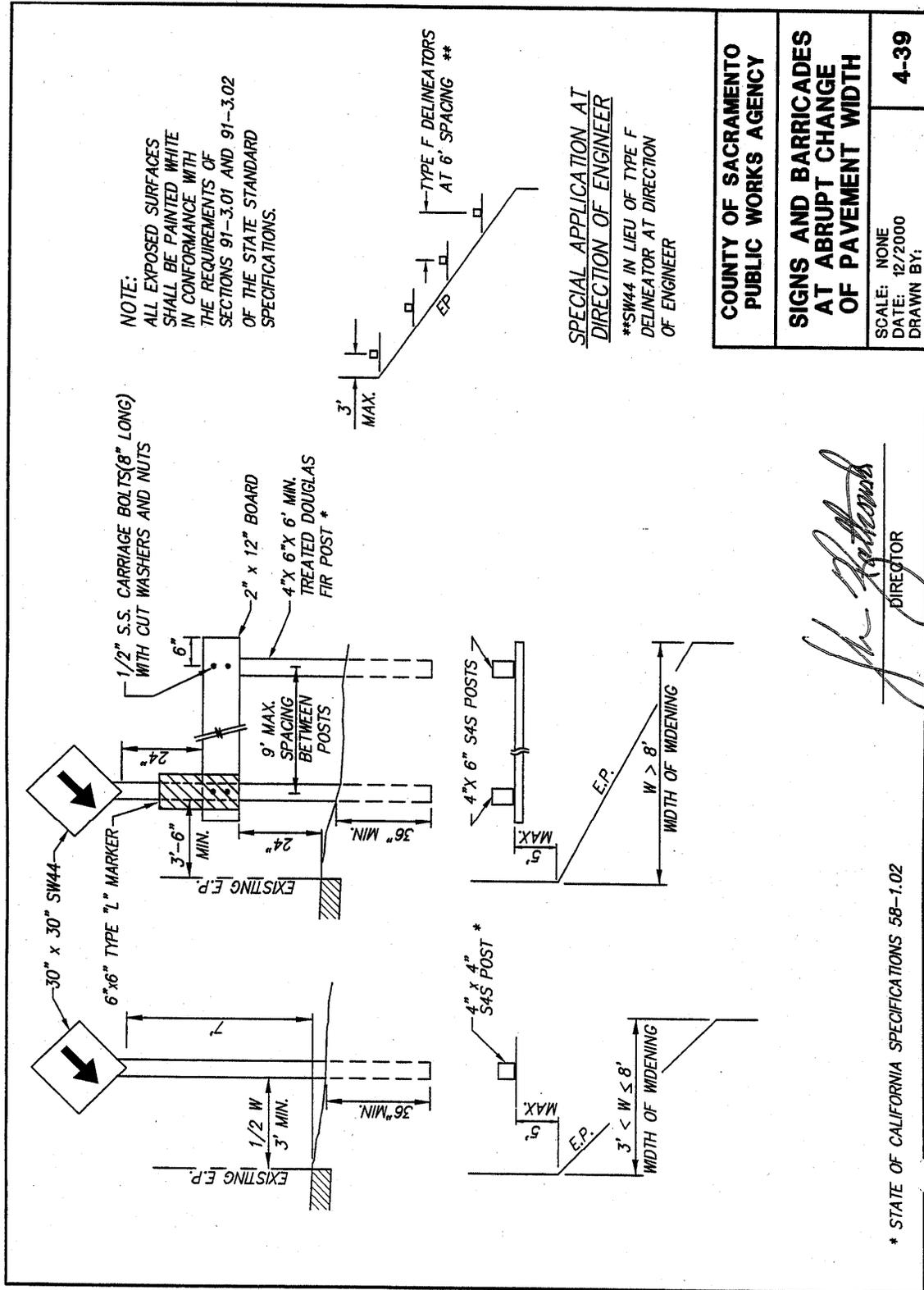
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 DIRECTOR

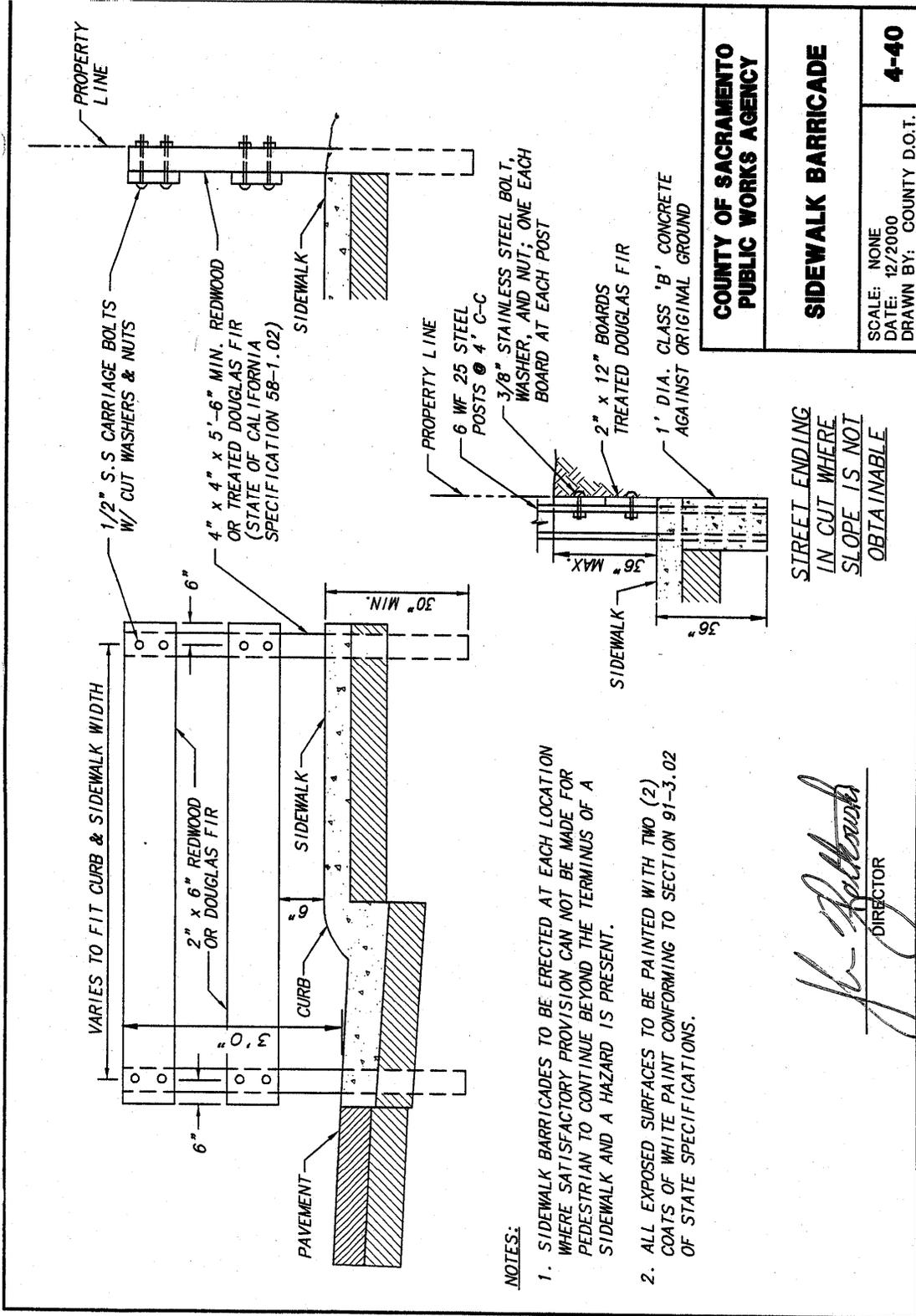
COUNTY OF SACRAMENTO PUBLIC WORKS AGENCY	
STREET NAME SIGN ON STREET LIGHT POLE PLACEMENT DETAIL	
SCALE: NONE DATE: 12/2000 DRAWN BY: COUNTY D.O.T.	4-35



[Signature]
DIRECTOR

COUNTY OF SACRAMENTO PUBLIC WORKS AGENCY	
STREET NAME SIGN INSTALLATION ON STREET LIGHT POLE	
SCALE: NONE DATE: 12/2000 DRAWN BY: COUNTY D.O.T.	4-36





COUNTY OF SACRAMENTO PUBLIC WORKS AGENCY	
SIDEWALK BARRICADE	
SCALE: NONE	4-40
DATE: 12/2000 DRAWN BY: COUNTY D.O.T.	

- NOTES:**
1. SIDEWALK BARRICADES TO BE ERECTED AT EACH LOCATION WHERE SATISFACTORY PROVISION CAN NOT BE MADE FOR PEDESTRIAN TO CONTINUE BEYOND THE TERMINUS OF A SIDEWALK AND A HAZARD IS PRESENT.
 2. ALL EXPOSED SURFACES TO BE PAINTED WITH TWO (2) COATS OF WHITE PAINT CONFORMING TO SECTION 91-3.02 OF STATE SPECIFICATIONS.

J. C. [Signature]
DIRECTOR

<p>CASE #1</p> <p>PREFERRED LOCATION WHERE UTILITY CONFLICTS AND PHYSICAL CONDITIONS ALLOW AND AN EASEMENT OR RIGHT OF WAY EXISTS BEHIND WALK.</p>	<p>CASE #2</p> <p>STANDARD LOCATION WHERE CASE #1 IS NOT REASONABLY ACHIEVABLE.</p> <p>* PEDESTRIAN CLEARANCE OF 36" MINIMUM IN FRONT OF THE POLE IS DESIRABLE. POLE CAN BE PLACED 12" MINIMUM FROM BACK OF CURB IF 36" MINIMUM PEDESTRIAN CLEARANCE IS PROVIDED BEHIND POLE. POLE TO BE PLACED WITH BACK OF POLE FLUSH WITH BACK OF WALK IF NOT IN CONFLICT WITH OTHER UTILITIES.</p>
<p>CASE #3</p> <p>ACCEPTABLE LOCATION ONLY WHERE CASES 1 OR 2 CANNOT BE REASONABLY ACHIEVED OR WHERE SPECIAL CONDITIONS DICTATE (I.E. WHERE SHORT FRONTAGE IMPROVEMENT CAUSES SIGNIFICANT DEVIATION FROM POLE LINE RESULTING IN UNDESIRABLE GUYING REQUIREMENTS).</p>	<p>CASE #4</p> <p>REFER TO NOTE 1</p> <p>* SEE NOTE 2 BELOW</p>

NOTES:

- WHERE STREET IMPROVEMENTS WILL ULTIMATELY BE CLASS A OR B, THE POLE SHOULD BE LOCATED IN CONFORMANCE TO THE APPROPRIATE CASE #1 OR #2 BASED ON THE FUTURE LOCATION OF THE STREET IMPROVEMENTS. IN THE CASE WHERE NO CURBS WOULD EVER BE ANTICIPATED, THE POLE SHOULD BE LOCATED 4 FEET MINIMUM FROM THE EDGE OF PAVEMENT SO AS NOT TO ENCRoACH IN AN ADJACENT DITCH AS SHOWN IN CASE #4.
- WHEN URBAN AREA ROADWAY IMPROVEMENT PROJECTS ARE UNDERTAKEN TO INCREASE ROADWAY CAPACITY OR SAFETY, UTILITY POLES MAY BE ALLOWED TO REMAIN AT THE EDGE OF PAVEMENT, WITH PREFERABLY 0.5 FEET MINIMUM CLEARANCE (CASE #4). HOWEVER, WHERE THE PRE-PROJECT UTILITY POLE LOCATION IS WITHIN THE PROPOSED PAVEMENT SECTION, UTILITY POLES SHOULD BE RELOCATED TO THE ULTIMATE LOCATION PER CASE #1 OR CASE #2 ABOVE, IN ORDER TO AVOID RELOCATION OF THE UTILITY POLE IN THE FUTURE.
- UTILITY POLE PLACEMENT UNDER THE FOLLOWING CONDITIONS IS SUBJECT TO APPROVAL BY THE CHIEF OF THE DEPARTMENT OF TRANSPORTATION:
 - A- IN VISIBILITY CONTROL AREA (SEE STANDARD DRAWING 4-20) AND IN THE RIGHT-OF-WAY IMMEDIATELY ADJACENT TO, OR AS AN EXTENSION OF, VISIBILITY CONTROL AREAS.
 - B- IN ADDITION TO THE REQUIREMENTS OF 3-A ABOVE, POLES LARGER THAN 18 INCHES IN DIAMETER WITHIN 9 FEET OF THE EDGE OF PAVEMENT OR EDGE LINE WHERE THERE IS NO SIDEWALK.
 - C- POLES LOCATED ON THE OUTSIDE OF SHARP CURVES, OR IN THE TANGENT OF THE CURVES WITHIN 200 FEET OF THE BEGINNING OR END OF CURVE (FOR 45 MPH OR GREATER SPEED LIMIT) OR 100 FEET (FOR A LESSER SPEED LIMIT) FOR CONSIDERATION OF GUARDRAIL OR SPECIAL DELINEATION FOR PURPOSES OF THIS SECTION, SHARP CURVES SHALL BE CONSIDERED THOSE WITH RADIUS OF LESS THAN 800 FEET FOR URBAN 2 LANE STREETS AND WITH RADIUS OF LESS THAN 2000 FEET FOR RURAL ROADS OR MULTI-LANE STREETS.
 - D- RISERS PROPOSED IN A LOCATION THAT WOULD REDUCE PEDESTRIAN CLEARANCE TO LESS THAN 36 INCHES SHALL NOT BE ALLOWED.

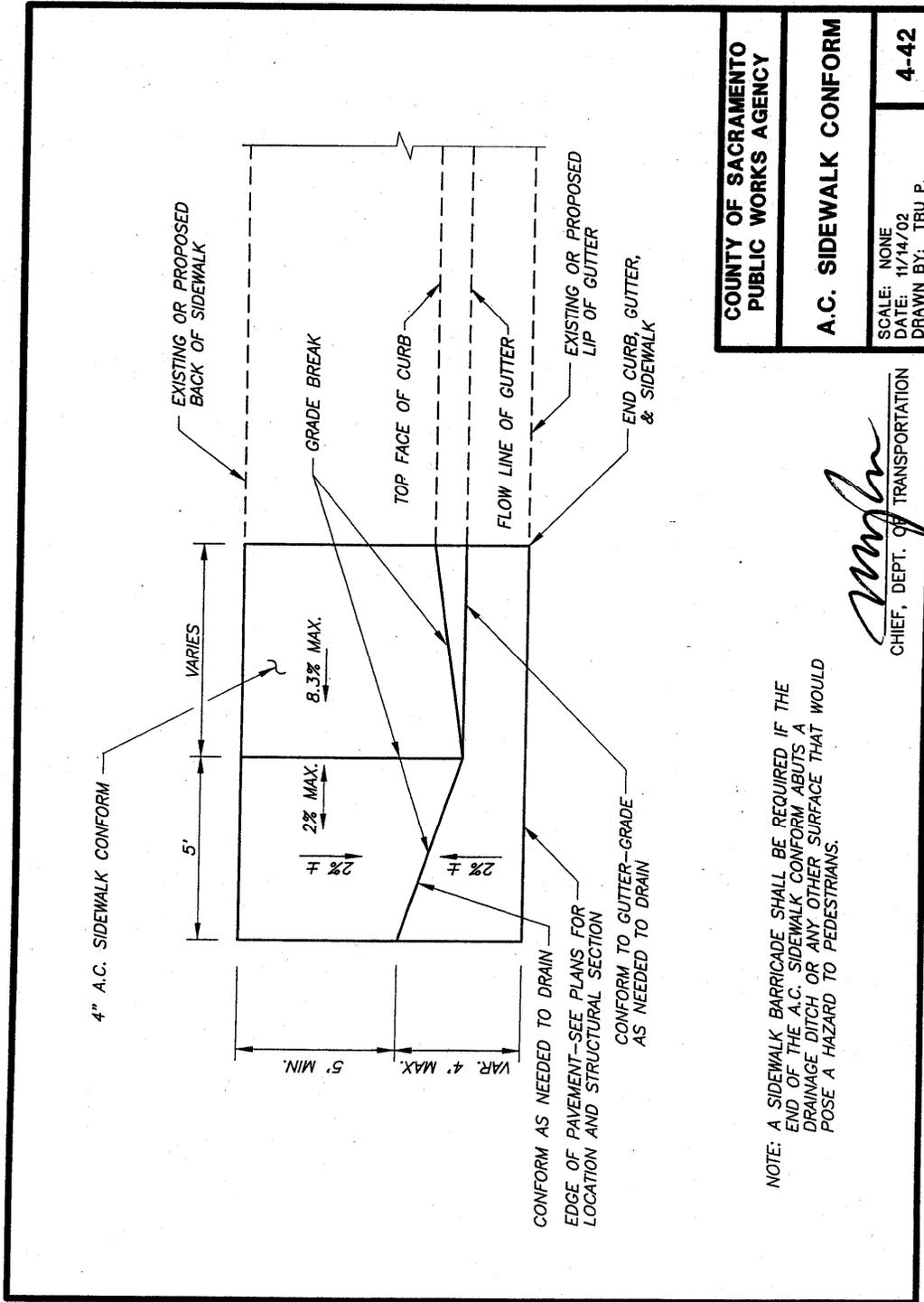
**COUNTY OF SACRAMENTO
PUBLIC WORKS AGENCY**

**UTILITY POLE
PLACEMENT LOCATIONS**

SCALE: NONE
DATE: 12/2000
DRAWN BY: COUNTY D.O.T.

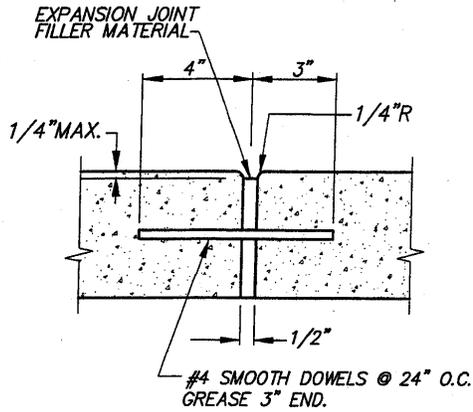
4-41

[Signature]
DIRECTOR

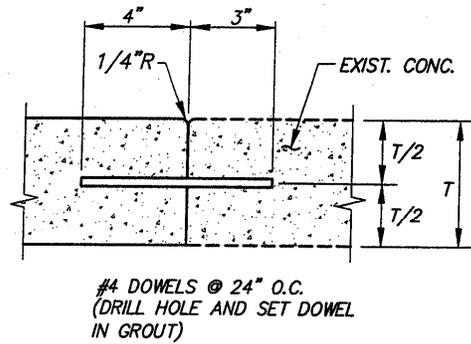


COUNTY OF SACRAMENTO PUBLIC WORKS AGENCY	
A.C. SIDEWALK CONFORM	
SCALE: NONE	4-42
DATE: 11/14/02	
DRAWN BY: TRU P.	

[Signature]
CHIEF, DEPT. OF TRANSPORTATION

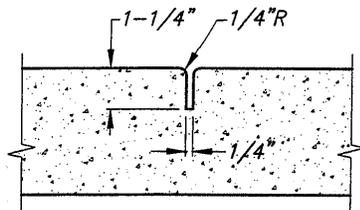


EXPANSION JOINT

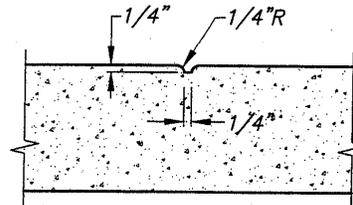


SIDEWALK CONTACT JOINT

(USE WHERE NEW CONSTRUCTION OF S/W AND
OR CURB & GUTTER ABUTTS EXISTING IMPROVEMENTS.)



WEAKENED PLANE JOINT



SCORE MARK

[Signature]
CHIEF, DEPT. OF TRANSPORTATION

<p>COUNTY OF SACRAMENTO PUBLIC WORKS AGENCY</p>	
<p>STANDARD CONCRETE JOINT DETAILS</p>	
<p>SCALE: NONE DATE: 12/02 DRAWN BY: TRU P.</p>	<p>4-43</p>

Appendix C: Capital Implementation Program Project List

Legend for Project List

Use Priorities

- A. Public input requests
 - 1. Government services.
 - 2. Commercial, business or multi-family residential.
 - 3. Single-family residential.

(Refer to Section 6.3 for detailed descriptions)

Condition Priorities

- 1) Reconstruct curb ramps at locations where existing curb ramps have an unsafe condition that may cause a trip and fall.
- 2) A new curb ramp will be installed at locations where there is no curb ramp to provide accessibility.
- 3) When a corner has one existing curb ramp and conditions allow for the construction of an additional curb ramp at the same corner, and provided that traffic controls allow for a safe path of travel, an additional curb ramp will be installed.
- 4) A curb ramp is constructed or reconstructed at a location with difficult physical conditions such as major utility conflicts, physical barriers or other constraints, which would create a hardship situation on the entity.
- 5) An existing curb ramp will be reconstructed when it does not meet current federal and state accessibility standards

(Refer to Section 6.3 for detailed descriptions)

Work Scope Codes

- (1) Complete ADA retrofit of signalized four-way intersection.
- (2) Complete ADA retrofit of controlled intersection.
- (3) Complete ADA retrofit of signalized T-intersection.
- (4) At signalized intersections, installation of new accessible pedestrian signals with push buttons and crosswalk striping for all crossing directions where crosswalks are required by the ADA Codes and Standards.
- (5) Partial ADA retrofit at four-way intersection, single-family residential area.

- (6) Partial ADA retrofit at T-intersection, single-family residential area.
- (7) One or more new single curb ramps where other curb ramps at the intersection are complying.
- (8) Renovation of existing curb ramp to remove hazardous conditions.
- (9) Installation of new curb, gutter and concrete sidewalk.
- (10) Partial curb, gutter and sidewalk installation to provide programmatic access.
- (11) Miscellaneous sidewalk or walkway repair or replacement.
- (12) Selected sidewalk and bus pad pavement as required for transit access.
- (13) Selected sidewalk and bus pad pavement as required for new mid-block crosswalk with pedestrian signals.
- (14) Roadway widening or installation of required asphalt conforms for accessible pedestrian access routes.
- (15) Removal of sidewalk barriers.

(Refer to Section 6.2 for detailed descriptions)

Funding Codes

- A. Measure A sales tax funds.
- B. Disabled Access to Transit Program, funded by Congestion Mitigation and Air Quality Program (Federal TEA-21 Program).
- C. State Transportation Improvement Program (STIP) (includes Measure A sales tax funds and specified developer fees).
- D. Regional Surface Transportation Program (includes Measure A sales tax funds and specified developer fees).
- E. Florin Beautification Project, funded by Regional Surface Transportation Program (Federal TEA-21 Program) and local Florin Road Partnership Business Improvement District.
- F. Florin Beautification Project, funded by State Transportation Improvement Program.
- G. Folsom Blvd. Enhancement Project, funded by State Transportation Improvement Program.
- H. Franklin Blvd. Streetscape Project, funded by Regional Surface Transportation Program (Federal TEA-21 Program) and Sacramento Housing and Redevelopment Association and the City of Sacramento.
- I. Fulton Avenue Enhancement Project, funded by Regional Surface Transportation Program (Federal TEA-21 Program) and Fulton Avenue Business Improvement District.
- J. Roadway Development Fee Capital Improvement Plan (includes Measure A sales tax funds).
- K. Elk Grove West Vineyard Public Facilities and Vineyard Springs Comprehensive Financing Plans.
- L. North Natomas Financing Plan, in conjunction with the City of Sacramento.
- M. Sacramento Area Flood Control Agency administered (from Measure A sales tax funds).
- N. Antelope Public Facilities Financing Plan Capital Improvement Program.

Sacramento County Department of Transportation ADA Transition Plan

- O. Greenback Lane Improvement Project, in cooperation with the City of Citrus Heights (includes Measure A sales tax funds and specified developer fees).
- P. Federal Safe Routes to School Program.
- Q. North Watt Avenue Enhancement Program, funded by State Transportation Improvement Program (STIP) (includes Measure A sales tax funds) and Sacramento Housing and Redevelopment Association.
- R. Funds from Developer fees.
- S. Sidewalk Continuity Project, from Measure A sales tax funds.
- T. Unused
- U. Unused
- V. Unused
- W. Unused
- X. (also called CR) Elderly and Disabled Access Program, improvements determined from resident requests.
- Y. Major Intersection Projects, from Measure A sales tax funds.
- Z. ADA Transition Plan Implementation Projects, from combination of Measure A sales tax funds, developer fees and other currently unspecified funds.

Sacramento County Department of Transportation ADA Transition Plan

Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
FY 2004-2005											
S. Sacto	55th St	Florin Rd	1	2	9	E	\$25,040		\$3,040	\$8,200	\$36,280
Carmichael	Auburn Blvd	Myrtle Ave	1	2	5	A	\$0	\$11,840	\$0	\$9,840	\$21,680
Arden Arcade	Avalon Dr	Marilona Dr	1	2	3	P	\$37,560		\$4,560	\$6,150	\$48,270
S. Sacto	Bowling Dr	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$12,300	\$26,340
N. Highlands	College Oak Dr	Myrtle Ave	1	2	5	A	\$0	\$11,840	\$0	\$9,840	\$21,680
S. Sacto	E Southgate Dr	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$12,300	\$26,340
Antelope	Falcon View Dr	N Loop Blvd	1	2	1	N	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560
S. Sacto	Fawn Way	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$12,300	\$26,340
S. Sacto	Franklin Blvd	41st Ave	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	42nd Ave	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	43rd Ave	1	2	9	H	\$12,520		\$1,520	\$8,200	\$22,240
S. Sacto	Franklin Blvd	44th Ave	1	2	9	H	\$12,520		\$1,520	\$8,200	\$22,240
S. Sacto	Franklin Blvd	45th Ave	1	2	9	H	\$12,520		\$1,520	\$8,200	\$22,240
S. Sacto	Franklin Blvd	46th Ave	1	2	9	H	\$12,520		\$1,520	\$8,200	\$22,240
S. Sacto	Franklin Blvd	47th Ave	1	2	1	H	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
S. Sacto	Franklin Blvd	48th Ave	1	2	9	H	\$12,520		\$1,520	\$12,300	\$26,340
S. Sacto	Franklin Blvd	51st Ave	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	52nd Ave	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	Cuny Ave	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	Florin Rd	1	2	1	E	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
S. Sacto	Franklin Blvd	Green Tree Dr	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	Turnbridge Dr	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	Williamsborough Dr	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Fulton Ave	Alta Arden Expy	1	2	1	I	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Arden Arcade	Fulton Ave	Arden Way	1	2	1	I	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Arden Arcade	Fulton Ave	Armstrong Dr	1	2	9	I	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Fulton Ave	Auburn Blvd	1	2	1	I	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Antelope	Watt Ave	Bainbridge Dr	1	2	2	Q	\$50,080		\$6,080	\$6,150	\$62,310
N. Highlands	Watt Ave	E St	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880
N. Highlands	Watt Ave	Karen Ln	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880
N. Highlands	Watt Ave	Karl Dr	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880

Sacramento County Department of Transportation ADA Transition Plan

Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Antelope	Watt Ave	Larchmont Dr	1	2	9	Q	\$12,520		\$1,520	\$12,300	\$26,340
N. Highlands	Watt Ave	Margaret Way	2	2	9	Q	\$12,520		\$760	\$1,750	\$15,030
N. Highlands	Watt Ave	McClellan Dr	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880
Antelope	Watt Ave	Mountain Oak Way	1	2	9	Q	\$12,520		\$1,520	\$12,300	\$26,340
Carmichael	Watt Ave	Myrtle Ave	1	2	1	Q	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
N. Highlands	Watt Ave	N Haven Dr	1	2	9	Q	\$12,520		\$1,520	\$12,300	\$26,340
N. Highlands	Watt Ave	Oak Dell Ave	1	2	9	Q	\$12,520		\$1,520	\$8,200	\$22,240
N. Highlands	Watt Ave	Orange Grove Ave	1	2	9	Q	\$12,520		\$1,520	\$8,200	\$22,240
N. Highlands	Watt Ave	Palm St	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880
Antelope	Watt Ave	Plymouth Dr	1	2	9	Q	\$12,520		\$1,520	\$12,300	\$26,340
N. Highlands	Watt Ave	Poplar Blvd	1	2	9	Q	\$12,520		\$1,520	\$6,560	\$20,600
Antelope	Watt Ave	Quinn Way	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880
N. Highlands	Watt Ave	Roseville Rd	1	2	9	Q	\$25,040		\$3,040	\$12,300	\$40,380
Antelope	Watt Ave	Turner Dr	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880
N. Highlands	Watt Ave	Wings Way	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Maryal Dr	Marilona Dr	1	2	3	P	\$37,560		\$4,560	\$1,750	\$43,870
S. Sacto	Florin Mall 1	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$1,750	\$15,790
S. Sacto	Mall Drive	Florin Rd	1	2	1	E	\$50,080	\$11,840	\$6,080	\$1,750	\$69,750
S. Sacto	Florin Mall 2	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$1,750	\$15,790
S. Sacto	65th Expy	Florin Rd	1	2	3	E	\$37,560	\$8,880	\$4,560	\$1,750	\$52,750
S. Sacto	Franklin Blvd	49th Ave	2	2	9	H	\$12,520		\$760	\$1,750	\$15,030
S. Sacto	Franklin Blvd	La Grande Blvd	2	2	9	H	\$12,520		\$760	\$1,750	\$15,030
Arden Arcade	Fulton Ave	Trade Wind Ave	2	2	9	I	\$12,520		\$760	\$1,750	\$15,030
Arden Arcade	Fulton Ave	Echo Way	2	2	9	I	\$12,520		\$760	\$1,750	\$15,030
Antelope	Watt Ave	Fairbairn Dr	2	2	9	Q	\$12,520		\$760	\$1,750	\$15,030
Antelope	Watt Ave	Antelope Blvd	2	2	1	Q	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710
Antelope	N Loop Blvd	Elverta Rd	3	2	3	N	\$37,560	\$8,880	\$0	\$350	\$46,790
Arden Arcade	GARFIELD AVE	GIBBONS DR	1	1	10	Z	\$145				\$145
Arden Arcade	COUNTRY HAVEN CT	EDISON AVE	1	1	10	Z	\$145				\$145
Arden Arcade	ENGLE RD	HALLELUJAH CT	1	1	10	Z	\$145				\$145
Arden Arcade	WHITNEY AVE	NORRIS AVE	1	1	10	Z	\$145				\$145
Arden Arcade	WHITNEY AVE	CONCETTA WAY	1	1	10	Z	\$145				\$145
Arden Arcade	MISSION AVE	WHITNEY AVE	1	1	10	Z	\$145				\$145

Sacramento County Department of Transportation ADA Transition Plan

Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	WHITNEY AVE	FOSTER WAY	1	1	10	Z	\$145				\$145
Arden Arcade	WATT AVE	LARCHMONT SQUARE LN	1	1	10	Z	\$145				\$145
Arden Arcade	WATT AVE	LYNNE WAY	1	1	10	Z	\$145				\$145
Arden Arcade	WALNUT AVE	LOVE WAY	1	1	10	Z	\$145				\$145
Arden Arcade	WATT AVE	GREENVIEW LN	1	1	10	Z	\$145				\$145
Arden Arcade	MARCONI AVE	WRIGHT ST	1	1	10	Z	\$145				\$145
Arden Arcade	DELROSE CT	GARFIELD AVE	1	1	10	Z	\$145				\$145
Arden Arcade	CHENU AVE	WATT AVE	1	1	10	Z	\$145				\$145
Arden Arcade	WATT AVE	KINGS WAY	1	1	10	Z	\$145				\$145
Arden Arcade	JULIE ANN CT	KENNETH AVE	1	1	10	Z	\$145				\$145
Arden Arcade	ELVYRA WAY	FULTON AVE	1	1	10	Z	\$290				\$290
Arden Arcade	KENTFIELD DR	WATT AVE	1	1	10	Z	\$145				\$145
Arden Arcade	ROSE CT	WALNUT AVE	1	1	10	Z	\$145				\$145
Arden Arcade	FULTON AVE	LOMA VISTA DR	1	1	10	Z	\$145				\$145
Arden Arcade	BUTANO DR	PARK TOWNE CIR	1	1	10	Z	\$290				\$290
Arden Arcade	LA MESA WAY	FULTON AVE	1	1	10	Z	\$290				\$290
Arden Arcade	WATT AVE	MARYAL DR	1	1	10	Z	\$290				\$290
Arden Arcade	COTTAGE WAY	FULTON AVE	1	1	10	Z	\$290				\$290
Arden Arcade	LANDON LN	COTTAGE WAY	1	1	10	Z	\$145				\$145
Arden Arcade	COTTAGE WAY	MORSE AVE	1	1	10	Z	\$145				\$145
Arden Arcade	RICHMOND ST	COTTAGE WAY	2	1	10	Z	\$145				\$145
Arden Arcade	TESLA WAY	FULTON AVE	1	1	10	Z	\$145				\$145
Arden Arcade	FULTON AVE	TOWER AVE	1	1	10	Z	\$145				\$145
Arden Arcade	WYDA WAY	HOWE AVE	1	1	10	Z	\$435				\$435
Arden Arcade	ALTA ARDEN EXPY	MORSE AVE	1	1	10	Z	\$145				\$145
Arden Arcade	WYDA WAY	WRIGHT ST	1	1	10	Z	\$145				\$145
Arden Arcade	FULTON AVE	ARMSTRONG DR	1	1	10	Z	\$290				\$290
Arden Arcade	HOWE AVE	ALTA ARDEN EXPY	1	1	10	Z	\$145				\$145
Arden Arcade	WATT AVE	WILLETT CT	1	1	10	Z	\$145				\$145
Arden Arcade	EASTERN AVE	ALLEY	1	1	10	Z	\$145				\$145
Arden Arcade	ARDEN WAY	FULTON AVE	1	1	10	Z	\$580				\$580
Arden Arcade	AVONDALE AVE	ARDEN WAY	1	1	10	Z	\$145				\$145
Arden Arcade	ARDEN WAY	WATT AVE	1	1	10	Z	\$290				\$290

Sacramento County Department of Transportation ADA Transition Plan

Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	ARDEN WAY	LAS PASAS WAY	1	1	10	Z	\$145				\$145
Arden Arcade	SAFARI CT	ARDEN WAY	1	1	10	Z	\$145				\$145
Arden Arcade	HURLEY WAY	RUSHDEN DR	1	1	10	Z	\$145				\$145
Arden Arcade	BELL ST	VILLANOVA CIR	2	1	10	Z	\$145				\$145
Arden Arcade	HOWE AVE	CADILLAC DR	1	1	10	Z	\$290				\$290
Arden Arcade	Fulton Ave	Carlsbad Ave	1	2	9	I	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Fulton Ave	Carson Way	1	2	2	I	\$50,080		\$6,080	\$8,200	\$64,360
Arden Arcade	Fulton Ave	Cottage Way	1	2	1	I	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Arden Arcade	Fulton Ave	Edison Ave	1	2	1	I	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Arden Arcade	Fulton Ave	El Camino Ave	1	2	1	I	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Arden Arcade	Fulton Ave	Elvyra Way	1	2	9	I	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Fulton Ave	Hernando Rd	1	2	9	I	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Fulton Ave	Juniper Ln	1	2	9	I	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Fulton Ave	La Mesa Way	1	2	9	I	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Fulton Ave	Loma Vista Dr	1	2	9	I	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Fulton Ave	Loma Vista Dr	1	2	9	I	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Fulton Ave	Marconi Ave	1	2	1	I	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Arden Arcade	Fulton Ave	Northrop Ave	1	2	5	A	\$0	\$11,840			\$11,840
Arden Arcade	Fulton Ave	Pope Ave	1	2	9	I	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Fulton Ave	Tesla Way	1	2	9	I	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Fulton Ave	Tioga Way	1	2	9	I	\$12,520		\$1,520	\$12,300	\$26,340
Arden Arcade	Fulton Ave	Tower Ave	1	2	9	I	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Fulton Ave	Valley Rd	1	2	9	I	\$12,520		\$1,520	\$12,300	\$26,340
N. Highlands	Garfield Ave	Greenback Ln	1	2	1	C	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Consumnes	Grant Line Rd	Calvine Rd	2	2	3	K	\$37,560	\$8,880	\$2,280	\$1,750	\$50,470
N. Highlands	Hackberry Ln	Winding Way	1	2	1	A	\$50,080	\$11,840	\$6,080	\$6,150	\$74,150
Orangevale	Hazel Ave	Cherry Ave	3	2	3	D	\$37,560	\$8,880		\$350	\$46,790
Orangevale	Hazel Ave	Oak Ave	1	2	1	D	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560
N. Highlands	I-80 ramps	Greenback Ln	1	2	9	C	\$50,080		\$6,080	\$12,300	\$68,460
Fair Oaks	Illinois Ave	Madison Ave	1	2	1	P	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Vineyard	Kingsbridge Dr	Calvine Rd	1	2	3	K	\$18,780		\$2,280	\$6,150	\$27,210
S. Sacto	Lincolnshire Dr	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$12,300	\$26,340
Arden Arcade	Marilona Dr	Marconi Ave	1	2	9	P	\$12,520		\$1,520	\$9,840	\$23,880

Sacramento County Department of Transportation ADA Transition Plan

Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs	
Arden Arcade	Maryal Dr	Avalon Dr	1	2	2	P	\$25,040		\$6,080	\$6,150	\$37,270	
Arden Arcade	Maryal Dr	El Camino Ave	1	2	9	P	\$25,040		\$3,040	\$9,840	\$37,920	
Antelope	N Loop Blvd	Diane Dr	1	2	3	N	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840	
Antelope	Palmerson Dr	N Loop Blvd	1	2	1	N	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840	
Antelope	Redwater Dr	N Loop Blvd	1	2	3	N	\$37,560	\$8,880	\$4,560	\$12,300	\$63,300	
S. Sacto	Sky Pkwy	65th St	1	2	3	E	\$37,560	\$8,880	\$4,560	\$12,300	\$63,300	
S. Sacto	Stockton Blvd	65th St	1	2	1	E	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200	
S. Sacto	Stockton Blvd	Florin Rd	1	2	1	E	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300	
Consumnes	Sunrise Blvd	Grant Line Rd	1	2	3	K	\$37,560	\$8,880	\$4,560	\$6,560	\$57,560	
Arden Arcade	Trinity River Dr	Coloma Rd	1	2	2	A	\$37,560	\$8,880	\$4,560	\$8,200	\$59,200	
S. Sacto	U.S. 99	Florin Rd	1	2	2	E	\$25,040		\$3,040	\$12,300	\$40,380	
Vineyard	Vineyard Rd	Calvine Rd	2	2	4	K	\$25,040		\$1,520	\$1,750	\$28,310	
Antelope	Walerga Rd	N Loop Blvd	1	2	1	N	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560	
N. Highlands	Watt Ave	A St	2	2	2	Q	\$50,080		\$3,040	\$1,750	\$54,870	
N. Highlands	Watt Ave	Airbase Dr	1	2	3	Q	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840	
Arden Arcade	Watt Ave	American River Dr	1	2	9	D	\$50,080		\$6,080	\$9,840	\$66,000	
Carmichael	Watt Ave	Auburn Blvd	1	2	1	A	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$200,000	\$200,000	\$25,000	\$100,000	\$525,000
Yearly Totals								\$2,856,245	\$584,800	\$325,960	\$940,800	\$4,707,805
FY 2005-2006												
N. Highlands	Andrea Blvd	Elkhorn Blvd	1	2	1	J	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840	
Carmichael	Annadale Ln	Auburn Blvd	1	2	4	S	\$12,520		\$1,520	\$8,200	\$22,240	
Arden Arcade	Annadale Ln	Edison Ave	1	2	4	S	\$25,040		\$3,040	\$6,560	\$34,640	
Arden Arcade	Annadale Ln	Merrily Way	1	2	4	S	\$25,040		\$3,040	\$6,560	\$34,640	
Antelope	Antelope North Rd	Olive Ave	1	2	3	N	\$25,040		\$3,040	\$1,750	\$29,830	
Antelope	Antelope North Rd	Poker Ln	3	2	3	N	\$25,040		\$0	\$350	\$25,390	
N. Highlands	Auburn Blvd	Manzanita Ave	1	2	2	B	\$25,040	\$5,920	\$3,040	\$8,200	\$42,200	
Carmichael	Auburn Blvd	Orange Grove Ave	1	2	2	B	\$37,560	\$8,880	\$4,560	\$8,200	\$59,200	
Arden Arcade	Avalon Dr	El Camino Ave	1	2	9	S	\$50,080		\$6,080	\$9,840	\$66,000	
Arden Arcade	Avalon Dr	Marconi Ave	1	2	9	S	\$25,040		\$3,040	\$9,840	\$37,920	
S. Sacto	Bacchini Ave	Florin Rd	1	2	9	F	\$12,520		\$1,520	\$6,150	\$20,190	
Arden Arcade	Barbarell Way	Marconi Ave	1	2	9	S	\$12,520		\$1,520	\$9,840	\$23,880	

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	Bell St	Northrop Ave	1	2	4	S	\$25,040		\$3,040	\$12,300	\$40,380
Carmichael	Bellue St	Moraga Dr	1	2	9	CR	\$12,520		\$1,520	\$6,150	\$20,190
N. Highlands	Bismarck Dr	Elkhorn Blvd	1	2	9	A	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Briggs Dr	Florin Rd	1	2	1	F	\$50,080	\$11,840	\$6,080	\$6,150	\$74,150
Arden Arcade	Burgundy Way	El Camino Ave	1	2	4	S	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Butano Dr	El Camino Ave	1	2	2	CR	\$50,080		\$6,080	\$8,200	\$64,360
N. Highlands	Butterball Way	Elkhorn Blvd	1	2	3	A	\$37,560		\$4,560	\$8,200	\$50,320
N. Highlands	Cantel Way	Elkhorn Blvd	1	2	3	A	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840
Arden Arcade	Carrisa Way	Marconi Ave	1	2	9	S	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Cathay Way	El Camino Ave	1	2	9	S	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Celia Ave	Florin Rd	2	2	9	F	\$12,520		\$760	\$1,750	\$15,030
S. Sacto	Chandler Dr	Florin Rd	1	2	9	F	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Crater Way	Elkhorn Blvd	1	2	9	J	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Devonshire Rd	Arden Way	1	2	4	S	\$25,040		\$3,040	\$9,840	\$37,920
Carmichael	Dewey Dr	Oakcrest Ave	1	2	2	CR	\$50,080		\$6,080	\$8,200	\$64,360
Carmichael	Dewey Dr	Pepperwood Way	1	2	9	CR	\$12,520		\$1,520	\$12,300	\$26,340
N. Highlands	College Oak Dr	Orange Grove Ave	1	2	1	B	\$50,080	\$11,840	\$6,080	\$1,750	\$69,750
N. Highlands	Dillingham Dr	Elkhorn Blvd	2	2	9	A	\$12,520		\$760	\$1,750	\$15,030
Vineyard	Elk Grove-Florin Rd	Florin Rd	1	2	1	F	\$50,080	\$11,840	\$6,080	\$1,750	\$69,750
Vineyard	N Kiefer Rd	Kiefer Blvd	2	2	2	S	\$50,080		\$3,040	\$1,750	\$54,870
N. Highlands	Harlequin Way	Elkhorn Blvd	2	2	9	CR	\$12,520		\$760	\$1,750	\$15,030
RL/Elverta	9th St	M St	2	2	2	CR	\$25,040		\$1,520	\$1,750	\$28,310
Arden Arcade	Ione St	El Camino Ave	1	2	9	S	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Ione St	Marconi Ave	1	2	9	S	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Kara Dr	Florin Rd	1	2	9	F	\$12,520		\$1,520	\$6,150	\$20,190
N. Highlands	Kimmel Dr	Elkhorn Blvd	1	2	3	A	\$37,560		\$4,560	\$9,840	\$51,960
S. Sacto	Kingsley St	Florin Rd	1	2	9	F	\$12,520		\$1,520	\$6,150	\$20,190
N. Highlands	Larchmont Dr	Elkhorn Blvd	1	2	3	A	\$37,560		\$4,560	\$9,840	\$51,960
S. Sacto	Lindale Dr	Florin Rd	1	2	9	F	\$25,040		\$3,040	\$8,200	\$36,280
S. Sacto	McComber St	Florin Rd	1	2	9	F	\$12,520		\$1,520	\$6,560	\$20,600
S. Sacto	McCurdy	Florin Rd	2	2	9	F	\$25,040		\$1,520	\$1,750	\$28,310
Carmichael	Miles Ln	Fair Oaks Blvd	1	2	4	S	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Morse Ave	Alta Arden Expy	1	2	2	S	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840

Sacramento County Department of Transportation ADA Transition Plan

Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	Munroe St	Fair Oaks Blvd	1	2	2	S	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Arden Arcade	Munroe St	Huntington Rd	1	2	4	S	\$25,040		\$3,040	\$9,840	\$37,920
Orangevale	Pershing Ave	Madison Ave	1	2	7	CR	\$12,520	\$2,960	\$1,520	\$12,300	\$29,300
N. Highlands	Plumber Way	Elkhorn Blvd	1	2	9	A	\$12,520		\$1,520	\$9,840	\$23,880
RL/Elverta	Rio Linda Blvd	Q St	1	2	2	CR	\$50,080		\$6,080	\$9,840	\$66,000
Antelope	Roseville Rd	Katella Way	1	2	9	CR	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Diablo Dr	Elkhorn Blvd	1	2	1	J	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
N. Highlands	Don Julio Blvd	Elkhorn Blvd	1	2	1	A	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
RL/Elverta	Dry Creek Rd	Ascot Ave	1	2	2	M	\$50,080		\$6,080	\$8,200	\$64,360
S. Sacto	Bowling Dr	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$12,300	\$26,340
Arden Arcade	Eastern Ave	Engle Rd	1	2	2	S	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560
Vineyard	Bradshaw Rd	Calvine Rd	1	2	2	K	\$50,080	\$11,840	\$6,080	\$6,150	\$74,150
N. Highlands	College Oak Dr	Myrtle Ave	1	2	5	A		\$11,840			\$11,840
S. Sacto	E Southgate Dr	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$12,300	\$26,340
S. Sacto	Fawn Way	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$12,300	\$26,340
S. Sacto	Franklin Blvd	41st Ave	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	42nd Ave	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	43rd Ave	1	2	9	H	\$12,520		\$1,520	\$8,200	\$22,240
S. Sacto	Franklin Blvd	44th Ave	1	2	9	H	\$12,520		\$1,520	\$8,200	\$22,240
S. Sacto	Franklin Blvd	45th Ave	1	2	9	H	\$12,520		\$1,520	\$8,200	\$22,240
S. Sacto	Franklin Blvd	46th Ave	1	2	9	H	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Eastern Ave	Hazelwood Ave	1	2	9	S	\$12,520		\$1,520	\$6,560	\$20,600
S. Sacto	Franklin Blvd	47th Ave	1	2	1	H	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
S. Sacto	Franklin Blvd	48th Ave	1	2	9	H	\$12,520		\$1,520	\$12,300	\$26,340
S. Sacto	Franklin Blvd	51st Ave	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	52nd Ave	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	Cuny Ave	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	Florin Rd	1	2	1	E	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
S. Sacto	Franklin Blvd	Green Tree Dr	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	Turnbridge Dr	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Franklin Blvd	Williamsborough Dr	1	2	9	H	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Eastern Ave	Lyle St	1	2	9	S	\$12,520		\$1,520	\$6,560	\$20,600
Arden Arcade	Eastern Ave	Marconi Ave	1	2	2	S	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs	
Arden Arcade	Eastern Ave	Ravenwood Ave	1	2	9	S	\$12,520		\$1,520	\$6,560	\$20,600	
Arden Arcade	Watson St	El Camino Ave	1	2	9	S	\$25,040		\$3,040	\$9,840	\$37,920	
Arden Arcade	Watt Ave	Barrington Rd	1	2	9	CR	\$12,520		\$1,520	\$9,840	\$23,880	
Arden Arcade	Watt Ave	Edison Ave	1	2	2	S	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840	
Arden Arcade	Watt Ave	El Camino Ave	1	2	1	B	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200	
Arden Arcade	Watt Ave	Robertson Ave	1	2	4	S	\$25,040		\$3,040	\$8,200	\$36,280	
Arden Arcade	Watt Ave	West Way	1	2	4	S	\$25,040		\$3,040	\$9,840	\$37,920	
N. Highlands	Weddigen Way	Elkhorn Blvd	1	2	9	A	\$25,040		\$3,040	\$8,200	\$36,280	
Carmichael	Wilmer St	Moraga Dr	1	2	9	CR	\$12,520		\$1,520	\$6,150	\$20,190	
Arden Arcade	Wright St	Cottage Way	1	2	9	CR	\$12,520		\$1,520	\$9,840	\$23,880	
N. Highlands	Garfield Ave	Greenback Ln	1	2	1	C	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200	
Cosumnes	Grant Line Rd	Calvine Rd	2	2	3	K	\$37,560	\$8,880	\$2,280	\$1,750	\$50,470	
N. Highlands	Hackberry Ln	Winding Way	1	2	1	A	\$50,080	\$11,840	\$6,080	\$6,150	\$74,150	
Orangevale	Hazel Ave	Cherry Ave	3	2	3	D	\$37,560	\$8,880		\$350	\$46,790	
Orangevale	Hazel Ave	Oak Ave	1	2	1	D	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560	
N. Highlands	I-80 ramps	Greenback Ln	1	2	9	C	\$50,080		\$6,080	\$12,300	\$68,460	
Fair Oaks	Illinois Ave	Madison Ave	1	2	1	P	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840	
S. Sacto	Edith St	Florin Rd	1	2	9	F	\$12,520		\$1,520	\$6,150	\$20,190	
Arden Arcade	Yorktown Ave	El Camino Ave	1	2	9	CR	\$12,520		\$1,520	\$8,200	\$22,240	
Arden Arcade	Fair Oaks Blvd	Grant Ave	1	2	2	B	\$37,560	\$8,880	\$4,560	\$12,300	\$63,300	
Arden Arcade	Fair Oaks Blvd	Landis Ave	1	2	8	B	\$6,260		\$760	\$12,300	\$19,320	
Vineyard	Kingsbridge Dr	Calvine Rd	1	2	3	K	\$18,780		\$2,280	\$6,150	\$27,210	
Cosumnes	Sunrise Blvd	Grant Line Rd	1	2	3	K	\$37,560	\$8,880	\$4,560	\$6,560	\$57,560	
Arden Arcade	Fairchild Dr	Fair Oaks Blvd	1	2	4	S	\$12,520		\$1,520	\$8,200	\$22,240	
Arden Arcade	Fulton Ave	Sierra Blvd	1	2	3	S	\$37,560	\$8,880	\$4,560	\$12,300	\$63,300	
S. Sacto	U.S. 99	Florin Rd	1	2	2	E	\$25,040		\$3,040	\$12,300	\$40,380	
Vineyard	Vineyard Rd	Calvine Rd	2	2	4	K	\$25,040		\$1,520	\$1,750	\$28,310	
S. Sacto	Gardner Ave	Florin Rd	1	2	9	F	\$25,040		\$3,040	\$1,750	\$29,830	
Arden Arcade	Garfield Ave	El Camino Ave	1	2	1	B	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840	
Cosumnes	Waterman Rd	Grant Line Rd	2	2	3	K	\$37,560	\$8,880	\$2,280	\$1,750	\$50,470	
Arden Arcade	Howe Ave	El Camino Ave	1	2	2	B	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							A	\$257,330	\$208,880	\$31,080	\$119,680	\$616,970
Yearly Totals								\$3,212,050	\$546,320	\$367,760	\$992,650	\$5,118,780

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
FY 2006-2007											
S. Sacto	Power Inn Rd	Florin Rd	1	2	1	F	\$50,080	\$11,840	\$6,080	\$6,150	\$74,150
S. Sacto	Power Inn Rd	Gerber Rd	1	2	2	S	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
S. Sacto	Power Inn Rd	Stevenson Ave	1	2	2	CR	\$50,080		\$6,080	\$9,840	\$66,000
S. Sacto	Reese Rd	Florin Rd	2	2	9	F	\$12,520		\$760	\$1,750	\$15,030
S. Sacto	Rimrock Dr	Florin Rd	1	2	9	F	\$25,040		\$3,040	\$9,840	\$37,920
Carmichael	Manzanita Ave	Bourbon Dr	1	2	9	CR	\$25,040		\$3,040	\$8,200	\$36,280
Carmichael	Manzanita Ave	Fair Oaks Blvd	1	2	7	CR	\$25,040	\$5,920	\$3,040	\$9,840	\$43,840
Carmichael	Samoa Way	Grant Ave	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
Arden Arcade	Montclair St	Whitney Ave	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320
Arden Arcade	Becerra Way	Whitney Ave	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Arden Arcade	Ronk Way	Whitney Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Eastern Ave	Whitney Ave	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Arden Arcade	Kirkland Way	Whitney Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Mission Ave	North Ave	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
Arden Arcade	Green Park Ln	North Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Carmichael	COYLE AVE	DEWEY DR	1	1	10	Z	\$145				\$145
Carmichael	MADISON AVE	TOOMBS ST	1	1	10	Z	\$145				\$145
Carmichael	SONORA WAY	MADISON AVE	1	1	10	Z	\$145				\$145
Carmichael	LEAVITT WAY	ELSINORE WAY	2	1	10	Z	\$145				\$145
Carmichael	EDGERLY WAY	MADISON AVE	1	1	10	Z	\$145				\$145
Carmichael	PALMTREE CT	DEWEY DR	1	1	10	Z	\$290				\$290
Carmichael	PALM AVE	DEWEY DR	1	1	10	Z	\$145				\$145
Carmichael	BOURBON DR	SCOTCH CT	1	1	10	Z	\$145				\$145
Carmichael	WINDING WAY	ZELINDA DR	1	1	10	Z	\$580				\$580
Carmichael	RAMPART DR	WINDING WAY	1	1	10	Z	\$145				\$145
Carmichael	NEW YORK AVE	FAIR OAKS BLVD	1	1	10	Z	\$435				\$435
Carmichael	FOUNTAINDALE WAY	CALIFORNIA AVE	1	1	10	Z	\$290				\$290
Carmichael	MURDOCK WAY	GROVER CT	2	1	10	Z	\$145				\$145
Carmichael	BARKER ELMS CT	CALIFORNIA AVE	1	1	10	Z	\$145				\$145
Carmichael	PANAMA AVE	PALM DR	1	1	10	Z	\$145				\$145
Arden Arcade	Norris Ave	Woodmark Ct	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Carmichael	Leafwood Dr	Stanley Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Carmichael	California Ave	Stanley Ave	1	2	2	Z	\$50,080		\$6,080	\$8,200	\$64,360
Carmichael	California Ave	Cole Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Clairidge Oak Ct	Robertson Ave	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Arden Arcade	Robertson Ave	Mayer Way	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Terry Way	Robertson Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Arden Arcade	Merrimac St	Robertson Ave	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
Arden Arcade	Birch St	Robertson Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Arden Arcade	Whitewood Dr	Robertson Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Whitewood Dr	Robertson Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Fair Oaks Blvd	Robertson Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	GARFIELD AVE	LOCUST AVE	1	1	10	Z	\$145				\$145
Arden Arcade	MILGRAY CT	GARFIELD AVE	1	1	10	Z	\$290				\$290
Arden Arcade	HACKBERRY LN	CYPRESS AVE	1	1	10	Z	\$145				\$145
Arden Arcade	CYPRESS AVE	MANZANITA AVE	1	1	10	Z	\$290				\$290
RL/Elverta	10th St	Arrowhead Ave	2	2	2	CR	\$25,040		\$1,520	\$1,750	\$28,310
RL/Elverta	10th St	E St	2	2	2	CR	\$25,040		\$1,520	\$1,750	\$28,310
RL/Elverta	10th St	G St	1	2	2	CR	\$50,080		\$6,080	\$6,560	\$62,720
RL/Elverta	10th St	Lomita Way	2	2	2	CR	\$25,040		\$1,520	\$1,750	\$28,310
RL/Elverta	10th St	Q St	1	2	2	CR	\$50,080		\$6,080	\$6,560	\$62,720
RL/Elverta	2nd St	M St	1	2	2	CR	\$50,080		\$6,080	\$8,200	\$64,360
RL/Elverta	2nd St	Q St	1	2	2	CR	\$50,080		\$6,080	\$6,560	\$62,720
Fair Oaks	San Juan Ave	Amy Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Fair Oaks	Leo's Ln	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
RL/Elverta	6th St	M St	1	2	2	CR	\$25,040		\$3,040	\$9,840	\$37,920
RL/Elverta	6th St	N St	1	2	2	CR	\$25,040		\$3,040	\$6,150	\$34,230
RL/Elverta	9th St	O St	2	2	2	CR	\$25,040		\$1,520	\$1,750	\$28,310
N. Highlands	Hillsdale Blvd	Tresler Ave	1	2	4	CR	\$25,040		\$3,040	\$1,750	\$29,830
Carmichael	Sunrise Blvd	Madison Ave	1	2	1	B	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
N. Highlands	Thomas Dr	Elkhorn Blvd	1	2	1	A	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
S. Sacto	Tokay Ave	Florin Rd	2	2	9	F	\$12,520		\$760	\$1,750	\$15,030
Arden Arcade	Verna Way	El Camino Ave	1	2	9	S	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Verna Way	Marconi Ave	1	2	9	S	\$12,520		\$1,520	\$9,840	\$23,880

Sacramento County Department of Transportation ADA Transition Plan

Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	Villa Vista Way	Marconi Ave	1	2	9	S	\$12,520		\$1,520	\$9,840	\$23,880
N. Highlands	Walerga Rd	Elkhorn Blvd	1	2	1	A	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Antelope	Walerga Rd	Elverta Rd	1	2	1	CR	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Arden Arcade	Walnut Ave	El Camino Ave	1	2	2	B	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
S. Sacto	Lincolnshire Dr	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$12,300	\$26,340
S. Sacto	Sky Pkwy	65th St	1	2	3	E	\$37,560	\$8,880	\$4,560	\$12,300	\$63,300
S. Sacto	Stockton Blvd	65th St	1	2	1	E	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
S. Sacto	Stockton Blvd	Florin Rd	1	2	1	E	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
S. Sacto	Florin Mall 1	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$1,750	\$15,790
S. Sacto	Mall Drive	Florin Rd	1	2	1	E	\$50,080	\$11,840	\$6,080	\$1,750	\$69,750
S. Sacto	Florin Mall 2	Florin Rd	1	2	9	E	\$12,520		\$1,520	\$1,750	\$15,790
S. Sacto	65th Expy	Florin Rd	1	2	3	E	\$37,560	\$8,880	\$4,560	\$1,750	\$52,750
S. Sacto	Franklin Blvd	49th Ave	2	2	9	H	\$12,520		\$760	\$1,750	\$15,030
S. Sacto	Franklin Blvd	La Grande Blvd	2	2	9	H	\$12,520		\$760	\$1,750	\$15,030
Arden Arcade	Rushden Dr	Hurley Way	1	2	2	CR	\$50,080		\$6,080	\$6,560	\$62,720
Vineyard	S Watt Ave	Alderson Ave	1	2	9	R	\$12,520		\$1,520	\$6,560	\$20,600
Vineyard	S Watt Ave	Canberra Dr	1	2	9	R	\$12,520		\$1,520	\$6,150	\$20,190
Vineyard	S Watt Ave	Frederic Dr	3	2	9	R	\$12,520		\$0	\$350	\$12,870
Vineyard	S Watt Ave	Jackson Rd	1	2	1	R	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560
Vineyard	S Watt Ave	Kiefer Blvd	1	2	1	R	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Fair Oaks	San Juan Ave	Sunset Ave	1	2	1	CR	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
N. Highlands	Schofield Way	Elkhorn Blvd	1	2	9	A	\$12,520		\$1,520	\$9,840	\$23,880
S. Sacto	Simon St	Florin Rd	1	2	9	F	\$12,520		\$1,520	\$6,150	\$20,190
N. Highlands	Sprig Dr	Elkhorn Blvd	1	2	3	A	\$37,560		\$4,560	\$8,200	\$50,320
S. Sacto	Stockton Blvd	Gerber Rd	1	2	3	CR	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840
S. Sacto	Strand St	Florin Rd	2	2	9	F	\$12,520		\$760	\$1,750	\$15,030
Fair Oaks	KENNETH AVE	KENNETH OAK WAY	1	1	10	Z	\$145				\$145
Fair Oaks	MADISON AVE	FAIR OAKS BLVD	1	1	10	Z	\$290				\$290
Fair Oaks	E CARRIAGE LN	FAIR OAKS BLVD	1	1	10	Z	\$290				\$290
Fair Oaks	SUNSET AVE	HAZEL AVE	1	1	10	Z	\$145				\$145
Fair Oaks	HALE RANCH LN	WINDING WAY	2	1	10	Z	\$145				\$145
Fair Oaks	HAZEL AVE	CURRAGH DOWNS DR	1	1	10	Z	\$290				\$290
N. Highlands	Watt Ave	A St	2	2	2	Q	\$50,080		\$3,040	\$1,750	\$54,870

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs	
N. Highlands	Watt Ave	Airbase Dr	1	2	3	Q	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840	
N. Highlands	Watt Ave	E St	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880	
N. Highlands	Watt Ave	Karen Ln	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880	
N. Highlands	Watt Ave	Karl Dr	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880	
N. Highlands	Watt Ave	Margaret Way	2	2	9	Q	\$12,520		\$760	\$1,750	\$15,030	
Arden Arcade	Rosyln Way	El Camino Ave	1	2	9	S	\$25,040		\$3,040	\$9,840	\$37,920	
S. Sacto	East Pkwy	Florin Rd	1	2	2	S	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300	
Arden Arcade	Eastern Ave	Alva Ct	1	2	9	S	\$12,520		\$1,520	\$6,150	\$20,190	
Arden Arcade	Eastern Ave	Annette St	1	2	3	S	\$37,560		\$4,560	\$6,560	\$48,680	
Arden Arcade	Eastern Ave	El Camino Ave	1	2	2	S	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840	
Vineyard	Bradshaw Rd	Elder Creek Rd	1	2	2	J	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560	
Vineyard	Bradshaw Rd	Florin Rd	2	2	2	J	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710	
Arden Arcade	Fair Oaks Blvd	North Ave	1	2	9	CR	\$12,520		\$1,520	\$9,840	\$23,880	
Consumnes	Fair Oaks Blvd	Sutter Ave	1	2	3	CR	\$37,560		\$4,560	\$9,840	\$51,960	
Fair Oaks	Fair Oaks Blvd	Winding Way	1	2	3	CR	\$37,560		\$4,560	\$6,560	\$48,680	
S. Sacto	Fletcher Farm Dr	Florin Rd	1	2	9	F	\$12,520		\$1,520	\$1,750	\$15,790	
S. Sacto	Florin-Perkins Rd	Florin Rd	1	2	1	F	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560	
S. Sacto	Franusich Ave	Florin Rd	1	2	9	F	\$12,520		\$1,520	\$1,750	\$15,790	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$688,280	\$268,080	\$72,880	\$154,240	\$1,183,480
Yearly Totals								\$3,535,685	\$546,320	\$399,680	\$764,440	\$5,246,125
FY 2007-2008												
N. Highlands	Gay Way	Georgia Dr	1	2	2	CR	\$25,040		\$6,080	\$6,150	\$37,270	
N. Highlands	Gay Way	N Haven Dr	1	2	2	CR	\$25,040		\$6,080	\$6,150	\$37,270	
N. Highlands	Hillsdale Blvd	Elkhorn Blvd	1	2	3	J	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840	
N. Highlands	Hillsdale Blvd	Madison Ave	1	2	1	CR	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840	
Cosumnes	Trib Crossing Dr	Silver Point Ln	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870	
Consumnes	Aerojet Rd	Folsom Blvd	2	2	2	G	\$50,080		\$3,040	\$1,750	\$54,870	
Vineyard	Bradshaw Rd	Gerber Rd	2	2	1	J	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710	
Arden Arcade	Fair Oaks Blvd	Angelina Ave	1	2	9	C	\$12,520		\$1,520	\$6,150	\$20,190	
Arden Arcade	Fair Oaks Blvd	Engle Rd	1	2	2	C	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840	
Arden Arcade	Fair Oaks Blvd	Landis Ave	1	2	9	C	\$12,520		\$1,520	\$12,300	\$26,340	
Arden Arcade	Fair Oaks Blvd	Lloyd Way	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880	

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	Fair Oaks Blvd	Robertson Ave	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Fair Oaks Blvd	Stanley Ave	1	2	2	C	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840
Fair Oaks	Hazel Ave	Bedford Ave	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Fair Oaks	Hazel Ave	Black Olive Ct	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Fair Oaks	Hazel Ave	Curragh Downs Dr	1	2	3	G	\$37,560	\$8,880	\$4,560	\$8,200	\$59,200
Consumnes	Hazel Ave	Folsom Blvd	1	2	3	G	\$37,560	\$8,880	\$4,560	\$1,750	\$52,750
Consumnes	Hazel Ave	Gold Country Blvd	1	2	9	C	\$25,040		\$3,040	\$6,560	\$34,640
Fair Oaks	Hazel Ave	Kalo Ct	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Fair Oaks	Hazel Ave	Kruitof Way	1	2	9	C	\$12,520		\$1,520	\$16,400	\$30,440
Fair Oaks	Hazel Ave	La Serena Dr	1	2	3	C	\$37,560	\$8,880	\$4,560	\$12,300	\$63,300
Arden Arcade	Bryan Way	Fair Oaks Blvd	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320
Fair Oaks	Hazel Ave	Madison Ave	1	2	1	C	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Fair Oaks	Hazel Ave	N Winding Way	1	2	9	C	\$12,520		\$1,520	\$6,560	\$20,600
Fair Oaks	Hazel Ave	Phoenix Ave	1	2	1	C	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Fair Oaks	Hazel Ave	Pomo Cir	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Fair Oaks	Hazel Ave	Quail Run Way	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Fair Oaks	Hazel Ave	Roediger Ln	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Fair Oaks	Hazel Ave	Sunset Ave	1	2	1	C	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Fair Oaks	Hazel Ave	Timm Ave	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Consumnes	Hazel Ave	Tributary Pt Dr	1	2	9	C	\$12,520		\$1,520	\$12,300	\$26,340
Fair Oaks	Hazel Ave	Van Gogh Cir	1	2	9	C	\$25,040		\$3,040	\$8,200	\$36,280
Fair Oaks	Hazel Ave	Van Gogh Cir	1	2	9	C	\$25,040		\$3,040	\$8,200	\$36,280
Fair Oaks	Hazel Ave	Vincent Ave	1	2	9	C	\$12,520		\$1,520	\$12,300	\$26,340
Arden Arcade	Fulton Ave	Valley Rd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	Heron Way	Marconi Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	Walnut Ave	Almond Way	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Walnut Ave	Von Way	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Fair Oaks	Hazel Ave	Winding Way	1	2	3	G	\$37,560	\$8,880	\$4,560	\$8,200	\$59,200
N. Natomas	Rio Linda Blvd	Elkhorn Blvd	2	2	1	L	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710
Arden Arcade	Watt Ave	Adams Rd	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Watt Ave	Arden Creek Rd	2	2	2	C	\$50,080		\$3,040	\$1,750	\$54,870
Arden Arcade	Watt Ave	Arden Way	1	2	3	C	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840
Arden Arcade	Watt Ave	Arden Way	1	2	1	C	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	Watt Ave	Ardenridge Dr	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Watt Ave	Ardenridge Ln	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Watt Ave	Barberry Ln	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Ben Lomond Dr	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Bodega Ct	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Watt Ave	Buena Vista Dr	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Watt Ave	Butano Dr	1	2	2	C	\$50,080		\$6,080	\$12,300	\$68,460
N. Highlands	Watt Ave	Channing Dr	1	2	9	N	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Watt Ave	Chenu Ave	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
N. Highlands	Watt Ave	Clara Way	1	2	9	N	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Watt Ave	Club Ln	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Cody Way	1	2	9	C	\$12,520		\$1,520	\$6,560	\$20,600
Arden Arcade	Watt Ave	Cosmos Ave	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Fulton Ave	Cottage Way	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
N. Highlands	Watt Ave	Don Julio Blvd	1	2	3	N	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840
Arden Arcade	Watt Ave	El Encino Way	1	2	9	C	\$12,520		\$1,520	\$12,300	\$26,340
Arden Arcade	Watt Ave	El Ricon Way	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
N. Highlands	Watt Ave	Elkhorn Blvd	1	2	1	N	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Antelope	Watt Ave	Elverta Rd	1	2	1	N	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
N. Highlands	Watt Ave	Grattan Way	1	2	9	N	\$12,520		\$1,520	\$12,300	\$26,340
Arden Arcade	Watt Ave	Greenview Ln	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Harmony Ln	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Humboldt Way	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Watt Ave	Hyde Way	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Kentfield Dr	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Watt Ave	Kings Way	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Fair Oaks Blvd	Carter Rd	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Watt Ave	La Brea Way	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
N. Highlands	Watt Ave	Larry Way	1	2	9	N	\$12,520		\$1,520	\$12,300	\$26,340
Arden Arcade	Watt Ave	Las Pajas Way	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Lerwick Rd	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Los Alamos Way	1	2	9	C	\$12,520		\$1,520	\$12,300	\$26,340
Arden Arcade	Watt Ave	Lynne Way	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	Watt Ave	Lynne Way	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Maplewood Ln	1	2	9	C	\$12,520		\$1,520	\$6,560	\$20,600
Arden Arcade	Watt Ave	Marconi Ave	1	2	1	C	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Arden Arcade	Watt Ave	Maryal Dr	1	2	9	C	\$12,520		\$1,520	\$6,560	\$20,600
Arden Arcade	Watt Ave	Mayfair Dr	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
N. Highlands	Watt Ave	Milton Way	1	2	9	N	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Wilhaggin Dr	Fair Oaks Blvd	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$1,750	\$69,750
Arden Arcade	Ashton Dr	Guildford Ct	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
Arden Arcade	Watt Ave	Northrop Ave	1	2	3	C	\$37,560	\$8,880	\$4,560	\$12,300	\$63,300
Arden Arcade	Watt Ave	Pope Ave	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Potter Ln	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	San Lucas Way	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	San Ysidro Way	1	2	1	C	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Arden Arcade	Watt Ave	Shady Ln	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Sierra View Ln	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	St. Mathews Dr	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Arden Arcade	Watt Ave	Tembrook Dr	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
N. Highlands	Watt Ave	Van Owen St	1	2	1	N	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Arden Arcade	Watt Ave	Wellington Dr	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Antelope	REDWATER DR	THORNBURY DR	1	1	10	Z	\$145				\$145
Antelope	FOBES DR	REDWATER DR	2	1	10	Z	\$145				\$145
Antelope	WINTER OAK WAY	PALMERSON DR	1	1	10	Z	\$145				\$145
Antelope	PALMERSON DR	MISTY PASS WAY	1	1	10	Z	\$145				\$145
Antelope	HIDDEN MEADOW WAY	FALCON VIEW DR	2	1	10	Z	\$145				\$145
Antelope	OLD DAIRY DR	PALMERSON DR	1	1	10	Z	\$145				\$145
Antelope	SUPPORO WAY	ALBERTVILLE WAY	1	1	10	Z	\$145				\$145
Antelope	MEADOW PASS WAY	MISTY PASS WAY	1	1	10	Z	\$290				\$290
Antelope	WALERGA RD	N LOOP BLVD	1	1	10	Z	\$145				\$145
Antelope	FALCON VIEW DR	LONESTAR WAY	2	1	10	Z	\$145				\$145
Antelope	BLACK EAGLE DR	NIGHT STAR CT	2	1	10	Z	\$145				\$145
Antelope	FALCON VIEW DR	FAN WOOD WAY	2	1	10	Z	\$290				\$290
Arden Arcade	Watt Ave	Wemberley Dr	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Antelope	FAN WOOD WAY	HILLSBROOK DR	1	1	10	Z	\$145				\$145

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Antelope	FALCON VIEW DR	FALCONWOOD WAY	1	1	10	Z	\$145				\$145
Antelope	KIRKCALDY WAY	OAKENSHIELD CIR	1	1	10	Z	\$290				\$290
Antelope	KIRKCALDY WAY	N LOOP BLVD	1	1	10	Z	\$290				\$290
Antelope	FAWNRIDGE CT	FOUNTAINARBOR WAY	1	1	10	Z	\$145				\$145
Antelope	FAWNRIDGE WAY	FALCON VIEW DR	1	1	10	Z	\$145				\$145
Antelope	AZTEC WAY	ELVERTA RD	1	1	10	Z	\$145				\$145
Antelope	ROSEVILLE RD	ADAGIO WAY	1	1	10	Z	\$145				\$145
Antelope	CRATER BUTTE WAY	CHIMANGO CT	1	1	10	Z	\$145				\$145
Antelope	LORAC VISTA DR	DON JULIO BLVD	1	1	10	Z	\$145				\$145
Antelope	ELVERTA RD	WALERGA RD	1	1	10	Z	\$145				\$145
Antelope	VALLEY QUAIL CT	WATT AVE	1	1	10	Z	\$145				\$145
Antelope	KINGLET WAY	DELANEY DR	1	1	10	Z	\$435				\$435
Antelope	BIG CLOUD WAY	HEATHSTON CT	1	1	10	Z	\$290				\$290
Antelope	DELANEY DR	GOLDFINCH WAY	1	1	10	Z	\$145				\$145
Antelope	SCOTLAND DR	WATT AVE	1	1	10	Z	\$145				\$145
Antelope	DAVIDSON DR	SCOTLAND DR	1	1	10	Z	\$145				\$145
Antelope	DAVIDSON DR	ORT WAY	1	1	10	Z	\$145				\$145
Antelope	ANTELOPE RD	BELERO DR	1	1	10	Z	\$145				\$145
Antelope	WATT AVE	BAINBRIDGE DR	1	1	10	Z	\$145				\$145
Arden Arcade	Watt Ave	Whitney Ave	1	2	1	C	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Arden Arcade	Watt Ave	William Way	1	2	9	C	\$12,520		\$1,520	\$9,840	\$23,880
Arden Arcade	Watt Ave	Windsor Dr	1	2	9	C	\$12,520		\$1,520	\$8,200	\$22,240
Fair Oaks	Hazel Ave	I-50 off-ramps	1	2	2	C	\$50,080		\$6,080	\$1,750	\$57,910
Fair Oaks	Hazel Ave	Overlook	1	2	9	C	\$12,520		\$1,520	\$1,750	\$15,790
N. Highlands	AUSPICIOUS WAY	LUXFORD CT	1	1	10	Z	\$145				\$145
N. Highlands	ANTELOPE RD	ROSEVILLE RD	1	1	10	Z	\$580				\$580
N. Highlands	MONOGRAM DR	PERCEPTIVE WAY	2	1	10	Z	\$145				\$145
N. Highlands	BUTTERBALL WAY	HOLWORTHY WAY	2	1	10	Z	\$145				\$145
N. Highlands	MONOGRAM DR	WOODVILLE LN	1	1	10	Z	\$145				\$145
Fair Oaks	Hazel Ave	Nimbus Rd	2	2	9	C	\$25,040		\$1,520	\$1,750	\$28,310
Fair Oaks	Hazel Ave	Cedar-Village	2	2	9	C	\$12,520		\$760	\$1,750	\$15,030
Fair Oaks	Hazel Ave	Amoruso Ave	2	2	9	C	\$12,520		\$760	\$1,750	\$15,030
Arden Arcade	Watt Ave	La Habra Way	2	2	9	C	\$12,520		\$760	\$1,750	\$15,030

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	Watt Ave	Winding Way	2	2	1	C	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710
Arden Arcade	Watt Ave	Yorktown Ave	1	2	9	C	\$12,520		\$1,520	\$1,750	\$15,790
Arden Arcade	Watt Ave	Bolivar St	2	2	1	N	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710
N. Highlands	DON JULIO BLVD	MONOGRAM DR	1	1	10	Z	\$145				\$145
N. Highlands	MONOGRAM DR	BESTOW WAY	2	1	10	Z	\$145				\$145
N. Highlands	ARUTAS DR	BLACKFIELD DR	1	1	10	Z	\$145				\$145
N. Highlands	GALBRATH DR	MCDERMOTT DR	1	1	10	Z	\$145				\$145
N. Highlands	GALBRATH DR	WALERGA RD	1	1	10	Z	\$145				\$145
N. Highlands	ROSEVILLE RD	STATIONERS WAY	1	1	10	Z	\$290				\$290
N. Highlands	MCDERMOTT DR	GOLDEN ASPEN DR	1	1	10	Z	\$145				\$145
N. Highlands	VISTA OAK WAY	ROCA WAY	1	1	10	Z	\$290				\$290
N. Highlands	ANDREA BLVD	ROCA WAY	2	1	10	Z	\$290				\$290
N. Highlands	SUNSET OAK CT	DIABLO DR	2	1	10	Z	\$290				\$290
N. Highlands	ROSEVILLE RD	GOLD RUN AVE	1	1	10	Z	\$290				\$290
N. Highlands	ELKHORN BLVD	LARCHMONT DR	1	1	10	Z	\$290				\$290
N. Highlands	PABLO DR	LANCELOT DR	1	1	10	Z	\$580				\$580
N. Highlands	LANCELOT DR	DIABLO DR	1	1	10	Z	\$290				\$290
N. Highlands	WATT AVE	ELKHORN BLVD	1	1	10	Z	\$580				\$580
N. Highlands	HILLSDALE BLVD	ELKHORN BLVD	1	1	10	Z	\$145				\$145
N. Highlands	BELL HILL DR	DIABLO DR	1	1	10	Z	\$290				\$290
N. Highlands	WATT AVE	VAN OWEN ST	1	1	10	Z	\$290				\$290
N. Highlands	CHANNING DR	WATT AVE	1	1	10	Z	\$145				\$145
N. Highlands	KEEMA AVE	GUTHRIE ST	1	1	10	Z	\$580				\$580
N. Highlands	WATT AVE	LARRY WAY	1	1	10	Z	\$290				\$290
N. Highlands	KEEMA AVE	WALERGA RD	1	1	10	Z	\$290				\$290
N. Highlands	WALERGA RD	PENWITH WAY	1	1	10	Z	\$145				\$145
N. Highlands	WATT AVE	I ST	1	1	10	Z	\$580				\$580
N. Highlands	GREENBACK LN	GARFIELD AVE	1	1	10	Z	\$435				\$435
N. Highlands	KIRKBY WY	WALERGA RD	1	1	10	Z	\$580				\$580
N. Highlands	ERNESTINE WAY	BRUCE WAY	1	1	10	Z	\$145				\$145
N. Highlands	VERNER AVE	GARFIELD AVE	1	1	10	Z	\$145				\$145
N. Highlands	MORAZAN ST	DON JULIO BLVD	1	1	10	Z	\$145				\$145
N. Highlands	DON JULIO BLVD	WATT AVE	1	1	10	Z	\$290				\$290

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs	
N. Highlands	WATT AVE	E ST	1	1	10	Z	\$435				\$435	
N. Highlands	WATT AVE	KARL DR	1	1	10	Z	\$435				\$435	
N. Highlands	WALERGA RD	WILLOWBROOK DR	2	1	10	Z	\$290				\$290	
N. Highlands	WALERGA RD	HILLSDALE BLVD	1	1	10	Z	\$290				\$290	
N. Highlands	HILLTOP DR	MANZANITA AVE	1	1	10	Z	\$290				\$290	
N. Highlands	CALCUTTA WAY	HAMILTON ST	1	1	10	Z	\$145				\$145	
N. Highlands	GLASSBORO WAY	CALCUTTA WAY	2	1	10	Z	\$145				\$145	
N. Highlands	GREENHOLME DR	HAMILTON ST	2	1	10	Z	\$145				\$145	
N. Highlands	N HAVEN DR	WATT AVE	1	1	10	Z	\$290				\$290	
N. Highlands	GLASSBORO WAY	GREENHOLME DR	2	1	10	Z	\$145				\$145	
N. Highlands	BUFFWOOD WAY	WALNUT AVE	2	1	10	Z	\$145				\$145	
N. Highlands	MADISON AVE	HACKBERRY LN	1	1	10	Z	\$290				\$290	
N. Highlands	PEACEKEEPER WAY	WATT AVE	1	1	10	Z	\$435				\$435	
N. Highlands	COLLEGE OAK DR	AMBER LN	2	1	10	Z	\$290				\$290	
N. Highlands	WATT AVE	ROSEVILLE RD	1	1	10	Z	\$290				\$290	
N. Highlands	LAVELLE WAY	CYCLAMEN WAY	2	1	10	Z	\$290				\$290	
N. Highlands	COLLINA PL	WALNUT AVE	2	1	10	Z	\$290				\$290	
N. Highlands	MIEKO WAY	MIEKO WAY	2	1	10	Z	\$145				\$145	
N. Highlands	TYLER ST	MIEKO WAY	1	1	10	Z	\$290				\$290	
N. Highlands	SHELL ST	JONKO AVE	2	1	10	Z	\$145				\$145	
N. Highlands	MYRTLE AVE	ROSEVILLE RD	1	1	10	Z	\$435				\$435	
N. Highlands	BRITTNEY LEE CT	MYRTLE AVE	1	1	10	Z	\$145				\$145	
N. Highlands	MYRTLE AVE	AUBURN BLVD	1	1	10	Z	\$145				\$145	
N. Highlands	ROSEVILLE RD	DUDLEY LOOP	2	1	10	Z	\$290				\$290	
N. Highlands	PONDERAY LN	PASADENA AVE	2	1	10	Z	\$145				\$145	
N. Highlands	PASADENA AVE	CREEK RD	1	1	10	Z	\$145				\$145	
N. Highlands	HACKBERRY LN	REGAN HALL LN	1	1	10	Z	\$145				\$145	
N. Highlands	WATT AVE	I 80	1	1	10	Z	\$145				\$145	
N. Highlands	ROSEVILLE RD	LONGVIEW DR	1	1	10	Z	\$145				\$145	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$487,960	\$235,520	\$58,440	\$136,850	\$918,770
Yearly Totals								\$3,126,970	\$525,600	\$360,160	\$1,022,490	\$5,035,220

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
FY 2008-2009											
RL/Elverta	Elwyn Ave	Artesia Rd	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Walerga Rd	Keema Ave	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$6,150	\$57,150
Orangevale	Lake Natoma Dr	Madison Ave	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$6,560	\$57,560
N. Highlands	Watt Ave	Palm St	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$12,300	\$63,300
Fair Oaks	Kenneth Ave	Hans Engel Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Winters St	Bell Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
S. Sacto	Farmington Way	Lawnwood Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
S. Sacto	Franklin Blvd	Turnbridge Dr	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
S. Sacto	Chandler Dr	Sunnyslope Dr	1	2	2	Z	\$25,040		\$6,080	\$1,750	\$32,870
S. Sacto	Lindale Dr	Sunnyslope Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
S. Sacto	Pritchard Rd	Florin Rd	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
S. Sacto	Franusich Ave	Florin Rd	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
S. Sacto	Palmer House Dr	Conrad Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Channing Dr	David Dr	2	2	2	CR	\$50,080		\$3,040	\$1,750	\$54,870
S. Sacto	Chris Ave	Florin Rd	1	2	9	F	\$12,520		\$1,520	\$1,750	\$15,790
N. Highlands	College Oak Dr	Winding Way	1	2	2	CR	\$50,080		\$6,080	\$9,840	\$66,000
S. Sacto	Kara Dr	Diana Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	Circle Pkwy	East Pkwy	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
S. Sacto	Carmi St	Samantha Way	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
S. Sacto	Palmer House Dr	Nomad Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	A Pkwy	East Pkwy	1	2	2	Z	\$50,080		\$6,080	\$1,750	\$57,910
S. Sacto	Palmer House Dr	Blackhawk Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Rimrock Dr	Lindale Dr	1	2	2	Z	\$50,080		\$6,080	\$12,300	\$68,460
S. Sacto	A Pkwy	Center Pkwy	1	2	2	Z	\$50,080		\$6,080	\$1,750	\$57,910
S. Sacto	Lindale Dr	Chandler Dr	1	2	2	Z	\$50,080		\$6,080	\$8,200	\$64,360
S. Sacto	Ewing Way	Lindale Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Lindale Dr	Flamingo Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
S. Sacto	Lindale Dr	Bama Ct	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Sahara Ct	Lindale Dr	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
S. Sacto	Lindale Dr	Fort Pitt Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
S. Sacto	Del Prado Way	Lindale Dr	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
S. Sacto	Center Pkwy	C Pkwy	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480

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S. Sacto	Center Pkwy	F Pkwy	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
S. Sacto	Stockton Blvd	Stacy Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
S. Sacto	Stockton Blvd	Massie Ct	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
S. Sacto	Power Inn Rd	Stevenson Ave	1	2	2	Z	\$50,080		\$6,080	\$12,300	\$68,460
Vineyard	Vineyard Rd	Caprilli Dr	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
S. Sacto	Short Rd	Calvine Rd	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
N. Natomas	El Centro Rd	Elverta Rd	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$1,750	\$69,750
N. Natomas	El Centro Rd	Elverta Rd	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$1,750	\$69,750
Orangevale	Shumway Dr	Buffalo Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Orangevale	Bullion Way	Buffalo Ave	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
Orangevale	Smokewood Ct	Winding Oak Dr	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Orangevale	Main Ave	Lakefair Ct	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Fair Oaks	Almond Ave	Greenback Ln	1	2	2	O	\$37,560		\$4,560	\$9,840	\$51,960
Orangevale	Beech Ave	Greenback Ln	1	2	2	O	\$37,560		\$4,560	\$12,300	\$54,420
Fair Oaks	Birks Ln	Greenback Ln	1	2	9	O	\$12,520		\$1,520	\$9,840	\$23,880
Antelope	Don Julio Blvd	Antelope Rd	1	2	1	N	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Antelope	Don Julio Blvd	N Loop Blvd	1	2	4	N	\$37,560		\$4,560	\$6,560	\$48,680
Antelope	Don Julio Blvd	Poker Ln	1	2	3	N	\$37,560	\$8,880	\$4,560	\$12,300	\$63,300
Fair Oaks	Fair Oaks Blvd	Greenback Ln	1	2	1	O	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Orangevale	Hazel Ave	Greenback Ln	1	2	1	O	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Fair Oaks	Hoffman Ln	Greenback Ln	2	2	9	O	\$12,520		\$760	\$1,750	\$15,030
Fair Oaks	Kenneth Ave	Greenback Ln	1	2	1	O	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Fair Oaks	Kifisia Way	Greenback Ln	1	2	9	O	\$12,520		\$1,520	\$8,200	\$22,240
Fair Oaks	Stratus Dr	Greenback Ln	1	2	9	O	\$12,520		\$1,520	\$9,840	\$23,880
Fair Oaks	Trajan Dr	Greenback Ln	1	2	2	O	\$37,560		\$4,560	\$12,300	\$54,420
Fair Oaks	Wittenham Way	Greenback Ln	1	2	9	O	\$12,520		\$1,520	\$8,200	\$22,240
Orangevale	Hickory Ave	Greenback Ln	2	2	1	O	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710
Vineyard	Bradshaw Rd	Hwy 50	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$1,750	\$52,750
Vineyard	Bradshaw Rd	Hwy 50	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$6,560	\$57,560
S. Sacto	46th Ave	44th St	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	54th St	47th Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
S. Sacto	Vista Ave	47th Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Burns Way	47th Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
S. Sacto	Leola Way	47th Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Laurine Way	47th Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
S. Sacto	Welty Way	47th Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
S. Sacto	Steiner Dr	48th Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Orangevale	Mellowstoffer	Greenback Ln	2	2	9	O	\$12,520		\$760	\$1,750	\$15,030
S. Sacto	Steiner Dr	Sitton Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Steiner Dr	50th Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Steiner Dr	51st St	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Steiner Dr	Frawley Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Steiner Dr	53rd St	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	42nd St	Elko Ct	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480
S. Sacto	Burdett Way	53rd Ave	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
S. Sacto	Wire Dr	53rd Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
S. Sacto	Prentiss Dr	53rd Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
S. Sacto	42nd St	Cuny Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
S. Sacto	Dawson Way	53rd Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
S. Sacto	Bowling Dr	Greenwich Cir	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
S. Sacto	Marburn Ct	47th St	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
S. Sacto	Burdett Way	53rd Ave	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320
S. Sacto	Steiner Dr	53rd Ave	1	2	2	Z	\$50,080		\$6,080	\$12,300	\$68,460
S. Sacto	Bowling Dr	Greenwich Cir	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	Palmer House Dr	Lindale Dr	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
N. Natomas	ANGEL WAY	W 2ND ST	1	1	10	Z	\$290				\$290
N. Natomas	RIO LINDA BLVD	L ST	1	1	10	Z	\$145				\$145
N. Natomas	2ND ST	EVCAR WAY	1	1	10	Z	\$145				\$145
N. Natomas	EVCAR WAY	RINETTI WAY	1	1	10	Z	\$290				\$290
N. Natomas	RIO LINDA BLVD	MONTAGUE WAY	1	1	10	Z	\$145				\$145
N. Natomas	STRIKER AVE	NATIONAL DR	1	1	10	Z	\$290				\$290
N. Natomas	SPORTS DR	GATEWAY PARK BLVD	2	1	10	Z	\$145				\$145
N. Natomas	N MARKET BLVD	NATIONAL DR	1	1	10	Z	\$145				\$145
N. Natomas	N MARKET BLVD	GATEWAY PARK BLVD	1	1	10	Z	\$145				\$145
Orangevale	W RANCH DR	HAZEL AVE	1	1	10	Z	\$145				\$145
Orangevale	ALMOND AVE	WHITE OWL CT	1	1	10	Z	\$290				\$290

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Orangevale	MENKE WAY	MENKE WAY	2	1	10	Z	\$145				\$145
Orangevale	ALMOND AVE	PHEASANT CLUB CT	1	1	10	Z	\$145				\$145
Orangevale	FAIR OAKS BLVD	LINDEN LIME CT	1	1	10	Z	\$145				\$145
Orangevale	CENTRAL PARK CT	CENTRAL AVE	1	1	10	Z	\$145				\$145
Orangevale	PECAN AVE	AUGUSTINE CT	2	1	10	Z	\$145				\$145
Orangevale	GREEN EYES WAY	GREEN TOP WAY	2	1	10	Z	\$145				\$145
Orangevale	BEECH AVE	GREENBACK LN	1	1	10	Z	\$435				\$435
Orangevale	GREENBACK LN	HAZEL AVE	1	1	10	Z	\$290				\$290
Orangevale	SHERRY DR	ROLOFF WAY	2	1	10	Z	\$145				\$145
Orangevale	SUEDE HILL CT	GARDEN TOWNE WAY	1	1	10	Z	\$290				\$290
Orangevale	ROCK CANYON WAY	AIRHILL WAY	1	1	10	Z	\$145				\$145
Orangevale	CHESTNUT AVE	HILARI WAY	2	1	10	Z	\$145				\$145
Orangevale	VILLAGE PARK CT	ILLINOIS AVE	1	1	10	Z	\$145				\$145
Orangevale	PHOENIX EAST CT	PHOENIX AVE	2	1	10	Z	\$435				\$435
Orangevale	RUNWAY DR	PHOENIX AVE	2	1	10	Z	\$145				\$145
Orangevale	SKYWAY DR	CEDARVILLAGE DR	1	1	10	Z	\$290				\$290
Orangevale	RUNWAY DR	GRUMMAN WAY	2	1	10	Z	\$435				\$435
Orangevale	HANGAR WAY	RUNWAY DR	2	1	10	Z	\$290				\$290
Orangevale	ERLE BLUNDEN WAY	RUNWAY DR	2	1	10	Z	\$145				\$145
Orangevale	SUNSET AVE	RUNWAY DR	1	1	10	Z	\$145				\$145
RL/Elverta	ELVERTA RD	DUTCH HAVEN BLVD	1	1	10	Z	\$145				\$145
RL/Elverta	Q ST	10TH ST	1	1	10	Z	\$145				\$145
RL/Elverta	10TH ST	TAKAMI CT	1	1	10	Z	\$290				\$290
RL/Elverta	9TH AVE	ANDERSON WOOD WAY	2	1	10	Z	\$435				\$435
RL/Elverta	ANDERSON WOOD WAY	10TH ST	1	1	10	Z	\$290				\$290
RL/Elverta	6TH ST	O ST	1	1	10	Z	\$145				\$145
RL/Elverta	2ND ST	SHADY WOODS WAY	2	1	10	Z	\$145				\$145
RL/Elverta	N ST	6TH ST	1	1	10	Z	\$145				\$145
RL/Elverta	OAK LN	CURVED BRIDGE RD	1	1	10	Z	\$145				\$145
RL/Elverta	OAK LN	9TH AVE	1	1	10	Z	\$145				\$145
RL/Elverta	W M ST	W 2ND ST	2	1	10	Z	\$145				\$145
RL/Elverta	W M ST	SUN ACER WAY	2	1	10	Z	\$290				\$290
RL/Elverta	M ST	2ND ST	2	1	10	Z	\$145				\$145

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RL/Elverta	6TH ST	M ST	1	1	10	Z	\$145				\$145	
RL/Elverta	7TH AVE	M ST	1	1	10	Z	\$145				\$145	
RL/Elverta	L ST	4TH ST	1	1	10	Z	\$145				\$145	
RL/Elverta	JAMIE CT	6TH ST	1	1	10	Z	\$145				\$145	
RL/Elverta	DRY CREEK RD	I ST	1	1	10	Z	\$290				\$290	
RL/Elverta	G ST	DRY CREEK RD	1	1	10	Z	\$145				\$145	
RL/Elverta	E ST	HAYER CIR	2	1	10	Z	\$145				\$145	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$462,920	\$200,000	\$63,000	\$146,550	\$872,470
Yearly Totals								\$3,152,495	\$336,160	\$383,720	\$803,600	\$4,675,975

FY 2009-2010

Fair Oaks	California Ave	Fair Oaks Blvd	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Orangevale	Amoruso Ave	Madison Ave	1	2	9	A	\$12,520		\$1,520	\$8,200	\$22,240
Orangevale	Blue Oak Dr	Madison Ave	2	2	9	A	\$12,520		\$760	\$1,750	\$15,030
Fair Oaks	Buena Ventura Way	Madison Ave	1	2	9	A	\$12,520		\$1,520	\$9,840	\$23,880
Fair Oaks	Buena Vista Ave	Madison Ave	1	2	9	A	\$12,520		\$1,520	\$9,840	\$23,880
Vineyard	Elk Grove-Florin Rd	Gerber Rd	2	2	2	K	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710
Fair Oaks	Fair Oaks Blvd	Dorian Way	1	2	9	A	\$12,520		\$1,520	\$8,200	\$22,240
Fair Oaks	Fair Oaks Blvd	E Carriage Ln	1	2	9	A	\$12,520		\$1,520	\$12,300	\$26,340
Fair Oaks	Fair Oaks Blvd	Kaula Dr	1	2	9	A	\$12,520		\$1,520	\$9,840	\$23,880
Fair Oaks	Fair Oaks Blvd	Madison Ave	1	2	1	A	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Fair Oaks	Fair Oaks Blvd	McMillan Dr	1	2	9	A	\$12,520		\$1,520	\$9,840	\$23,880
Fair Oaks	Fair Oaks Blvd	Oahu Dr	1	2	9	A	\$12,520		\$1,520	\$9,840	\$23,880
Antelope	Watt Ave	Tolman Ln	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
Antelope	Choctaw Ct	Navaho Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Antelope	Chippewa Ct	Navaho Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Antelope	Mohican Way	Navaho Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Fair Oaks	Fair Oaks Blvd	Orange Tree Ct	1	2	9	A	\$12,520		\$1,520	\$8,200	\$22,240
Fair Oaks	Fair Oaks Blvd	Sunset Ave	1	2	1	A	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Fair Oaks	Fair Oaks Blvd	Westcamp Rd	1	2	9	A	\$12,520		\$1,520	\$12,300	\$26,340
Fair Oaks	Fair Oaks Blvd	Woodleaf Dr	1	2	9	A	\$12,520		\$1,520	\$9,840	\$23,880
Orangevale	Flyway Dr	Madison Ave	1	2	9	A	\$12,520		\$1,520	\$8,200	\$22,240
Fair Oaks	Greenbrier Way	Madison Ave	1	2	9	A	\$12,520		\$1,520	\$9,840	\$23,880

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Fair Oaks	Kenneth Ave	Madison Ave	1	2	1	A	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Vineyard	S Watt Ave	Alder Ave	1	2	2	R	\$50,080		\$6,080	\$6,560	\$62,720
Vineyard	S Watt Ave	Elder Creek Rd	3	2	1	R	\$50,080	\$11,840	\$0	\$350	\$62,270
Antelope	Aztec Way	Navaho Dr	1	2	2	Z	\$50,080		\$6,080	\$12,300	\$68,460
Antelope	Aztec Way	Navaho Dr	1	2	2	Z	\$50,080		\$6,080	\$12,300	\$68,460
Antelope	Teton Way	Navaho Dr	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Antelope	Teton Way	Navaho Dr	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Antelope	Mohican Way	Navaho Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Antelope	Seneca Way	Navaho Dr	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Antelope	Blackfoot Way	Pima Way	1	2	2	Z	\$50,080		\$6,080	\$1,750	\$57,910
Antelope	Blackfoot Way	Navaho Dr	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Antelope	Seneca Way	Navaho Dr	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Vineyard	S Watt Ave	Osage Ave	1	2	2	R	\$50,080		\$6,080	\$6,560	\$62,720
Fair Oaks	Shire Ct	Madison Ave	1	2	9	A	\$12,520		\$1,520	\$9,840	\$23,880
Orangevale	Walnut Ave	Madison Ave	1	2	9	A	\$25,040		\$3,040	\$8,200	\$36,280
N. Highlands	Walnut Ave	Madison Ave	1	2	9	A	\$25,040		\$3,040	\$8,200	\$36,280
Antelope	Blackfoot Way	Pima Way	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
Antelope	Delaney Dr	Davidson Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Antelope	Sullivan Dr	Davidson Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Antelope	34th St	U St	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
Antelope	Component Way	Antelope Rd	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Antelope	La Tour Dr	Antelope Rd	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320
N. Highlands	Monument Dr	Auspicious Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Danfield Cir	Bainbridge Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Danfield Cir	Bainbridge Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Worthington Dr	Bainbridge Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Arden Arcade	Fair Oaks Blvd	Captain Ct	1	2	9	A	\$12,520		\$1,520	\$1,750	\$15,790
Orangevale	McKay St	Madison Ave	2	2	1	A	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710
Carmichael	Valoma St	Madison Ave	2	2	9	A	\$12,520		\$760	\$1,750	\$15,030
N. Highlands	Worthington Dr	Bainbridge Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Holbrook Way	Bainbridge Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Wrigley Cir	Bainbridge Dr	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
N. Highlands	Bainbridge Dr	Wrigley Cir	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920

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N. Highlands	Arutas Dr	Bainbridge Dr	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310
N. Highlands	Larchmont Dr	Bainbridge Dr	1	2	2	Z	\$50,080		\$6,080	\$12,300	\$68,460
N. Highlands	Salazar Dr	Bainbridge Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
N. Highlands	Wrigley Cir	Bainbridge Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Fair Oaks	Winding Oak Dr	Madison Ave	2	2	2	A	\$50,080		\$3,040	\$1,750	\$54,870
Carmichael	Greenback Ln	Madison Ave	2	2	3	A	\$37,560	\$8,880	\$2,280	\$1,750	\$50,470
RL/Elverta	U.S. 99	Elverta Rd	2	2	9	A	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710
RL/Elverta	Rio Linda Blvd	Elverta Rd	2	2	2	A	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710
N. Highlands	Bainbridge Dr	Wrigley Cir	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Meath Way	Bainbridge Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Dutch Flat Dr	Bainbridge Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Worthington Dr	Bainbridge Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
N. Highlands	Worthington Dr	Bainbridge Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Walerga Rd	Bainbridge Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Bainbridge Dr	Rutherford Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Monument Dr	Auspicious Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Winthrop Ct	Bainbridge Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Vineyard	S Watt Ave	Tokay Ln	3	2	9	R	\$12,520		\$0	\$350	\$12,870
Vineyard	S Watt Ave	43rd Ave	3	2	9	R	\$12,520		\$0	\$350	\$12,870
Vineyard	S Watt Ave	Wayne Ct	3	2	9	R	\$12,520		\$0	\$350	\$12,870
Vineyard	S Watt Ave	Fruitridge Rd	3	2	1	R	\$50,080	\$11,840	\$0	\$350	\$62,270
N. Highlands	Quinn Way	Bainbridge Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Larchmont Dr	Delhaven Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Larchmont Dr	Galbrath Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Larchmont Dr	Ramsey Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Larchmont Dr	Turner Dr	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
N. Highlands	Jonothan Way	Turner Dr	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Antelope	Watt Ave	Turner Dr	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
N. Highlands	Cimarron Way	Silverthorne Cir	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
N. Highlands	La Cienega Dr	Melrose Dr	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
N. Highlands	Diablo Dr	Olympic Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	Craighurst Dr	Larchmont Dr	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
N. Highlands	Longdale Dr	Keema Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230

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N. Highlands	Guthrie St	Keema Ave	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310	
N. Highlands	Pinebrook Way	Silverthorne Cir	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830	
N. Highlands	Larchmont Dr	Colette Way	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270	
N. Highlands	Silverthorne Cir	Klamath Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230	
N. Highlands	Larry Way	Thomas Dr	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680	
N. Highlands	La Cienega Dr	Larchmont Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230	
N. Highlands	Cimarron Way	Silverthorne Cir	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270	
N. Highlands	Watt Ave	I St	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200	
N. Highlands	Greenback Ln	I 80	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300	
N. Highlands	McCloud Dr	Jack London Cir	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720	
Fair Oaks	Fair Oaks Blvd	Greenback Ln	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200	
N. Highlands	Brinef Dr	Garfield Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830	
N. Highlands	Santa Fe Way	San Ardo Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230	
N. Highlands	Santa Fe Way	San Ardo Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Arden Arcade	Eastern Ave	Cottage Way	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$6,150	\$57,150	
N. Highlands	MYRTLE AVE	COLLEGE OAK DR	1	1	10	Z	\$145			\$6,560	\$6,705	
Arden Arcade	SAN JUAN AVE	WINDING WAY	1	1	10	Z	\$145			\$6,560	\$6,705	
Arden Arcade	SAN JUAN AVE	WALNUT RD	1	1	10	Z	\$290			\$6,560	\$6,850	
Arden Arcade	WATT AVE	AUBURN BLVD	1	1	10	Z	\$290			\$6,560	\$6,850	
Arden Arcade	FAIR OAKS BLVD	STANLEY AVE	1	1	10	Z	\$145			\$6,560	\$6,705	
Carmichael	FAIR OAKS BLVD	PALM DR	1	1	10	Z	\$145			\$6,560	\$6,705	
Arden Arcade	EL CAMINO AVE	FALLWATER LN	1	1	10	Z	\$435			\$6,560	\$6,995	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$200,000	\$200,000	\$25,000	\$100,000	\$525,000
Yearly Totals								\$3,406,715	\$371,680	\$378,400	\$860,670	\$5,017,465

FY 2010-2011

Antelope	Navaho Dr	Mohican Way	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
Antelope	Navaho Dr	Arapaho Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	San Juan Ave	San Nita Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	San Juan Ave	Julep Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Walnut Ave	Laurelview Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Bonnie Jean Way	Winding Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	Pasadena Ave	Winding Way	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200

Sacramento County Department of Transportation ADA Transition Plan

Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	Norris Ave	Auburn Blvd	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	San Juan Ave	Oak Knoll Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Walnut Ave	Hope Ln	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Hillsdale Blvd	Oberon Ave	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
Fair Oaks	Hazel Ave	Vincent Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Fair Oaks	Illinois Ave	Great Oak Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Fair Oaks	Fair Oaks Blvd	Temple Park Rd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	Field St	Poplar Blvd	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Fair Oaks	Hazel Ave	Phoenix Ave	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320
N. Highlands	Garfield Ave	Madison Ave	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
N. Highlands	Hackberry Ln	Madison Ave	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Fair Oaks	San Juan Ave	Gail Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Fair Oaks	Cocoa Palm Way	Kaula Dr	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
Fair Oaks	Hazel Ave	Bedford Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Jackson St	Woodhue Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Watt Ave	Roseville Rd	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Fair Oaks	San Juan Ave	Pheasant Rd	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
N. Highlands	Date Ave	Judy Ct	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Carmichael	Heritage Dr	St James Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Carmichael	Brookglen Way	St James Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Fair Oaks	San Juan Ave	Cardinal Rd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Carmichael	Crestview Dr	St James Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
N. Highlands	Tyler St	Ashcroft Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	JAMES WAY	ARNOLD AVE	2	1	10	Z	\$145				\$145
N. Highlands	A ST	WATT AVE	1	1	10	Z	\$580				\$580
N. Highlands	GEORGIA DR	A ST	1	1	10	Z	\$580				\$580
N. Highlands	ROSEVILLE RD	A ST	1	1	10	Z	\$145				\$145
N. Highlands	PALM AVE	HARRISON ST	2	1	10	Z	\$145				\$145
N. Highlands	PALM AVE	HAMILTON ST	2	1	10	Z	\$145				\$145
N. Highlands	HILLSDALE BLVD	PALM AVE	1	1	10	Z	\$290				\$290
N. Highlands	WATT AVE	PALM ST	1	1	10	Z	\$290				\$290
N. Highlands	TYLER ST	TERRACE DOWNS WAY	2	1	10	Z	\$145				\$145
N. Highlands	GARFIELD AVE	YELLOW PINE WAY	1	1	10	Z	\$290				\$290

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
N. Highlands	WINGS WAY	WATT AVE	1	1	10	Z	\$290				\$290
N. Highlands	MANZANITA AVE	SHADOW CREEK DR	1	1	10	Z	\$290				\$290
N. Highlands	COLLEGE OAK DR	SUMMERWOOD CIR	2	1	10	Z	\$145				\$145
N. Highlands	ROCKWELL DR	N AIRWAY DR	1	1	10	Z	\$290				\$290
N. Highlands	COLLEGE OAK DR	CHIPPENDALE DR	2	1	10	Z	\$145				\$145
N. Highlands	RENICK WAY	MADISON AVE	1	1	10	Z	\$145				\$145
N. Highlands	MADISON AVE	JACKSON ST	1	1	10	Z	\$290				\$290
N. Highlands	GARFIELD AVE	MADISON AVE	1	1	10	Z	\$290				\$290
Carmichael	Perth Way	Palm Dr	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480
Carmichael	California Ave	Mauer Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Bryce St	Marconi Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Morse Ave	Marconi Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Morse Ave	Marconi Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Ashbourne Dr	Marconi Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Darwin St	Helena Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Arden Arcade	La Paz Way	Marconi Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	Kino St	Marconi Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	Carrisa Way	Marconi Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Vineyard	Bradshaw Rd	Lincoln Village Dr	1	2	3	CR	\$37,560	\$8,880	\$4,560	\$9,840	\$60,840
Vineyard	Bradshaw Rd	Calvine Rd	1	2	2	K	\$50,080	\$11,840	\$6,080	\$6,150	\$74,150
Vineyard	Bradshaw Rd	Elder Creek Rd	1	2	2	J	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560
Vineyard	Bradshaw Rd	Florin Rd	2	2	2	J	\$50,080	\$11,840	\$3,040	\$1,750	\$66,710
Consumnes	Waterman Rd	Grant Line Rd	2	2	3	K	\$37,560	\$8,880	\$2,280	\$1,750	\$50,470
Arden Arcade	Walnut Ave	Whitney Ave	1	2	1	CR	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560
Arden Arcade	Walnut Ave	Winding Way	1	2	3	CR	\$37,560		\$4,560	\$6,150	\$48,270
Arden Arcade	Garfield Ave	Marconi Ave	1	2	1	CR	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Arden Arcade	Howe Ave	Arden Way	1	2	1	CR	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Arden Arcade	Howe Ave	Hurley Way	1	2	1	CR	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Arden Arcade	Howe Ave	Marconi Ave	1	2	1	CR	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Arden Arcade	Walnut Ave	Engle Rd	1	2	1	CR	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560
Arden Arcade	Walnut Ave	Marconi Ave	1	2	1	CR	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
N. Highlands	Jolana Ln	Myrtle Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Perina Way	Myrtle Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs	
N. Highlands	Brittney Lee Ct	Myrtle Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Fair Oaks	Shamrock Dr	Sunset Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230	
Fair Oaks	Medford St	Sunset Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230	
Carmichael	Ward Ln	Sunset Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380	
N. Highlands	Donna Cir	Winona Way	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830	
N. Highlands	Donna Cir	Winona Way	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830	
Carmichael	Jan Dr	Moraga Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270	
N. Highlands	Garfield Ave	Kiva Dr	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830	
N. Highlands	Garfield Ave	Winding Way	1	2	1	CR	\$50,080	\$11,840	\$6,080	\$6,150	\$74,150	
N. Highlands	Gay Way	David Dr	1	2	2	CR	\$50,080		\$6,080	\$6,150	\$62,310	
N. Highlands	Watt Ave	McClellan Dr	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880	
N. Highlands	Watt Ave	N Haven Dr	1	2	9	Q	\$12,520		\$1,520	\$12,300	\$26,340	
N. Highlands	Watt Ave	Oak Dell Ave	1	2	9	Q	\$12,520		\$1,520	\$8,200	\$22,240	
N. Highlands	Watt Ave	Orange Grove Ave	1	2	9	Q	\$12,520		\$1,520	\$8,200	\$22,240	
N. Highlands	Watt Ave	Palm St	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880	
N. Highlands	Watt Ave	Poplar Blvd	1	2	9	Q	\$12,520		\$1,520	\$6,560	\$20,600	
N. Highlands	Watt Ave	Roseville Rd	1	2	9	Q	\$25,040		\$3,040	\$12,300	\$40,380	
N. Highlands	Watt Ave	Wings Way	1	2	9	Q	\$12,520		\$1,520	\$9,840	\$23,880	
Arden Arcade	La Colina Way	Marconi Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920	
Carmichael	Royal Palm Way	Palm Dr	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480	
Arden Arcade	Fulton Ave	El Sutton Ln	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280	
Arden Arcade	Garfield Ave	La Colina Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230	
Arden Arcade	Garfield Ave	Clark Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230	
Cosumnes	GOLD COUNTRY BLVD	HAZEL AVE	1	1	10	Z	\$145			\$6,560	\$6,705	
Cosumnes	HAZEL AVE	TRIBUTARY PT DR	1	1	10	Z	\$290			\$6,560	\$6,850	
Cosumnes	TRIBUTARY LN	TRIBUTARY PT DR	2	1	10	Z	\$290			\$1,750	\$2,040	
Cosumnes	TRIBUTARY LN	TRIBUTARY PT DR	1	1	10	Z	\$145			\$6,560	\$6,705	
Cosumnes	TRIB CROSSING DR	GOLD POINTE LN	2	1	10	Z	\$290			\$1,750	\$2,040	
Cosumnes	HAZEL AVE	FOLSOM BLVD	2	1	10	Z	\$290			\$1,750	\$2,040	
Cosumnes	TRIB CROSSING DR	TRIBUTARY PT DR	1	1	10	Z	\$145			\$6,560	\$6,705	
Cosumnes	GOLD COUNTRY BLVD	BLUE LEDGE DR	1	1	10	Z	\$145			\$6,560	\$6,705	
Cosumnes	DARK CANYON DR	GOLD COUNTRY BLVD	1	1	10	Z	\$145			\$6,560	\$6,705	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$613,740	\$223,680	\$75,160	\$202,720	\$1,115,300

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Yearly Totals							\$3,011,585	\$419,040	\$360,160	\$902,350	\$4,693,135
FY 2011-2012											
RL/Elverta	Rio Linda Blvd	Aldea Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
RL/Elverta	Rio Linda Blvd	Rafael Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
RL/Elverta	Gibson Ranch Rd	Elverta Rd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Orangevale	Hazel Ave	Creek Oaks Ln	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
RL/Elverta	Rio Linda Blvd	Savoy Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Orangevale	Hickory Ave	Shoshone Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Orangevale	Wachtel Way	Oak Ave	1	2	2	Z	\$50,080		\$6,080	\$1,750	\$57,910
Orangevale	Redwing Ct	Oak Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Orangevale	Filbert Ave	Oak Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Orangevale	Granite Ave	Oak Ave	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310
Orangevale	Chestnut Ave	Oak Ave	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310
RL/Elverta	10th St	Quiet Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Main Ave	Ottomon Way	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Orangevale	Main Ave	Ottomon Way	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
RL/Elverta	2nd St	Q St	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
Orangevale	Beech Ave	Mavis Ave	1	2	2	Z	\$50,080		\$6,080	\$12,300	\$68,460
RL/Elverta	Dorado St	Q St	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
RL/Elverta	Belcamp St	Q St	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
RL/Elverta	Rio Linda Blvd	Paladin Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
RL/Elverta	7th St	Quebec Way	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
RL/Elverta	Rio Linda Blvd	Archway Ave	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Orangevale	Hazel Ave	Leue Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Orangevale	Bobby St	Elm Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Orangevale	Tahiti Ct	Elm Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Orangevale	Pecan Ave	Elm Ave	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
RL/Elverta	Rio Linda Blvd	Bradley Way	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
RL/Elverta	8th Ave	N St	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
RL/Elverta	Oak Ln	N St	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
RL/Elverta	9th St	Oak Ln	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Orangevale	Kenneth Ave	Cortadera Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640

Sacramento County Department of Transportation ADA Transition Plan

Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
RL/Elverta	5th St	L St	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
Orangevale	Fair Oaks Blvd	Linden Lime Ct	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Orangevale	Fair Oaks Blvd	Sundance Dr	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
RL/Elverta	6th St	K St	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310
Orangevale	Kenneth Ave	Central Ave	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
Orangevale	Kilwood Ct	Lake Natoma Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Carmichael	MERCY CT	DEWEY DR	1	1	8	Z	\$5,760			\$12,300	\$18,060
Carmichael	SUNRISE BLVD	MADISON AVE	1	1	8	Z	\$5,760			\$8,200	\$13,960
Carmichael	MADISON AVE	PRIMROSE DR	1	1	8	Z	\$5,760			\$8,200	\$13,960
Carmichael	MADISON AVE	SAN JUAN AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
Carmichael	MADISON AVE	FLEETWOOD DR	1	1	8	Z	\$5,760			\$9,840	\$15,600
Carmichael	MADISON AVE	OGILBY WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Carmichael	EDGERLY WAY	MADISON AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
Carmichael	RUTLAND DR	MADISON AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
Carmichael	OAKRIDGE CT	WILDRIDGE DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Carmichael	SUNRISE HILLS DR	WILDRIDGE DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Carmichael	PALM AVE	DEWEY DR	1	1	8	Z	\$5,760			\$12,300	\$18,060
Carmichael	MADISON AVE	SCHUYLER DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Carmichael	MULDROW RD	MANZANITA AVE	2	1	8	Z	\$5,760			\$1,750	\$7,510
Carmichael	BOURBON DR	RYE WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Carmichael	WINDING WAY	MANZANITA AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
Carmichael	SALMAAN DR	CAMRAY CIR	1	1	8	Z	\$5,760			\$16,400	\$22,160
Carmichael	SALMAAN DR	CAMRAY CIR	1	1	8	Z	\$5,760			\$16,400	\$22,160
Carmichael	ADANA CIR	SALMAAN DR	1	1	8	Z	\$5,760			\$16,400	\$22,160
Carmichael	WISAM CT	SADEK WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Carmichael	LINCOLN AVE	CALIFORNIA AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
Carmichael	LINCOLN AVE	LINCOLN CREEK CIR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Carmichael	PARADISE DR	LINCOLN AVE	2	1	8	Z	\$5,760			\$1,750	\$7,510
Carmichael	HOLLISTER AVE	MURDOCK WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Carmichael	OAK TOP WAY	BANNISTER RD	2	1	8	Z	\$5,760			\$1,750	\$7,510
Carmichael	PATTYPEART WAY	PATTYPEART WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Carmichael	BARKER ELMS CT	CALIFORNIA AVE	1	1	8	Z	\$5,760			\$12,300	\$18,060
Carmichael	GRANT PARK DR	LANDIS AVE	2	1	8	Z	\$5,760			\$1,750	\$7,510

Sacramento County Department of Transportation ADA Transition Plan

Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Carmichael	KENNETH AVE	ARBORETA CT	1	1	8	Z	\$5,760			\$16,400	\$22,160
Orangevale	FILBERT AVE	TERRAMORE DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Orangevale	MADISON AVE	MAIN AVE	1	1	8	Z	\$11,520			\$8,200	\$19,720
Orangevale	MADISON AVE	MAIN AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
Orangevale	PERSHING AVE	PECAN AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
Orangevale	PECAN AVE	PERSHING AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
Orangevale	AMERIGO AVE	HAZEL AVE	1	1	8	Z	\$11,520			\$6,560	\$18,080
Orangevale	MADISON AVE	MADISON GREEN LN	1	1	8	Z	\$5,760			\$8,200	\$13,960
Orangevale	BLUE OAK DR	CAMAS CT	2	1	8	Z	\$5,760			\$1,750	\$7,510
Orangevale	MONTIA CT	BLUE OAK DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Orangevale	BLUE OAK DR	PLANTAIN CIR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Orangevale	BEAUREGARD WAY	WINDING OAK DR	1	1	8	Z	\$5,760			\$12,300	\$18,060
Orangevale	PLANTAIN CIR	BLUE OAK DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Orangevale	LEMON TREE CT	BLUE OAK DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Orangevale	BLUE OAK DR	BUTTERWOOD CIR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Orangevale	LONG CANYON DR	LARIAT CT	1	1	8	Z	\$5,760			\$6,560	\$12,320
Orangevale	BUTTERWOOD CIR	BUTTERWOOD CIR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Orangevale	WINDSOCK AVE	WINDING OAK DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Orangevale	WINDING OAK DR	VISTA DEL ORO WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Orangevale	WINDING OAK DR	SADDLE RIDGE WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Orangevale	SUNSET AVE	QUAIL MEADOW WAY	1	1	8	Z	\$5,760			\$16,400	\$22,160
Vineyard	Waterton Way	La Riviera Dr	1	2	2	Z	\$50,080		\$6,080	\$9,840	\$66,000
Vineyard	Lanier Way	La Riviera Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Vineyard	La Riviera Dr	Vancouver Dr	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Vineyard	La Riviera Dr	Tuolumne Dr	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Vineyard	Salmon Falls Dr	Tuolumne Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Vineyard	Warrego Way	Salmon Falls Dr	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Vineyard	Waterton Way	Salmon Falls Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Posada Way	Caldera Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Vineyard	Mirada St	Trujillo Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Vineyard	Nikol St	Caldera Way	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480
Vineyard	Posada Way	Caldera Way	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480
Vineyard	Eisenhower Dr	Rosemont Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230

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Vineyard	Floradora Dr	Rosemont Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Thom Way	Rosemont Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Tango St	Caldera Way	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480
Vineyard	Moonbeam Dr	Goethe Rd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Vineyard	Fusilier Way	Rosemont Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Rosemont Dr	Brunner Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Rosemont Dr	Goldilocks Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Bradshaw Rd	Ecology Ln	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Vineyard	Branch Center Rd	Ecology Ln	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Vineyard	Rosemont Dr	New Dawn Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Pageant Dr	Eisenhower Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Vineyard	Huntsman Dr	Eisenhower Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Brunner Dr	S Port Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	Sampson Blvd	Iowa Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	Sampson Blvd	Iowa Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
S. Sacto	Sampson Blvd	Iowa Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	Vista Ave	Iowa Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	Vista Ave	Iowa Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	Bonniemae Way	Iowa Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	Ethel Way	Iowa Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	Laurine Way	Iowa Ave	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310
S. Sacto	Nona Way	Iowa Ave	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
S. Sacto	44th St	Iowa Ave	1	2	2	Z	\$50,080		\$6,080	\$9,840	\$66,000
S. Sacto	35th Ave	Mendocino Blvd	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
S. Sacto	44th St	34th Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	35th Ave	Mendocino Blvd	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
S. Sacto	44th St	35th Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	41st St	39th Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
S. Sacto	41st St	39th Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	44th St	40th Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
S. Sacto	Laurine Way	Lemon Hill Ave	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310
S. Sacto	41st Ave	44th St	1	2	2	Z	\$50,080		\$6,080	\$12,300	\$68,460
S. Sacto	41st St	41st Ave	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310

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S. Sacto	Vista Ave	Lemon Hill Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	44th St	42nd Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	42nd Ave	44th St	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Sampson Blvd	42nd Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	42nd Ave	44th St	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	41st St	42nd Ave	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
S. Sacto	44th St	43rd Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	41st St	43rd Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	Vista Ave	44th Ave	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
Orangevale	WILDRIDGE DR	SUNRISE BLVD	1	1	10	Z	\$145				\$145
S. Sacto	65TH ST	STOCKTON BLVD	1	1	10	Z	\$145				\$145
S. Sacto	65TH ST	SKY PKWY	1	1	10	Z	\$145				\$145
S. Sacto	STOCKTON BLVD	LINDALE DR	1	1	10	Z	\$145				\$145
S. Sacto	FAWN WAY	FLORIN RD	1	1	10	Z	\$145				\$145
Rancho Murieta	MURIETA PKWY	MARR DR	1	1	10	Z	\$145				\$145
S. Sacto	W NICHOLS AVE	E NICHOLS AVE	2	1	10	Z	\$145				\$145
S. Sacto	15TH AVE	E NICHOLS AVE	2	1	10	Z	\$145				\$145
S. Sacto	ORINDA WAY	44TH ST	1	1	10	Z	\$145				\$145
S. Sacto	18TH AVE	W NICHOLS AVE	2	1	10	Z	\$145				\$145
S. Sacto	44TH ST	PERRY AVE	1	1	10	Z	\$145				\$145
S. Sacto	FRUITRIDGE RD	44TH ST	1	1	10	Z	\$145				\$145
S. Sacto	FRUITRIDGE RD	DEL NORTE BLVD	1	1	10	Z	\$580				\$580
S. Sacto	RIZZO CT	MLK JR BLVD	1	1	10	Z	\$145				\$145
S. Sacto	LEMON HILL AVE	LUCERO DR	1	1	10	Z	\$145				\$145
S. Sacto	MLK JR BLVD	43RD AVE	1	1	10	Z	\$145				\$145
S. Sacto	49TH AVE	MLK JR BLVD	1	1	10	Z	\$145				\$145
S. Sacto	65TH ST	SAVINGS PL	1	1	10	Z	\$145				\$145
S. Sacto	TRAIL WOODS DR	POWER INN RD	1	1	10	Z	\$145				\$145
S. Sacto	BRITTANY PARK DR	WESTPORT CIR	1	1	10	Z	\$145				\$145
S. Sacto	BRITTANY PARK DR	ELK GROVE-FLORIN RD	1	1	10	Z	\$435				\$435
S. Sacto	DARIEN CIR	BRITTANY PARK DR	1	1	10	Z	\$145				\$145
S. Sacto	VINTAGE PARK DR	BEDFORD COVE WAY	1	1	10	Z	\$145				\$145
S. Sacto	BAYTOWN WAY	VINTAGE PARK DR	1	1	10	Z	\$145				\$145

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs	
S. Sacto	VINTAGE PARK DR	FINTOWN CT	1	1	10	Z	\$145				\$145	
S. Sacto	FINTOWN CT	ORANMORE CT	1	1	10	Z	\$145				\$145	
S. Sacto	NEW POINT DR	CALVINE RD	1	1	10	Z	\$290				\$290	
Vineyard	LA RIVIERA DR	RAMP	1	1	10	Z	\$145				\$145	
Vineyard	LA RIVIERA DR	RAMP	1	1	10	Z	\$145				\$145	
Vineyard	WATT AVE	RAMP	1	1	10	Z	\$145				\$145	
Vineyard	BRADSHAW RD	COUNTRYROADS DR	1	1	10	Z	\$145				\$145	
Vineyard	ROSEMONT DR	AMERICANA WAY	1	1	10	Z	\$145				\$145	
Vineyard	ROSEMONT DR	SPARTAN WAY	1	1	10	Z	\$145				\$145	
Vineyard	BRADVIEW DR	KRISTI CT	2	1	10	Z	\$145				\$145	
Vineyard	WATT AVE	FOLSOM BLVD	1	1	10	Z	\$290				\$290	
Vineyard	S WATT AVE	MANLOVE RD	1	1	10	Z	\$145				\$145	
Vineyard	PROVINCETOWN WAY	AUBERGINE WAY	1	1	10	Z	\$145				\$145	
Vineyard	KIEFER BLVD	SCOTTSBORO DR	1	1	10	Z	\$145				\$145	
Vineyard	MCROBERTS DR	WOODRING DR	1	1	10	Z	\$145				\$145	
Vineyard	WETHERSFIELD DR	MUSTIC WAY	1	1	10	Z	\$145				\$145	
Vineyard	AUBERGINE WAY	WICKFORD WAY	1	1	10	Z	\$145				\$145	
Vineyard	WESTPORTER DR	PORTWOOD CT	2	1	10	Z	\$145				\$145	
Vineyard	FLORIN RD	S WATT AVE	1	1	10	Z	\$145				\$145	
Vineyard	BOTHWELL DR	VINTAGE PARK DR	1	1	10	Z	\$145				\$145	
Vineyard	KINGSBRIDGE DR	CALVINE RD	1	1	10	Z	\$580				\$580	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$200,435	\$200,000	\$25,000	\$100,000	\$525,435
Yearly Totals								\$3,250,810	\$200,000	\$359,400	\$1,094,650	\$4,904,860

FY 2012-2013

Arden Arcade	Eastern Ave	Alley	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Walnut Ave	San Marque Cir	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Carmichael	Wingfield Way	Shelfield Dr	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320
Arden Arcade	Walnut Ave	San Marque Cir	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Carmichael	Carmelo Dr	Shelfield Dr	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	Alta Arden Expy	Wyda Way	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Brier Way	Thor Way	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480
Arden Arcade	Bowerwood Dr	Thor Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	Mission Ave	Thor Way	1	2	2	Z	\$50,080		\$6,080	\$16,400	\$72,560
Arden Arcade	Marione Dr	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	Morse Ave	Via Grande	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Carmichael	Carmelo Dr	Bentley Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	Marione Dr	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	Morse Ave	Via Grande	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Eastern Ave	Alley	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Arden Arcade	Eastern Ave	Alley	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Carmichael	McClaren Dr	Gary Way	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Avondale Ave	Arden Way	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320
Arden Arcade	Stewart Rd	Arden Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	Mission Ave	Arden Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	Mission Ave	Arden Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	Mendota Way	Arden Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	Copenhagen Way	Arden Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Carmichael	Carmelo Dr	Alder Glen Ct	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	Eastern Ave	Alley	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Mission Ave	Valmonte Dr	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Fair Oaks Blvd	Paloma Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Carmichael	Gary Way	Ivanhoe Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Carmichael	Gary Way	Shelley Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Carmichael	McClaren Dr	Ivanhoe Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Fair Oaks Blvd	Menlo Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Hurley Way	Bell St	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560
Arden Arcade	Eastern Ave	Alley	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Arden Arcade	Lake Oak Ct	Fair Oaks Blvd	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$6,560	\$74,560
Arden Arcade	Columbia Dr	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Fallen Leaf Way	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Treehouse Ln	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Howe Ave	Spanos Ct	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Ashton Dr	Offham Ct	1	2	2	Z	\$50,080		\$6,080	\$1,750	\$57,910
Arden Arcade	Kevington Ct	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Rodante Way	Fair Oaks Blvd	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870

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Arden Arcade	La Sierra Dr	San Lucas Way	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
Arden Arcade	Estates Dr	Ramel Way	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	San Lucas Way	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Parkfair Dr	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Estates Dr	Crondall Dr	1	2	2	Z	\$37,560		\$4,560	\$16,400	\$58,520
Arden Arcade	Mills Rd	Royce Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Mills Rd	Latham Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Arden Arcade	Moffatt Way	Kadema Dr	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Clunie Dr	Kadema Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Carmichael	SAN JUAN AVE	PALM AVE	1	1	8	Z	\$5,760			\$8,200	\$13,960
Fair Oaks	SUNSET AVE	SAN JUAN AVE	1	1	8	Z	\$5,760			\$12,300	\$18,060
Carmichael	PASADENA AVE	AUBURN BLVD	1	1	8	Z	\$5,760			\$8,200	\$13,960
Carmichael	SAN JUAN AVE	WALNUT RD	1	1	8	Z	\$5,760			\$9,840	\$15,600
Carmichael	WALNUT RD	SAN JUAN AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
Carmichael	SUNRISE BLVD	FAIR OAKS BLVD	1	1	8	Z	\$5,760			\$12,300	\$18,060
Arden Arcade	WATT AVE	AUBURN BLVD	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	SAN JUAN AVE	FAIR OAKS BLVD	1	1	8	Z	\$5,760			\$9,840	\$15,600
Carmichael	MARCONI AVE	FAIR OAKS BLVD	1	1	8	Z	\$5,760			\$9,840	\$15,600
Carmichael	FAIR OAKS BLVD	PALM DR	1	1	8	Z	\$5,760			\$12,300	\$18,060
Arden Arcade	TOPP CT	FAIR OAKS BLVD	1	1	8	Z	\$5,760			\$12,300	\$18,060
Arden Arcade	EL CAMINO AVE	BELL ST	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	FULTON AVE	EL CAMINO AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	EL CAMINO AVE	MEADOWBROOK RD	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	EL CAMINO AVE	YORKTOWN AVE	1	1	8	Z	\$5,760			\$8,200	\$13,960
Arden Arcade	EL CAMINO AVE	BUTANO DR	1	1	8	Z	\$5,760			\$8,200	\$13,960
Arden Arcade	EL CAMINO AVE	WALNUT AVE	1	1	8	Z	\$5,760			\$8,200	\$13,960
Arden Arcade	EL CAMINO AVE	CAMINO PARK CT	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	EL CAMINO AVE	FAIR OAKS BLVD	1	1	8	Z	\$5,760			\$12,300	\$18,060
Antelope	PALMERSON DR	OAK FLAT WAY	1	1	8	Z	\$11,520			\$6,150	\$17,670
Antelope	MEADOW PASS WAY	DON JULIO BLVD	1	1	8	Z	\$5,760			\$6,560	\$12,320
Antelope	FORRESTER WAY	PALMERSON DR	1	1	8	Z	\$11,520			\$6,150	\$17,670
Antelope	REDWATER DR	N LOOP BLVD	1	1	8	Z	\$5,760			\$12,300	\$18,060
Antelope	BLACK SADDLE DR	BIG SKY DR	1	1	8	Z	\$5,760			\$6,560	\$12,320

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Antelope	BLACK SADDLE DR	DRIVER RANCH CT	1	1	8	Z	\$5,760			\$9,840	\$15,600
Antelope	BOULDER CREEK WAY	BIG SKY DR	1	1	8	Z	\$11,520			\$9,840	\$21,360
Antelope	BROWN OTTER DR	N COUNTRY DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Antelope	BLACKFOOT WAY	SUN MAIDEN WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Antelope	SCOTLAND DR	ANGUS WAY	1	1	8	Z	\$11,520			\$6,560	\$18,080
Antelope	BLACKFOOT WAY	SUN MAIDEN WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
Antelope	DAVIDSON DR	SCOTLAND DR	1	1	8	Z	\$11,520			\$6,150	\$17,670
Antelope	DAVIDSON DR	ORT WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
Antelope	LIGHT FOOT CT	BLACK BEAR DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Antelope	BLACK BEAR DR	LITTLE ROCK DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Antelope	U ST	MIDTOWN DR	1	1	8	Z	\$11,520			\$6,560	\$18,080
Antelope	COMMONWEALTH DR	ANTELOPE RD	1	1	8	Z	\$11,520			\$9,840	\$21,360
Antelope	WATT AVE	BAINBRIDGE DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Arden Arcade	KIMBERLY HILL CT	MANZANITA AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	LOCUST AVE	SCRANTON CIR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	LOCUST AVE	SCRANTON CIR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Arden Arcade	MILGRAY CT	GARFIELD AVE	1	1	8	Z	\$5,760			\$6,150	\$11,910
Arden Arcade	CYPRESS AVE	GARFIELD AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	GARFIELD AVE	TRIPLETT CT	1	1	8	Z	\$5,760			\$6,150	\$11,910
Arden Arcade	REGGIE WAY	LINDA LOU DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	WATT AVE	EDISON AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	ENGLE RD	SARECO CT	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	WHITNEY AVE	FOSTER WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	ROBERTSON AVE	PETTY LN	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	ROBERTSON AVE	KOBROCK WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Arden Arcade	MARCONI AVE	EASTERN AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	MARCONI AVE	WALNUT AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	HALSTED AVE	HOMWOOD WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Arden Arcade	MISSION AVE	KNAPP WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
Arden Arcade	COTTAGE WAY	FULTON AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	COTTAGE WAY	MORSE AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	WYDA WAY	BELL ST	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	THOR WAY	WALNUT AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320

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Arden Arcade	MORSE AVE	VIA GRANDE	1	1	8	Z	\$5,760			\$6,150	\$11,910
Arden Arcade	KUBEL CIR	AVONDALE AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	EASTERN AVE	ALLEY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	AVONDALE AVE	ARDEN WAY	1	1	8	Z	\$5,760			\$8,200	\$13,960
Arden Arcade	LA SIERRA DR	ARDEN WAY	1	1	8	Z	\$5,760			\$8,200	\$13,960
Arden Arcade	HURLEY WAY	BELL ST	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	BELL ST	CLINTON RD	2	1	8	Z	\$5,760			\$1,750	\$7,510
Arden Arcade	FAIR OAKS BLVD	SAVERIEN DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	NORTHROP AVE	HOWE AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
Arden Arcade	SIERRA BLVD	WOODSIDE SIERRA	2	1	8	Z	\$5,760			\$1,750	\$7,510
Arden Arcade	SIERRA BLVD	WOODSIDE OAKS	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	FULTON AVE	FAIR OAKS BLVD	1	1	8	Z	\$5,760			\$6,560	\$12,320
Arden Arcade	FAIR OAKS BLVD	MUNROE ST	1	1	8	Z	\$5,760			\$12,300	\$18,060
Arden Arcade	WYNDGATE RD	AMERICAN RIVER DR	1	1	8	Z	\$5,760			\$8,200	\$13,960
N. Highlands	REIMS WAY	SAINT TROPEZ WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
N. Highlands	SAINT TROPEZ WAY	CHERBOURG DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	CHERBOURG DR	DIEPPE WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	VERSAILLES WAY	CHERBOURG DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	CHERBOURG DR	SAINT MORITZ CT	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	GLENEAGLE WAY	CANDLESTICK WAY	1	1	8	Z	\$11,520			\$16,400	\$27,920
N. Highlands	MONOGRAM DR	MONUMENT DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
N. Highlands	TUPELO DR	ROCA WAY	1	1	8	Z	\$11,520			\$6,560	\$18,080
N. Highlands	TUPELO DR	CANDLESTICK WAY	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	TUPELO DR	OFFIELD CT	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	ABERFELDY WAY	TUPELO DR	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	TUPELO DR	ANDREA BLVD	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	VISTA OAK WAY	ROCA WAY	1	1	8	Z	\$11,520			\$6,150	\$17,670
N. Highlands	SAWTOOTH CT	DIABLO DR	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	LINDA OAK CT	ROCA WAY	1	1	8	Z	\$11,520			\$6,150	\$17,670
N. Highlands	DIABLO DR	BOOTJACK DR	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	PABLO DR	ROCA WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
N. Highlands	EAGLE VIEW WAY	PABLO DR	1	1	8	Z	\$5,760			\$16,400	\$22,160
N. Highlands	ANDREA BLVD	ROCA WAY	2	1	8	Z	\$11,520			\$1,750	\$13,270

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
N. Highlands	ELKHORN BLVD	BUTTERBALL WAY	1	1	8	Z	\$5,760			\$8,200	\$13,960
N. Highlands	SUNSET OAK CT	DIABLO DR	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	SPRIG DR	BLACK DUCK WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	SPRIG DR	REDHEAD WAY	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	PABLO DR	PHLOX CT	1	1	8	Z	\$11,520			\$6,560	\$18,080
N. Highlands	ELKHORN BLVD	DON JULIO BLVD	1	1	8	Z	\$5,760			\$12,300	\$18,060
N. Highlands	ANDREA BLVD	SUTTER OAK CT	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	ANDREA BLVD	KILKENNY DR	1	1	8	Z	\$5,760			\$9,840	\$15,600
N. Highlands	BLUEBILL WAY	SPRIG DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	BUTTERBALL WAY	HARLEQUIN WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	BARBARA LEE CIR	KARM WAY	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	BARBARA LEE CIR	KARM WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	ELKHORN BLVD	CANTEL WAY	1	1	8	Z	\$5,760			\$9,840	\$15,600
N. Highlands	KARM WAY	KARM WAY	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	SPRIG DR	BUTTERBALL WAY	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	HARLEQUIN WAY	BLACKJACK WAY	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	KARM WAY	CONCERT WAY	1	1	8	Z	\$11,520			\$6,560	\$18,080
N. Highlands	KARM WAY	KARM WAY	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	SPECKLE WAY	SPRIG DR	2	1	8	Z	\$11,520			\$1,750	\$13,270
N. Highlands	SAKO CT	WEATHERBY WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	DON JULIO BLVD	FOX VALLEY CIR	1	1	8	Z	\$5,760			\$6,150	\$11,910
N. Highlands	DON JULIO BLVD	FOX VALLEY CIR	1	1	8	Z	\$5,760			\$6,150	\$11,910
N. Highlands	HILLSDALE BLVD	WEATHERBY WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
N. Highlands	WEATHERBY WAY	RUGER CT	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	KINGBIRD WAY	KEEMA AVE	1	1	8	Z	\$11,520			\$6,560	\$18,080
N. Highlands	KINGBIRD WAY	OSPREY CT	1	1	8	Z	\$5,760			\$6,150	\$11,910
N. Highlands	KINGBIRD WAY	FOXTAIL CT	1	1	8	Z	\$5,760			\$6,150	\$11,910
N. Highlands	WEATHERBY WAY	ANDREA BLVD	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	ANDREA BLVD	HILLSDALE BLVD	1	1	8	Z	\$5,760			\$9,840	\$15,600
N. Highlands	WALERGA RD	PENWITH WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
N. Highlands	MCCLOUD DR	OGDEN NASH WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
N. Highlands	SHILOH WAY	SHILOH WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	WATT AVE	E ST	1	1	8	Z	\$5,760			\$9,840	\$15,600

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N. Highlands	BRINEF DR	GARFIELD AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
N. Highlands	MODOC WAY	WALNUT AVE	1	1	8	Z	\$5,760			\$6,150	\$11,910
N. Highlands	FLETCHER CT	WALNUT AVE	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	GLASSBORO WAY	CALCUTTA WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	BUFFWOOD WAY	ATLANTA WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	GLASSBORO WAY	GREENHOLME DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	BUFFWOOD WAY	WALNUT AVE	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	WALNUT AVE	PALM AVE	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	COLLEGE OAK DR	MISSIE WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	RESCUE CT	SHAVER CT	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	WILLOW ROCK WAY	KOHLER RD	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	BOBBIE JO CT	MARY KATE DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	SPRINGFIELD WAY	DARKWOODS CT	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	SPRINGFIELD WAY	COLLEGE OAK DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	CHRIS ANN CT	HACKBERRY LN	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	TYLER ST	ZACHARY WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	SENATE AVE	PRESIDENT AVE	1	1	8	Z	\$5,760			\$6,150	\$11,910
N. Highlands	GREENLAWN WAY	WALLABY WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	MADISON AVE	POLK ST	1	1	8	Z	\$5,760			\$6,560	\$12,320
N. Highlands	HILLSDALE BLVD	MADISON AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
N. Highlands	INTERSTATE AVE	MADISON AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
N. Highlands	TRESLER AVE	HARRISON ST	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	HARRISON ST	BAPTIST CT	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	LAVELLE WAY	CYCLAMEN WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	OMNI DR	CONNECTICUT DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	OMNI DR	HELAMAN CT	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	MIEKO WAY	MIEKO WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	TYLER ST	MIEKO WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
N. Highlands	HACKBERRY LN	MULDROW RD	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	BELLINGER CT	POLK ST	1	1	8	Z	\$5,760			\$6,150	\$11,910
N. Highlands	WATT AVE	MYRTLE AVE	1	1	8	Z	\$5,760			\$9,840	\$15,600
N. Highlands	MYRTLE AVE	HARRISON ST	2	1	8	Z	\$5,760			\$1,750	\$7,510
N. Highlands	TYLER ST	MYRTLE AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320

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N. Highlands	MYRTLE AVE	AUBURN BLVD	1	1	8	Z	\$5,760			\$8,200	\$13,960	
N. Highlands	I 80	WATT AVE	1	1	8	Z	\$5,760			\$8,200	\$13,960	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$200,000	\$200,000	\$25,000	\$100,000	\$525,000
Yearly Totals								\$2,716,920	\$223,680	\$199,800	\$1,359,210	\$4,499,610

FY 2013-2014

N. Natomas	Camelot St	Elkhorn Blvd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Vineyard	Whitewater Way	Stansberry Way	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310
Vineyard	Whitewater Way	Linda Rio Dr	1	2	2	Z	\$50,080		\$6,080	\$16,400	\$72,560
Vineyard	Stansberry Way	La Riviera Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Vineyard	Payette Dr	La Riviera Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Vineyard	Woodman Way	La Riviera Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Vineyard	Rogue River Dr	La Riviera Dr	1	2	2	Z	\$50,080		\$6,080	\$9,840	\$66,000
Vineyard	Ramp	La Riviera Dr	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$12,300	\$63,300
Vineyard	Ramp	La Riviera Dr	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$1,750	\$52,750
Vineyard	Ramp	La Riviera Dr	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$8,200	\$59,200
Vineyard	Ramp	La Riviera Dr	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$8,200	\$59,200
Vineyard	Linda Rio Dr	La Riviera Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Vineyard	Rosemont Dr	Fashion Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Bradshaw Rd	Preservation Way	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
Vineyard	Rosemont Dr	Sutters Gold Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Huntsman Dr	Parfait Dr	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310
Vineyard	Rosemont Dr	Mojave Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Tallyho Dr	Kiefer Blvd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Vineyard	Kiefer Blvd	Thornhill Dr	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Vineyard	Tallyho Dr	Kiefer Blvd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Vineyard	Rosemont Dr	Kiefer Blvd	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320
Vineyard	Bradshaw Rd	Conservation Rd	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Vineyard	Thornhill Dr	Alderson Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Vineyard	Agriculture Ln	Traffic Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Vineyard	Wildrose Way	Alderson Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Vineyard	Service Rd	Kiefer Blvd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Vineyard	Thornhill Dr	Brydon Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230

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Vineyard	Youth Center Ct	Kiefer Blvd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380	
Vineyard	Thornhill Dr	Nasreen Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270	
N. Natomas	KENORA ST	2ND ST	1	1	8	Z	\$5,760			\$8,200	\$13,960	
N. Natomas	CHERRI LYNN AVE	W 2ND ST	1	1	8	Z	\$5,760			\$6,150	\$11,910	
N. Natomas	SUNVIEW WAY	2ND ST	1	1	8	Z	\$5,760			\$8,200	\$13,960	
N. Natomas	WITHINGTON AVE	2ND ST	1	1	8	Z	\$5,760			\$8,200	\$13,960	
N. Natomas	2ND ST	LINN WAY	1	1	8	Z	\$5,760			\$8,200	\$13,960	
N. Natomas	QUADRA AVE	QUADRA AVE	2	1	8	Z	\$5,760			\$1,750	\$7,510	
N. Natomas	2ND ST	JUBILEE WAY	1	1	8	Z	\$5,760			\$8,200	\$13,960	
N. Natomas	W ELKHORN BLVD	2ND ST	1	1	8	Z	\$5,760			\$8,200	\$13,960	
N. Natomas	ELKHORN BLVD	RIO LINDA BLVD	1	1	8	Z	\$5,760			\$12,300	\$18,060	
N. Natomas	DEL PASO RD	KENMAR RD	2	1	8	Z	\$5,760			\$1,750	\$7,510	
N. Natomas	STRIKER AVE	STADIUM LN	2	1	8	Z	\$5,760			\$1,750	\$7,510	
N. Natomas	N MARKET BLVD	SIERRA POINT DR	1	1	8	Z	\$5,760			\$6,560	\$12,320	
N. Natomas	VISTA PARK CT	NATIONAL DR	1	1	8	Z	\$5,760			\$8,200	\$13,960	
N. Natomas	LENNANE DR	NATIONAL DR	1	1	8	Z	\$5,760			\$12,300	\$18,060	
N. Natomas	SIERRA POINT DR	NATIONAL DR	1	1	8	Z	\$5,760			\$6,560	\$12,320	
N. Natomas	N FREEWAY BLVD	LENNANE DR	1	1	8	Z	\$5,760			\$6,560	\$12,320	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$2,278,320	\$244,400	\$277,320	\$548,900	\$3,348,940
Yearly Totals								\$3,296,960	\$279,920	\$389,800	\$910,660	\$4,877,340
FY 2014-2015												
N. Highlands	Diablo Dr	Everest Way	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870	
N. Highlands	Don Julio Blvd	Dewar Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920	
N. Highlands	Don Julio Blvd	Providence Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920	
N. Highlands	Hillsdale Blvd	McCloud Dr	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960	
N. Highlands	McCloud Dr	Everest Way	1	2	2	Z	\$50,080		\$6,080	\$1,750	\$57,910	
N. Highlands	Sagebrush Way	McCloud Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230	
N. Highlands	Bruce Way	Ernestine Way	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680	
N. Highlands	Oakhollow Dr	Robert Frost Way	1	2	2	Z	\$37,560		\$4,560	\$16,400	\$58,520	
N. Highlands	Oakhollow Dr	Ravenstone Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
N. Highlands	Grattan Way	Don Julio Blvd	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920	
N. Highlands	Bolivar Ave	Don Julio Blvd	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920	

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
N. Highlands	Hillsdale Blvd	Robert Frost Way	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
N. Highlands	Claussen Way	Larry Way	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
N. Highlands	Bruce Way	Larry Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Orangevale	Lake Natoma Dr	Margo Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Lake Natoma Dr	Woodminster Cir	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Orangevale	Filbert Ave	Westeria Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Diablo Dr	Everest Way	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Orangevale	Lake Natoma Dr	Woodminster Cir	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Sunday Dr	Stephen Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Cornelia Way	Stephen Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Orangevale	Hazel Ave	Fortuna Way	1	2	2	Z	\$37,560		\$4,560	\$12,300	\$54,420
N. Highlands	Gay Way	Stephen Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
N. Highlands	Duff Ct	Stephen Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Queen Ct	Stephen Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Orangevale	Lake Natoma Dr	Overwood Ct	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Lake Natoma Dr	Beachwood Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Illinois Ave	Windshire Ln	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
N. Highlands	Channing Dr	N Haven Dr	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
N. Highlands	Hillsdale Blvd	Diablo Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Diablo Dr	Oakbank Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
N. Highlands	Lila Ln	N Haven Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Revelstok Dr	Diablo Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Jeanine Dr	Diablo Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Hazel Ave	Van Moore Ln	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Orangevale	Lake Natoma Dr	Tonkin Dr	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
Orangevale	Tonkin Dr	Lake Natoma Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Karl Dr	Santa Fe Way	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310
N. Highlands	Bliss Ct	Karl Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Verner Ave	Pioneer Way	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
N. Highlands	Brett Dr	Hillsdale Blvd	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
N. Highlands	Rio Verde Way	Karl Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	El Campo Way	Karl Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	San Marcos Way	Karl Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Orangevale	Kilwood Ct	Lake Natoma Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Rosario Blvd	Karl Dr	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
N. Highlands	Brett Dr	Revelstok Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Orangevale	Illinois Ave	Boca Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Orangevale	Snowberry Way	Lake Natoma Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Patte Way	Lake Natoma Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Santa Fe Way	Elmo Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Hazel Ave	Aksarben Dr	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Orangevale	Chestnut Ave	Rich Hill Dr	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Orangevale	Dalton Way	Lake Natoma Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Main Ave	Lake Natoma Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Lake Natoma Dr	Tonkin Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Orangevale	Tonkin Dr	Lake Natoma Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Dalton Way	Lake Natoma Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Snipes Blvd	Lake Natoma Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Orangevale	Beech Ave	Pershing Ave	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Orangevale	Beech Ave	Pershing Ave	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Orangevale	Fairvale Way	Pershing Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Santa Fe Way	Elmo Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Orangevale	Pershing Ave	Baxter Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Baxter Way	Pershing Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Rosebud Ln	Auburn Blvd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Orangevale	Walnut Ave	Madison Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
N. Highlands	Santa Fe Way	Ellis Ct	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Orangevale	Pershing Ave	Baxter Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Baxter Way	Pershing Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Hazel Ave	Lance Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Orangevale	Amerigo Ave	Hazel Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Fair Oaks	Leafcrest Way	Treecrest Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
N. Highlands	Manzanita Ave	Casa Alegre	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Fair Oaks	McKay St	Wightman Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Orangevale	Hazel Ave	Leedy Ln	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	A St	A St	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300

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N. Highlands	A St	A St	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
N. Highlands	Aero Ct	A St	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Poplar Blvd	A St	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
Fair Oaks	Fair Oaks Blvd	Leafcrest Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	Palm Ave	Garfield Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Hazel Ave	Jonnie Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Orangevale	Main Ave	Golden Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Fair Oaks	Mckay St	Deseret Ave	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Orangevale	Hazel Ave	Britland Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Orangevale	Main Ave	Bullion Way	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310
N. Highlands	Georgia Dr	McClellan Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
N. Highlands	Watt Ave	McClellan Dr	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	Poplar Blvd	McClellan Dr	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
Orangevale	Illinois Ave	Sheraton Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Poplar Blvd	Payne Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Orangevale	Main Ave	Shumway Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Orangevale	Main Ave	Drift Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Fair Oaks	Valonia St	Madison Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Fair Oaks	Chicago Ave	Madison Ave	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Fair Oaks	Hazel Ave	Madison Ave	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
Fair Oaks	Kahala Ct	Madison Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Rockwell Dr	Wings Way	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
N. Highlands	Walnut Ave	Madison Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Fair Oaks	Chicago Ave	Kaula Dr	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
N. Highlands	Polk St	Myrtle Ave	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Carmichael	Panay Ct	Kenneth Ave	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480
Carmichael	Los Feliz Way	Kenneth Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Carmichael	Lost Deer Ln	Van Alstine Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Orangevale	OAK AVE	BIRUTA AVE	2	1	8	Z	\$11,520			\$1,750	\$13,270
Orangevale	KROGH CT	SUNCREEK WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Orangevale	KENNETH AVE	MENKE WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Orangevale	MENKE WAY	MENKE WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Orangevale	SUNCREEK WAY	MENKE WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910

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Orangevale	MENKE WAY	ALMOND AVE	1	1	8	Z	\$5,760			\$6,150	\$11,910	
Orangevale	DRYWOOD WAY	CROSSOAK WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510	
Orangevale	KENNETH AVE	MURICATIA DR	1	1	8	Z	\$11,520			\$6,560	\$18,080	
Orangevale	FAIR OAKS BLVD	SUNDANCE DR	1	1	8	Z	\$5,760			\$8,200	\$13,960	
Orangevale	SKYVIEW DR	WOODLAKE HILLS DR	2	1	8	Z	\$5,760			\$1,750	\$7,510	
Orangevale	OAKWIND CT	KENNETH AVE	2	1	8	Z	\$5,760			\$1,750	\$7,510	
Orangevale	SHERRY DR	ROLOFF WAY	2	1	8	Z	\$11,520			\$1,750	\$13,270	
Orangevale	SHERRY DR	ROLOFF WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510	
Orangevale	RENEE ANN ST	CHESTNUT AVE	2	1	8	Z	\$5,760			\$1,750	\$7,510	
Orangevale	SUEDE HILL CT	GARDEN TOWNE WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320	
Orangevale	THELEN CT	ILLINOIS AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320	
Orangevale	AIRHILL WAY	WALNUT AVE	2	1	8	Z	\$5,760			\$1,750	\$7,510	
Orangevale	CHESTNUT AVE	ROCK CANYON WAY	2	1	8	Z	\$11,520			\$1,750	\$13,270	
Orangevale	WALNUT AVE	SIRL WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320	
Orangevale	QUAIL OAK WAY	ILLINOIS AVE	1	1	8	Z	\$5,760			\$6,150	\$11,910	
Orangevale	TERRAMORE DR	FILBERT AVE	2	1	8	Z	\$11,520			\$1,750	\$13,270	
Orangevale	VISTA DOME CT	WINDING OAK DR	1	1	8	Z	\$5,760			\$6,560	\$12,320	
Orangevale	WINDING OAK DR	VISTA DEL ORO WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$200,000	\$200,000	\$25,000	\$100,000	\$525,000
Yearly Totals								\$3,541,360	\$259,200	\$411,080	\$932,890	\$5,144,530
FY 2015-2016												
RL/Elverta	Buckboard Dr	G St	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
RL/Elverta	Dry Creek Rd	E St	1	2	2	Z	\$50,080		\$6,080	\$12,300	\$68,460	
S. Sacto	65TH ST	STOCKTON BLVD	1	1	8	Z	\$5,760			\$8,200	\$13,960	
S. Sacto	FLORIN MALL DR	FLORIN RD	1	1	8	Z	\$5,760			\$9,840	\$15,600	
S. Sacto	FAWN WAY	FLORIN RD	1	1	8	Z	\$5,760			\$12,300	\$18,060	
RL/Elverta	ZUIDER ZEE CIR	DUTCH HAVEN BLVD	2	1	8	Z	\$11,520			\$1,750	\$13,270	
RL/Elverta	U ST	10TH ST	1	1	8	Z	\$5,760			\$16,400	\$22,160	
RL/Elverta	10TH ST	U ST	1	1	8	Z	\$5,760			\$6,560	\$12,320	
RL/Elverta	LUCILE WAY	10TH ST	1	1	8	Z	\$5,760			\$6,560	\$12,320	
RL/Elverta	8TH ST	SUN VISTA CT	1	1	8	Z	\$5,760			\$6,560	\$12,320	
RL/Elverta	OAK LN	9TH AVE	1	1	8	Z	\$5,760			\$8,200	\$13,960	

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RL/Elverta	W M ST	SUN ACER WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
RL/Elverta	M ST	BIRNAM ST	2	1	8	Z	\$11,520			\$1,750	\$13,270
RL/Elverta	M ST	2ND ST	2	1	8	Z	\$5,760			\$1,750	\$7,510
RL/Elverta	SHADY WOODS WAY	M ST	1	1	8	Z	\$5,760			\$8,200	\$13,960
RL/Elverta	7TH AVE	M ST	1	1	8	Z	\$5,760			\$9,840	\$15,600
RL/Elverta	E ST	SKI PARK CT	2	1	8	Z	\$5,760			\$1,750	\$7,510
RL/Elverta	E ST	HAYER CIR	2	1	8	Z	\$5,760			\$1,750	\$7,510
RL/Elverta	E ST	HAYER CIR	2	1	8	Z	\$5,760			\$1,750	\$7,510
S. Sacto	16TH AVE	47TH ST	1	1	8	Z	\$5,760			\$6,560	\$12,320
S. Sacto	STOCKTON BLVD	16TH AVE	1	1	8	Z	\$5,760			\$12,300	\$18,060
S. Sacto	STOCKTON BLVD	17TH AVE	1	1	8	Z	\$5,760			\$12,300	\$18,060
S. Sacto	16TH AVE	44TH ST	1	1	8	Z	\$5,760			\$9,840	\$15,600
S. Sacto	43RD ST	16TH AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
S. Sacto	STOCKTON BLVD	PARKER AVE	1	1	8	Z	\$5,760			\$12,300	\$18,060
S. Sacto	48TH ST	PARKER AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
S. Sacto	ROOSEVELT AVE	STOCKTON BLVD	1	1	8	Z	\$5,760			\$8,200	\$13,960
S. Sacto	48TH ST	ROOSEVELT AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
S. Sacto	22ND AVE	MLK JR BLVD	1	1	8	Z	\$5,760			\$9,840	\$15,600
S. Sacto	BAKER AVE	STOCKTON BLVD	1	1	8	Z	\$5,760			\$8,200	\$13,960
S. Sacto	48TH ST	BAKER AVE	2	1	8	Z	\$11,520			\$1,750	\$13,270
S. Sacto	SIERRA VISTA AVE	44TH ST	1	1	8	Z	\$5,760			\$9,840	\$15,600
S. Sacto	FRUITRIDGE RD	DEWEY BLVD	1	1	8	Z	\$5,760			\$8,200	\$13,960
S. Sacto	FRUITRIDGE RD	44TH ST	1	1	8	Z	\$5,760			\$9,840	\$15,600
S. Sacto	LEMON HILL AVE	LUCERO DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
S. Sacto	41ST AVE	FRANKLIN BLVD	1	1	8	Z	\$5,760			\$9,840	\$15,600
S. Sacto	43RD AVE	FRANKLIN BLVD	1	1	8	Z	\$5,760			\$8,200	\$13,960
S. Sacto	49TH AVE	MLK JR BLVD	1	1	8	Z	\$5,760			\$6,560	\$12,320
S. Sacto	FLORIN RD	FLETCHER FARM DR	1	1	8	Z	\$5,760			\$16,400	\$22,160
S. Sacto	POWER INN RD	FLORIN RD	1	1	8	Z	\$5,760			\$6,150	\$11,910
S. Sacto	FLORIN RD	FRANKLIN BLVD	1	1	8	Z	\$11,520			\$12,300	\$23,820
S. Sacto	POWER INN RD	BLACKHAWK DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
S. Sacto	PRITCHARD RD	FLORINTOWN WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
S. Sacto	PALMER HOUSE DR	GREYHAWK CT	1	1	8	Z	\$5,760			\$9,840	\$15,600

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
S. Sacto	STOCKTON BLVD	CHANDLER DR	1	1	8	Z	\$5,760			\$8,200	\$13,960
S. Sacto	POWER INN RD	LOUCRETA DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
S. Sacto	A PKWY	FRANKLIN BLVD	1	1	8	Z	\$5,760			\$12,300	\$18,060
S. Sacto	POWER INN RD	68TH AVE	1	1	8	Z	\$5,760			\$6,150	\$11,910
S. Sacto	DARLA WAY	HOMEFIELD WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	SALMON FALLS DR	BARRACUDA WAY	1	1	8	Z	\$5,760			\$8,200	\$13,960
Vineyard	HANKS ST	COUNTRYROADS DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	HUNTSMAN DR	MICRON AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	ASTRAL DR	OLD PLACERVILLE RD	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	FOLSOM BLVD	STARFIRE DR	1	1	8	Z	\$5,760			\$12,300	\$18,060
Vineyard	OLD PLACERVILLE RD	HAPPY LN	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	CALDERA WAY	STARINA WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	HEIRLOOM WAY	CALDERA WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	FIRELIGHT WAY	CALDERA WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	HUNTSMAN DR	GYPSY WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	MANLOVE RD	CAL CENTER DR	1	1	8	Z	\$5,760			\$8,200	\$13,960
Vineyard	HUNTSMAN DR	THILOW DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	HUNTSMAN DR	ROSEMONT DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	ROSEMONT DR	ZORINA WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	ROSEMONT DR	CONTEMPO DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	ROSEMONT DR	AMERICANA WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	ROSEMONT DR	FRENHAM WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	ROSEMONT DR	REDGOLD WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	TANGO ST	NEW DAWN DR	1	1	8	Z	\$5,760			\$8,200	\$13,960
Vineyard	AMERICANA WAY	BLUE RIVER CT	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	WATT AVE	FOLSOM BLVD	1	1	8	Z	\$5,760			\$8,200	\$13,960
Vineyard	AMERICANA WAY	HUNTSMAN DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	REEDSPORT CT	CONTEMPO DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	MARCOLA CT	CONTEMPO DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	MAYHEW RD	MIRANDY DR	1	1	8	Z	\$5,760			\$9,840	\$15,600
Vineyard	FABERGE WAY	MIRANDY DR	1	1	8	Z	\$11,520			\$6,150	\$17,670
Vineyard	TILLAMOOK WAY	MIRANDY DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	BIRCH TREE WAY	GLEN ALDER WAY	1	1	8	Z	\$11,520			\$6,560	\$18,080

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Vineyard	MIRANDY DR	CONTEMPO DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	N KIEFER BLVD	TILLAMOOK WAY	1	1	8	Z	\$5,760			\$9,840	\$15,600
Vineyard	AUTUMNWOOD DR	KIEFER BLVD	1	1	8	Z	\$5,760			\$8,200	\$13,960
Vineyard	S WATT AVE	FREDRIC AVE	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	WESTPORTER DR	LUTHERAN CIR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	LILIBET AVE	TALLYHO DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	ROSEPORT WAY	COBBLEWOOD CT	1	1	8	Z	\$11,520			\$6,560	\$18,080
Vineyard	S PORT DR	NASREEN DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	TALLYHO DR	CLENDENEN WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	ASHGROVE WAY	FIRGROVE CT	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	CLENDENEN WAY	OZRO CT	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	ELMGROVE CT	BIRCHGROVE WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	S PORT DR	NEWHALL DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	TALLYHO DR	NEWHALL DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	HOLLYGROVE CT	NEWHALL DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	BAIRNSDALE WAY	CANBERRA DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	TALLYHO DR	OXWOOD DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	ROSE RIVER WAY	LEAFMONT WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	NASREEN DR	NEWHALL DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	CANBERRA DR	CANBERRA DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	TALLYHO DR	PLUMGROVE WAY	2	1	8	Z	\$11,520			\$1,750	\$13,270
Vineyard	KANGAROO CT	CANBERRA DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	PLUMGROVE WAY	APPLEGATE DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	WINDSONG CT	THORNHILL DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	TALLYHO DR	CEDARGROVE DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	TALLYHO DR	JUSTIN WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	BAIRNSDALE WAY	CANBERRA DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Vineyard	THORNHILL DR	CANBERRA DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	CANBERRA DR	WYALONG WAY	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	GLEN INNES WAY	CANBERRA DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	TALLYHO DR	CLENDENEN WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510
Vineyard	BIRDSONG CT	THORNHILL DR	1	1	8	Z	\$5,760			\$6,150	\$11,910
Vineyard	MAYHEW RD	OXWOOD DR	1	1	8	Z	\$5,760			\$16,400	\$22,160

Sacramento County Department of Transportation ADA Transition Plan

Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs	
Vineyard	OXWOOD DR	LEAFMONT WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320	
Vineyard	CLARECASTLE CT	CASTLEBAR WAY	2	1	8	Z	\$5,760			\$1,750	\$7,510	
Vineyard	PRAIRIE TRAIL WAY	THORNHILL DR	1	1	8	Z	\$5,760			\$6,150	\$11,910	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$2,052,240	\$220,720	\$222,600	\$685,960	\$3,181,520
Yearly Totals								\$2,812,800	\$220,720	\$231,720	\$1,403,480	\$4,668,720

FY 2016-2017

N. Highlands	Plumber Way	Goddard Way	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
N. Highlands	Larchmont Dr	Goddard Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Thomas Dr	Plymouth Dr	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
N. Highlands	Cantel Way	Plymouth Dr	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
N. Highlands	Schofield Way	Painter Way	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
N. Highlands	Larchmont Dr	Painter Way	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
N. Highlands	Andrea Blvd	Roble Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Thomas Dr	Lankershim Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	Thomas Dr	Milton Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
N. Highlands	Larchmont Dr	Gothberg Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	Diablo Dr	Woodforest Dr	1	2	2	Z	\$37,560		\$4,560	\$12,300	\$54,420
N. Highlands	Blackjack Way	Walerga Rd	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Diablo Dr	Forestwood Dr	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	Floral Dr	Larchmont Dr	1	2	2	Z	\$50,080		\$6,080	\$9,840	\$66,000
N. Highlands	Watt Ave	Lankershim Way	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
N. Highlands	Fox Valley Cir	Don Julio Blvd	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Fox Valley Cir	Don Julio Blvd	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Cantel Way	Milton Way	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
N. Highlands	Aqua Ct	Don Julio Blvd	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
N. Highlands	Watt Ave	Milton Way	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320
N. Highlands	Thomas Dr	Floral Dr	1	2	2	Z	\$50,080		\$6,080	\$1,750	\$57,910
N. Highlands	Larchmont Dr	Melrose Dr	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
N. Highlands	Pearson Ln	Melrose Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Cantel Way	Floral Dr	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480
N. Highlands	Watt Ave	Floral Dr	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
N. Highlands	Graylock Ln	Melrose Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
N. Highlands	Andrea Blvd	Filaree Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	Ridley Way	Thomas Dr	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480
N. Highlands	Stoneman Dr	Melrose Dr	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
N. Highlands	Thomas Dr	Van Owen St	1	2	2	Z	\$37,560		\$4,560	\$16,400	\$58,520
Fair Oaks	KENNETH AVE	KENNETH OAK WAY	1	1	8	Z	\$5,760			\$12,300	\$18,060
Fair Oaks	DUTCHESS CT	SUNBONNET DR	2	1	8	Z	\$5,760			\$1,750	\$7,510
Fair Oaks	SPRING GLEN DR	SUNBONNET DR	1	1	8	Z	\$5,760			\$6,560	\$12,320
Fair Oaks	FAIR OAKS BLVD	NIESSEN WAY	1	1	8	Z	\$5,760			\$6,560	\$12,320
Fair Oaks	TREECREST AVE	MCKAY ST	2	1	8	Z	\$5,760			\$1,750	\$7,510
Fair Oaks	ILLINOIS AVE	LINCOLN VILLA WAY	1	1	8	Z	\$5,760			\$8,200	\$13,960
Fair Oaks	QUAIL HILL WAY	QUAIL MEADOW WAY	1	1	8	Z	\$5,760			\$16,400	\$22,160
Fair Oaks	OLD WINDING WAY	BIJAN CT	1	1	8	Z	\$5,760			\$6,560	\$12,320
Fair Oaks	SACRAMENTO ST	FAIR OAKS BLVD	1	1	8	Z	\$5,760			\$9,840	\$15,600
S. Sacto	Franklin Blvd	47th Ave	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
S. Sacto	Franklin Blvd	47th Ave	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Antelope	Pima Way	Navaho Dr	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
N. Highlands	Fair Play Dr	Bainbridge Dr	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
N. Highlands	Watt Ave	Grattan Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	Walerga Rd	Larchmont Dr	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
N. Highlands	Diablo Dr	Klamath Dr	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
N. Highlands	Don Julio Blvd	Garland Ct	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	Belva Way	Melrose Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
N. Highlands	Don Julio Blvd	Scotia Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
N. Highlands	Pinebrook Way	Tacomac Dr	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
N. Highlands	Tacomac Dr	Everest Way	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
N. Highlands	Watt Ave	Don Julio Blvd	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840
Carmichael	California Ave	Kiernan Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Arden Arcade	Watt Ave	Kings Way	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$8,200	\$76,200
Arden Arcade	Catalina Dr	El Camino Ave	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
Arden Arcade	Howe Ave	Cottage Way	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
N. Highlands	Garfield Ave	Winding Way	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$6,150	\$74,150
Fair Oaks	Watkins Dr	Sacramento St	1	2	2	Z	\$50,080		\$6,080	\$6,560	\$62,720
Carmichael	Clover Knoll Ct	Lincoln Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs	
Arden Arcade	Millrace Rd	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920	
Arden Arcade	Midland Way	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380	
Arden Arcade	Landwood Way	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280	
Arden Arcade	Hazelwood Ave	El Camino Ave	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320	
Arden Arcade	Via Camino Ave	El Camino Ave	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320	
Arden Arcade	Upham Ct	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280	
Arden Arcade	Lillian Ln	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380	
Arden Arcade	Pueblo St	Cypress Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Arden Arcade	Jane Ct	Cypress Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Arden Arcade	Garfield Ave	Aris Way	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830	
Arden Arcade	Queenston Ct	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280	
Arden Arcade	Champlain Ln	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380	
Arden Arcade	Grover Ct	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280	
Arden Arcade	Dell Rd	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380	
Arden Arcade	Rochdale Dr	Gibbons Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Arden Arcade	Linus Way	Gibbons Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Carmichael	California Ave	Casita Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920	
Arden Arcade	Payton St	Edison Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Arden Arcade	Norris Ave	Edison Ave	1	2	2	Z	\$50,080		\$6,080	\$1,750	\$57,910	
Arden Arcade	Brownson St	Edison Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230	
Arden Arcade	Brownson St	Edison Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830	
Arden Arcade	Belcrest Way	Engle Rd	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Arden Arcade	Belcrest Way	Engle Rd	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Arden Arcade	Casa Rosa Way	Engle Rd	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Arden Arcade	Vega Ct	Engle Rd	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$400,000	\$400,000	\$50,000	\$200,000	\$1,050,000
Yearly Totals								\$2,805,600	\$471,040	\$335,760	\$886,510	\$4,498,910
FY 2017-2018												
S. Sacto	49th St	Parker Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
S. Sacto	45th St	Parker Ave	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
S. Sacto	49th St	Roosevelt Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830	
S. Sacto	45th St	Roosevelt Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230	

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
S. Sacto	44th St	Roosevelt Ave	1	2	2	Z	\$50,080		\$6,080	\$12,300	\$68,460
S. Sacto	44th St	22nd Ave	1	2	2	Z	\$37,560		\$4,560	\$12,300	\$54,420
S. Sacto	44th St	26th Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	44th St	26th Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Sampson Blvd	Fruitridge Rd	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320
S. Sacto	Bonniemae Way	Fruitridge Rd	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Ethel Way	Fruitridge Rd	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
S. Sacto	Laurine Way	Fruitridge Rd	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Nona Way	Fruitridge Rd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
S. Sacto	O'Dea Dr	Fruitridge Rd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
S. Sacto	Mendocino Blvd	32nd Ave	1	2	4	Z	\$25,040		\$3,040	\$16,400	\$44,480
S. Sacto	44th St	O'dea Dr	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	Sampson Blvd	44th Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
S. Sacto	44th St	45th Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
S. Sacto	47th Ave	Steiner Dr	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$12,300	\$63,300
S. Sacto	47th St	50th Ave	1	2	2	Z	\$37,560		\$4,560	\$1,750	\$43,870
S. Sacto	Steiner Dr	Austin Way	1	2	2	Z	\$50,080		\$6,080	\$9,840	\$66,000
S. Sacto	Livingston Way	47th St	1	2	2	Z	\$37,560		\$4,560	\$6,560	\$48,680
S. Sacto	Franklin Blvd	Florin Rd	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300
S. Sacto	Grand Cru Dr	Calvine Rd	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$1,750	\$69,750
S. Sacto	43rd St	14th Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
S. Sacto	Stockton Blvd	15th Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
S. Sacto	44th St	18th Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
S. Sacto	44th St	18th Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Walnut Ave	Foothill Dr	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Wright St	Wulff Ln	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Ethan Way	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Moretti Way	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Tamarack Way	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	Pasadena Ave	Auburn Blvd	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	Wright St	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	Gwen Rd	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	Morse Ave	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs
Arden Arcade	Sunview Ave	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Borica Way	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	Borica Way	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	Cambon Way	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	RosylN Way	El Camino Ave	1	2	2	Z	\$37,560		\$4,560	\$9,840	\$51,960
Arden Arcade	El Vita Way	El Camino Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380
Arden Arcade	Mission Ave	Nottingham Cir	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Venus Dr	Laurelwood Way	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Arden Arcade	Mission Ave	Laurelwood Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Garfield Ave	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830
Arden Arcade	Byron Rd	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Cortez Ln	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Venus Dr	Cottage Way	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Arden Arcade	Venus Dr	Cottage Way	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Arden Arcade	Mercury Way	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Daphne Ave	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Adonis Way	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Park Ridge Ct	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Morpheus Ln	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Santa Lucia Way	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640
Arden Arcade	Bircher Way	Cottage Way	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Arden Arcade	Maddox Ct	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Eric Rd	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Oak Leaf Ave	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Carob Ct	Cottage Way	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Walnut Ave	Cottage Way	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$6,150	\$57,150
Arden Arcade	Root Ave	Kenneth Ave	1	2	4	Z	\$25,040		\$3,040	\$6,150	\$34,230
Arden Arcade	Gunn Rd	Kenneth Ave	1	2	2	Z	\$50,080		\$6,080	\$6,150	\$62,310
Arden Arcade	Marchita Way	Fair Oaks Blvd	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280
Arden Arcade	Walnut Ave	Oak Leaf Ave	1	2	2	Z	\$37,560		\$4,560	\$6,150	\$48,270
Arden Arcade	Coloma Rd	Neiretto Ct	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Arden Arcade	Howe Ave	Dawn Way	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920
Carmichael	Fair Oaks Blvd	Wedgewood Ave	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380

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Community	Street 1 (typically NS Street)	Street 2 (typically EW Street)	Use	Condi- tion	Work Scope	Fund- ing	Curb Ramps	Signals	Cross- walks	Side walks	Total Costs	
Carmichael	Fair Oaks Blvd	Frontier Way	1	2	4	Z	\$25,040		\$3,040	\$12,300	\$40,380	
Arden Arcade	Mission Ave	Oxbow Dr	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Arden Arcade	Mission Ave	Nelroy Way	1	2	4	Z	\$25,040		\$3,040	\$6,560	\$34,640	
Carmichael	Fair Oaks Blvd	Oak Ave	1	2	4	Z	\$25,040		\$3,040	\$8,200	\$36,280	
Carmichael	Boyer Dr	Oak Ave	1	2	4	Z	\$25,040		\$3,040	\$1,750	\$29,830	
Carmichael	Shelfield Dr	Newbury Way	1	2	2	Z	\$37,560		\$4,560	\$8,200	\$50,320	
Arden Arcade	Juniper Ln	Fulton Ave	1	2	4	Z	\$25,040		\$3,040	\$9,840	\$37,920	
Arden Arcade	Howe Ave	Alta Arden Expy	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$9,840	\$77,840	
Arden Arcade	Morse Ave	Arden Way	1	2	1	Z	\$50,080	\$11,840	\$6,080	\$12,300	\$80,300	
Arden Arcade	Fair Oaks Blvd	Jacob Ln	1	2	2	Z	\$37,560	\$8,880	\$4,560	\$1,750	\$52,750	
Unspecified Locations for Curb Ramps, Sidewalks, Crosswalks and Accessible Signals							AZ	\$400,000	\$400,000	\$50,000	\$200,000	\$1,050,000
Yearly Totals								\$2,766,280	\$474,000	\$337,280	\$845,920	\$4,423,480

Appendix D: Bus Stop Transition Plan (BSTP)

Sacramento County Department of Transportation



Americans with Disabilities Act Transition Plan Bus Stop Transition Plan (BSTP) Supplement



April 9, 2013

This Update To The SacDOT "ADA Transition Plan Including Bus Stop Supplement" Was Made Possible By A Caltrans Transportation Planning Grant In The Environmental Justice Planning Category For Fiscal Year 2010-2011.

Environmental Justice Transportation Planning

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Appendix D: Appendices:

BSTP Appendix 1	Bus Stop Prioritization Score Card (27 Pgs.)
BSTP Appendix 2	Bus Stop Overall Priority Ranking and Improvement Costs (28 Pages)
BSTP Appendix 3	Alphabetical Bus Stop Priority Ranking and Improvement Costs (28 Pages)
BSTP Appendix 4	Bus Stop Street Improvement Details, Class A, B & C Streets (4 Pages)
BSTP Appendix 5	Bus Stop Maintenance Agreement – SacDOT & RT (13 Pages)
BSTP Appendix 6	RT Bus Route Maps (3 Pages)
BSTP Appendix 7	Sacramento County Corridor Map (1 Page)
BSTP Appendix 8	Bus Stop Survey Samples & Website Ridership Survey Summary (19 Pages)
BSTP Appendix 9	Grievance Form (1 Page)

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Section 1: Introduction and Administrative Information

A stated goal of the Sacramento County Department of Transportation (SacDOT) is the commitment to delivering quality municipal services and transportation facilities on which our neighborhoods and businesses depend. This includes encouraging and supporting many alternative transportation choices, including the regional bus transportation network.

The primary service provider for bus service in the county is Sacramento Regional Transit (RT). RT began operations in April 1973. Their annual ridership has steadily increased on the bus and light rail systems from 14 million in 1987 to over 31 million passengers in 2010. Weekday bus ridership averages about 51,000 passengers per day (see **BSTP Appendix 6** for RT Route Maps). In addition to RT, other adjacent jurisdictions such as Folsom (Folsom Stage Lines) and Elk Grove (E-Trans) also have several bus stops within the unincorporated county right-of-way.

On February 1, 2011 the County Board of Supervisors authorized SacDOT to enter into a formal maintenance agreement with RT to provide a framework for the partnership between SacDOT and RT which outlines cost sharing related to the installation, accessibility and maintenance of the RT bus stops in the unincorporated County. The “Bus Stop Installation and Maintenance Agreement” was executed by the County Board of Supervisors on March 16, 2011, and became effective on July 1, 2011. A copy of this agreement is attached in **BSTP Appendix 5**.

SacDOT initiated this Bus Stop Transition Plan (BSTP) supplement to establish the needs, policies, priorities and practices for implementing bus stop accessibility improvements within the unincorporated county right-of-way and to enhance transit as an alternative transportation choice in the unincorporated county. SacDOT began working on this ADABSTP in March 2011, with the award of a state sponsored Environmental Justice and Community-Based Transportation Planning Grant. A goal of this BSTP is to further optimize the pedestrian experience when utilizing public bus stops by providing safe and useable facilities for all pedestrians, and to assure compliance with all federal, state, and local regulations and standards.

Section 2: Public Participation

SacDOT encouraged public participation during the development of this BSTP supplement. This process included persons with disabilities and those representing disability service organizations. The outreach included the following components:

- **Advisory Groups:** Two advisory committees, the Sacramento County Disability Advisory Commission Physical Access Subcommittee (DAC) and the Regional Transit Mobility Advisory Council (MAC). Both groups contain members of the disabled community. The

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DAC and MAC have monthly public meetings and are highly recognized for their advocacy to encourage access for the disabled community.

- Website: A project website was created to disseminate information about the project: <http://www.msa2.saccounty.net/transportation/Pages/Project-BusStopADA.aspx> This website describes the project scope of work and an invitation for the public ridership to participate with their feedback.
- SacDOT Ridership Survey: SacDOT provided an online ridership survey to determine ridership trends and to learn of any accessibility issues that some riders may be having at specific bus stops. See **BSTP Appendix 8** for a summary of the Website Ridership Survey results.
- Press Release: SacDOT published a Press Release to cover the project kick-off and invite the public to our online ridership survey to help identify needs, see **BSTP Appendix 8**.
- Outreach to RT Staff to gather bus stop location information.

Section 3: Inventory Efforts

SacDOT conducted a needs assessment survey for 1,029 bus stop locations in the unincorporated Sacramento County. These site surveys were developed for each bus stop location by a trained accessibility surveyor. The needs assessment for many bus stop locations were obtained from Google Street View, often requiring an actual site visit for confirmation to measure slopes or distances. A one page summary was prepared for each bus stop location showing the needs identified in the survey. The summary sheet includes: the bus stop location; the unique bus stop number assigned by Regional Transit; ADA infrastructure upgrade needs; approximate contracted costs to provide those needs; and photos were taken of the bus stop from the “bus arriving perspective” and from the “bus departing” perspective (see **BSTP Appendix 8** for samples of these “Bus Stop Site Surveys”). These improvement needs and associated costs for each bus stop location were tabulated and prioritized in a spreadsheet to create a master database that can be sorted and searched (see **BSTP Appendix 3**). This inventory of prioritized bus stop needs will be used for developing future bus stop improvement projects. However, each bus stop survey of needs and cost estimates are for preliminary planning and budgetary purposes only. A more thorough future site visit and formal design will be conducted for each bus stop when funding for upgrade improvements becomes available.

SacDOT obtained from RT their current master database spreadsheet of all active bus stops for all bus service providers that operate in the unincorporated county. The spreadsheet contains a total of 1,029 active bus stop locations in the unincorporated County. Eighteen of those active stops are served by E-Trans (City of Elk Grove) and three stops are served by Folsom Stage Lines (City of Folsom). The remaining 1,008 stops are served by RT. The total bus stops will fluctuate in the future as some routes get modified, and some inactive stops will become active, and vice versa.

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Occasionally new stops are also created with a previously un-assigned bus stop number. Whether a bus stop is active or inactive, it will always retain its unique identifying bus stop number. This BSTP will be updated on an as-needed basis for all active bus stops in the unincorporated County. Any newly constructed or reconstructed bus stops will be built to all applicable local, state and federal accessibility codes and standards.

Section 4: Bus Stop Accessibility Standards

County Street Improvement Standards:

The County Board of Supervisors approved an update to the Street Improvement Standards on September 15, 2009. These Street Improvement Standards include three primary “classes” of street cross sections, Class A, B, and C streets. The distinctions of these street classes are briefly noted below:

- Class A Street: Street with concrete curb, gutter and sidewalk.
- Class B Street: Same as Class A with no sidewalk.
- Class C Street: Total paved street width (32’ or 36’) with no curb, gutter or sidewalk.

The 2009 Street Improvement Standards provide a bus turn out for a new Class A Street. However, that detail provides for a 125’ long by 7’ deep bus turnout that may not be appropriate for retrofitting most existing bus stops, due to the impacts to the existing adjoining properties and driveways. Therefore this Bus Stop Transition Plan introduces additional improvement details for retrofitting bus stops on Class A, B, and C Streets to provide ADA compliance while also minimizing the impacts to existing adjoining properties. These new bus stop details for Class A, B, and C Streets are provided in **Appendix 4** of this BSTP supplement. These street improvement details will be formally incorporated into the County Street Improvement Standards when they are next updated.

Section 5: Steps To Compliance

This BSTP supplement addresses those items that are unique to bus stop accessibility within the unincorporated County. This BSTP supplement addresses the following four aspects of accessibility:

Step 1: Identify physical obstacles in the public entity's facilities that limit the accessibility of its programs or activities to individuals with disabilities;

Step 2: Describe in detail the methods that will be used to make the facilities accessible;

Step 3: Specify the schedule for taking the steps necessary to achieve compliance with this section and, if the time period of the transition plan is longer than one year, identify steps that will

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be taken during each year of the transition period; and

Step 4: Describe the grievance procedure process.

Each of these four steps will be addressed in greater detail below as they pertain to the bus stops within the unincorporated county.

Step 1. Identify physical obstacles in the public entity's facilities that limit the accessibility of its programs or activities to individuals with disabilities.

In order to fully determine what constitutes being an obstacle at a bus stop, it is important to understand what infrastructure is required in order to provide accessibility with regards to the 2010 ADA Standards and California Title 24. If the required infrastructure is not present, or not adequate in size or slope gradient, then that would constitute an obstacle. The following are key highlights of bus stop requirements taken from the “2010 ADA Standards for Accessible Design”:

810 Transportation Facilities

810.1 General. *Transportation facilities shall comply with 810.*

810.2.1 Surface. *Bus stop boarding and alighting areas shall have a firm, stable surface.*

810.2.2 Dimensions. *Bus boarding and alighting areas shall provide a clear length of 96 inches (2440 mm), measured perpendicular to the curb or vehicle roadway edge, and a clear width of 60 inches (1525 mm), measured parallel to the vehicle roadway. Public entities shall ensure that the construction of bus boarding and alighting areas comply with 810.2.2, to the extent the construction specifications are within their control.*

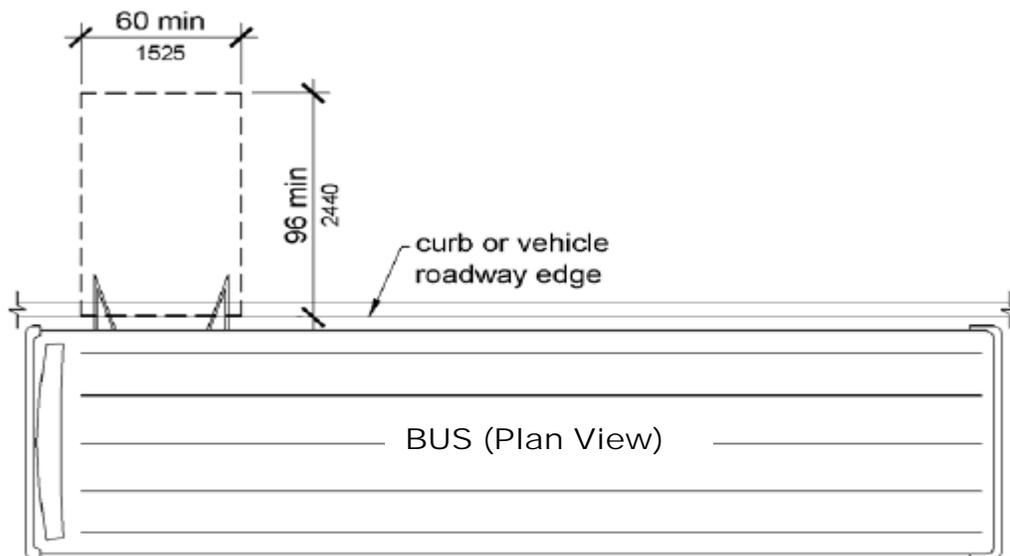


Figure 810.2.2 Dimensions of Bus Boarding and Alighting Areas

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810.2.3 Connection. *Bus stop boarding and alighting areas shall be connected to streets, sidewalks, or pedestrian paths by an accessible route complying with 402.*

810.2.4 Slope. *Parallel to the roadway, the slope of the bus stop boarding and alighting area shall be the same as the roadway, to the maximum extent practicable. Perpendicular to the roadway, the slope of the bus stop boarding and alighting area shall not be steeper than 1:48.*

(Note: Per 218.4 Bus Shelters. Where provided, bus shelters shall comply with 810.3 below)

810.3 Bus Shelters. *Bus shelters shall provide a minimum clear floor or ground space complying with 305 entirely within the shelter. Bus shelters shall be connected by an accessible route complying with 402 to a boarding and alighting area complying with 810.2.*

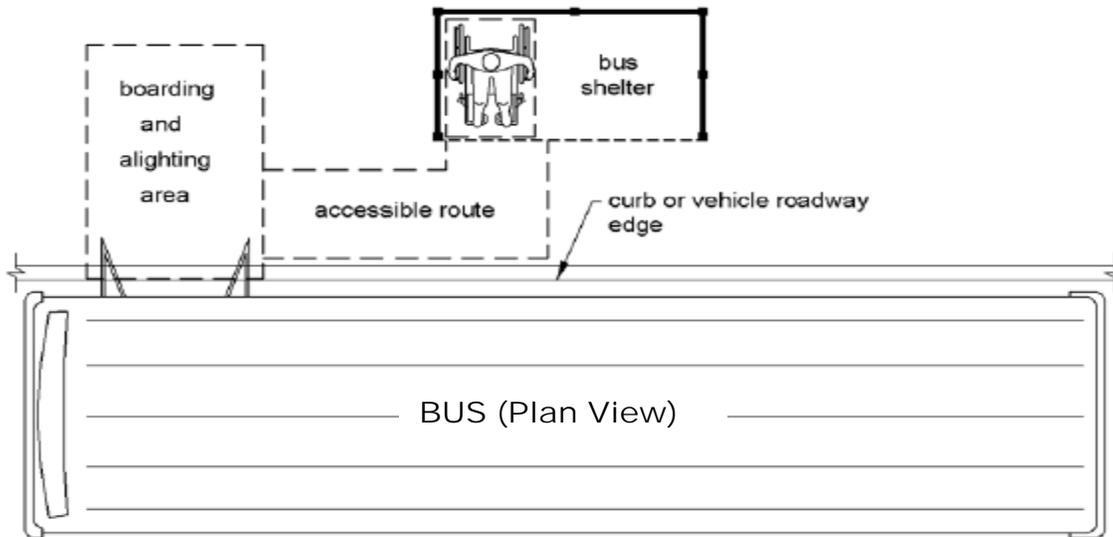


Figure 810.3 Bus Shelters

In addition to the above requirements, the “**California Building Code, Title 24, Section 1121B.2.1 Bus stops and terminals**” has additional criteria. Specifically CBC Section 1121B.2 requires that “*newly constructed bus stop pads must provide a square curb surface between the pad and road or other detectable warning in accordance with Section 1133B.8.5.*”

Also, **CBC Title 24 Section 1121B.2.1** states that the, “*Bus stop pads shall be at the same slope as the roadway in the direction parallel to roadway, and maximum one unit vertical in 50 units horizontal (2-percent slope) perpendicular to roadway*”. This slope is slightly more stringent than the 2010 ADA Standards which allows a slope not steeper than 1:48 (2.08%). The more stringent of federal, state or county codes will govern the construction of any bus stop within the unincorporated County.

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In addition to the bus stop infrastructure requirements noted above, SacDOT also identified missing sidewalk segments along Class A Streets, such that if the missing section were in-filled, then it would provide at least one accessible path from the bus stop to an adjacent intersection. If that accessible path led to an intersection that did not have curb ramps, then costs for both the sidewalk infill and the curb ramp upgrades were included in the one page summary for that proposed bus stop upgrade project to provide accessibility.

Step 2. Describe in detail the methods that will be used to make the facilities accessible

Fifty-five (5.3%) of the 1,029 surveyed bus stops were found to be ADA compliant. These compliant stops have a 5' x 8' boarding area with compliant slope gradients, a vertical curb fronting the boarding area, and are served by an accessible path of travel to at least one adjacent accessible intersection.

In cases where bus stops were determined to need a new 5' x 8' boarding pad, and the location lacked sufficient existing right-of-way to provide one, then the cost of additional right-of-way acquisition was included in the project cost for providing a compliant boarding pad. Existing bus stops often have an adequate boarding pad area, however it may require replacement to meet the desired slope gradient perpendicular to the curb. The replacement of the boarding pad may also require a section of adjoining sidewalk to be removed and replaced to accommodate a transition zone for the new variation to the adjusted boarding pad slope and curb height.

Current street improvement standards require vertical (Type 2) curbs on all Class A streets except on the following streets where rolled curb is allowed: 32' streets (Minor Residential); 38' streets (Primary Residential); cul-de-sacs; and certain instances of sidewalk infill less than 300' in length.

As noted above, the CBC Title 24 dictates the use of either a vertical curb or a detectable warning surface at the traffic edge of the 8' x 5' boarding pad. Many existing bus stops are on streets constructed with rolled curbs, therefore a Type 2 vertical curb improvements will need to be constructed. The rolled (Type 1A) curb and adjacent sidewalk will be removed to provide for a vertical curb (Type 2) and a sidewalk transition zone match the adjacent curb height while maintaining all other compliant slope gradients. The vertical curb and sidewalk transition details are shown in the new street improvement standards in **BSTP Appendix 4**.

For bus stops on Class C streets where sidewalks (or curbing) are not included in the standard street cross-section, a bus stop concrete boarding pad will be positioned at the edge of pavement with a detectable warning surface set into the concrete at the traffic edge of the boarding pad, as shown in **BSTP Appendix 4**.

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Step 3. Specify the schedule for taking the steps necessary to achieve compliance with this section and, if the time period of the transition plan is longer than one year, identify steps that will be taken during each year of the transition period.

To properly schedule anticipated accessibility improvements, each bus stop was first prioritized based on seven different scoring categories. This complete list was then sorted based on priority, then the highest scores were segmented into groups to match anticipated budget funding projection for that particular fiscal year. The process outlining the prioritization, anticipated funding sources and the scheduling of improvements is further detailed below:

Prioritization of Accessibility Improvements

The first step in determining a multi-year schedule for compliance of the 1,029 total bus stops was to prioritize them based on points accumulated in seven important scoring categories. Total points were assigned to each bus stop based on that bus stop's applicability to each category (see **BSTP Appendix 1**). The bus stops with the highest total points will rank higher on the priority list and will be the first considered for accessibility improvements when funding is available. When bus stops share the same overall priority score, the total daily passenger load for each stop was used to break the tie.

When SacDOT receives a request from someone who is unable to access a specific bus stop location in the unincorporated county, and requests improvements be provided, then that bus stop will be placed at the top of the priority list with the other highest ranking locations for consideration in the next improvement project. This is that same prioritization system that SacDOT has used with curb ramps since the 2005 Transition Plan was approved and it has proven to be a very reasonable practice. Otherwise, the seven scoring categories for determining priority ranking are as follows:

Scoring Categories

Transit Center: RT has eight major Transit Centers, these are high traffic bus stops where passengers transfer from one route to another. If a bus stop is a designated Transit Center it is given five points.

Corridor Route: Sacramento County has identified 14 commercial corridors (see **BSTP Appendix 7**) where an effort is being made to foster economic development and urban revitalization by improving the overall quality of the corridor and the communities that they serve. If a bus stop is within these corridor routes it was given 4 points.

RT Ridership: SacDOT obtained recent ridership data from RT that provided the average daily counts of the total passengers that got on and off at each specific bus stop from all the routes that serve that stop. The daily ridership count averages covered the period from April

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1, 2012 through June 16, 2012. The ridership score was based on the total “on and off” counts at each stop, and the stops were separated into five scoring categories of 4, 3, 2, 1, or 0 points. The following table describes the ridership scoring category:

Points Allocated To A Bus Stop Based On Average Daily Passengers Getting On or Off		
Points Allocated	Total Passengers Getting On and Off Per Day	Qualifying Number of Bus Stops
4 (Top 25% of total use)	Between 935 and 252	13
3 (Next 25% of total use)	Between 251 and 71	59
2 (Next 25% of total use)	Between 70 and 30	155
1 (Next 25% of total use)	Between 29 and 0.5	718
0	Less than 0.5	84

Hospital, Medical or Skilled Nursing Facility: Bus stops located within a reasonably short distance from a hospital, medical or a skilled nursing facility were given 4 points.

Retirement Facility: Bus stops within a reasonably short distance from a retirement facility were given a score of three points.

Schools and Government Offices: Any bus stop that is within a reasonably short distance of a K-12 school, college or government offices was given three points.

Other Trip Generators: This wide ranging category addresses those bus stops that served “other” trip generation destinations such as neighborhood parks, commercial businesses, and/or strip malls. Stops serving these other trip generating locations were given two points. Larger malls such as Sunrise, Florin, Arden Fair, etc. were given up to six points based on the very high concentration of adjoining “trip generators”.

Funding Sources of the Accessibility Improvements

There are a number of existing and potential programs and funding sources for capital improvement projects included in the ADA Capital Implementation Plan. These programs are described in this section.

On-Going Capital Improvement Programs

These programs are operated by or coordinated with SacDOT on an on-going, annual basis. The extent of funding levels may be fixed or may vary yearly. These programs include the following:

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1) Curb, Gutter and Sidewalk Maintenance Program

The traditional SacDOT Curb, Gutter and Sidewalk Maintenance Program identifies curbs, gutters and sidewalks that are in need of repair or replacement and develops a priority list for their inclusion into the maintenance program. The Curb, Gutter and Sidewalk Maintenance Program has two facets: permanent replacement and temporary repair. This program will now also incorporate the accessibility improvements that are within the project limits of this maintenance program. A similar priority system has been used for this maintenance program, based upon such factors as severity of damage, the amount of pedestrian traffic, and the proximity to schools, parks, bus stops and hospitals.

2) Elderly and Disabled Accessibility Project

Traditionally the Elderly and Disabled Accessibility Project primarily replaces curb, gutter and sidewalk with new curb ramps. These curb ramps and sidewalks are often installed near schools and hospitals, at driveways, or at other curb, gutter and sidewalk locations where accessibility for persons with disabilities could be improved. Accessible pedestrian signals are also installed as part of this program. With the implementation of this ADABSTP, bus stop accessibility improvements will also be considered. Improvement needs are often determined by constituents' requests. The Elderly and Disabled Accessibility Project is funded by Measure A funds at a current funding level of \$300,000 per year.

3) Caltrans Construction Projects

Caltrans construction and renovation of roadways and facilities along State highways within the unincorporated County typically includes new curb ramps and other accessibility-related improvements. While SacDOT does not directly manage these projects, it coordinates locations and details of the work with Caltrans. The two highest ranked bus stops based on the ridership “on and off” usage is the Watt Avenue overpass that spans I-80 and is therefore in the jurisdiction of Caltrans.

4) Private Developer Construction Projects adjacent to the County Right-of Way

There is typically private construction throughout the unincorporated County that has direct impact on improvements within the SacDOT right-of-way. As a condition of the approval of a building permit, Developers are typically required to construct or improve the property frontage which includes the sidewalk, curb ramps and bus stops directly adjacent to the subject property.

5) Regional Transit Cost Sharing of Bus Stop Facility Improvements

As discussed previously, RT and SacDOT entered into a shared “Bus Stop Installation and Maintenance Agreement” that became effective on July 1, 2011. This agreement (Section 1.F.(4.) “Cost Allocation for Concrete Pads”) states that: “*RT will reimburse COUNTY for 50% of COUNTY’s actual costs for installation of concrete pads in sidewalks for passenger*

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waiting areas and in COUNTY streets for bus loading areas if the scope of work and cost estimate were approved in advance by RT.”

6) State and Federal Grants and Other Funding Sources

SacDOT has been successful with many state and federal grant awards. SacDOT will aggressively pursue grant and other funding opportunities where bus stop ADA improvements would be a strong candidate for funding award.

7) Sacramento County Capital Improvement Projects

These programs include on-going Sacramento County capital improvement projects and maintenance programs, as well as specific projects and funding sources allocated in the SacDOT seven year Transportation Improvement Plan (TIP).

Scheduling of Accessibility Improvements

The total estimated cost of improvements identified in this BSTP supplement is \$36,561,150 to bring all bus stops within the unincorporated county to current accessibility standards. Some of these improvements will be accomplished through the on-going capital improvement programs. In addition, the County and RT propose to collectively allocate funding specifically to construct the highest priority improvements in the BSTP. An investment of \$250,000 in the 2013/14 fiscal year, and \$500,000 in subsequent years will allow accessibility improvements to be made that impact 25% of the ridership within two years and 50% of the ridership after four years. The County and RT may also jointly pursue funding from state and federal grant programs to leverage available local funding sources and make an even larger investment to complete additional priority investments.

This ADABSTP will be a dynamic plan due to a changing population base of customers, changing neighborhood demands, bus route modifications, and other factors. Likewise, it is highly probable that some of these bus stop enhancements will occur as a result of other unrelated street and sidewalk improvement projects, developer in-fill, or other means. As such, SacDOT will anticipate updating this BSTP and funding plan every 4-7 years, or on an as needed basis.

Step 4. Describe the grievance procedure process.

Grievance Procedure

The existing SacDOT ADA Transition Plan already has a grievance process in place that is fully applicable to bus stops in the unincorporated county. Likewise, SacDOT maintains a web page (<http://www.sacdot.com/Pages/ADADocumentsandResources.aspx>) that contains information regarding the grievance process, including forms that can be printed out for submitting a grievance, or to make comments and recommendations. That existing webpage and the existing grievance procedure will be used for bus stops. As stated previously, when SacDOT receives a special

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request, or a grievance from someone unable to access a specific bus stop location in the unincorporated county, then accessibility improvements for that bus stop location will be placed at the top of the priority list with the other highest ranking locations for consideration in the next available improvement project.

RT has a similar ADA grievance process in place that extends into other jurisdictions where they provide service beyond the unincorporated county. The RT webpage that provides the resources for grievance related issues is <http://www.sacrt.com/adacomplaintsprocess.stm>.

When SacDOT receives a bus stop related special request or a grievance for ADA improvements in the unincorporated county it will be shared with RT. And conversely, when RT receives a special request or grievance in the unincorporated county, then RT will share that with SacDOT. This will sharing of priorities will allow both jurisdictions to better manage and coordinate the overall priority list and accessibility projects.

The Bus Stop Grievance Procedure for ADA accessibility will follow the same process as what has been in place in the ADA Transition Plan since 2005. The procedures and Grievance Form are duplicated here below in the **BSTP Appendix 9**:

Grievance Procedures and Instructions

Step 1: File a Grievance Form

The complainant should fill out the ADA Complaint / Grievance Form shown below, giving all of the information requested. The ADA Complaint / Grievance Form should be filed in writing with the SacDOT Program Access Coordinator within 60 days of the alleged disability-related discrimination. A copy of the ADA Complaint / Grievance Form shall be forwarded by the Program Access Coordinator to the Chief of the Disability Compliance Office. Upon request, reasonable accommodations will be provided in completing the form, or alternative formats of the form will be provided. The ADA Complaint / Grievance Procedure and Form may be obtained from and sent to the Sacramento County Department of Transportation, Program Access Coordinator, 906 G Street, Suite 510, Sacramento, CA., 95814, Telephone: (916) 874-6291, TTY (916) 875-7105.

Step 2: An Investigation is Conducted

A notice of receipt shall be mailed to the complainant by registered mail within five days of the receipt of the complaint or grievance, and the SacDOT Program Access Coordinator or another authorized representative shall begin an investigation into the merits of the complaint within 60 days. If necessary, the SacDOT Program Access Coordinator or another authorized representative may contact the complainant directly to obtain additional facts or documentation relevant to the grievance. If the complainant alleges misconduct on the part of the SacDOT Program Access Coordinator, another authorized representative may be appointed by the Director of SacDOT to undertake the investigation if the allegations can be

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substantiated. If the complainant does not wish to be contacted personally, he/she should indicate it on the ADA Complaint / Grievance Form.

After the grievance is received, the complaint shall be brought before the ADA Oversight Committee, co-chaired by the Chief of the Disability Compliance Office and the SacDOT Program Access Coordinator. The co-chairs shall meet on an ad-hoc committee to resolve the grievance.

Step 3: A Written Decision is Prepared and Forwarded to the Complainant

The Chief of the Disability Compliance Office and the SacDOT Program Access Coordinator shall prepare a written decision, after full consideration of the grievance merits, no later than 75 days following the receipt of the grievance. If the complaint alleges misconduct on the part of the SacDOT Program Access Coordinator, another authorized representative may be appointed by the Director of SacDOT to prepare the written decision if the allegations can be substantiated. A copy of the written decision shall be mailed to the complainant by registered mail no later than five days after preparation of the written decision. A copy of the written decision also shall be mailed to the Chief of the Disability Compliance Office.

Step 4: A Complainant May Appeal the Decision

If the complainant is dissatisfied with the written decision, the complainant may file a written appeal with either, at the complainant's option, the Director of SacDOT, or with the Chief of the Disability Compliance Office, no later than 30 days from the date of the mailing of the decision. The appeal must contain a statement of the reasons why the complainant is dissatisfied with the written decision, and must be signed by the complainant, or by someone authorized to sign on the complainant's behalf. A notice of receipt shall be mailed to the complainant by registered mail within five days of the receipt of the appeal. The appeal reviewers, consisting of the Chief of the Disability Compliance Office, and the SacDOT Program Access Coordinator, shall act upon the appeal no later than 60 days after receipt, and a copy of the appeal reviewers' written decision shall be mailed to the complainant by registered mail no later than five days after preparation of the decision. The decision of the appeal reviewer shall be final. A copy of the written decision also shall be mailed to the Chief of the Disability Compliance Office.

The SacDOT Program Access Coordinator, the Director of SacDOT, and the Chief of the Disability Compliance Office shall maintain the confidentiality of all files and records relating to grievances filed, unless disclosure is authorized or required by law. Any retaliation, coercion, intimidation, threat, interference or harassment for the filing of a grievance, or used to restrain a complainant from filing, is prohibited and should be reported immediately to the Chief of the Disability Compliance Office.

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Section 6: Conclusion

This BSTP supplement identifies \$36,561,150 of improvements that will improve public transit accessibility alternatives for people living or visiting within the unincorporated Sacramento County. This Plan, along with the existing County Pedestrian Master Plan, ADA Transition Plan and Bicycle Master Plan will help SacDOT continue to provide quality municipal services and transportation facilities on which our neighborhoods and businesses depend.

BSTP Appendix 1 - Bus Stop Prioritization Score Card

Bus Stop #	Overall Priority Rank	On Street	Dir.	Far/ Near	At Street	Ave. Daily Riders (On & Off)	Transit Centers (5 or 0)	Corridor Route (4 or 0)	RT Ridership (4,3,2,or1)	Medical or Nursing Ctr. (4, 0)	Retire-ment Facil. (3, 0)	Schools & Gov't Office (3,0)	Other Trip Generators (2,0)	Total Priority Score
377	14	WATT/MANLOVE LRS	SB	F	BAY 2	262		4	4	4			2	14
378	73	WATT/MANLOVE LRS	EB	F	BAY 1	5		4	1	4			2	11
447	573	HAZEL AVE	NB	F	NIMBUS RD	0			0				4	4
453	230	FAIR OAKS BLVD	EB	N	MARSHALL AVE	16		4	1				2	7
454	829	KILZER AVE	NB	F	DUDLEY BLVD	1			1				2	3
459	830	NORTHROP AVE	WB	F	EVELYN LN	1			1				2	3
479	412	FORCUM AVE	SB	F	DUDLEY BLVD	4			1			3	2	6
495	709	A/C ROAD	EB	N	PATROL ROAD	8			1				2	3
496	360	KIEFER BLVD	WB	F	MAYHEW RD	18			1			3	2	6
512	831	DUDLEY BLVD	EB	F	FORCUM AVE	1			1				2	3
520	408	PEACEKEEPER WAY	WB	F	DUDLEY BLVD	4			1			3	2	6
544	409	DUDLEY BLVD	SB	N	PEACEKEEPER WAY	4			1			3	2	6
563	1	ORANGE GROVE AVE	WB	F	COLLEGE OAK DR	587	5		4	4	3	3	2	21
565	808	ORANGE GROVE AVE	WB	N	PASADENA AVE	2			1				2	3
566	231	AUBURN BLVD	WB	F	ORANGE GROVE AVE	15		4	1				2	7
567	201	AUBURN BLVD	WB	F	JONES RD	30		4	1				2	7
599	878	FORCUM AVE	NB	N	DUDLEY BLVD	0			0			3		3
629	685	JAMES WAY	WB	F	SKVARLA AVE	9			1				2	3
630	13	AUBURN BLVD	EB	F	WATT AVE	544		4	4	4			2	14
631	46	AUBURN BLVD	EB	F	ANNADALE LN	74		4	3		3		2	12
632	91	AUBURN BLVD	EB	F	PASADENA AVE	25		4	1		3		2	10
633	258	AUBURN BLVD	EB	F	BERESFORD WAY	10		4	1				2	7
634	302	AUBURN BLVD	EB	F	NORRIS AVE	4		4	1				2	7
636	240	ORANGE GROVE AVE	EB	F	AUBURN BLVD	13		4	1				2	7
637	511	ORANGE GROVE AVE	EB	F	PASADENA AVE	34			2				2	4
638	698	ORANGE GROVE AVE	EB	F	SYCAMORE AVE	8			1				2	3
639	749	COLLEGE OAK DR	NB	F	ORANGE GROVE AVE	5			1				2	3
640	7	COLLEGE OAK DR	NB	F	MYRTLE AVE	416		4	4			3	4	15
641	122	COLLEGE OAK DR	NB	N	AUBURN BLVD	75		4	3				2	9
642	69	AUBURN BLVD	EB	F	MADISON AVE	32		4	2		3		2	11
643	21	AUBURN BLVD	EB	N	KOHLER RD	130		4	4		3		2	13
644	217	AUBURN BLVD	EB	F	HEMLOCK ST	22		4	1				2	7
645	144	AUBURN BLVD	EB	N	PALM AVE	50		4	2				2	8
646	246	AUBURN BLVD	EB	F	GARFIELD AVE	12		4	1				2	7
647	133	AUBURN BLVD	EB	N	GARFIELD AVE	66		4	2				2	8
650	508	JAMES WAY	WB	F	DUDLEY BLVD	35			2				2	4
651	178	DUDLEY AVE	SB	F	PALM AVE	21			1	4	3			8
660	986	RIO LINDA BLVD	SB	F	SAVOY AVE	1			1					1
665	9	COLLEGE OAK DR	SB	F	ORANGE GROVE AVE	218			4	4	3	2	2	15

BSTP Appendix 1 - Bus Stop Prioritization Score Card

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666	785	PASADENA AVE	SB	F	WINDING WY	3			1				2	3
667	553	PASADENA AVE	SB	F	PASADENA AVE	4			1			3		4
674	177	MISSION AVE	SB	F	ENGLE RD	23			1	4		3		8
675	189	MISSION AVE	SB	N	WHITNEY AVE	44			2			3	2	7
676	976	WHITNEY AVE	WB	F	CONCETTA WAY	2			1					1
677	417	WHITNEY AVE	WB	N	MULHOLLAND WY	2			1			3	2	6
678	381	WHITNEY AVE	WB	F	NORRIS AVE	10			1			3	2	6
679	766	WHITNEY AVE	WB	F	BECCERRA WAY	4			1				2	3
680	933	WHITNEY AVE	WB	F	MONTCLAIRE ST	7			1					1
681	241	WHITNEY AVE	WB	N	WATT AVE	13		4	1				2	7
721	203	ELVERTA RD	EB	N	WATT AVE	28		4	1				2	7
723	1019	DRY CREEK RD	NB	F	ASCOT AVE	0			0					0
724	786	DRY CREEK RD	NB	F	C ST	3			1				2	3
725	855	DRY CREEK RD	NB	N	E ST	1			1				2	3
726	253	DRY CREEK RD	NB	N	G ST	11			1			6		7
727	447	ELKHORN BLVD	WB	F	DRY CREEK RD	17			1				4	5
728	963	ELVERTA RD	WB	F	BELLINGRATH DR	3			1					1
730	823	ELKHORN BLVD	WB	F	6TH ST	2			1				2	3
731	645	ELKHORN BLVD	WB	F	RIO LINDA BLVD	13			1				2	3
732	943	2ND ST	NB	F	JUBILEE WAY	5			1					1
733	964	2ND ST	NB	F	WITHINGTON AVE	3			1					1
734	987	2ND ST	NB	F	DOLPHIN WY	1			1					1
735	955	2ND ST	NB	N	M ST	4			1					1
736	733	M ST	EB	F	RIO LINDA	6			1				2	3
737	856	M ST	EB	F	5TH ST	1			1				2	3
738	832	M ST	EB	F	6TH ST	1			1				2	3
739	743	M ST	EB	N	FRONT ST	6			1				2	3
740	397	M ST	EB	F	8TH ST	6			1			3	2	6
741	767	OAK LANE	NB	F	9TH ST	4			1				2	3
743	546	10TH ST	NB	N	Q ST	6			1			3		4
744	965	Q ST	WB	N	8TH ST	3			1					1
745	966	Q ST	WB	N	7TH ST	3			1					1
746	824	Q ST	WB	N	FRONT ST	2			1				2	3
747	717	RIO LINDA BLVD	NB	F	Q ST	7			1				2	3
748	977	RIO LINDA BLVD	NB	F	U ST	2			1					1
749	909	RIO LINDA BLVD	NB	F	U ST	0			0				2	2
750	825	RIO LINDA BLVD	NB	F	DELANO ST	2			1				2	3
751	833	ELVERTA RD	EB	F	EL MODENA AVE	1			1				2	3
752	1021	ELVERTA RD	EB	N	EL VERANO AVE	0			0					0

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754	1022	ELVERTA RD	EB	F	PALLADAY RD	0			0					0
755	988	ELVERTA RD	EB	F	16TH ST	1			1					1
756	978	ELVERTA RD	EB	F	CHERRY BROOK DR	2			1					1
757	989	ELVERTA RD	EB	N	BELLINGRATH DR	1			1					1
758	750	ELVERTA RD	EB	F	DUTCH HAVEN BLVD	5			1				2	3
759	809	ELVERTA RD	EB	F	28TH ST	2			1				2	3
760	566	ELVERTA RD	EB	F	ELECTRIC AVE	1			1			3		4
761	152	ELVERTA RD	WB	F	WATT AVE	42		4	2				2	8
762	897	ELVERTA RD	WB	F	28TH ST	0			0				2	2
763	751	ELVERTA RD	WB	F	DUTCH HAVEN BLVD	5			1				2	3
764	990	ELVERTA RD	WB	F	RIVERGREEN DR	1			1					1
765	1023	ELVERTA RD	WB	N	16TH ST	0			0					0
766	991	ELVERTA RD	WB	F	16th St	1			1					1
767	1024	ELVERTA RD	WB	F	PALLADAY RD	0			0					0
768	1025	ELVERTA RD	WB	N	EL VERANO AVE	0			0					0
769	992	ELVERTA RD	WB	F	EL MODENA AVE	1			1					1
770	567	ELVERTA RD	WB	F	RIO LINDA BLVD	1			1			3		4
771	810	RIO LINDA BLVD	SB	N	DELANO ST	2			1				2	3
772	910	RIO LINDA BLVD	SB	N	U ST	0			0				2	2
773	993	RIO LINDA BLVD	SB	N	U ST	1			1					1
774	768	Q ST	EB	F	RIO LINDA BLVD	4			1				2	3
775	979	Q ST	EB	F	FRONT ST	2			1					1
776	994	Q ST	EB	F	7TH ST	1			1					1
777	967	Q ST	EB	F	8TH ST	3			1					1
778	549	10TH ST	SB	F	Q ST	5			1			3		4
779	811	10TH ST	SB	N	O ST	2			1				2	3
780	787	OAK LANE	SB	N	9TH ST	3			1				2	3
781	391	M ST	WB	N	8TH ST	7			1			3	2	6
782	744	M ST	WB	F	FRONT ST	6			1				2	3
783	898	M ST	WB	F	6TH ST	0			0				2	2
784	857	M ST	WB	N	5TH ST	1			1				2	3
785	769	M ST	WB	N	RIO LINDA	4			1				2	3
786	944	2ND ST	SB	F	M ST	5			1					1
787	980	2ND ST	SB	F	DOLPHIN WY	2			1					1
788	956	2ND ST	SB	F	WITHINGTON AVE	4			1					1
789	938	2ND ST	SB	F	MONTICELLO AVE	6			1					1
791	636	ELKHORN BLVD	EB	F	RIO LINDA BLVD	15			1				2	3
792	826	ELKHORN BLVD	EB	N	6TH ST	2			1				2	3
794	346	ELKHORN BLVD	EB	N	DRY CREEK RD	34			2				4	6

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796	259	DRY CREEK RD	SB	N	G ST	10			1			6		7
797	670	DRY CREEK RD	SB	N	E ST	11			1				2	3
798	770	DRY CREEK RD	SB	N	C ST	4			1				2	3
799	1020	DRY CREEK RD	SB	N	ASCOT AVE	0			0					0
1074	340	BUTANO DR	WB	F	PARK TOWNE CIR	43			2				4	6
1075	671	BUTANO DR	SB	N	COTTAGE WAY	11			1				2	3
1076	372	BUTANO DR	SB	N	COTTAGE WAY	12			1		3		2	6
1077	779	COTTAGE WAY	WB	F	KINCAID WAY	4			1				2	3
1091	195	ETHAN WAY	NB	N	COTTAGE WAY	35			2			3	2	7
1092	728	ETHAN WAY	NB	F	KEITH WAY	7			1				2	3
1093	925	ETHAN WAY	NB	F	DELMA WAY	16			1					1
1094	576	EL CAMINO AVE	EB	F	ETHAN WAY	30			1				2	3
1101	718	ARDEN WAY	EB	F	ETHAN WAY	7			1				2	3
1102	361	ARDEN WAY	EB	F	HOWE AVE	18			1			3	2	6
1103	600	ARDEN WAY	EB	F	BELL ST	23			1				2	3
1104	382	ARDEN WAY	EB	F	CLINTON RD	10			1			3	2	6
1105	446	ARDEN WAY	EB	F	FULTON AVE	21		4	1					5
1106	771	ARDEN WAY	EB	F	BERKSHIRE WAY	4			1				2	3
1107	148	ARDEN WAY	EB	F	MORSE AVE	46			2	4			2	8
1108	145	ARDEN WAY	EB	F	PROFESSIONAL DR	49			2	4			2	8
1109	102	ARDEN WAY	EB	F	WATT AVE	8		4	1			3	2	10
1110	834	ARDEN WAY	EB	F	LA PLAYA WAY	1			1				2	3
1111	995	ARDEN WAY	EB	F	LA SIERRA DR	1			1					1
1112	858	ARDEN WAY	EB	F	ESPERANZA DR	1			1				2	3
1113	672	ARDEN WAY	EB	F	EASTERN AVE	11			1				2	3
1114	996	ARDEN WAY	EB	F	SURITA ST	1			1					1
1115	859	ARDEN WAY	EB	N	MISSION AVE	1			1				2	3
1116	1018	ARDEN WAY	EB	N	PALOMA AVE	0			0					0
1127	45	COTTAGE WAY	EB	F	MORSE AVE	82			3	4		3	2	12
1128	759	COTTAGE WAY	EB	F	KINCAID WAY	5			1				2	3
1129	352	BUTANO DR	NB	F	COTTAGE WAY	22			1		3		2	6
1130	699	BUTANO DR	NB	F	COTTAGE WAY	8			1				2	3
1131	186	BUTANO DR	EB	F	PARK TOWNE CIR	74			3				4	7
1139	12	FAIR OAKS BLVD	SB	N	TOPP CT	65		4	2	4	3		2	15
1141	247	FAIR OAKS BLVD	SB	F	EL CAMINO AVE	12		4	1				2	7
1143	325	FAIR OAKS BLVD	SB	F	SHELFIELD DR	1		4	1				2	7
1144	428	FAIR OAKS BLVD	SB	F	GARFIELD AVE	0		4	0				2	6
1146	326	FAIR OAKS BLVD	SB	F	WESTMINSTER CT	1		4	1				2	7
1147	100	ARDEN WAY	WB	F	FAIR OAKS BLVD	10		4	1		3		2	10

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1148	981	ARDEN WAY	WB	F	PARK PLACE DR	2			1					1
1149	835	ARDEN WAY	WB	F	MISSION AVE	1			1				2	3
1150	860	ARDEN WAY	WB	F	PLUTO WAY	1			1				2	3
1151	646	ARDEN WAY	WB	F	EASTERN AVE	13			1				2	3
1152	911	ARDEN WAY	WB	F	CATHAY WAY	0			0				2	2
1153	997	ARDEN WAY	WB	N	MAPLE GLEN RD	1			1					1
1154	836	ARDEN WAY	WB	N	LA PLAYA WAY	1			1				2	3
1155	63	ARDEN WAY	WB	F	WATT AVE	43		4	2			3	2	11
1156	219	ARDEN WAY	WB	F	PROFESSIONAL DR	21			1	4			2	7
1157	605	ARDEN WAY	WB	F	MORSE AVE	21			1				2	3
1158	772	ARDEN WAY	WB	F	AVONDALE AVE	4			1				2	3
1159	223	ARDEN WAY	WB	F	FULTON AVE	18		4	1				2	7
1160	647	ARDEN WAY	WB	N	WRIGHT ST	13			1				2	3
1161	623	ARDEN WAY	WB	F	BELL ST	18			1				2	3
1162	659	ARDEN WAY	WB	F	HOWE AVE	12			1				2	3
1175	356	FAIR OAKS BLVD	NB	F	ARDEN WAY	20			1		3		2	6
1176	837	FAIR OAKS BLVD	NB	F	MARIONE DR	1			1				2	3
1177	879	FAIR OAKS BLVD	NB	F	AINSLEY CT	0			0		3			3
1178	899	FAIR OAKS BLVD	NB	F	GARFIELD AVE	0			0				2	2
1180	900	FAIR OAKS BLVD	NB	F	CLAREMONT RD	0			0				2	2
1181	26	FAIR OAKS BLVD	NB	F	VAN ALSTINE AVE	78		4	3	4			2	13
1182	50	FAIR OAKS BLVD	NB	F	KENNETH AVE	42		4	2	4			2	12
1183	303	FAIR OAKS BLVD	NB	F	FRONTIER WAY	4		4	1				2	7
1205	501	SUNRISE BLVD	SB	F	MADISON AVE	39			2				2	4
1206	788	SUNRISE BLVD	SB	F	WILDRIDGE DR	3			1				2	3
1207	660	SUNRISE BLVD	SB	F	SUNSET AVE	12			1				2	3
1210	838	DEWEY DR	SB	N	WINDING WAY	1			1				2	3
1213	293	DEWEY DR	SB	F	COYLE AVE	5			1	4			2	7
1214	719	DEWEY DR	SB	F	TEMPLETON DR	7			1				2	3
1215	752	DEWEY DR	SB	F	MADISON AVE	5			1				2	3
1216	559	DEWEY DR	SB	F	OAKCREST AVE	2			1			3		4
1217	425	DEWEY DR	SB	F	WILL ROGERS DR	1			1			3	2	6
1218	560	DEWEY DR	SB	F	MORAGA DR	2			1			3		4
1220	968	WINDING WY	WB	F	BARRETT RD	3			1					1
1222	780	JAN DR	SB	F	WINDING WY	4			1				2	3
1223	563	JAN DR	SB	N	RAMPART DR	2			1			3		4
1224	564	JAN DR	SB	N	OTIS CT	2			1			3		4
1225	998	CALIFORNIA AVE	SB	N	LINCOLN AVE	1			1					1
1226	286	CALIFORNIA AVE	SB	F	FAIR OAKS BLVD	6		4	1				2	7

BSTP Appendix 1 - Bus Stop Prioritization Score Card

Bus Stop #	Overall Priority Rank	On Street	Dir.	Far/ Near	At Street	Ave. Daily Riders (On & Off)	Transit Centers (5 or 0)	Corridor Route (4 or 0)	RT Ridership (4,3,2,or1)	Medical or Nursing Ctr. (4, 0)	Retire-ment Facil. (3, 0)	Schools & Gov't Office (3,0)	Other Trip Generators (2,0)	Total Priority Score
1227	476	CALIFORNIA AVE	SB	F	CASITA AVE	0			0			3	2	5
1228	477	CALIFORNIA AVE	SB	N	SUTTER AVE	0			0			3	2	5
1229	969	CALIFORNIA AVE	SB	N	GRANT AVE	3			1					1
1230	839	CALIFORNIA AVE	SB	N	LANDIS AVE	1			1				2	3
1231	999	CALIFORNIA AVE	SB	N	STANLEY AVE	1			1					1
1232	1000	CALIFORNIA AVE	SB	N	HOMESWEET WAY	1			1					1
1233	901	CALIFORNIA AVE	SB	N	PALM DR	0			0				2	2
1234	902	PALM DR	WB	F	PANAMA AVE	0			0				2	2
1236	350	MARCONI AVE	WB	F	LA VISTA AVE	25			1			3	2	6
1237	734	MARCONI AVE	WB	F	GARFIELD AVE	6			1				2	3
1238	613	MARCONI AVE	WB	F	LINDEN LN	20			1				2	3
1239	503	MARCONI AVE	WB	F	WALNUT AVE	37			2				2	4
1240	648	MARCONI AVE	WB	F	ROOT AVE	13			1				2	3
1241	363	MARCONI AVE	WB	F	MISSION AVE	17			1			3	2	6
1242	735	MARCONI AVE	WB	F	GREENWOOD AVE	6			1				2	3
1243	584	MARCONI AVE	WB	F	EASTERN AVE	26			1				2	3
1244	478	MARCONI AVE	WB	F	NORRIS AVE	0			0			3	2	5
1245	389	MARCONI AVE	WB	F	BECCERRA WAY	8			1			3	2	6
1246	720	MARCONI AVE	WB	N	MONTCLAIRE ST	7			1				2	3
1247	58	MARCONI AVE	WB	F	WATT AVE	68		4	2			3	2	11
1248	789	MARCONI AVE	WB	F	ASHBOURNE DR	3			1				2	3
1249	721	MARCONI AVE	WB	F	LACY LN	7			1				2	3
1250	577	MARCONI AVE	WB	F	CALDERWOOD LN	29			1				2	3
1251	86	MARCONI AVE	WB	F	FULTON AVE	49		4	2				4	10
1252	642	MARCONI AVE	WB	F	WRIGHT ST	14			1				2	3
1253	729	MARCONI AVE	WB	F	BELL ST	7			1				2	3
1263	483	MARCONI AVE	EB	F	DARWIN ST	58			2				2	4
1264	637	MARCONI AVE	EB	F	PYRAMID WAY	15			1				2	3
1265	912	MARCONI AVE	EB	N	BELL ST	0			0				2	2
1266	592	MARCONI AVE	EB	F	WRIGHT ST	25			1				2	3
1267	87	MARCONI AVE	EB	F	FULTON AVE	45		4	2				4	10
1268	585	MARCONI AVE	EB	N	CORABEL LN	26			1				2	3
1269	700	MARCONI AVE	EB	F	SANTA PAULA CT	8			1				2	3
1270	812	MARCONI AVE	EB	N	ASHBOURNE DR	2			1				2	3
1271	83	MARCONI AVE	EB	F	WATT AVE	76		4	3		3			10
1272	736	MARCONI AVE	EB	N	MONTCLAIRE ST	6			1				2	3
1273	371	MARCONI AVE	EB	N	BECCERRA WAY	13			1			3	2	6
1274	383	MARCONI AVE	EB	N	NORRIS AVE	10			1			3	2	6
1275	790	MARCONI AVE	EB	F	EASTERN AVE	3			1				2	3

BSTP Appendix 1 - Bus Stop Prioritization Score Card

Bus Stop #	Overall Priority Rank	On Street	Dir.	Far/ Near	At Street	Ave. Daily Riders (On & Off)	Transit Centers (5 or 0)	Corridor Route (4 or 0)	RT Ridership (4,3,2,or1)	Medical or Nursing Ctr. (4, 0)	Retire-ment Facil. (3, 0)	Schools & Gov't Office (3,0)	Other Trip Generators (2,0)	Total Priority Score
1276	773	MARCONI AVE	EB	N	GREENWOOD AVE	4			1				2	3
1277	357	MARCONI AVE	EB	F	MISSION AVE	20			1			3	2	6
1278	661	MARCONI AVE	EB	F	ROOT AVE	12			1				2	3
1279	515	MARCONI AVE	EB	F	WALNUT AVE	31			2				2	4
1280	606	MARCONI AVE	EB	F	GUNN LN	21			1				2	3
1281	400	MARCONI AVE	EB	F	GARFIELD AVE	6			1			3	2	6
1282	375	MARCONI AVE	EB	N	LA VISTA AVE	11			1			3	2	6
1284	774	PALM DR	EB	N	PANAMA AVE	4			1				2	3
1286	1001	CALIFORNIA AVE	NB	N	MERRY LN	1			1					1
1287	1002	CALIFORNIA AVE	NB	F	STANLEY AVE	1			1					1
1288	1003	CALIFORNIA AVE	NB	N	LANDIS AVE	1			1					1
1289	982	CALIFORNIA AVE	NB	F	GRANT AVE	2			1					1
1290	880	CALIFORNIA AVE	NB	N	SUTTER AVE	0			0			3		3
1292	317	CALIFORNIA AVE	NB	F	FAIR OAKS BLVD	2		4	1				2	7
1293	1004	CALIFORNIA AVE	NB	N	LINCOLN AVE	1			1					1
1294	571	JAN DR	NB	N	JASPAR CT	1			1			3		4
1295	565	JAN DR	NB	N	RAMPART DR	2			1			3		4
1296	827	WINDING WY	EB	F	JAN DR	2			1				2	3
1298	1005	WINDING WY	EB	N	BARRETT RD	1			1					1
1299	840	DEWEY DR	NB	F	WINDING WAY	1			1				2	3
1300	568	DEWEY DR	NB	F	PAPAYA DR	1			1			3		4
1301	881	DEWEY DR	NB	F	WILL ROGERS DR	0			0			3		3
1302	1006	DEWEY DR	NB	F	HILLHURST DR	1			1					1
1304	673	DEWEY DR	NB	F	MADISON AVE	11			1				2	3
1305	841	DEWEY DR	NB	F	PALM AVE	1			1				2	3
1341	331	SAN JUAN AVE	SB	F	MADISON AVE	1			1	4			2	7
1342	686	SAN JUAN AVE	SB	N	GAIL WAY	9			1				2	3
1343	204	SAN JUAN AVE	SB	N	PHEASANT RD	28			1	4			2	7
1344	682	SAN JUAN AVE	SB	N	SUNSET AVE	10			1				2	3
1346	701	SAN JUAN AVE	SB	F	HEATHER RD	8			1				2	3
1348	775	FAIR OAKS BLVD	WB	F	SAN JUAN AVE	4			1				2	3
1349	522	FAIR OAKS BLVD	WB	F	GROVER LN	15			1		3			4
1350	614	FAIR OAKS BLVD	WB	F	HOLLISTER AVE	20			1				2	3
1351	103	FAIR OAKS BLVD	WB	N	MARSHALL AVE	7			1	4	3		2	10
1353	248	FAIR OAKS BLVD	WB	F	MILES LANE	12		4	1				2	7
1354	96	FAIR OAKS BLVD	WB	F	CALIFORNIA AVE	14		4	1			3	2	10
1355	161	FAIR OAKS BLVD	WB	F	WAYSIDE LN	39		4	2				2	8
1356	136	FAIR OAKS BLVD	SB	N	GIBBONS DR	62		4	2				2	8
1357	66	FAIR OAKS BLVD	SB	F	ENGLE RD	35		4	2		3		2	11

BSTP Appendix 1 - Bus Stop Prioritization Score Card

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1358	92	FAIR OAKS BLVD	SB	F	GRANT AVE	24		4	1			3	2	10
1359	17	FAIR OAKS BLVD	SB	N	LANDIS AVE	29		4	1	4		3	2	14
1360	71	FAIR OAKS BLVD	SB	N	ANGELINA AVE	15		4	1	4			2	11
1361	164	FAIR OAKS BLVD	SB	F	ROBERTSON AVE	37		4	2				2	8
1363	172	FAIR OAKS BLVD	SB	F	MARCONI AVE	32		4	2				2	8
1365	304	EL CAMINO AVE	WB	F	FAIR OAKS BLVD	4		4	1				2	7
1366	620	EL CAMINO AVE	WB	F	EL TONAS WAY	19			1				2	3
1367	649	EL CAMINO AVE	WB	F	GARFIELD AVE	13			1				2	3
1368	188	EL CAMINO AVE	WB	N	GUNN LN	45			2			3	2	7
1369	776	EL CAMINO AVE	WB	F	WALNUT AVE	4			1				2	3
1370	710	EL CAMINO AVE	WB	N	LAURENCE AVE	8			1				2	3
1371	485	EL CAMINO AVE	WB	F	LANDWOOD WAY	52			2				2	4
1372	615	EL CAMINO AVE	WB	F	MISSION AVE	20			1				2	3
1373	364	EL CAMINO AVE	WB	F	GREENWOOD AVE	17			1			3	2	6
1374	199	EL CAMINO AVE	WB	F	EASTERN AVE	31			2			3	2	7
1375	791	EL CAMINO AVE	WB	F	AVALON DR	3			1				2	3
1376	792	EL CAMINO AVE	WB	F	IONE ST	3			1				2	3
1377	631	EL CAMINO AVE	WB	F	ANDRADE WAY	16			1				2	3
1378	42	EL CAMINO AVE	WB	F	BUTANO DR	100		4	4				4	12
1379	36	EL CAMINO AVE	WB	N	WATT AVE	162		4	4				4	12
1380	127	EL CAMINO AVE	WB	F	WATT AVE	27		4	1				4	9
1381	277	EL CAMINO AVE	WB	F	YORKTOWN AVE	7		4	1				2	7
1382	694	EL CAMINO AVE	WB	F	MORSE AVE	9			1				2	3
1383	598	EL CAMINO AVE	WB	F	MORSE AVE	25			1				2	3
1384	722	EL CAMINO AVE	WB	F	BELPORT LN	7			1				2	3
1385	484	EL CAMINO AVE	WB	F	PARKWOOD DR	58			2				2	4
1386	272	EL CAMINO AVE	WB	F	FULTON AVE	8		4	1				2	7
1387	358	EL CAMINO AVE	WB	F	WRIGHT ST	20			1			3	2	6
1388	438	EL CAMINO AVE	WB	F	BELL ST	72			3				2	5
1389	895	EL CAMINO AVE	WB	F	HOWE AVE	31			2					2
1421	153	WHITNEY AVE	EB	F	WATT AVE	42		4	2				2	8
1422	760	WHITNEY AVE	EB	F	MONTCLAIRE ST	5			1				2	3
1423	723	WHITNEY AVE	EB	F	BECCERRA WAY	7			1				2	3
1424	376	WHITNEY AVE	EB	F	NORRIS AVE	11			1			3	2	6
1425	351	WHITNEY AVE	EB	F	EASTERN AVE	23			1			3	2	6
1427	957	WHITNEY AVE	EB	F	CONCETTA WAY	4			1					1
1428	193	MISSION AVE	NB	F	WHITNEY AVE	40			2			3	2	7
1437	813	PASADENA AVE	NB	N	WINDING WY	2			1				2	3
1438	294	MYRTLE AVE	EB	F	AUBURN BLVD	5		4	1				2	7

BSTP Appendix 1 - Bus Stop Prioritization Score Card

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1439	687	MYRTLE AVE	EB	N	COLLEGE OAK DR	9			1				2	3
1455	386	EL CAMINO AVE	EB	F	HOWE AVE	9			1			3	2	6
1456	601	EL CAMINO AVE	EB	F	BELL ST	23			1				2	3
1457	420	EL CAMINO AVE	EB	F	GWEN DR	1			1			3	2	6
1458	6	EL CAMINO AVE	EB	F	FULTON AVE	76		4	3	4		3	2	16
1459	753	EL CAMINO AVE	EB	F	PARKWOOD DR	5			1				2	3
1460	593	EL CAMINO AVE	EB	F	MEADOWBROOK RD	25			1				2	3
1461	730	EL CAMINO AVE	EB	F	MORSE AVE	7			1				2	3
1462	745	EL CAMINO AVE	EB	F	DRAYTON DR	6			1				2	3
1463	38	EL CAMINO AVE	EB	F	YORKTOWN AVE	147		4	4				4	12
1464	35	EL CAMINO AVE	EB	F	WATT AVE	180		4	4				4	12
1465	683	EL CAMINO AVE	EB	F	BUTANO DR	10			1				2	3
1466	662	EL CAMINO AVE	EB	F	ANDRADE WAY	12			1				2	3
1467	802	EL CAMINO AVE	EB	F	IONE ST	3			1				2	3
1468	680	EL CAMINO AVE	EB	F	AVALON DR	11			1				2	3
1469	190	EL CAMINO AVE	EB	F	EASTERN AVE	43			2			3	2	7
1470	398	EL CAMINO AVE	EB	N	GREENWOOD AVE	6			1			3	2	6
1471	487	EL CAMINO AVE	EB	F	MISSION AVE	49			2				2	4
1472	724	EL CAMINO AVE	EB	N	LANDWOOD WAY	7			1				2	3
1473	746	EL CAMINO AVE	EB	F	VIA CAMINO AVE	6			1				2	3
1474	695	EL CAMINO AVE	EB	F	UPHAM CT	9			1				2	3
1475	514	EL CAMINO AVE	EB	F	WALNUT AVE	32			2				2	4
1476	612	EL CAMINO AVE	EB	F	GUNN RD	21			1				2	3
1477	632	EL CAMINO AVE	EB	F	GARFIELD AVE	16			1				2	3
1478	711	EL CAMINO AVE	EB	F	EL TONAS WAY	8			1				2	3
1481	207	FAIR OAKS BLVD	NB	F	PALM DR	27		4	1				2	7
1482	175	FAIR OAKS BLVD	NB	N	ROBERTSON AVE	30		4	2				2	8
1483	53	FAIR OAKS BLVD	NB	F	STANLEY AVE	30		4	2	4			2	12
1484	128	FAIR OAKS BLVD	NB	F	LANDIS AVE	1		4	1				4	9
1485	27	FAIR OAKS BLVD	NB	F	GRANT AVE	21		4	1		3	3	2	13
1486	119	FAIR OAKS BLVD	NB	F	SHIRLEY AVE	81		4	3				2	9
1488	160	FAIR OAKS BLVD	EB	F	WAYSIDE LN	40		4	2				2	8
1489	455	FAIR OAKS BLVD	EB	F	CALIFORNIA AVE	10		4	1					5
1490	278	FAIR OAKS BLVD	EB	F	QUEENSTON CT	7		4	1				2	7
1492	95	FAIR OAKS BLVD	EB	N	CHAMPLAIN LN	15			1	4	3		2	10
1493	586	FAIR OAKS BLVD	EB	F	HOLLISTER AVE	26			1				2	3
1494	547	FAIR OAKS BLVD	EB	F	DELL RD	6			1		3			4
1495	702	SAN JUAN AVE	NB	F	FAIR OAKS BLVD	8			1				2	3
1497	684	SAN JUAN AVE	NB	F	WINDING WAY	10			1				2	3

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1499	594	SAN JUAN AVE	NB	F	SUNSET AVE	25			1				2	3
1500	267	SAN JUAN AVE	NB	F	FAIRWAY TWO AVE	9			1	4			2	7
1501	842	SAN JUAN AVE	NB	F	GAIL WAY	1			1				2	3
1519	147	AUBURN BLVD	WB	F	MORSE AVE	47		4	2				2	8
1520	237	FULTON AVE	SB	F	AUBURN BLVD	14		4	1				2	7
1521	138	FULTON AVE	SB	F	EDISON AVE	55		4	2				2	8
1522	260	FULTON AVE	SB	F	POPE AVE	10		4	1				2	7
1523	279	FULTON AVE	SB	F	CARSON WAY	7		4	1				2	7
1524	55	FULTON AVE	SB	F	MARCONI AVE	91		4	3				4	11
1525	444	FULTON AVE	SB	F	MARCONI AVE	22		4	1					5
1526	213	FULTON AVE	SB	N	EL CAMINO AVE	23		4	1				2	7
1527	341	FULTON AVE	SB	F	EL CAMINO AVE	39		4	2					6
1528	465	FULTON AVE	SB	F	HERNANDO RD	3		4	1					5
1529	165	FULTON AVE	SB	F	COTTAGE WAY	37		4	2				2	8
1530	170	FULTON AVE	SB	F	ALTA ARDEN EXWY	33		4	2				2	8
1531	149	FULTON AVE	SB	F	ARDEN WAY	46		4	2				2	8
1532	154	FULTON AVE	SB	F	WITTKOP WAY	42		4	2				2	8
1533	120	FULTON AVE	SB	F	HURLEY WY	80		4	3				2	9
1534	18	FULTON AVE	SB	F	LARKSPUR LN	22		4	1	4	3		2	14
1535	345	FULTON AVE	SB	F	NORTHROP AVE	36		4	2					6
1536	72	MUNROE ST	SB	F	SIERRA BLVD	10		4	1	4			2	11
1537	342	MUNROE ST	SB	F	FAIR OAKS BLVD	39		4	2					6
1549	173	MUNROE ST	NB	F	FAIR OAKS BLVD	31		4	2				2	8
1550	312	FULTON AVE	NB	F	SIERRA BLVD	3		4	1				2	7
1551	52	FULTON AVE	NB	F	NORTHROP AVE	31		4	2	4			2	12
1552	93	FULTON AVE	NB	F	LARKSPUR LN	24		4	1		3		2	10
1553	123	FULTON AVE	NB	F	HURLEY WY	71		4	3				2	9
1554	214	FULTON AVE	NB	F	MAISON WAY	23		4	1				2	7
1555	162	FULTON AVE	NB	F	ARDEN WAY	39		4	2				2	8
1556	168	FULTON AVE	NB	F	ALTA ARDEN EXWY	34		4	2				2	8
1557	205	FULTON AVE	NB	F	COTTAGE WAY	28		4	1				2	7
1558	466	FULTON AVE	NB	F	LOMA VISTA DR	3		4	1					5
1559	141	FULTON AVE	NB	F	EL CAMINO AVE	54		4	2				2	8
1560	249	FULTON AVE	NB	F	ELVYRA WAY	12		4	1				2	7
1561	215	FULTON AVE	NB	N	MARCONI AVE	23		4	1				2	7
1562	16	FULTON AVE	NB	F	MARCONI AVE	82		4	3			3	4	14
1564	327	FULTON AVE	NB	F	CARSON WAY	1		4	1				2	7
1565	273	FULTON AVE	NB	N	POPE AVE	8		4	1				2	7
1566	139	FULTON AVE	NB	F	EDISON AVE	55		4	2				2	8

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1596	261	AUBURN BLVD	EB	F	FULTON AVE	10		4	1				2	7
1597	206	AUBURN BLVD	EB	N	MORSE AVE	28		4	1				2	7
1598	295	AUBURN BLVD	EB	N	WATT AVE	5		4	1				2	7
1611	429	65TH ST	WB	F	53RD AVE	0		4	0				2	6
1612	30	65TH ST	WB	N	SKY PKWY	637		4	4				4	12
1613	70	65TH ST	WB	N	FLORIN RD	30		4	2			3	2	11
1646	180	MORSE AVE	SB	F	COTTAGE WAY	13			1	4	3			8
1648	887	MORSE AVE	SB	F	GERALD AVE	55			2					2
1649	891	MORSE AVE	SB	F	ARDEN WAY	37			2					2
1650	640	MORSE AVE	SB	F	TRUSSEL WAY	15			1				2	3
1651	712	MORSE AVE	SB	F	BERKSHIRE WAY	8			1				2	3
1652	192	MORSE AVE	SB	N	HURLEY WAY	41			2		3		2	7
1653	554	MORSE AVE	SB	F	HAMPTON RD	4			1			3		4
1654	384	MORSE AVE	SB	N	NORTHROP AVE	10			1			3	2	6
1655	421	NORTHROP AVE	WB	F	JONAS AVE	1			1			3	2	6
1656	338	NORTHROP AVE	WB	F	FULTON AVE	46		4	2					6
1657	621	NORTHROP AVE	WB	F	CARRO DR	19			1				2	3
1658	650	NORTHROP AVE	WB	F	DORNAJO WAY	13			1				2	3
1659	579	NORTHROP AVE	WB	F	BELL ST	28			1				2	3
1660	348	NORTHROP AVE	WB	N	HOWE AVE	28			1		3		2	6
1662	62	NORTHROP AVE	EB	F	HOWE AVE	49			2	4	3		2	11
1663	506	NORTHROP AVE	EB	F	BELL ST	36			2				2	4
1664	624	NORTHROP AVE	EB	F	PARK	18			1				2	3
1665	617	NORTHROP AVE	EB	F	CARRO DR	20			1				2	3
1666	337	NORTHROP AVE	EB	F	FULTON AVE	48		4	2					6
1668	1007	NORTHROP AVE	EB	F	JONAS AVE	1			1					1
1669	731	MORSE AVE	NB	F	NORTHROP AVE	7			1				2	3
1670	535	MORSE AVE	NB	F	AMBERWOOD RD	9			1			3		4
1671	198	MORSE AVE	NB	F	HURLEY WAY	33			2		3		2	7
1672	634	MORSE AVE	NB	F	BERKSHIRE WAY	16			1				2	3
1673	628	MORSE AVE	NB	F	TRUSSEL WAY	17			1				2	3
1674	130	MORSE AVE	NB	F	ARDEN WAY	72			3			3	2	8
1675	931	MORSE AVE	NB	F	VIA GRANDE	9			1					1
1677	108	MORSE AVE	NB	F	COTTAGE WAY	4			1	4	3		2	10
1844	212	STOCKTON BLVD	SB	F	15TH AVE	24		4	1				2	7
1845	171	STOCKTON BLVD	SB	F	17TH AVE	33		4	2				2	8
1846	166	STOCKTON BLVD	SB	F	PARKER AVE	35		4	2				2	8
1847	218	STOCKTON BLVD	SB	N	ROOSEVELT AVE	22		4	1				2	7
1852	169	STOCKTON BLVD	SB	N	SOUTHWEST AVE	34		4	2				2	8

BSTP Appendix 1 - Bus Stop Prioritization Score Card

Bus Stop #	Overall Priority Rank	On Street	Dir.	Far/ Near	At Street	Ave. Daily Riders (On & Off)	Transit Centers (5 or 0)	Corridor Route (4 or 0)	RT Ridership (4,3,2,or1)	Medical or Nursing Ctr. (4, 0)	Retire-ment Facil. (3, 0)	Schools & Gov't Office (3,0)	Other Trip Generators (2,0)	Total Priority Score
1853	318	STOCKTON BLVD	SB	F	GORDON DR	2		4	1				2	7
1854	81	STOCKTON BLVD	SB	F	MCPAHON DR	124		4	4				2	10
1860	32	STOCKTON BLVD	SB	F	65TH ST	257		4	4				4	12
1861	209	STOCKTON BLVD	SB	F	LINDALE AVE	26		4	1				2	7
1884	118	STOCKTON BLVD	NB	F	65TH ST	89		4	3				2	9
2018	111	FLORIN MALL	NB	F	66TH AVE	3		4	1		3		2	10
2019	268	FLORIN MALL	NB	N	FLORIN RD	9		4	1				2	7
2020	39	FLORIN RD	WB	N	STOCKTON BLVD	130		4	4				4	12
2021	124	FLORIN RD	WB	N	FLORIN MALL DR	71		4	3				2	9
2023	262	SKY PKWY	NB	F	65TH ST	10		4	1				2	7
2024	11	SKY PKWY	NB	F	65TH ST	114		4	4			3	4	15
2025	104	SKY PKWY	WB	N	KLINGON CT	6		4	1		3		2	10
2026	98	N PARKWAY	NB	F	SKY PKWY	13		4	1		3		2	10
2027	525	STEINER RD	NB	N	53RD AVE	13			1			3		4
2028	934	STEINER RD	NB	N	AUSTIN WAY	7			1					1
2029	347	STEINER RD	NB	N	51ST ST	30			1			3	2	6
2030	374	STEINER RD	NB	F	SITTON WAY	12			1			3	2	6
2037	541	CENTER PKWY	SB	F	GOYA PKWY	7			1			3		4
2038	540	CENTER PKWY	SB	N	SOUTH PKWY	8			1			3		4
2154	945	CENTER PKWY	NB	F	EAST PKWY	5			1					1
2155	843	CENTER PKWY	NB	F	H PKWY	1			1				2	3
2160	134	FRANKLIN BLVD	NB	F	MEADOWGATE DR	65		4	2				2	8
2161	448	FOREST PARKWAY	EB	F	FRANKLIN BLVD	14		4	1					5
2162	542	FOREST PARKWAY	WB	F	CENTER PARKWAY	7			1			3		4
2163	445	FOREST PARKWAY	WB	N	FRANKLIN BLVD	22		4	1					5
2165	40	FLORIN RD	EB	F	FLORIN MALL DR	119		4	4				4	12
2166	430	STOCKTON BLVD	NB	F	FLORIN RD	0		4	0				2	6
2168	339	FLORIN MALL	SB	F	FLORIN RD	44		4	2					6
2169	28	FLORIN MALL	SB	F	66TH AVE	17		4	1		3	3	2	13
2170	280	ORANGE AVE	EB	F	FLORIN MALL DR	7		4	1				2	7
2171	296	ORANGE AVE	EB	N	CITRUS AVE	5		4	1				2	7
2172	844	CHANDLER DR	EB	N	LINDALE DR	1			1				2	3
2174	281	FRANKLIN BLVD	NB	N	A PARKWAY	7		4	1				2	7
2217	582	FRUITRIDGE RD	WB	F	44TH ST	27			1				2	3
2218	616	FRUITRIDGE RD	WB	F	ENRICO BLVD	20			1				2	3
2291	625	FRUITRIDGE RD	EB	F	FRANKLIN BLVD	18			1				2	3
2292	607	FRUITRIDGE RD	EB	F	M L KING BL	21			1				2	3
2293	587	FRUITRIDGE RD	EB	F	MENDOCINO BLVD	26			1				2	3
2294	688	FRUITRIDGE RD	EB	F	42ND ST	9			1				2	3

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2295	608	FRUITRIDGE RD	EB	F	44TH ST	21			1				2	3
2296	754	FRUITRIDGE RD	EB	N	BONNIE MAE WAY	5			1				2	3
2297	651	FRUITRIDGE RD	EB	N	SAMPSON BLVD	13			1				2	3
2311	114	FLORIN RD	WB	F	65TH ST	238		4	3				2	9
2312	56	FLORIN RD	WB	F	EAST PKWY	84		4	3				4	11
2313	37	FLORIN RD	WB	F	BOWLING DR	151		4	4				4	12
2372	23	FLORIN RD	EB	F	FRANKLIN BLVD	108		4	4		3		2	13
2373	85	FLORIN RD	EB	F	BOWLING DR	63		4	2				4	10
2374	31	FLORIN RD	EB	F	EAST PKWY	386		4	4				4	12
2375	431	STOCKTON BLVD	NB	F	ELSIE AVE	0		4	0				2	6
2376	456	STOCKTON BLVD	NB	F	SUNCOUNTRY LN	10		4	1					5
2377	181	GERBER RD	EB	F	STOCKTON BLVD	12		4	1		3			8
2378	460	GERBER RD	EB	F	PAR PARKWY	6		4	1					5
2379	221	GERBER RD	EB	F	PALMER HOUSE DR	20		4	1				2	7
2381	443	POWER INN RD	NB	F	GERBER RD	24		4	1					5
2382	920	SCOTTSDALE DR	WB	F	POWER INN RD	26			1					1
2383	689	PALMER HOUSE DR	NB	F	SCOTTSDALE DR	9			1				2	3
2385	527	PALMER HOUSE DR	NB	F	PICKOI CIR	12			1			3		4
2387	543	LINDALE DR	WB	F	PALMER HOUSE DR	7			1			3		4
2388	558	LINDALE DR	WB	F	RIMROCK DR	3			1			3		4
2389	970	LINDALE DR	WB	F	FORT PITT WAY	3			1					1
2390	793	LINDALE DR	WB	F	SAHARA CT	3			1				2	3
2391	457	STOCKTON BLVD	SB	F	66TH AVE	8		4	1					5
2392	432	ORANGE AVE	EB	N	CITRUS AVE	0		4	0				2	6
2393	467	ORANGE AVE	EB	N	FLORIN MALL DR	3		4	1					5
2395	794	LINDALE DR	EB	F	SAHARA CT	3			1				2	3
2396	971	LINDALE DR	EB	F	DEL CORONADO WAY	3			1					1
2397	555	LINDALE DR	EB	F	EWING WAY	4			1			3		4
2398	532	LINDALE DR	EB	F	PALMER HOUSE DR	10			1			3		4
2399	524	PALMER HOUSE DR	SB	N	PICKOI CIR	13			1			3		4
2400	725	PALMER HOUSE DR	SB	F	PICKOI CIR	7			1				2	3
2401	761	SCOTTSDALE DR	EB	F	PALMER HOUSE DR	5			1				2	3
2402	919	SCOTTSDALE DR	EB	N	POWER INN RD	27			1					1
2403	923	POWER INN RD	SB	F	MAC FADDEN DR	17			1					1
2404	222	GERBER RD	WB	F	POWER INN RD	19		4	1				2	7
2405	451	GERBER RD	WB	F	PALMER HOUSE DR	12		4	1					5
2406	468	GERBER RD	WB	F	PALMER HOUSE DR	3		4	1					5
2407	452	GERBER RD	WB	F	WHISPER WILLOW DR	12		4	1					5
2408	183	STOCKTON BLVD	SB	F	GERBER RD	6		4	1		3			8

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2409	238	STOCKTON BLVD	SB	F	MASSIE CT 1	14		4	1				2	7
2587	681	M L KING JR BLVD	SB	F	FRUITRIDGE RD	11			1				2	3
2588	504	M L KING JR BLVD	SB	F	32ND AVE	37			2				2	4
2589	512	M L KING JR BLVD	SB	F	41ST AVE	34			2				2	4
2590	505	M L KING JR BLVD	SB	F	44TH AVE	37			2				2	4
2591	703	M L KING JR BLVD	SB	N	47TH AVE	8			1				2	3
2592	167	FRANKLIN BLVD	SB	F	47TH AVE	35		4	2				2	8
2593	305	FRANKLIN BLVD	SB	F	M L KING BLVD	4		4	1				2	7
2594	232	FRANKLIN BLVD	SB	F	52ND AVE	15		4	1				2	7
2595	263	FRANKLIN BLVD	SB	F	TURNBRIDGE DR	10		4	1				2	7
2597	47	FRANKLIN BLVD	NB	F	FLORIN RD	74		4	3		3		2	12
2598	282	FRANKLIN BLVD	NB	F	GREEN TREE DR	7		4	1				2	7
2599	233	FRANKLIN BLVD	NB	F	TURNBRIDGE DR	15		4	1				2	7
2600	234	FRANKLIN BLVD	NB	F	CUNY AVE	15		4	1				2	7
2601	208	FRANKLIN BLVD	NB	F	M L KING BLVD	27		4	1				2	7
2602	297	FRANKLIN BLVD	NB	N	47TH AVE	5		4	1				2	7
2603	509	M L KING JR BLVD	NB	F	47TH AVE	35			2				2	4
2604	499	M L KING JR BLVD	NB	F	44TH AVE	40			2				2	4
2605	903	M L KING JR BLVD	NB	N	41ST AVE	0			0				2	2
2606	663	M L KING JR BLVD	NB	N	32ND AVE	12			1				2	3
2609	588	M L KING JR BLVD	NB	N	23RD ST	26			1				2	3
2662	604	44TH ST	SB	F	14TH AVE	22			1				2	3
2664	580	44TH ST	SB	F	18TH AVE	28			1				2	3
2665	517	44TH ST	SB	F	ROOSEVELT AVE	21			1			3		4
2666	526	23RD AVE	WB	F	44TH ST	13			1			3		4
2667	781	44TH ST	SB	N	25TH ST	4			1				2	3
2670	652	44TH ST	SB	N	FRUITRIDGE RD	13			1				2	3
2671	674	44TH ST	SB	F	FRUITRIDGE RD	11			1				2	3
2672	518	44TH ST	SB	F	IOWA AVE	20			1			3		4
2675	618	44TH ST	SB	F	37TH AVE	20			1				2	3
2676	704	44TH ST	SB	F	39TH AVE	8			1				2	3
2678	726	47TH AVE	EB	F	46TH ST	7			1				2	3
2679	713	44TH ST	SB	F	41ST AVE	8			1				2	3
2680	589	44TH ST	SB	F	44TH AVE	26			1				2	3
2681	578	44TH ST	SB	F	45TH AVE	29			1				2	3
2682	609	47TH AVE	EB	F	47TH ST	21			1				2	3
2683	367	STEINER RD	SB	F	47TH AVE	17			1			3	2	6
2684	392	STEINER RD	SB	F	51ST ST	7			1			3	2	6
2685	927	STEINER RD	SB	F	53RD AVE	12			1					1

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2686	539	STEINER RD	SB	F	53RD AVE	9			1				3	4
2688	182	SKY PKWY	EB	F	6TH PKWY	10		4	1		3			8
2689	65	SKY PKWY	WB	N	KLINGON CT	37		4	2		3		2	11
2690	10	SKY PKWY	SB	N	65TH ST	162		4	4			3	4	15
2691	365	47TH AVE	WB	F	STEINER DR	17			1			3	2	6
2692	482	47TH AVE	WB	F	SAMPSON BLVD	60			2				2	4
2693	638	47TH AVE	WB	F	LAURINE WAY	15			1				2	3
2694	892	44TH ST	NB	F	47TH AVE	36			2					2
2695	690	44TH ST	NB	F	44TH AVE	9			1				2	3
2696	387	44TH ST	NB	N	LEMON HILL AVE	9			1			3	2	6
2699	602	44TH ST	NB	N	39TH AVE	23			1				2	3
2700	355	44TH ST	NB	F	37TH AVE	21			1			3	2	6
2703	528	44TH ST	NB	F	IOWA AVE	12			1			3		4
2704	510	44TH ST	NB	F	FRUITRIDGE RD	35			2				2	4
2706	653	44TH ST	NB	F	SIERRA VISTA AVE	13			1				2	3
2707	516	44TH ST	NB	N	23RD AVE	25			1			3		4
2708	556	44TH ST	NB	N	ROOSEVELT AVE	4			1			3		4
2709	626	44TH ST	NB	N	18TH AVE	18			1				2	3
2710	705	44TH ST	NB	N	ORINDA WAY	8			1				2	3
2711	629	44TH ST	NB	N	14TH AVE	17			1				2	3
2719	675	WINDING WY	EB	F	SUNRISE BLVD	11			1				2	3
2749	131	FOLSOM BLVD	WB	F	LA RIVIERA DR	69		4	2				2	8
2750	254	FOLSOM BLVD	WB	N	NORCADE CIR	11		4	1				2	7
2751	155	FOLSOM BLVD	WB	F	LA QUINTA DR	42		4	2				2	8
2752	15	FOLSOM BLVD	WB	N	MANLOVE RD	112		4	4	4			2	14
2755	5	FOLSOM BLVD	EB	F	MANLOVE RD	34	5	4	2	4			2	17
2756	264	FOLSOM BLVD	EB	F	LA QUINTA DR	10		4	1				2	7
2757	158	FOLSOM BLVD	EB	F	STARFIRE DR	41		4	2				2	8
2795	255	FAIR OAKS BLVD	EB	F	SUNRISE BLVD	11		4	1				2	7
2796	226	FAIR OAKS BLVD	NB	N	CALIFORNIA AVE	17		4	1				2	7
2797	654	SUNRISE BLVD	NB	F	WINDING WAY	13			1				2	3
2798	655	SUNRISE BLVD	NB	F	SUNSET AVE	13			1				2	3
2799	904	SUNRISE BLVD	NB	F	CAVALLI WAY	0			0				2	2
2800	737	SUNRISE BLVD	NB	F	WILDRIDGE DR	6			1				2	3
2824	622	BRADSHAW RD	SB	F	COUNTRY ROADS DR	19			1				2	3
2825	488	BRADSHAW RD	SB	F	OLD PLACER RD	49			2				2	4
2826	583	GOETHE RD	WB	F	BRADSHAW RD	27			1				2	3
2846	905	GOETHE RD	EB	F	BRANCH CTR RD	0			0				2	2
2873	762	KIEFER BLVD	EB	F	S WATT AVE	5			1				2	3

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2874	676	KIEFER BLVD	EB	F	BRYDON WAY	11			1				2	3
2884	335	KIEFER BLVD	EB	F	SOUTHPORT DR	113			4				2	6
2885	479	KIEFER BLVD	EB	F	TALLYHO DR	67			2				2	4
2886	595	KIEFER BLVD	EB	F	HUNTSMAN DR	25			1				2	3
2887	498	KIEFER BLVD	EB	F	TALLYHO DR	41			2				2	4
2888	366	KIEFER BLVD	EB	F	MAYHEW RD	17			1			3	2	6
2889	353	BRANCH CTR	NB	F	AGRICULTURE LN	21			1			3	2	6
2891	413	BRANCH CTR	NB	N	PRESERVATION WAY	3			1			3	2	6
2892	422	BRANCH CTR	NB	F	PRESERVATION WAY	1			1			3	2	6
2903	502	ELKHORN BLVD	WB	F	DIABLO DR	39			2				2	4
2904	706	ELKHORN BLVD	WB	F	CRATER WAY	8			1				2	3
2905	393	ELKHORN BLVD	WB	F	ANDREA BLVD	7			1			3	2	6
2906	795	ELKHORN BLVD	WB	N	HILLSDALE BLVD	3			1				2	3
2907	377	ELKHORN BLVD	WB	F	DON JULIO BLVD	11			1			3	2	6
2908	714	ELKHORN BLVD	WB	F	SPRIG DR	8			1				2	3
2909	590	ELKHORN BLVD	WB	F	BUTTERBALL WAY	26			1				2	3
2910	889	ELKHORN BLVD	WB	F	WALERGA RD	44			2					2
2911	523	ELKHORN BLVD	WB	F	LARCHMONT DR	14			1			3		4
2912	368	ELKHORN BLVD	WB	F	THOMAS DR	15			1			3	2	6
2913	250	ELKHORN BLVD	WB	F	CANTEL WAY	12		4	1				2	7
2923	239	FAIR OAKS BLVD	SB	F	MADISON AVE	14		4	1				2	7
2924	319	FAIR OAKS BLVD	SB	F	WOODLEAF DR	2		4	1				2	7
2925	470	FAIR OAKS BLVD	SB	N	ORANGETREE CT	2		4	1					5
2927	287	FAIR OAKS BLVD	SB	F	SUNSET AVE	6		4	1				2	7
2928	313	FAIR OAKS BLVD	SB	F	MONTE PARK AVE	3		4	1				2	7
2929	458	FAIR OAKS BLVD	SB	N	WINDING WAY	8		4	1					5
2931	433	FAIR OAKS BLVD	SB	N	WINDING WAY	0		4	0				2	6
2932	328	MAIN ST	SB	F	SACRAMENTO ST	1		4	1				2	7
2934	242	FAIR OAKS BLVD	SB	N	CALIFORNIA AVE	13		4	1				2	7
2935	251	FAIR OAKS BLVD	WB	N	SUNRISE BLVD	12		4	1				2	7
2945	436	LUCE AVE	NB	F	PEACEKEEPER WAY	74			3				2	5
2954	174	LA RIVIERA DR	NB	F	FOLSOM BLVD	31		4	2				2	8
2955	1008	LA RIVIERA DR	WB	F	LINDA RIO DR	1			1					1
2956	828	LA RIVIERA DR	WB	F	STANSBERRY WAY	2			1				2	3
2957	738	LA RIVIERA DR	WB	F	ROGUE RIVER DR	6			1				2	3
2958	557	LA RIVIERA DR	WB	F	WATERTON WAY	4			1			3		4
2959	935	LA RIVIERA DR	WB	F	LANIER WAY	7			1					1
2960	668	LA RIVIERA DR	WB	F	WOODMAN WAY	12			1				2	3
2961	603	LA RIVIERA DR	WB	N	WATT AVE	23			1				2	3

BSTP Appendix 1 - Bus Stop Prioritization Score Card

Bus Stop #	Overall Priority Rank	On Street	Dir.	Far/ Near	At Street	Ave. Daily Riders (On & Off)	Transit Centers (5 or 0)	Corridor Route (4 or 0)	RT Ridership (4,3,2,or1)	Medical or Nursing Ctr. (4, 0)	Retire-ment Facil. (3, 0)	Schools & Gov't Office (3,0)	Other Trip Generators (2,0)	Total Priority Score
2962	220	WATT AVE	NB	N	FAIR OAKS BLVD	21		4	1				2	7
2963	715	WATT AVE	NB	F	NORTHROP AVE	8			1				2	3
2964	796	WATT AVE	NB	F	BODEGA CT	3			1				2	3
2965	932	WATT AVE	NB	F	SAN YSIDRO WAY	8			1					1
2966	569	WATT AVE	NB	F	EL RICON WAY	1			1			3		4
3000	677	LA RIVIERA DR	EB	F	WATT AVE	11			1				2	3
3001	803	LA RIVIERA DR	EB	F	WOODMAN WAY	3			1				2	3
3002	958	LA RIVIERA DR	EB	F	LANIER WAY	4			1					1
3003	552	LA RIVIERA DR	EB	F	WATERTON WAY	5			1			3		4
3004	845	LA RIVIERA DR	EB	F	ROGUE RIVER DR	1			1				2	3
3005	641	LA RIVIERA DR	EB	F	PAYETTE DR	15			1				2	3
3006	691	LA RIVIERA DR	EB	N	TUOLUMNE DR	9			1				2	3
3007	306	LA RIVIERA DR	EB	N	FOLSOM BLVD	4		4	1				2	7
3026	747	SUNRISE BLVD	NB	F	COLOMA RD	6			1				2	3
3027	893	SUNRISE BLVD	NB	F	GOLD EXPRESS DR	34			2					2
3028	696	SUNRISE BLVD	NB	F	GOLD COUNTRY BLVD	9			1				2	3
3032	474	FAIR OAKS BLVD	NB	F	WINDING WAY	1		4	1					5
3033	283	FAIR OAKS BLVD	EB	N	WATKINS DR	7		4	1				2	7
3034	112	FAIR OAKS BLVD	NB	N	FAIR OAKS BLVD	3		4	1			3	2	10
3035	459	FAIR OAKS BLVD	NB	N	ORANGE AVE	7		4	1					5
3036	329	FAIR OAKS BLVD	NB	N	MONTE PARK AVE	1		4	1				2	7
3037	307	FAIR OAKS BLVD	NB	F	SUNSET AVE	4		4	1				2	7
3038	74	FAIR OAKS BLVD	NB	N	WESTCAMP RD	5		4	1	4			2	11
3039	288	FAIR OAKS BLVD	NB	F	WOODLEAF DR	6		4	1				2	7
3040	252	FAIR OAKS BLVD	NB	F	MADISON AVE	12		4	1				2	7
3042	804	FAIR OAKS BLVD	NB	F	SUNRISE E WAY	3			1				2	3
3043	805	FAIR OAKS BLVD	NB	N	SUNBONNET DR	3			1				2	3
3049	142	WATT AVE	SB	F	ROSEVILLE RD	53		4	2				2	8
3052	489	WATT AVE	SB	F	ELVERTA RD	49			2				2	4
3054	732	WATT AVE	SB	F	NAVAHO DR	7			1				2	3
3055	379	WATT AVE	SB	F	DAVIDSON DR	11			1			3	2	6
3056	216	WATT AVE	SB	F	U ST	23		4	1				2	7
3057	269	WATT AVE	SB	F	LARCHMONT DR	9		4	1				2	7
3058	289	WATT AVE	SB	F	Q ST	6		4	1				2	7
3059	320	WATT AVE	SB	F	TURNER DR	2		4	1				2	7
3060	137	WATT AVE	SB	F	ELKHORN BLVD	62		4	2				2	8
3061	235	WATT AVE	SB	F	VAN OWEN ST	15		4	1				2	7
3062	256	WATT AVE	SB	F	I ST	11		4	1				2	7
3063	330	WATT AVE	SB	F	DON JULIO BLVD	1		4	1				2	7

BSTP Appendix 1 - Bus Stop Prioritization Score Card

Bus Stop #	Overall Priority Rank	On Street	Dir.	Far/ Near	At Street	Ave. Daily Riders (On & Off)	Transit Centers (5 or 0)	Corridor Route (4 or 0)	RT Ridership (4,3,2,or1)	Medical or Nursing Ctr. (4, 0)	Retire-ment Facil. (3, 0)	Schools & Gov't Office (3,0)	Other Trip Generators (2,0)	Total Priority Score
3064	143	WATT AVE	SB	F	FREEDOM PARK DR	51		4	2				2	8
3065	163	WATT AVE	SB	F	KARL DR	39		4	2				2	8
3066	146	WATT AVE	SB	F	N HAVEN DR	48		4	2				2	8
3067	125	WATT AVE	SB	F	A ST	69		4	2			3		9
3068	82	WATT AVE	SB	F	WINGS WAY	112		4	4				2	10
3069	43	WATT AVE	SB	N	POPLAR BLVD	91		4	3			3	2	12
3070	135	WATT AVE	SB	F	MYRTLE AVE	63		4	2				2	8
3071	150	WATT AVE	SB	F	WINONA WAY	46		4	2				2	8
3072	210	WATT AVE	SB	F	ORANGE GROVE AVE	26		4	1				2	7
3073	22	WATT AVE	SB	F	LONGVIEW DR	120		4	4			3	2	13
3074	140	WATT AVE	SB	F	AUBURN BLVD	55		4	2				2	8
3075	60	WATT AVE	SB	N	EDISON AVE	66		4	2			3	2	11
3076	88	WATT AVE	SB	F	WHITNEY AVE	41		4	2				4	10
3077	121	WATT AVE	SB	F	POPE AVE	78		4	3				2	9
3078	54	WATT AVE	SB	F	MARCONI AVE	93		4	3				4	11
3079	77	WATT AVE	SB	F	CHENU AVE	166		4	4				2	10
3080	34	WATT AVE	SB	F	EL CAMINO AVE	195		4	4				4	12
3081	113	WATT AVE	SB	F	ARDEN WAY	1		4	1			3	2	10
3082	475	WATT AVE	SB	F	BARRINGTON RD	1		4	1					5
3083	453	WATT AVE	SB	F	HYDE WAY	12		4	1					5
3084	469	WATT AVE	SB	F	HURLEY WAY	3		4	1					5
3085	265	WATT AVE	SB	N	WINDSOR DR	10		4	1				2	7
3086	227	WATT AVE	SB	N	NORTHROP AVE	17		4	1				2	7
3087	344	WATT AVE	SB	F	COSMOS AVE	37		4	2					6
3089	57	WATT AVE	NB	F	BUTANO DR	77		4	3				4	11
3090	33	WATT AVE	NB	F	EL CAMINO AVE	252		4	4				4	12
3091	151	WATT AVE	NB	F	KINGS WAY	44		4	2				2	8
3092	19	WATT AVE	NB	F	MARCONI AVE	145		4	4		3		2	13
3093	298	WATT AVE	NB	F	LYNNE WAY 1	5		4	1				2	7
3094	228	WATT AVE	NB	F	LYNNE WAY 2	17		4	1				2	7
3095	84	WATT AVE	NB	F	WHITNEY AVE	66		4	2				4	10
3096	59	WATT AVE	NB	F	EDISON AVE	67		4	2			3	2	11
3098	336	WATT AVE	NB	F	LONGVIEW DR	50		4	2					6
3099	76	WATT AVE	NB	F	MARGARET WAY	783		4	4				2	10
3100	116	WATT AVE	NB	F	WINONA WAY	96		4	3				2	9
3101	20	WATT AVE	NB	F	MYRTLE AVE	143		4	4			3	2	13
3102	25	WATT AVE	NB	F	ROSEVILLE RD	80		4	3	4			2	13
3103	61	WATT AVE	NB	F	POPLAR BLVD	64		4	2			3	2	11
3104	270	WATT AVE	NB	F	AIR BASE DR	9		4	1				2	7

BSTP Appendix 1 - Bus Stop Prioritization Score Card

Bus Stop #	Overall Priority Rank	On Street	Dir.	Far/ Near	At Street	Ave. Daily Riders (On & Off)	Transit Centers (5 or 0)	Corridor Route (4 or 0)	RT Ridership (4,3,2,or1)	Medical or Nursing Ctr. (4, 0)	Retire-ment Facil. (3, 0)	Schools & Gov't Office (3,0)	Other Trip Generators (2,0)	Total Priority Score
3106	224	WATT AVE	NB	F	MCCLELLAN DR	18		4	1				2	7
3107	202	WATT AVE	NB	F	A ST	30		4	1				2	7
3108	176	WATT AVE	NB	F	N HAVEN DR	30		4	2				2	8
3109	442	WATT AVE	NB	F	KARL DR	27		4	1					5
3110	51	WATT AVE	NB	F	E ST	33		4	2	3		3		12
3112	271	WATT AVE	NB	F	BOLIVAR AVE	9		4	1				2	7
3113	343	WATT AVE	NB	F	CHANNING DR	39		4	2					6
3115	211	WATT AVE	NB	F	ELKHORN BLVD	26		4	1				2	7
3116	471	WATT AVE	NB	F	PLYMOUTH DR	2		4	1					5
3117	321	WATT AVE	NB	F	TURNER DR	2		4	1				2	7
3118	464	WATT AVE	NB	F	QUINN WAY	4		4	1					5
3119	257	WATT AVE	NB	F	QUINN WAY	11		4	1				2	7
3121	449	WATT AVE	NB	F	ANTELOPE RD	14		4	1					5
3122	380	WATT AVE	NB	F	BLACKFOOT WAY	11			1			3	2	6
3123	370	WATT AVE	NB	F	NAVAHO DR	14			1			3	2	6
3124	581	WATT AVE	NB	F	TIMBERLANE PL	28			1				2	3
3125	739	WATT AVE	NB	F	ELVERTA RD	6			1				2	3
3130	8	65TH ST	NB	F	FLORIN RD	284	5	4	4				2	15
3131	29	65TH ST	NB	F	SKY PKWY	687		4	4				4	12
3132	156	65TH ST	NB	F	STOCKTON BLVD	42		4	2				2	8
3179	885	HOWE AVE	SB	F	MARCONI AVE	67			2					2
3180	610	HOWE AVE	SB	F	RASSY WAY	21			1				2	3
3181	886	HOWE AVE	SB	F	EL CAMINO AVE	61			2					2
3182	407	HOWE AVE	NB	F	DELMA WAY	5			1			3	2	6
3183	197	HOWE AVE	SB	F	COTTAGE WAY	34			2			3	2	7
3184	495	HOWE AVE	SB	F	WYDA WAY	43			2				2	4
3185	435	HOWE AVE	SB	F	ARDEN WAY	90			3				2	5
3187	894	HOWE AVE	SB	F	HALLMARK DR	32			2					2
3188	115	HOWE AVE	SB	F	HURLEY WAY	128			4			3	2	9
3189	490	HOWE AVE	SB	N	VILLAGE	48			2				2	4
3190	493	HOWE AVE	SB	F	NORTHROP AVE	45			2				2	4
3191	48	HOWE AVE	SB	F	SIERRA BLVD	56		4	2	4			2	12
3273	748	MADISON AVE	EB	F	SUNRISE BLVD	6			1				2	3
3274	308	MADISON AVE	EB	F	FAIR OAKS BLVD	4		4	1				2	7
3275	913	MADISON AVE	EB	N	KAHALA CT	0			0				2	2
3276	550	MADISON AVE	EB	F	CHICAGO AVE	5			1			3		4
3277	561	MADISON AVE	EB	F	BUENA VISTA AVE	2			1			3		4
3278	401	MADISON AVE	EB	F	KENNETH AVE	5			1		3		2	6
3279	914	MADISON AVE	EB	F	VALONIA ST	0			0				2	2

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3280	814	MADISON AVE	EB	N	ILLINOIS AVE	2			1				2	3
3281	664	MADISON AVE	EB	N	HAZEL AVE	12			1				2	3
3282	678	MADISON AVE	EB	F	HAZEL AVE	11			1				2	3
3283	861	MADISON AVE	EB	F	FLYWAY DR	1			1				2	3
3284	972	MADISON AVE	EB	F	WINDING OAK DR	3			1					1
3285	973	MADISON AVE	EB	N	BLUE OAK DR	3			1					1
3286	322	MADISON AVE	EB	N	PERSHING AVE	2		4	1				2	7
3287	159	MAIN AVE	NB	F	MADISON AVE	41		4	2				2	8
3288	101	GREENBACK LN	WB	F	MAIN AVE	9		4	1		3		2	10
3289	75	GREENBACK LN	WB	F	WALNUT AVE	5		4	1	4			2	11
3290	314	GREENBACK LN	WB	F	CHESTNUT AVE	3		4	1				2	7
3291	315	GREENBACK LN	WB	F	FILBERT AVE	3		4	1				2	7
3292	309	GREENBACK LN	WB	N	PECAN AVE	4		4	1				2	7
3293	236	GREENBACK LN	WB	F	HAZEL AVE	15		4	1				2	7
3294	290	GREENBACK LN	WB	N	HICKORY AVE	6		4	1				2	7
3295	105	GREENBACK LN	WB	F	ALMOND AVE	6		4	1		3		2	10
3296	472	GREENBACK LN	WB	F	KENNETH AVE	2		4	1					5
3297	184	GREENBACK LN	WB	F	TRAJAN DR	1		4	1			3		8
3298	185	GREENBACK LN	WB	F	HOFFMAN LN	1		4	1			3		8
3350	132	AUBURN BLVD	WB	F	MANZANITA AVE	68		4	2				2	8
3351	284	AUBURN BLVD	WB	F	ROSEBUD LN	7		4	1				2	7
3352	44	AUBURN BLVD	WB	F	GARFIELD AVE	87		4	3		3		2	12
3353	157	AUBURN BLVD	WB	N	HEMLOCK ST	42		4	2				2	8
3354	89	AUBURN BLVD	WB	F	KOHLER RD	26		4	1		3		2	10
3355	78	AUBURN BLVD	WB	F	MADISON AVE	143		4	4				2	10
3356	441	COLLEGE OAK DR	SB	F	AUBURN BLVD	29		4	1					5
3357	434	COLLEGE OAK DR	SB	N	MYRTLE AVE	97			3				2	5
3451	574	HAZEL AVE	SB	N	PITMAN LANE LN	0			0				4	4
3452	862	HAZEL AVE	SB	F	OAK AVE	1			1				2	3
3453	882	HAZEL AVE	SB	F	ALMOND VIEW CT	0			0			3		3
3454	815	HAZEL AVE	SB	F	CENTRAL AVE	2			1				2	3
3455	915	HAZEL AVE	SB	N	GREENHALGH LN	0			0				2	2
3456	99	HAZEL AVE	SB	F	GREENBACK LN	11		4	1		3		2	10
3457	946	HAZEL AVE	SB	F	PERSHING AVE	5			1					1
3458	633	HAZEL AVE	SB	F	MADISON AVE	16			1				2	3
3459	863	HAZEL AVE	SB	F	PHOENIX AVE	1			1				2	3
3460	399	HAZEL AVE	SB	F	SUNSET AVE	6			1		3		2	6
3461	418	HAZEL AVE	SB	F	N WIND WAY	2			1			3	2	6
3462	797	HAZEL AVE	SB	F	WINDING WY	3			1				2	3

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3463	864	HAZEL AVE	SB	F	VISAGE CIR	1			1				2	3
3464	575	HAZEL AVE	NB	F	CENTRAL AVE	0			0				4	4
3465	1009	HAZEL AVE	NB	N	AKSARBEN DR	1			1					1
3468	1010	HAZEL AVE	NB	F	CURRAGH DOWN S DR	1			1					1
3469	1027	HAZEL AVE	NB	F	DEGAS CT	0			0					0
3470	846	HAZEL AVE	NB	N	WINDING WY	1			1				2	3
3471	423	HAZEL AVE	NB	F	LA SERENA DR	1			1			3	2	6
3472	410	HAZEL AVE	NB	F	SUNSET AVE	4			1		3		2	6
3473	883	HAZEL AVE	NB	F	BEDFORD AVE	0			0		3			3
3474	916	HAZEL AVE	NB	F	PHOENIX AVE	0			0				2	2
3475	777	HAZEL AVE	NB	F	MADISON AVE	4			1				2	3
3476	983	HAZEL AVE	NB	F	LEEDY LN	2			1					1
3477	798	HAZEL AVE	NB	N	HAZEL HILL CT.	3			1				2	3
3478	129	HAZEL AVE	NB	F	GREENBACK LN	0		4	0			3	2	9
3479	917	HAZEL AVE	NB	F	CALVERT AVE	0			0				2	2
3480	847	HAZEL AVE	NB	F	CENTRAL AVE	1			1				2	3
3481	402	ELM AVE	WB	F	HAZEL AVE	5			1			3	2	6
3482	929	ELM AVE	WB	N	BEECH AVE	10			1					1
3483	414	BEECH AVE	NB	F	MAVIS AVE	3			1			3	2	6
3503	243	AIRBASE DR	EB	F	WATT AVE	13		4	1				2	7
3504	480	MADISON AVE	EB	F	JACKSON ST	66			2				2	4
3505	94	MADISON AVE	EB	F	HARRISON ST	19		4	1			3	2	10
3508	126	MADISON AVE	WB	F	HILLSDALE BLVD	38		4	2			3		9
3509	179	MADISON AVE	WB	F	HARRISON ST	17		4	1			3		8
3510	491	MADISON AVE	WB	F	JACKSON ST	48			2				2	4
3520	107	HILLSDALE BLVD	NB	F	MADISON AVE	5		4	1			3	2	10
3521	782	HILLSDALE BLVD	NB	F	FRIZELL AVE	4			1				2	3
3522	939	HILLSDALE BLVD	NB	F	CHAPEL WAY	6			1					1
3523	926	HILLSDALE BLVD	NB	F	STOCKBRIDGE AVE	13			1					1
3524	437	HILLSDALE BLVD	NB	F	PALM AVE	74			3				2	5
3525	596	HILLSDALE BLVD	NB	F	GREENHOLME DR	25			1				2	3
3526	890	HILLSDALE BLVD	EB	N	WALERGA RD	41			2					2
3527	665	HILLSDALE BLVD	NB	F	WALERGA RD	12			1				2	3
3528	533	HILLSDALE BLVD	NB	F	BRETT DR	10			1			3		4
3529	551	HILLSDALE BLVD	NB	F	DIABLO DR	5			1			3		4
3530	415	HILLSDALE BLVD	NB	N	ROBERT FROST WAY	3			1			3	2	6
3531	529	HILLSDALE BLVD	NB	F	MC CLOUD DR	11			1			3		4
3532	940	HILLSDALE BLVD	NB	F	TACOMIC DR	6			1					1
3533	545	ANDREA BLVD	EB	F	HILLSDALE BLVD	7			1			3		4

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3540	497	ANDREA BLVD	WB	F	ELKHORN BLVD	42			2				2	4
3541	806	ANDREA BLVD	WB	N	HILLSDALE BLVD	3			1				2	3
3542	947	HILLSDALE BLVD	SB	F	ANDREA BLVD	5			1					1
3543	941	HILLSDALE BLVD	SB	N	TACOMIC DR	6			1					1
3544	520	HILLSDALE BLVD	SB	F	MC CLOUD DR	16			1			3		4
3545	385	HILLSDALE BLVD	SB	F	ROBERT FROST WAY	10			1			3	2	6
3547	530	HILLSDALE BLVD	SB	F	BRETT DR	11			1			3		4
3548	884	HILLSDALE BLVD	SB	F	WALERGA RD	68			2					2
3549	481	HILLSDALE BLVD	SB	N	GREENHOLME DR	66			2				2	4
3550	888	HILLSDALE BLVD	SB	F	PALM AVE	50			2					2
3551	1029	HILLSDALE BLVD	SB	F	HAMILTON ST	0			0					0
3552	536	HILLSDALE BLVD	SB	F	FRIZELL AVE	9			1			3		4
3587	906	MADISON AVE	WB	F	LINCOLN OAKS DR	0			0				2	2
3588	395	MADISON AVE	WB	N	DEWEY DR	7			1			3	2	6
3589	310	COYLE AVE	WB	F	DEWEY DR	4			1	4			2	7
3590	225	COYLE AVE	WB	F	SOUTHBROOK WAY	18			1	4			2	7
3591	388	COYLE AVE	WB	F	PARK OAKS DR	8			1			3	2	6
3592	323	COYLE AVE	WB	F	HESPER WAY	2			1	4			2	7
3593	959	COYLE AVE	WB	F	WOODLEIGH DR	4			1					1
3594	299	COYLE AVE	WB	F	CASA ALEGRE	5			1	4			2	7
3595	740	MANZANITA AVE	SB	N	NONNIE AVE	6			1				2	3
3596	403	MANZANITA AVE	SB	N	PEPPERMILL CT	5			1		3		2	6
3597	67	MANZANITA AVE	SB	F	MADISON AVE	35			2	4	3		2	11
3598	707	MANZANITA AVE	SB	F	MULDROW RD	8			1				2	3
3599	921	MANZANITA AVE	SB	F	VERDE CRUZ WAY	18			1					1
3600	64	MANZANITA AVE	SB	F	WINDING WAY	38		4	2		3		2	11
3601	106	MANZANITA AVE	SB	F	WINDMILL WAY	6		4	1		3		2	10
3602	311	MANZANITA AVE	SB	F	LOCUST AVE	4		4	1				2	7
3603	97	MANZANITA AVE	SB	F	CYPRESS AVE	14		4	1		3		2	10
3614	90	MANZANITA AVE	NB	F	FAIR OAKS BLVD	26		4	1		3		2	10
3615	274	MANZANITA AVE	NB	F	LINCOLN AVE	8		4	1				2	7
3616	109	MANZANITA AVE	NB	F	JAN DR	4		4	1		3		2	10
3617	68	MANZANITA AVE	NB	F	WINDING WAY	35		4	2		3		2	11
3618	390	MANZANITA AVE	NB	F	BOURBON DR	8			1			3	2	6
3619	692	MANZANITA AVE	NB	N	MULDROW RD	9			1				2	3
3620	597	MANZANITA AVE	NB	F	MADISON AVE	25			1				2	3
3621	404	MANZANITA AVE	NB	N	ELLERSLEE DR	5			1		3		2	6
3622	816	MANZANITA AVE	NB	F	MALEVILLE AVE	2			1				2	3
3623	266	COYLE AVE	EB	F	MANZANITA AVE	10			1	4			2	7

BSTP Appendix 1 - Bus Stop Prioritization Score Card

Bus Stop #	Overall Priority Rank	On Street	Dir.	Far/ Near	At Street	Ave. Daily Riders (On & Off)	Transit Centers (5 or 0)	Corridor Route (4 or 0)	RT Ridership (4,3,2,or1)	Medical or Nursing Ctr. (4, 0)	Retire-ment Facil. (3, 0)	Schools & Gov't Office (3,0)	Other Trip Generators (2,0)	Total Priority Score
3624	960	COYLE AVE	EB	F	WOODLEIGH DR	4			1					1
3625	110	COYLE AVE	EB	F	HESPER WAY	4			1	4		3	2	10
3626	373	COYLE AVE	EB	N	MILBURN ST	12			1			3	2	6
3627	229	COYLE AVE	EB	N	ST CHARLES DR	17			1	4			2	7
3628	300	COYLE AVE	EB	F	ST CHARLES DR	5			1	4			2	7
3629	656	MADISON AVE	EB	F	DEWEY DR	13			1				2	3
3630	907	MADISON AVE	EB	N	LINCOLN OAKS DR	0			0				2	2
3631	918	MADISON AVE	EB	N	LINDA SUE WAY	0			0				2	2
3632	741	MADISON AVE	EB	N	SAN JUAN AVE	6			1				2	3
3634	817	MADISON AVE	EB	F	MARIPOSA AVE	2			1				2	3
3635	924	MADISON AVE	EB	F	PALMYRA DR	17			1					1
3696	936	ELVERTA RD	WB	F	BIG SKY DR	7			1					1
3769	324	DON JULIO BLVD	EB	F	WATT AVE	2		4	1				2	7
3770	755	DON JULIO BLVD	EB	F	BOLIVAR AVE	5			1				2	3
3771	763	DON JULIO BLVD	EB	F	LARRY WAY	5			1				2	3
3772	362	DON JULIO BLVD	EB	N	BELVA WAY	18			1			3	2	6
3773	537	DON JULIO BLVD	EB	F	GARLAND CT	9			1		3			4
3774	974	DON JULIO BLVD	EB	F	LARCHMONT DR	3			1					1
3775	359	DON JULIO BLVD	EB	F	CRAIGHURST DR	19			1			3	2	6
3776	440	WALERGA RD	NB	F	DON JULIO BLVD	37			2		3			5
3777	630	WALERGA RD	NB	F	ELKHORN BLVD	17			1				2	3
3778	635	WALERGA RD	NB	F	GALBRATH DR	16			1				2	3
3780	948	WALERGA RD	NB	F	HOLWORTHY WAY	5			1					1
3781	865	ANTELOPE RD	WB	F	ANTELOPE HILLS DR	1			1				2	3
3782	679	ANTELOPE RD	WB	N	HOLBROOK WAY	11			1				2	3
3783	657	ANTELOPE RD	WB	F	BLACK BEAR DR	13			1				2	3
3860	194	BRANCH CTR	NB	N	GOETHE RD	37			2			3	2	7
3861	405	BRANCH CTR	SB	N	PRESERVATION WAY	5			1			3	2	6
3862	424	BRANCH CTR	SB	F	PRESERVATION WAY	1			1			3	2	6
3864	378	BRANCH CTR	SB	N	AGRICULTURE LN	11			1			3	2	6
3865	41	KIEFER BLVD	WB	F	BRANCH CENTER	116			4			6	2	12
3866	666	KIEFER BLVD	WB	N	N KIEFER BLVD	12			1				2	3
3867	818	KIEFER BLVD	WB	F	ARDERLY CT	2			1				2	3
3868	486	KIEFER BLVD	WB	F	HUNTSMAN DR	50			2				2	4
3869	513	KIEFER BLVD	WB	F	TALLYHO DR	34			2				2	4
3870	494	KIEFER BLVD	WB	N	SOUTHPORT DR	44			2				2	4
3871	866	KIEFER BLVD	WB	F	AUTUMNWOOD DR	1			1				2	3
3872	819	S WATT AVE	NB	F	KIEFER BLVD	2			1				2	3
3894	4	HOWE AVE	NB	F	FAIR OAKS BLVD	114		4	4	4	3		2	17

BSTP Appendix 1 - Bus Stop Prioritization Score Card

Bus Stop #	Overall Priority Rank	On Street	Dir.	Far/ Near	At Street	Ave. Daily Riders (On & Off)	Transit Centers (5 or 0)	Corridor Route (4 or 0)	RT Ridership (4,3,2,or1)	Medical or Nursing Ctr. (4, 0)	Retire-ment Facil. (3, 0)	Schools & Gov't Office (3,0)	Other Trip Generators (2,0)	Total Priority Score
3895	49	HOWE AVE	NB	F	SIERRA BLVD	53		4	2	4			2	12
3896	507	HOWE AVE	NB	F	NORTHROP AVE	36			2				2	4
3897	496	HOWE AVE	NB	N	VILLAGE	43			2				2	4
3898	79	HOWE AVE	NB	F	HURLEY WAY	133			4	4			2	10
3899	611	HOWE AVE	NB	N	VILLAFONT WAY	21			1				2	3
3900	24	HOWE AVE	NB	F	ARDEN WAY	101			4	4		3	2	13
3901	500	HOWE AVE	NB	F	WYDA WAY	40			2				2	4
3902	848	HOWE AVE	NB	N	COTTAGE WAY	1			1				2	3
3903	200	HOWE AVE	NB	F	DELMA WAY	31			2			3	2	7
3904	492	HOWE AVE	NB	F	EL CAMINO AVE	47			2				2	4
3905	643	HOWE AVE	NB	F	TALLAC ST	14			1				2	3
3950	548	DON JULIO BLVD	WB	N	CRAIGHURST DR	6			1		3			4
3951	922	DON JULIO BLVD	WB	F	PEARSON LN	18			1					1
3953	354	DON JULIO BLVD	WB	N	SCOTIA WAY	21			1		3		2	6
3954	807	DON JULIO BLVD	WB	F	BELVA WAY	3			1				2	3
3955	764	DON JULIO BLVD	WB	N	LARRY WAY	5			1				2	3
3956	799	DON JULIO BLVD	WB	F	BOLIVAR AVE	3			1				2	3
3957	285	DON JULIO BLVD	WB	F	SAN MARTIN ST	7		4	1				2	7
3960	244	ELKHORN BLVD	EB	F	WATT AVE	13		4	1				2	7
3961	450	ELKHORN BLVD	EB	F	CANTEL WAY	13		4	1					5
3962	534	ELKHORN BLVD	EB	F	THOMAS DR	10			1			3		4
3963	439	ELKHORN BLVD	EB	F	LARCHMONT DR	63			2			3		5
3964	693	ELKHORN BLVD	EB	F	WALERGA RD	9			1				2	3
3965	669	ELKHORN BLVD	EB	F	BUTTERBALL WAY	12			1				2	3
3966	930	ELKHORN BLVD	EB	F	SPRIG DR	10			1					1
3967	394	ELKHORN BLVD	EB	F	DON JULIO BLVD	7			1			3	2	6
3968	667	ELKHORN BLVD	EB	F	HILLSDALE BLVD	12			1				2	3
3969	349	ELKHORN BLVD	EB	F	ANDREA BLVD	26			1			3	2	6
3970	396	ELKHORN BLVD	EB	F	CRATER WAY	7			1			3	2	6
3976	908	N MARKET BLVD	WB	F	NORTH FREEWAY BLVD	0			0				2	2
3987	742	NATIONAL DR	SB	F	N MARKET BLVD	6			1				2	3
3988	756	NATIONAL DR	SB	N	NATIONAL DR	5			1				2	3
3989	196	NATIONAL DR	WB	F	LENNANE DR	35			2			3	2	7
3990	849	SIERRA POINT DR	NB	N	N MARKET BLVD	1			1				2	3
3993	850	SIERRA POINT DR	SB	F	N MARKET BLVD	1			1				2	3
3994	191	NATIONAL DR	EB	N	LENNANE DR	43			2			3	2	7
3995	757	NATIONAL DR	NB	F	NATIONAL DR	5			1				2	3
3996	851	NATIONAL DR	NB	N	N MARKET BLVD	1			1				2	3
4000	245	ANTELOPE RD	EB	F	WATT AVE	13		4	1				2	7

BSTP Appendix 1 - Bus Stop Prioritization Score Card

Bus Stop #	Overall Priority Rank	On Street	Dir.	Far/ Near	At Street	Ave. Daily Riders (On & Off)	Transit Centers (5 or 0)	Corridor Route (4 or 0)	RT Ridership (4,3,2,or1)	Medical or Nursing Ctr. (4, 0)	Retire-ment Facil. (3, 0)	Schools & Gov't Office (3,0)	Other Trip Generators (2,0)	Total Priority Score
4001	949	ANTELOPE RD	EB	F	MOUNTAIN OAK WAY	5			1					1
4002	852	ANTELOPE RD	EB	F	HOLBROOK WAY	1			1				2	3
4003	896	WALERGA RD	SB	F	ANTELOPE RD	30			2					2
4004	961	WALERGA RD	SB	F	BAINBRIDGE DR	4			1					1
4005	627	WALERGA RD	SB	F	GALBRATH DR	18			1				2	3
4006	599	WALERGA RD	SB	F	ELKHORN BLVD	24			1				2	3
4007	531	WALERGA RD	SB	N	DON JULIO BLVD	11			1		3			4
4083	716	ELSIE AVE	EB	F	IONA WAY	8			1				2	3
4084	419	ELSIE AVE	EB	F	HALBRITE WAY	2			1		3		2	6
4085	544	ELSIE AVE	EB	F	WILBUR WAY	7			1		3			4
4086	783	COTTONWOOD LN	SB	F	ELSIE AVE	4			1				2	3
4087	369	COTTONWOOD LN	SB	N	IVY CREEK DR	14			1			3	2	6
4088	187	COTTONWOOD LN	NB	F	IVY CREEK DR	58			2			3	2	7
4089	697	COTTONWOOD LN	NB	N	ELSIE AVE	9			1				2	3
4090	538	ELSIE AVE	WB	N	WILBUR WAY	9			1		3			4
4091	411	ELSIE AVE	WB	N	HEMINGWAY DR	4			1		3		2	6
4092	644	ELSIE AVE	WB	N	IONA WAY	14			1				2	3
4096	778	POWER INN RD	NB	F	ELSIE AVE	4			1				2	3
4097	950	POWER INN RD	NB	F	HAYSTACK DR	5			1					1
4098	461	GERBER RD	EB	F	POWER INN RD	6		4	1					5
4099	951	GERBER RD	EB	F	FERNRIDGE DR	5			1					1
4100	406	GERBER RD	WB	N	REESE RD	5			1			3	2	6
4105	784	GERBER RD	WB	N	FERNRIDGE DR	4			1				2	3
4106	291	POWER INN RD	SB	F	GERBER RD	6		4	1				2	7
4107	952	POWER INN RD	SB	N	SKYWOODS WAY	5			1					1
4413	462	ELSIE AVE	WB	F	ROBINETTE RD	6		4	1					5
4414	473	ELSIE AVE	WB	F	LA FIESTA WAY	2		4	1					5
4415	301	ELSIE AVE	EB	F	LA FIESTA WAY	5		4	1				2	7
4702	800	N MARKET BLVD	EB	F	S MARKET CT	3			1				2	3
4708	619	N MARKET BLVD	EB	N	SIERRA POINT DR	19			1				2	3
4712	708	N MARKET BLVD	EB	F	NATIONAL DR	8			1				2	3
4743	463	ANTELOPE RD	SB	F	ROSEVILLE RD	6			1				4	5
4753	519	N MARKET BLVD	WB	F	SPORTS DR	18			1			3		4
4754	639	N MARKET BLVD	WB	F	NORTHGATE BLVD	15			1				2	3
4774	316	FAIR OAKS BLVD	NB	F	MCMILLAN DR	3		4	1				2	7
4775	1011	FAIR OAKS BLVD	NB	F	NIESSEN WAY	1			1					1
4777	275	ELSIE AVE AVE	WB	N	STOCKTON BLVD	8		4	1				2	7
4778	276	ELSIE AVE	EB	F	STOCKTON BLVD	8		4	1				2	7
4808	984	RIO LINDA BLVD	NB	N	SAVOY AVE	2			1					1

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4809	820	ANTELOPE RD	WB	F	GREYWOLF DR	2			1				2	3
5235	1026	ELVERTA RD	EB	F	16TH ST	0			0					0
5243	1028	HAZEL AVE	EB	N	LAKE NIMBUS DR	0			0					0
5270	801	ANTELOPE RD	WB	N	WALERGA RD	3			1				2	3
5275	292	FAIR OAKS BLVD	WB	N	OLD WINDING WAY	6		4	1				2	7
5346	591	ANDREA BLVD	NB	N	ELKHORN BLVD	26			1				2	3
5358	821	TEMESCAL ST	NB	N	CALIFORNIA AVE	2			1				2	3
5359	853	HAZEL AVE	SB	F	GLORI DAWN DR	1			1				2	3
5360	765	FAIR OAKS BLVD	SB	N	WALNUT AVE	5			1				2	3
5361	758	SAN JUAN AVE	SB	F	LINCOLN AVE	5			1				2	3
5362	854	SAN JUAN AVE	SB	F	WINDING WAY 2-S	1			1				2	3
5364	822	SAN JUAN AVE	NB	F	SANTA SUSANNA WY	2			1				2	3
5367	867	ANTELOPE RD	EB	N	ANTELOPE HILLS DR	1			1				2	3
6505	658	FRANKLIN BLVD	NB	N	VILLAGE STAR DR	13			1				2	3
6512	928	FRANKLIN BLVD	SB	F	CALVINE RD	12			1					1
6514	1012	FRANKLIN BLVD	SB	F	ADALIS DR	1			1					1
6515	953	FRANKLIN BLVD	SB	F	LAKE PLEASANT DR	5			1					1
9800	3	WATT AVE	NB	Mid	I-80 LRT	897	5	4	4				6	19
9801	2	WATT AVE	SB	Mid	I-80 LRT	935	5	4	4				6	19
9817	80	WATT/MANLOVE LRS	SB	F	BAY 3	132		4	4				2	10
9903	117	I-80 SERVICE DR	SB	Mid	WATT LOWER LEVEL	94		4	3				2	9
9926	416	OLD PLACERVILLE RD	EB	F	MACREADY AVE	3			1			3	2	6
9935	975	EDISON AVE	SB	F	GIBBONS DR	3			1					1
9936	985	EDISON AVE	NB	N	AMBERLEY WAY	2			1					1
9937	942	EDISON AVE	WB	N	TERRA VISTA WAY	6			1					1
9938	954	EDISON AVE	EB	N	MIRA VISTA WAY	5			1					1
9939	521	MISSION AVE	NB	N	ENGLE RD	16			1			3		4
9940	562	ENGLE RD	WB	F	MIRADERA ST	2			1			3		4
9941	570	ENGLE RD	EB	F	MIRADERA ST	1			1			3		4
9942	937	EASTERN AVE	SB	N	ENGLE RD	7			1					1
9943	962	EASTERN AVE	NB	F	ENGLE RD	4			1					1
9996	727	LUCE AVE	NB	N	PALM ST	7			1				2	3
9997	454	PALM ST	EB	N	DUDLEY BLVD	11			1	4				5
11045 Folsom	332	MADISON AVE	NB	N	MAIN AVE	0		4	1				2	7
11069 Folsom	333	MAIN AVE	SB	N	GREENBACK LANE	0		4	1				2	7
11160 Folsom	334	MAIN AVE	NB	F	MADISON AVE	0		4	1				2	7

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95015 E-Tran	1013	AMAND GEORGE WAY	NB	F	CALVINE RD	0			1					1
95016 E-Tran	1014	TURQUOISE WAY	SB	N	CALVINE RD	0			1					1
95017 E-Tran	868	CALVINE RD	WB	F	AMAND GEORGE WAY	0			1				2	3
95018 E-Tran	572	CALVINE RD	WB	F	JORDAN RANCH RD	0			1			3		4
95019 E-Tran	1015	CALVINE RD	WB	F	WATERMAN RD	0			1					1
95020 E-Tran	869	CALVINE RD	WB	F	GRAND CRU DR	0			1				2	3
95040 E-Tran	876	CALVINE RD	WB	F	POWER INN	0			1				2	3
95043 E-Tran	877	POWER INN RD	SB	F	CALVINE RD	0			1				2	3
95293 E-Tran	870	CALVINE RD	EB	F	POWER INN	0			1				2	3
95333 E-Tran	871	CALVINE RD	WB	F	ELK GROVE FLORIN RD	0			1				2	3
95334 E-Tran	426	CALVINE RD	WB	F	VINTAGE PARK RD	0			1			3	2	6
95335 E-Tran	1016	CALVINE RD	WB	N	HARDESTER DR	0			1					1
95336 E-Tran	1017	CALVINE RD	WB	F	NEW POINT DR	0			1					1
95337 E-Tran	427	CALVINE RD	WB	F	CLIFFCREST DR	0			1			3	2	6
95338 E-Tran	872	CALVINE RD	WB	F	AUBERRY DR	0			1				2	3
95588 E-Tran	873	BRADSHAW	NB	F	VINTAGE PARK DR	0			1				2	3
95592 E-Tran	874	BRADSHAW	SB	F	VINTAGE PARK DR	0			1				2	3
95690 E-Tran	875	CALVINE RD	WB	F	BRADSHAW RD	0			1				2	3

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
1	563	Orange Grove Ave	WB	F	College Oak Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2013/14
2	9801	Watt Ave	SB	Mid	I-80 LRT	\$0	\$0	\$0	\$0	\$0	\$0	2013/14
3	9800	Watt Ave	NB	Mid	I-80 LRT	\$0	\$0	\$0	\$0	\$0	\$0	2013/14
4	3894	Howe Ave	NB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2013/14
5	2755	Folsom Blvd	EB	F	Manlove Rd	\$0	\$10,000	\$10,000	\$0	\$5,000	\$25,000	2013/14
6	1458	El Camino Ave	EB	F	Fulton Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2013/14
7	640	College Oak Dr	NB	F	Myrtle Ave	\$20,000	\$5,000	\$30,000	\$20,000	\$5,000	\$80,000	2013/14
8	3130	65th St	NB	F	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2013/14
9	665	College Oak Dr	SB	F	Orange Grove Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2013/14
10	2690	Sky Pkwy	SB	N	65th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
11	2024	Sky Pkwy	NB	F	65th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
12	1139	Fair Oaks Blvd	SB	N	Topp Ct	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
13	630	Auburn Blvd	EB	F	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
14	377	Watt/Manlove	SB	F	Lt Rail Sta Bay 2	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
15	2752	Folsom Blvd	WB	N	Manlove Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
16	1562	Fulton Ave	NB	F	Marconi Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
17	1359	Fair Oaks Blvd	SB	N	Landis Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
18	1534	Fulton Ave	SB	F	Larkspur Ln	\$0	\$0	\$0	\$0	\$0	\$0	2014/15
19	3092	Watt Ave	NB	F	Marconi Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
20	3101	Watt Ave	NB	F	Myrtle Ave	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	2014/15
21	643	Auburn Blvd	EB	N	Kohler Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
22	3073	Watt Ave	SB	F	Longview Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
23	2372	Florin Rd	EB	F	Franklin Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
24	3900	Howe Ave	NB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
25	3102	Watt Ave	NB	F	Roseville Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
26	1181	Fair Oaks Blvd	NB	F	Van Alstine Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
27	1485	Fair Oaks Blvd	NB	F	Grant Ave	\$0	\$0	\$0	\$0	\$0	\$0	2014/15
28	2169	Florin Mall	SB	F	66th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
29	3131	65th St	NB	F	Sky Pkwy	\$0	\$0	\$0	\$0	\$0	\$0	2014/15
30	1612	65th St	WB	N	Sky Pkwy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
31	2374	Florin Rd	EB	F	Easy Pkwy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
32	1860	Stockton Blvd	SB	F	65th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
33	3090	Watt Ave	NB	F	El Camino Ave	\$0	\$0	\$0	\$0	\$0	\$0	2015/16
34	3080	Watt Ave	SB	F	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
35	1464	El Camino Ave	EB	F	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
36	1379	El Camino Ave	WB	N	Watt Ave	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	2015/16
37	2313	Florin Rd	WB	F	Bowling Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
38	1463	El Camino Ave	EB	F	Yorktown Ave	\$0	\$0	\$0	\$0	\$0	\$0	2015/16

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
39	2020	Florin Rd	WB	N	Stockton Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
40	2165	Florin Rd	EB	F	Florin Mall Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
41	3865	Kiefer Blvd	WB	F	Branch Ctr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
42	1378	El Camino Ave	WB	F	Butano Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2015/16
43	3069	Watt Ave	SB	N	Poplar Blvd	\$0	\$0	\$0	\$0	\$0	\$0	2015/16
44	3352	Auburn Blvd	WB	F	Garfield Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
45	1127	Cottage Way	EB	F	Morse Ave	\$0	\$0	\$0	\$0	\$0	\$0	2015/16
46	631	Auburn Blvd	EB	F	Annadale Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
47	2597	Franklin Blvd	NB	F	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
48	3191	Howe Ave	SB	F	Sierra Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2015/16
49	3895	Howe Ave	NB	F	Sierra Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
50	1182	Fair Oaks Blvd	NB	F	Kenneth Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
51	3110	Watt Ave	NB	F	E St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
52	1551	Fulton Ave	NB	F	Northrop Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2015/16
53	1483	Fair Oaks Blvd	NB	F	Stanley Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
54	3078	Watt Ave	SB	F	Marconi Ave	\$0	\$0	\$0	\$0	\$0	\$0	2015/16
55	1524	Fulton Ave	SB	F	Marconi Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
56	2312	Florin Rd	WB	F	East Pkwy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17
57	3089	Watt Ave	NB	F	Butano Dr	\$0	\$0	\$0	\$0	\$0	\$0	2016/17
58	1247	Marconi Ave	WB	F	Watt Ave	\$20,000	\$5,000	\$20,000	\$0	\$5,000	\$50,000	2016/17
59	3096	Watt Ave	NB	F	Edison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
60	3075	Watt Ave	SB	N	Edison Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17
61	3103	Watt Ave	NB	F	Poplar Blvd	\$0	\$0	\$0	\$0	\$0	\$0	2016/17
62	1662	Northrop Ave	EB	F	Howe Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
63	1155	Arden Way	WB	F	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
64	3600	Manzanita Ave	SB	F	Winding Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
65	2689	Sky Pkwy	WB	N	Klingon Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
66	1357	Fair Oaks Blvd	SB	F	Engle Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17
67	3597	Manzanita Ave	SB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
68	3617	Manzanita Ave	NB	F	Winding Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17
69	642	Auburn Blvd	EB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
70	1613	65th St	WB	N	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17
71	1360	Fair Oaks Blvd	SB	N	Angelina Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
72	1536	Munroe St	SB	F	Sierra Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17
73	378	Watt/Manlove	EB	F	Lt Rail Sta Bay 1	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	2016/17
74	3038	Fair Oaks Blvd	NB	N	Westcamp Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
75	3289	Greenback Ln	WB	F	Walnut Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
76	3099	Watt Ave	NB	F	Margaret Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
77	3079	Watt Ave	SB	F	Chenu Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
78	3355	Auburn Blvd	WB	F	Madison Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
79	3898	Howe Ave	NB	F	Hurley Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
80	9817	S Watt/Manlove LRS	SB	F	Bay 3	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
81	1854	Stockton Blvd	SB	F	McMahon Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
82	3068	Watt Ave	SB	F	Wings way	\$0	\$0	\$0	\$0	\$0	\$0	2017/18
83	1271	Marconi Ave	EB	F	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
84	3095	Watt Ave	NB	F	Whitney Way	\$0	\$0	\$0	\$0	\$0	\$0	2017/18
85	2373	Florin Rd	EB	F	Bowling Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
86	1251	Marconi Ave	WB	F	Fulton Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
87	1267	Marconi Ave	EB	F	Fulton Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
88	3076	Watt Ave	SB	F	Whitney Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
89	3354	Auburn Blvd	WB	F	Kohler Rd	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	2017/18
90	3614	Manzanita Ave	NB	F	Fair Oaks Blvd	\$0	\$0	\$0	\$0	\$0	\$0	2017/18
91	632	Auburn Blvd	EB	F	Pasadena Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
92	1358	Fair Oaks Blvd	SB	F	Grant Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
93	1552	Fulton Ave	NB	F	Larkspur Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
94	3505	Madison Ave	EB	F	Harrison St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2018/19
95	1492	Fair Oaks Blvd	EB	N	Champlain Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2018/19
96	1354	Fair Oaks Blvd	WB	F	California Ave	\$20,000	\$0	\$25,000	\$160,000	\$5,000	\$210,000	2018/19
97	3603	Manzanita Ave	SB	F	Cypress Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2018/19
98	2026	N Parkway	NB	F	Sky Pkwy	\$20,000	\$5,000	\$60,000	\$0	\$5,000	\$90,000	2018/19
99	3456	Hazel Ave	SB	F	Greenback Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2018/19
100	1147	Arden Way	WB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2018/19
101	3288	Greenback Ln	WB	F	Main Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2018/19
102	1109	Arden Way	EB	F	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2018/19
103	1351	Fair Oaks Blvd	WB	N	Miles Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
104	2025	Sky Pkwy	WB	N	Klingon Ct	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2019/20
105	3295	Greenback Ln	WB	F	Almond Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
106	3601	Manzanita Ave	SB	F	Windmill Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
107	3520	Hillsdale Blvd	NB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
108	1677	Morse Ave	NB	F	Cottage Way	\$0	\$0	\$0	\$0	\$0	\$0	2019/20
109	3616	Manzanita Ave	NB	F	Jan Dr	\$20,000	\$0	\$80,000	\$0	\$5,000	\$105,000	2019/20
110	3625	Coyle Ave	EB	F	Hesper Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
111	2018	Florin Mall Dr	NB	F	66th Ave	\$20,000	\$0	\$0	\$80,000	\$5,000	\$105,000	2019/20
112	3034	Fair Oaks Blvd	NB	N	Winding Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2019/20
113	3081	Watt Ave	SB	F	Arden Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2019/20
114	2311	Florin Rd	WB	F	65th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2019/20

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
115	3188	Howe Ave	SB	F	Hurley Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2019/20
116	3100	Watt Ave	NB	F	Winona Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
117	9903	On I-80 Service Dr	SB	Mid	Watt Lower Level	\$0	\$0	\$0	\$0	\$0	\$0	2019/20
118	1884	Stockton Blvd	NB	F	65th St	\$0	\$0	\$0	\$0	\$0	\$0	2019/20
119	1486	Fair Oaks Blvd	NB	F	Shirley Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
120	1533	Fulton Ave	SB	F	Hurley Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
121	3077	Watt Ave	SB	F	Pope Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
122	641	College Oak Dr	NB	N	Auburn Blvd	\$20,000	\$5,000	\$20,000	\$10,000	\$5,000	\$60,000	2020/21
123	1553	Fulton Ave	NB	F	Hurley Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
124	2021	Florin Rd	WB	N	Florin Mall Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
125	3067	Watt Ave	SB	F	A St	\$0	\$0	\$0	\$0	\$0	\$0	2020/21
126	3508	Madison Ave	WB	F	Hillsdale Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2020/21
127	1380	El Camino Ave	WB	F	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2020/21
128	1484	Fair Oaks Blvd	NB	F	Landis Ave	\$0	\$0	\$0	\$0	\$0	\$0	2020/21
129	3478	Hazel Ave	NB	F	Greenback Ln	\$0	\$0	\$0	\$0	\$0	\$0	2020/21
130	1674	Morse Ave	NB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2020/21
131	2749	Folsom Blvd	WB	F	La Riviera Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2020/21
132	3350	Auburn Blvd	WB	F	Manzanita Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
133	647	Auburn Blvd	EB	N	Rosebud Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
134	2160	Franklin Blvd	NB	F	Meadowgate Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
135	3070	Watt Ave	SB	F	Myrtle Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2020/21
136	1356	Fair Oaks Blvd	SB	N	Gibbons Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
137	3060	Watt Ave	SB	F	Elkhorn Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
138	1521	Fulton Ave	SB	F	Edison Ave	\$20,000	\$0	\$20,000	\$0	\$5,000	\$45,000	2020/21
139	1566	Fulton Ave	NB	F	Edison Ave	\$20,000	\$0	\$10,000	\$0	\$5,000	\$35,000	2021/22
140	3074	Watt Ave	SB	F	Auburn Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
141	1559	Fulton Ave	NB	F	El Camino Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
142	3049	Watt Ave	SB	F	Roseville Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
143	3064	Watt Ave	SB	F	Freedom Park Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
144	645	Auburn Blvd	EB	N	Palm Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
145	1108	Arden Way	EB	F	Professional Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
146	3066	Watt Ave	SB	F	N Haven Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
147	1519	Auburn Blvd	WB	F	Morse Ave	\$20,000	\$0	\$10,000	\$20,000	\$5,000	\$55,000	2021/22
148	1107	Arden Way	EB	F	Morse Avey	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
149	1531	Fulton Ave	SB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
150	3071	Watt Ave	SB	F	Winona Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
151	3091	Watt Ave	NB	F	Kings Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
152	761	Elverta Rd	WB	F	Watt Ave	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	2021/22

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
153	1421	Whitney Ave	EB	F	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
154	1532	Fulton Ave	SB	F	Wittkop Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
155	2751	Folsom Blvd	WB	F	La Quinta Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
156	3132	65th St	NB	F	Stockton Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
157	3353	Auburn Blvd	WB	N	Hemlock St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
158	2757	Folsom Blvd	EB	F	Starfire Dr	\$0	\$0	\$0	\$0	\$0	\$0	2022/23
159	3287	Main Ave	NB	F	Madison Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
160	1488	Fair Oaks Blvd	EB	F	Wayside Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
161	1355	Fair Oaks Blvd	WB	F	Wayside Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
162	1555	Fulton Ave	NB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
163	3065	Watt Ave	SB	F	Karl Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
164	1361	Fair Oaks Blvd	SB	F	Robertson Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
165	1529	Fulton Ave	SB	F	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
166	1846	Stockton Blvd	SB	F	Parker Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
167	2592	Franklin Blvd	SB	F	47th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
168	1556	Fulton Ave	NB	F	Alta Arden Expy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
169	1852	Stockton Blvd	SB	N	Southwest Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
170	1530	Fulton Ave	SB	F	Alta Arden Expwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
171	1845	Stockton Blvd	SB	F	17th Ave	\$0	\$0	\$0	\$0	\$0	\$0	2022/23
172	1363	Fair Oaks Blvd	SB	F	Marconi Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
173	1549	Munroe St	NB	F	Fair Oaks Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
174	2954	La Riviera Dr	NB	F	Folsom Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
175	1482	Fair Oaks Blvd	NB	N	Robertson Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
176	3108	Watt Ave	NB	F	N Haven Dr	\$20,000	\$0	\$0	\$120,000	\$5,000	\$145,000	TBA
177	674	Mission Ave	SB	F	Engle Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
178	651	Dudley Blvd	SB	F	Palm Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
179	3509	Madison Ave	WB	F	Harrison St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
180	1646	Morse Ave	SB	F	Cottage Way	\$0	\$0	\$0	\$0	\$0	\$0	TBA
181	2377	Gerber Rd	EB	F	Stockton Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
182	2688	Sky Pkwy	EB	F	6th Pkwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
183	2408	Stockton Blvd	SB	F	Gerber Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
184	3297	Greenback Ln	WB	F	Trajan Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
185	3298	Greenback Ln	WB	F	Hoffman Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
186	1131	Butano Dr	EB	F	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
187	4088	Cottonwood Ln	NB	F	Ivy Creek Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
188	1368	El Camino Ave	WB	N	Gunn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
189	675	Mission Ave	SB	N	Whitney Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
190	1469	El Camino Ave	EB	F	Eastern Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
191	3994	National Dr	EB	N	Lennane Dr	\$20,000	\$0	\$0	\$60,000	\$5,000	\$85,000	TBA
192	1652	Morse Ave	SB	N	Hurley Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
193	1428	Mission Ave	NB	F	Whitney Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
194	3860	Branch Ctr	NB	N	Goethe Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
195	1091	Ethan Way	NB	N	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
196	3989	National Dr	WB	F	Lennane Dr	\$20,000	\$0	\$0	\$60,000	\$5,000	\$85,000	TBA
197	3183	Howe Ave	SB	F	Cottage Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
198	1671	Morse Ave	NB	F	Hurley Way	\$20,000	\$0	\$20,000	\$160,000	\$5,000	\$205,000	TBA
199	1374	El Camino Ave	WB	F	Eastern Ave	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	TBA
200	3903	Howe Ave	NB	F	Delma Way	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	TBA
201	567	Auburn Blvd	WB	F	Jones Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
202	3107	Watt Ave	NB	F	A St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
203	721	Elverta Rd	EB	N	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
204	1343	San Juan Ave	SB	N	Pheasant Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
205	1557	Fulton Ave	NB	F	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
206	1597	Auburn Blvd	EB	N	Morse Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
207	1481	Fair Oaks Blvd	NB	F	Palm Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA
208	2601	Franklin Blvd	NB	F	M L King Blvd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
209	1861	Stockton Blvd	SB	F	Lindale Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
210	3072	Watt Ave	SB	F	Orange Grove Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
211	3115	Watt Ave	NB	F	Elkhorn Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
212	1844	Stockton Blvd	SB	F	15th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
213	1526	Fulton Ave	SB	N	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
214	1554	Fulton Ave	NB	F	Maison Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
215	1561	Fulton Ave	NB	N	Marconi Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
216	3056	Watt Ave	SB	F	U St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
217	644	Auburn Blvd	EB	F	Hemlock St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
218	1847	Stockton Blvd	SB	N	Roosevelt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
219	1156	Arden Way	WB	F	Professional Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
220	2962	Watt Ave	NB	N	Fair Oaks Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
221	2379	Gerber Rd	EB	F	Palmer House Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
222	2404	Gerber Rd	WB	F	Power Inn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
223	1159	Arden Way	WB	F	Fulton Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
224	3106	Watt Ave	NB	F	McClellan Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
225	3590	Coyle Ave	WB	F	Southbrook Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
226	2796	Fair Oaks BLvd	NB	N	California Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
227	3086	Watt Ave	SB	N	Northup Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
228	3094	Watt Ave	NB	F	Lynn Way 1	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
229	3627	Coyle Ave	EB	N	St Charles Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
230	453	Fair Oaks Blvd	EB	N	Marshall Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
231	566	Auburn Blvd	WB	F	Orange Grove Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
232	2594	Franklin Blvd	SB	F	52nd Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
233	2599	Franklin Blvd	NB	F	Turnbridge Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
234	2600	Franklin Blvd	NB	F	Cuny Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
235	3061	Watt Ave	SB	F	Van Owen St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
236	3293	Greenback Ln	WB	F	Hazel Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
237	1520	Fulton Ave	SB	F	Auburn Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
238	2409	Stockton Blvd	SB	F	Massie Ct	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
239	2923	Fair Oaks Blvd	SB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
240	636	Orange Grove Ave	EB	F	Auburn Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
241	681	Whitney Ave	WB	N	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
242	2934	Fair Oaks Blvd	SB	N	California Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
243	3503	Airbase Dr	EB	F	Watt Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
244	3960	Elkhorn Blvd	EB	F	Watt Ave	\$20,000	\$5,000	\$10,000	\$80,000	\$5,000	\$120,000	TBA
245	4000	Antelope Rd	EB	F	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
246	646	Auburn Blvd	EB	F	Garfield Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
247	1141	Fair Oaks Blvd	SB	F	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
248	1353	Fair Oaks Blvd	WB	F	Marshall Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
249	1560	Fulton Ave	NB	F	Elvyra Way	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	TBA
250	2913	Elkhorn Blvd	WB	F	Cantel Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
251	2935	Fair Oaks Blvd	WB	N	Sunrise Blvd	\$20,000	\$0	\$80,000	\$5,000	\$5,000	\$110,000	TBA
252	3040	Fair Oaks Blvd	NB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
253	726	Dry Creek Rd	NB	N	G St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
254	2750	Folsom Blvd	WB	N	Norcade Cir	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
255	2795	Fair Oaks BLvd	EB	F	Sunrise Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
256	3062	Watt Ave	SB	F	I St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
257	3119	Watt Ave	NB	F	Quinn Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
258	633	Auburn Blvd	EB	F	Beresford Way	\$20,000	\$5,000	\$5,000	\$8,000	\$5,000	\$43,000	TBA
259	796	Dry Creek Rd	SB	N	G St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
260	1522	Fulton Ave	SB	F	Pope Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
261	1596	Auburn Blvd	EB	F	Fulton BLvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
262	2023	Sky Pkwy	NB	F	65th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
263	2595	Franklin Blvd	SB	F	Turnbridge Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA
264	2756	Folsom Blvd	EB	F	La Duinta Dr	\$20,000	\$5,000	\$0	\$5,000	\$5,000	\$35,000	TBA
265	3085	Watt Ave	SB	N	Windsor Way	\$20,000	\$5,000	\$20,000	\$60,000	\$5,000	\$110,000	TBA
266	3623	Coyle Ave	EB	F	Manzanita Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
267	1500	San Juan Ave	NB	F	Fairway Two Ave	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
268	2019	Florin Mall Dr	NB	N	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
269	3057	Watt Ave	SB	F	Larchmont Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
270	3104	Watt Ave	NB	F	Air Base Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
271	3112	Watt Ave	NB	F	Bolivar Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
272	1386	El Camino Ave	WB	F	Fulton Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
273	1565	Fulton Ave	NB	N	Pope Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
274	3615	Manzanita Ave	NB	F	Lincoln Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
275	4777	Elsie Ave	WB	N	Stockton Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
276	4778	Elsie Ave	EB	F	Stockton Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
277	1381	El Camino Ave	WB	F	Yorktown Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
278	1490	Fair Oaks Blvd	EB	F	Queenston Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
279	1523	Fulton Ave	SB	F	Carson Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
280	2170	Orange Ave	EB	F	Florin Mall Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
281	2174	Franklin Blvd	NB	N	A Parkway	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
282	2598	Franklin Blvd	NB	F	Green Tree Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
283	3033	Fair Oaks Blvd	EB	N	Watkins Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
284	3351	Auburn Blvd	WB	F	Rosebud Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
285	3957	Don Julio Blvd	WB	F	San Martin St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
286	1226	California Ave	SB	F	Fair Oaks Blvd	\$30,000	\$0	\$0	\$0	\$5,000	\$35,000	TBA
287	2927	Fair Oaks Blvd	SB	F	Sunset Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
288	3039	Fair Oaks Blvd	NB	F	Woodleaf Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
289	3058	Watt Ave	SB	F	Q St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
290	3294	Greenback Ln	WB	N	Hickory Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
291	4106	Power Inn Rd	SB	F	Gerber Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
292	5275	Fair Oaks Blvd	WB	N	Old Winding Wy	\$25,000	\$0	\$10,000	\$0	\$5,000	\$40,000	TBA
293	1213	Dewey Dr	SB	F	Coyle Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
294	1438	Myrtle Ave	EB	F	Auburn Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
295	1598	Auburn Blvd	EB	N	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
296	2171	Orange Ave	EB	N	Citrus Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
297	2602	Franklin Blvd	NB	N	47th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
298	3093	Watt Ave	NB	F	Lynn Way 1	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
299	3594	Coyle Ave	WB	F	Casa Alegre	\$25,000	\$0	\$0	\$0	\$5,000	\$30,000	TBA
300	3628	Coyle Ave	EB	F	St Charles Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
301	4415	Elsie Ave	EB	F	La Fiesta Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
302	634	Auburn Blvd	EB	F	Norris Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
303	1183	Fair Oaks Blvd	NB	F	Frontier Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
304	1365	El Camino Ave	WB	F	Fair Oaks Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

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305	2593	Franklin Blvd	SB	F	M L King Blvd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
306	3007	La Riviera Dr	EB	N	Folsom Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
307	3037	Fair Oaks Blvd	NB	F	Sunset Ave	\$20,000	\$0	\$0	\$150,000	\$5,000	\$175,000	TBA
308	3274	Madison Ave	EB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
309	3292	Greenback Ln	WB	N	Pecan Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
310	3589	Coyle Ave	WB	F	Dewey Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
311	3602	Manzanita Ave	SB	F	Locust Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
312	1550	Fulton Ave	NB	F	Sierra Blvd	\$20,000	\$5,000	\$20,000	\$15,000	\$5,000	\$65,000	TBA
313	2928	Fair Oaks Blvd	SB	F	Monte Park Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
314	3290	Greenback Ln	WB	F	Chestnut Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
315	3291	Greenback Ln	WB	F	Filbert Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
316	4774	Fair Oaks Blvd	NB	F	McMillan Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
317	1292	California Ave	NB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$75,000	\$80,000	\$5,000	\$185,000	TBA
318	1853	Stockton Blvd	SB	F	Gordon DR	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
319	2924	Fair Oaks Blvd	SB	F	Woodleak Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
320	3059	Watt Ave	SB	F	Owens Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
321	3117	Watt Ave	NB	F	Turner Dr	\$20,000	\$5,000	\$0	\$0	\$15,000	\$40,000	TBA
322	3286	Madison Ave	EB	N	Pershing Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
323	3592	Coyle Ave	WB	F	Hesper Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
324	3769	Don Julio Blvd	EB	F	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
325	1143	Fair Oaks Blvd	SB	F	Shelfield Dr	\$20,000	\$5,000	\$0	\$20,000	\$5,000	\$50,000	TBA
326	1146	Fair Oaks Blvd	SB	F	Westminster Ct	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	TBA
327	1564	Fulton Ave	NB	F	Carson Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
328	2932	Main St	SB	F	Sacramento St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
329	3036	Fair Oaks Blvd	NB	N	Monte Park Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
330	3063	Watt Ave	SB	F	Don Julio Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
331	1341	San Juan Ave	SB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
332	11069	Main Ave	NB	N	Greenback Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
333	11145	Madison Ave	SB	N	Main Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
334	11160	Main Ave	NB	F	Madison Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
335	2884	Kiefer Blvd	EB	F	Southport Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
336	3098	Watt Ave	NB	F	Longview Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
337	1666	Northrop Ave	EB	F	Fulton Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
338	1656	Northrop Ave	WB	F	Fulton Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
339	2168	Florin Mall	SB	F	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
340	1074	Butano Dr	WB	F	Park Townr Cir	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
341	1527	Fulton Ave	SB	F	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
342	1537	Munroe St	SB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

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343	3113	Watt Ave	NB	F	Channing Dr	\$20,000	\$0	\$0	\$0	\$15,000	\$35,000	TBA
344	3087	Watt Ave	SB	F	Cosmos Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
345	1535	Fulton Ave	SB	F	Northrop Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
346	794	Elkhorn Blvd	EB	N	Dry Creek Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
347	2029	Steiner Rd	NB	N	51st St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
348	1660	Northrop Ave	WB	N	Howe Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
349	3969	Elkhorn Blvd	EB	F	Andrea Blvd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
350	1236	Marconi Ave	WB	F	La Vista Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
351	1425	Whitney Ave	EB	F	Eastern Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
352	1129	Butano Dr	NB	F	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
353	2889	Branch Ctr	NB	F	Agriculture Ln	\$0	\$0	\$0	\$0	\$0	\$0	TBA
354	3953	Don Julio Blvd	WB	N	Scotia Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
355	2700	44th St	NB	F	37th Ave	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
356	1175	Fair Oaks Blvd	NB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
357	1277	Marconi Ave	EB	F	Mission Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
358	1387	El Camino Ave	WB	F	Wright St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
359	3775	Don Julio Blvd	EB	F	Craighurst Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
360	496	Kiefer Blvd	WB	F	Mayhew Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
361	1102	Arden Way	EB	F	Howe Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
362	3772	Don Julio Blvd	EB	N	Belva Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
363	1241	Marconi Ave	WB	F	Mission Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
364	1373	El Camino Ave	WB	F	Greenwood Ave	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	TBA
365	2691	47th Ave	WB	F	Steiner Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
366	2888	Kiefer Blvd	EB	F	Mayhew Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
367	2683	Steiner Rd	SB	F	47th Ave	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
368	2912	Elkhorn Blvd	WB	F	Thomas Dr	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
369	4087	Cottonwood Ln	SB	N	Ivy Creek Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
370	3123	Watt Ave	NB	F	Navaho Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
371	1273	Marconi Ave	EB	N	Becerra way	\$20,000	\$0	\$5,000	\$40,000	\$5,000	\$70,000	TBA
372	1076	Butano Dr	SB	N	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
373	3626	Coyle Ave	EB	N	Milburn St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
374	2030	Steiner Rd	NB	F	Sitton Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
375	1282	Marconi Ave	EB	N	La Vista Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
376	1424	Whitney Ave	EB	F	Norris Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
377	2907	Elkhorn Blvd	WB	F	Don Julio Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
378	3864	Branch Ctr	SB	N	Agriculture Ln	\$0	\$0	\$0	\$0	\$0	\$0	TBA
379	3055	Watt Ave	SB	F	Davidson Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
380	3122	Watt Ave	NB	F	Blackfoot Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
381	678	Whitney Ave	WB	F	Norris Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
382	1104	Arden Way	EB	F	Clinton Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
383	1274	Marconi Ave	EB	N	Norris Ave	\$20,000	\$0	\$18,000	\$92,000	\$5,000	\$135,000	TBA
384	1654	Morse Ave	SB	N	Northrop Ave	\$20,000	\$0	\$20,000	\$60,000	\$5,000	\$105,000	TBA
385	3545	Hillsdale Blvd	SB	F	Robert Frost Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
386	1455	El Camino Ave	EB	F	Howe Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
387	2696	44th St	NB	N	Lemon Hill Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
388	3591	Coyle Ave	WB	F	Park Oaks Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
389	1245	Marconi Ave	WB	F	Becerra Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
390	3618	Manzanita Ave	NB	F	Bourbon Dr	\$20,000	\$5,000	\$22,500	\$40,000	\$5,000	\$92,500	TBA
391	781	M St	WB	N	8th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
392	2684	Steiner Rd	SB	F	51st Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
393	2905	Elkhorn Blvd	WB	F	Andrea Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
394	3967	Elkhorn Blvd	EB	F	Don Julio Blvd	\$20,000	\$0	\$0	\$0	\$10,000	\$30,000	TBA
395	3588	Madison Ave	WB	N	Dewey Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
396	3970	Elkhorn Blvd	EB	F	Crater Way	\$20,000	\$0	\$0	\$0	\$10,000	\$30,000	TBA
397	740	M St	EB	F	8th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
398	1470	El Camino Ave	EB	N	Greenwood Ave	\$20,000	\$0	\$10,000	\$40,000	\$5,000	\$75,000	TBA
399	3460	Hazel Ave	SB	F	Sunset Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
400	1281	Marconi Ave	EB	F	Garfield Ave	\$20,000	\$0	\$18,000	\$80,000	\$5,000	\$123,000	TBA
401	3278	Madison Ave	EB	F	Kenneth Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
402	3481	Elm Ave	WB	F	Hazel Ave	\$25,000	\$5,000	\$5,000	\$40,000	\$5,000	\$80,000	TBA
403	3596	Manzanita Ave	SB	N	Peppermill Ct	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
404	3621	Manzanita Ave	NB	N	Ellerslee Dr	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	TBA
405	3861	Branch Ctr	SB	N	Preservation Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
406	4100	Gerber Rd	WB	N	Reese Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
407	3182	Howe Ave	NB	F	Delma St	\$20,000	\$10,000	\$20,000	\$60,000	\$25,000	\$135,000	TBA
408	520	Peacekeeper Way	WB	F	Dudley Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
409	544	Dudley Blvd	SB	N	Peacekeeper Way	\$30,000	\$0	\$0	\$0	\$5,000	\$35,000	TBA
410	3472	Hazel Ave	NB	F	Sunset Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
411	4091	Elsie Ave	WB	N	Hemingway Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
412	479	Forcum Ave	SB	F	Dudley Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
413	2891	Branch Ctr	NB	N	Preservation Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
414	3483	Beech Ave	NB	F	Mavis Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
415	3530	Hillsdale Blvd	NB	N	Robert Frost Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
416	9926	Old Placerville Rd	EB	F	Macready Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
417	677	Whitney Ave	WB	N	Mulholland Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
418	3461	Hazel Ave	SB	F	N Winding Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
419	4084	Elsie Ave	EB	F	Halbrite Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
420	1457	El Camino Ave	EB	F	Gwen Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
421	1655	Northrop Ave	WB	F	Jonas Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
422	2892	Branch Ctr	NB	F	Preservation Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
423	3471	Hazel Ave	NB	F	La Serena Dr	\$20,000	\$5,000	\$5,000	\$60,000	\$5,000	\$95,000	TBA
424	3862	Branch Ctr	SB	F	Preservation Way	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
425	1217	Dewey Dr	SB	F	Will Rogers Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
426	95334	Calvine Rd	WB	F	Vintage Park Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
427	95337	Calvine Rd	WB	F	Cliffcrest Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA
428	1144	Fair Oaks Blvd	SB	F	Garfield Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
429	1611	65th St	WB	F	53rd Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
430	2166	Stockton Blvd	NB	F	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
431	2375	Stockton Blvd	NB	F	Elsie Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
432	2392	Orange Ave	EB	N	Citrus Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
433	2931	Fair Oaks Blvd	SB	N	Winding Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
434	3357	College Oak Dr	SB	N	Myrtle Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
435	3185	Howe Ave	SB	F	Arden way	\$20,000	\$0	\$0	\$50,000	\$0	\$70,000	TBA
436	2945	Luce Ave	NB	F	Peacekeeper Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
437	3524	Hillsdale Blvd	NB	F	Palm Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
438	1388	El Camino Ave	WB	F	Bell St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
439	3963	Elkhorn Blvd	EB	F	Larchmont Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
440	3776	Walerga Rd	NB	F	Don Julio Blvd	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
441	3356	College Oak Dr	SB	F	Auburn Blvd	\$25,000	\$5,000	\$21,750	\$20,000	\$5,000	\$76,750	TBA
442	3109	Watt Ave	NB	F	Karl Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
443	2381	Power Inn Rd	NB	F	Gerber Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
444	1525	Fulton Ave	SB	F	Marconi Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
445	2163	Forest Pkwy	WB	N	Franklin Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
446	1105	Arden Way	EB	F	Fulton Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
447	727	Elkhorn Blvd	WB	F	Dry Creek Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
448	2161	Forest Pkwy	EB	F	Franklin Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
449	3121	Watt Ave	NB	F	Antelope Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
450	3961	Elkhorn Blvd	EB	F	Cantel Way	\$20,000	\$5,000	\$10,000	\$80,000	\$5,000	\$120,000	TBA
451	2405	Gerber Rd	WB	F	Palmer House Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
452	2407	Gerber Rd	WB	F	Whisper Willow Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
453	3083	Watt Ave	SB	F	Hyde Way	\$20,000	\$5,000	\$15,000	\$60,000	\$5,000	\$105,000	TBA
454	9997	Palm St	EB	N	Dudley Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
455	1489	Fair Oaks Blvd	EB	F	California Ave	\$20,000	\$5,000	\$100,000	\$120,000	\$5,000	\$250,000	TBA
456	2376	Stockton Blvd	NB	F	Suncountry Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
457	2391	Stockton Blvd	SB	F	66th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
458	2929	Fair Oaks Blvd	SB	N	Winding Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
459	3035	Fair Oaks Blvd	NB	N	Orange Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
460	2378	Gerber Rd	EB	F	Par Parkway	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
461	4098	Gerber Rd	EB	F	Power Inn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
462	4413	Elsie Ave	WB	F	Robinette Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
463	4743	Antelope Rd	SB	F	Roseville Rd/Daly Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
464	3118	Watt Ave	NB	F	Quinn Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
465	1528	Fulton Ave	SB	F	Hernando Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
466	1558	Fulton Ave	NB	F	Loma Vista Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
467	2393	Orange Ave	EB	N	Florin Mall Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
468	2406	Gerber Rd	WB	F	Gold Rod Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
469	3084	Watt Ave	SB	F	Hurley Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
470	2925	Fair Oaks Blvd	SB	N	Orangetree Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
471	3116	Watt Ave	NB	F	Plymouth Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
472	3296	Greenback Ln	WB	F	Kenneth Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
473	4414	Elsie Ave	WB	F	La Fiesta Wy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
474	3032	Fair Oaks Blvd	NB	F	Winding Way	\$20,000	\$5,000	\$20,000	\$120,000	\$5,000	\$170,000	TBA
475	3082	Watt Ave	SB	F	Barrington Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
476	1227	California Ave	SB	F	Casita Ave	\$20,000	\$0	\$50,000	\$20,000	\$5,000	\$95,000	TBA
477	1228	California Ave	SB	N	Sutter Ave	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	TBA
478	1244	Marconi Ave	WB	F	Norris Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
479	2885	Kiefer Blvd	EB	F	Tallyho Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
480	3504	Madison Ave	EB	F	Jackson St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
481	3549	Hillsdale Blvd	SB	N	Greenholme Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
482	2692	47th Ave	WB	F	Sampson Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
483	1263	Marconi Ave	EB	F	Darwin St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
484	1385	El Camino Ave	WB	F	Parkwood Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
485	1371	El Camino Ave	WB	F	Landwood Way	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
486	3868	Kiefer Blvd	WB	F	Huntsman Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
487	1471	El Camino Ave	EB	F	Mission Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
488	2825	Bradshaw Rd	SB	F	Old Placer Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
489	3052	Watt Ave	SB	F	Elverta Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
490	3189	Howe Ave	SB	N	Village Dr	\$20,000	\$5,000	\$0	\$0	\$45,000	\$70,000	TBA
491	3510	Madison Ave	WB	F	Jackson St	\$0	\$0	\$0	\$0	\$0	\$0	TBA
492	3904	Howe Ave	NB	F	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
493	3190	Howe Ave	SB	F	Northrop Ave	\$20,000	\$0	\$0		\$5,000	\$25,000	TBA
494	3870	Kiefer Blvd	WB	N	Southport Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
495	3184	Howe Ave	SB	F	Wyda Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
496	3897	Howe Ave	NB	N	Village Dr	\$20,000	\$0	\$0	\$10,000	\$5,000	\$35,000	TBA
497	3540	Andrea Blvd	WB	F	Elkhorn Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
498	2887	Kiefer Blvd	EB	F	Tallyho Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
499	2604	M L King Blvd	NB	F	44th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
500	3901	Howe Ave	NB	F	Wyda Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
501	1205	Sunrise Blvd	SB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
502	2903	Elkhorn Blvd	WB	F	Diablo Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
503	1239	Marconi Ave	WB	F	Walnut Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
504	2588	M L King Jr Blvd	SB	F	32nd Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
505	2590	M L King Jr Blvd	SB	F	44th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
506	1663	Northrop Ave	EB	F	Bell St	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
507	3896	Howe Ave	NB	F	Northrop Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
508	650	James Way	WB	F	Dudley Blvd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
509	2603	M L King Blvd	NB	F	47th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
510	2704	44th St	NB	F	Fruitridge Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
511	637	Orange Grove Ave	EB	F	Pasadena Ave	\$20,000	\$5,000	\$7,500	\$20,000	\$5,000	\$57,500	TBA
512	2589	M L King Jr Blvd	SB	F	41st Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
513	3869	Kiefer Blvd	WB	F	Tallyho Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
514	1475	El Camino Ave	EB	F	Walnut Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
515	1279	Marconi Ave	EB	F	Walnut Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
516	2707	44th St	NB	N	23rd Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
517	2665	44th St	SB	F	Roosevelt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
518	2672	44th St	SB	F	Iowa Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
519	4753	N Market Blvd	WB	F	Sports Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
520	3544	Hillsdale Blvd	SB	F	McCloud Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
521	9939	Mission Ave	NB	N	Engle Rd	\$25,000	\$0	\$0	\$80,000	\$5,000	\$110,000	TBA
522	1349	Fair Oaks Blvd	WB	F	Grover Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
523	2911	Elkhorn Blvd	WB	F	Larchmont Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
524	2399	Palmer House Dr	SB	N	Pickoi Cir	\$20,000	\$0	\$0	\$60,000	\$5,000	\$85,000	TBA
525	2027	Steiner Rd	NB	N	53rd Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
526	2666	23rd Ave	WB	F	44th St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
527	2385	Palmer House Dr	NB	F	Pickoi Cir	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
528	2703	44th St	NB	F	Iowa Ave	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
529	3531	Hillsdale Blvd	NB	F	McCloud Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
530	3547	Hillsdale Blvd	SB	F	Brett Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
531	4007	Walerga Rd	SB	N	Don Julio Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
532	2398	Lindale Dr	EB	F	Palmer House Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
533	3528	Hillsdale Blvd	NB	F	Brett Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
534	3962	Elkhorn Blvd	EB	F	Thomas Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
535	1670	Morse Ave	NB	F	Amberwood Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
536	3552	Hillsdale Blvd	SB	F	Frizell Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
537	3773	Don Julio Blvd	EB	F	Garland Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
538	4090	Elsie Ave	WB	N	Wilbur Wy	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
539	2686	Steiner Rd	SB	F	53rd Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
540	2038	Center Pkwy	SB	N	South Pkwy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
541	2037	Center Pkwy	SB	F	Goya Pkwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
542	2162	Forest Pkwy	WB	F	Center Pkwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
543	2387	Lindale Dr	WB	F	Palmer House Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
544	4085	Elsie Ave	EB	F	Wilbur Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
545	3533	Andrea Blvd	EB	F	Hillsdale Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
546	743	10th St	NB	N	Q St	\$20,000	\$0	\$15,000	\$40,000	\$5,000	\$80,000	TBA
547	1494	Fair Oaks Blvd	EB	F	Dell Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
548	3950	Don Julio Blvd	WB	N	Craighurst Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
549	778	10th St	SB	F	Q St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
550	3276	Madison Ave	EB	F	Chicago Ave	\$20,000	\$5,000	\$20,000	\$60,000	\$5,000	\$110,000	TBA
551	3529	Hillsdale Blvd	NB	F	Diablo Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
552	3003	La Riviera Dr	EB	F	Waterton Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
553	667	Pasadena Ave	SB	F	Cypress Ave	\$20,000	\$5,000	\$5,000	\$10,000	\$5,000	\$45,000	TBA
554	1653	Morse Ave	SB	F	Hampton Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
555	2397	Lindale Dr	EB	F	Ewing Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
556	2708	44th St	NB	N	Roosevelt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
557	2958	La Riviera Dr	WB	F	Waterman Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
558	2388	Lindale Dr	WB	F	Rimrock Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
559	1216	Dewey Dr	SB	F	Oakcrest Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
560	1218	Dewey Dr	SB	F	Moraga DR	\$20,000	\$0	\$12,000	\$0	\$5,000	\$37,000	TBA
561	3277	Madison Ave	EB	F	Buena Vista Ave	\$20,000	\$5,000	\$20,000	\$80,000	\$5,000	\$130,000	TBA
562	9940	Engle Rd	WB	F	Miradera St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
563	1223	Jan Dr	SB	N	Rampart Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
564	1224	Jan Dr	SB	N	Otis Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
565	1295	Jan Dr	NB	N	Rampart Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
566	760	Elverta Rd	EB	F	Electric Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
567	770	Elverta Rd	WB	F	Rio Linda Blvd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
568	1300	Dewey Dr	NB	F	Papaya Dr	\$30,000	\$0	\$0	\$0	\$5,000	\$35,000	TBA
569	2966	Watt Ave	NB	F	El Ricon Way	\$20,000	\$0	\$20,000	\$5,000	\$5,000	\$50,000	TBA
570	9941	Engle Rd	EB	F	Miradera St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
571	1294	Jan Dr	NB	N	Jaspar Ct	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
572	95018	Calvine Rd	WB	F	Kingsbridge Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA
573	447	Hazel Ave	NB	F	Nimbus Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
574	3451	Hazel Ave	SB	N	Pitman Lane Ln	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
575	3464	Hazel Ave	NB	F	Central Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
576	1094	El Camino Ave	EB	F	Ethan Way	\$20,000	\$0	\$5,000	\$5,000	\$5,000	\$35,000	TBA
577	1250	Marconi Ave	WB	F	Calderwood Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
578	2681	44th St	SB	F	45th Ave	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
579	1659	Northrop Ave	WB	F	Bell St	\$30,000	\$0	\$0	\$0	\$5,000	\$35,000	TBA
580	2664	44th St	SB	F	18th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
581	3124	Watt Ave	NB	F	Timberlane Pl	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
582	2217	Fruitridge Rd	WB	F	44th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
583	2826	Goethe Rd	WB	F	Branch Center Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
584	1243	Marconi Ave	WB	F	Eastern Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
585	1268	Marconi Ave	EB	N	Corabel Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
586	1493	Fair Oaks Blvd	EB	F	Hollister Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
587	2293	Fruitridge Rd	EB	F	Mendocino Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
588	2609	M L King Blvd	NB	N	23rd St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
589	2680	44th St	SB	F	44th Ave	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
590	2909	Elkhorn Blvd	WB	F	Butterball Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
591	5346	Andrea Blvd	NB	N	Elkhorn Blvd	\$20,000	\$5,000	\$19,500	\$0	\$5,000	\$49,500	TBA
592	1266	Marconi Ave	EB	F	Wright St	\$20,000	\$10,000	\$15,000	\$0	\$5,000	\$50,000	TBA
593	1460	El Camino Ave	EB	F	Meadowbrook Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
594	1499	San Juan Ave	NB	F	Sunset Ave	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
595	2886	Kiefer Blvd	EB	F	Huntsman Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
596	3525	Hillsdale Blvd	NB	F	Greenholme Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
597	3620	Manzanita Ave	NB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
598	1383	El Camino Ave	WB	F	Morse Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
599	4006	Walerga Rd	SB	F	Elkhorn Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
600	1103	Arden Way	EB	F	Bell St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
601	1456	El Camino Ave	EB	F	Bell St	\$20,000	\$0	\$0	\$120,000	\$5,000	\$145,000	TBA
602	2699	44th St	NB	N	39th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
603	2961	La Riviera Dr	WB	N	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
604	2662	44th St	SB	F	14th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
605	1157	Arden Way	WB	F	Morse Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
606	1280	Marconi Ave	EB	F	Gunn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
607	2292	Fruitridge Rd	EB	F	ML King Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
608	2295	Fruitridge Rd	EB	F	44th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
609	2682	47th Ave	EB	F	47th St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
610	3180	Howe Ave	SB	F	Rassy Way	\$20,000	\$5,000			\$5,000	\$30,000	TBA
611	3899	Howe Ave	NB	N	Villa Font Way	\$20,000	\$5,000	\$0	\$0	\$15,000	\$40,000	TBA
612	1476	El Camino Ave	EB	F	Gunn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
613	1238	Marconi Ave	WB	F	Linden Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
614	1350	Fair Oaks Blvd	WB	F	Hollister Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
615	1372	El Camino Ave	WB	F	Mission Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
616	2218	Fruitridge Rd	WB	F	Enrico Blvd	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
617	1665	Northrop Ave	EB	F	Carro Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
618	2675	44th St	SB	F	37th Ave	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
619	4708	N Market Blvd	EB	N	Sierra Point Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
620	1366	El Camino Ave	WB	F	El Tonas Way	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
621	1657	Northrop Ave	WB	F	Carro Dr	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
622	2824	Bradshaw Rd	SB	F	Country Roads Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
623	1161	Arden Way	WB	F	Bell St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
624	1664	Northrop Ave	EB	F	Park	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
625	2291	Fruitridge Rd	EB	F	Franklin Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
626	2709	44th St	NB	N	18th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
627	4005	Walerga Rd	SB	F	Galbrath Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
628	1673	Morse Ave	NB	F	Trussel Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
629	2711	44th St	NB	N	14th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
630	3777	Walerga Rd	NB	F	Elkhorn Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
631	1377	El Camino Ave	WB	F	Andrade Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
632	1477	El Camino Ave	EB	F	Garfield Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
633	3458	Hazel Ave	SB	F	Madison Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
634	1672	Morse Ave	NB	F	Berkshire Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
635	3778	Walerga Rd	NB	F	Galbrath Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
636	791	Elkhorn Blvd	EB	F	Rio Linda Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
637	1264	Marconi Ave	EB	F	Pyramid Way	\$20,000	\$20,000	\$55,000	\$0	\$5,000	\$100,000	TBA
638	2693	47th Ave	WB	F	Laurine Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
639	4754	N Market Blvd	WB	F	Northgate Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
640	1650	Morse Ave	SB	F	Trussel Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
641	3005	La Riviera Dr	EB	F	Payette Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
642	1252	Marconi Ave	WB	F	Wright St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
643	3905	Howe Ave	NB	F	Tallac Street	\$15,000	\$2,400	\$6,000	\$60,000	\$3,000	\$86,400	TBA
644	4092	Elsie Ave	WB	N	Iona Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
645	731	Elkhorn Blvd	WB	F	Rio Linda Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
646	1151	Arden Way	WB	F	Eastern Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
647	1160	Arden Way	WB	N	Wright St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
648	1240	Marconi Ave	WB	F	Root Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
649	1367	El Camino Ave	WB	F	Garfield Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
650	1658	Northrop Ave	WB	F	Dornajo Way	\$20,000	\$5,000	\$0	\$20,000	\$5,000	\$50,000	TBA
651	2297	Fruitridge Rd	EB	N	Sampson Blvd	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
652	2670	44th St	SB	N	Fruitridge Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
653	2706	44th St	NB	F	Sierra Vista Ave	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
654	2797	Sunrise Blvd	NB	F	Winding Way	\$20,000	\$0	\$0	\$0	\$10,000	\$30,000	TBA
655	2798	Sunrise Blvd	NB	F	Sunset Ave	\$20,000	\$5,000	\$0	\$20,000	\$5,000	\$50,000	TBA
656	3629	Madison Ave	EB	F	Dewey Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
657	3783	Antelope Rd	WB	F	Black Bear Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
658	6505	Franklin Blvd	NB	N	Village Star Dr	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
659	1162	Arden Way	WB	F	Howe Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
660	1207	Sunrise Blvd	SB	F	Sunset Ave	\$50,000	\$5,000	\$0	\$0	\$5,000	\$60,000	TBA
661	1278	Marconi Ave	EB	F	Root Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
662	1466	El Camino Ave	EB	F	Andrade Way	\$20,000	\$5,000	\$20,000	\$40,000	\$5,000	\$90,000	TBA
663	2606	M L King Blvd	NB	N	32nd Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
664	3281	Madison Ave	EB	N	Hazel Ave	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	TBA
665	3527	Hillsdale Blvd	NB	F	Walerga Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
666	3866	Kiefer Blvd	WB	N	N Kiefer Blvd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
667	3968	Elkhorn Blvd	EB	F	Hillsdale Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
668	2960	La Riviera Dr	WB	F	Woodman Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
669	3965	Elkhorn Blvd	EB	F	Butterball Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
670	797	Dry Creek Rd	SB	N	E St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
671	1075	Butano Dr	SB	N	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$10,000	\$35,000	TBA
672	1113	Arden Way	EB	F	Eastern Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
673	1304	Dewey Dr	NB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
674	2671	44th St	SB	F	Fruitridge Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
675	2719	Winding Way	EB	F	Sunrise Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
676	2874	Kiefer Blvd	EB	F	Brydon Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
677	3000	La Riviera Dr	EB	F	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
678	3282	Madison Ave	EB	F	Hazel Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
679	3782	Antelope Rd	WB	N	Holbrook Way	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
680	1468	El Camino Ave	EB	F	Avalon Dr	\$20,000	\$0	\$40,000	\$60,000	\$5,000	\$125,000	TBA
681	2587	M L King Jr Blvd	SB	F	Fruitridge Rd	\$20,000	\$0	\$0	\$160,000	\$5,000	\$185,000	TBA
682	1344	San Juan Ave	SB	N	Sunset Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
683	1465	El Camino Ave	EB	F	Butano Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
684	1497	San Juan Ave	NB	F	Winding Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
685	629	James Way	WB	F	Skvarla Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
686	1342	San Juan Ave	SB	N	Gail Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
687	1439	Myrtle Ave	EB	N	College Oak Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
688	2294	Fruitridge Rd	EB	F	42nd St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
689	2383	Palmer House Dr	NB	F	Scottsdale Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
690	2695	44th St	NB	F	44th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
691	3006	La Riviera Dr	EB	N	Tuolumne Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
692	3619	Manzanita Ave	NB	N	Muldrow Rd	\$20,000	\$5,000	\$22,500	\$40,000	\$5,000	\$92,500	TBA
693	3964	Elkhorn Blvd	EB	F	Walerga Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
694	1382	El Camino Ave	WB	F	Morse Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
695	1474	El Camino Ave	EB	F	Upham Ct	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
696	3028	Sunrise Blvd	NB	F	Gold Express Blvd	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
697	4089	Cottonwood Ln	NB	N	Elsie Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
698	638	Orange Grove Ave	EB	F	Sycamore Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
699	1130	Butano Dr	NB	F	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
700	1269	Marconi Ave	EB	F	Santa Paula Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
701	1346	San Juan Ave	SB	F	Heather Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
702	1495	San Juan Ave	NB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
703	2591	M L King Jr Blvd	SB	N	47th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
704	2676	44th St	SB	F	39th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
705	2710	44th St	NB	N	Orinda Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
706	2904	Elkhorn Blvd	WB	F	Crater Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
707	3598	Manzanita Ave	SB	F	Muldrow Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
708	4712	N Market Blvd	EB	F	National Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
709	495	AC Street	EB	N	Patrol Road	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
710	1370	El Camino Ave	WB	N	Laurence Ave	\$20,000	\$0	\$15,000	\$80,000	\$5,000	\$120,000	TBA
711	1478	El Camino Ave	EB	F	El Tonas Way	\$20,000	\$5,000	\$20,000	\$60,000	\$5,000	\$110,000	TBA
712	1651	Morse Ave	SB	F	Berkshire Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
713	2679	44th St	SB	F	41st St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
714	2908	Elkhorn Blvd	WB	F	Sprig Dr	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
715	2963	Watt Ave	NB	F	Northrop Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
716	4083	Elsie Ave	EB	F	Iona Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
717	747	Rio Linda Blvd	NB	F	Q St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
718	1101	Arden Way	EB	F	Ethan Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
719	1214	Dewey Dr	SB	F	Templeton Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
720	1246	Marconi Ave	WB	N	Montclair St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
721	1249	Marconi Ave	WB	F	Lacy Lane	\$20,000	\$5,000	\$85,000	\$60,000	\$5,000	\$175,000	TBA
722	1384	El Camino Ave	WB	F	Belport Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
723	1423	Whitney Ave	EB	F	Becerra Way	\$20,000	\$0	\$15,000	\$0	\$5,000	\$40,000	TBA
724	1472	El Camino Ave	EB	N	Landwood way	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	TBA
725	2400	Palmer House Dr	SB	F	Pickoi Cir	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
726	2678	47th Ave	EB	F	46th St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
727	9996	Luce Ave	NB	N	Palm St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
728	1092	Ethan Way	NB	F	Keith Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
729	1253	Marconi Ave	WB	F	Bell St	\$20,000	\$5,000	\$80,000	\$60,000	\$5,000	\$170,000	TBA
730	1461	El Camino Ave	EB	F	Morse Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
731	1669	Morse Ave	NB	F	Northrup Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
732	3054	Watt Ave	SB	F	Navaho Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
733	736	M St	EB	F	Rio Linda Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
734	1237	Marconi Ave	WB	F	Garfield Ave	\$20,000	\$0	\$29,000	\$106,000	\$5,000	\$160,000	TBA
735	1242	Marconi Ave	WB	F	Greenwood Ave	\$20,000	\$0	\$10,000	\$16,000	\$5,000	\$51,000	TBA
736	1272	Marconi Ave	EB	N	Montclair St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
737	2800	Sunrise Blvd	NB	F	Wildridge Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
738	2957	La Riviera Dr	WB	F	Rogue River Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
739	3125	Watt Ave	NB	F	Elverta Rd	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
740	3595	Manzanita Ave	SB	N	Nonnie Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
741	3632	Madison Ave	EB	N	San Juan Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
742	3987	National Dr	SB	F	N Market Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
743	739	M St	EB	N	Front St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
744	782	M St	WB	F	Front St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
745	1462	El Camino Ave	EB	F	Drayton Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
746	1473	El Camino Ave	EB	F	Via Camino Ave	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
747	3026	Sunrise Blvd	NB	F	Coloma Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
748	3273	Madison Ave	EB	F	Sunrise Blvd	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
749	639	College Oak Dr	NB	F	Orange Grove Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
750	758	Elverta Rd	EB	F	Dutch Haven Blvd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
751	763	Elverta Rd	WB	F	Dutch Haven Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
752	1215	Dewey Dr	SB	F	Madison Ave	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
753	1459	El Camino Ave	EB	F	Parkwood Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
754	2296	Fruitridge Rd	EB	N	Bonniemae Way	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
755	3770	Don Julio Blvd	EB	F	Bolivar Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
756	3988	National Dr	SB	N	National Dr	\$20,000	\$0	\$0	\$20,000	\$10,000	\$50,000	TBA
757	3995	National Dr	NB	F	National Dr	\$20,000	\$0	\$0	\$20,000	\$5,000	\$45,000	TBA
758	5361	San Juan Ave	SB	F	Lincoln Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
759	1128	Cottage Way	EB	F	Kincaid Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
760	1422	Whitney Ave	EB	F	Montclair St	\$20,000	\$0	\$25,000	\$80,000	\$5,000	\$130,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
761	2401	Scottsdale Dr	EB	F	Palmer House Dr	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
762	2873	Kiefer Blvd	EB	F	S Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
763	3771	Don Julio Blvd	EB	F	Larry Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
764	3955	Don Julio Blvd	WB	N	Larry Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
765	5360	Fair Oaks Blvd	SB	N	Walnut Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
766	679	Whitney Ave	WB	F	Becerra Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
767	741	Oak Lane	NB	F	9th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
768	774	Q St	EB	F	Rio Linda Blvd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
769	785	M St	WB	N	Rio Linda Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
770	798	Dry Creek Rd	SB	N	C St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
771	1106	Arden Way	EB	F	Berkshire Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
772	1158	Arden Way	WB	F	Avondale Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
773	1276	Marconi Ave	EB	N	Greenwood Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
774	1284	Palm Dr	EB	N	Panama Ave	\$20,000	\$5,000	\$0	\$48,000	\$5,000	\$78,000	TBA
775	1348	Fair Oaks Blvd	WB	F	San Juan Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
776	1369	El Camino Ave	WB	F	Walnut Ave	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
777	3475	Hazel Ave	NB	F	Madison Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
778	4096	Power Inn Rd	NB	F	Elsie Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
779	1077	Cottage Way	WB	F	Kincaid Way	\$20,000	\$0	\$15,000	\$20,000	\$5,000	\$60,000	TBA
780	1222	Jan Dr	SB	F	Winding Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
781	2667	44th St	SB	N	25th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
782	3521	Hillsdale Blvd	NB	F	Frizell Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
783	4086	Cottonwood Ln	SB	F	Elsie Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
784	4105	Gerber Rd	WB	N	Fernridge Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
785	666	Pasadena Ave	SB	F	Pasadena Ave	\$20,000	\$5,000	\$10,000	\$10,000	\$5,000	\$50,000	TBA
786	724	Dry Creek Rd	NB	F	C St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
787	780	Oak Ln	SB	N	9th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
788	1206	Sunrise Blvd	SB	F	Wildridge Dr	\$50,000	\$5,000	\$0	\$0	\$5,000	\$60,000	TBA
789	1248	Marconi Ave	WB	F	Ashbourne Dr	\$20,000	\$0	\$30,000	\$60,000	\$5,000	\$115,000	TBA
790	1275	Marconi Ave	EB	F	Eastern Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
791	1375	El Camino Ave	WB	F	Avalon Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
792	1376	El Camino Ave	WB	F	Ione St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
793	2390	Lindale Dr	WB	F	Sahara Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
794	2395	Lindale Dr	EB	F	Sahara Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
795	2906	Elkhorn Blvd	WB	N	Hillsdale Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
796	2964	Watt Ave	NB	F	Bodega Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
797	3462	Hazel Ave	SB	F	Winding Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
798	3477	Hazel Ave	NB	N	Hazel Hill Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
799	3956	Don Julio Blvd	WB	F	Bolivar Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
800	4702	N Market Blvd	EB	F	S Market Ct	\$20,000	\$5,000	\$42,000	\$0	\$5,000	\$72,000	TBA
801	5270	Antelope Rd	WB	N	Walerga Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
802	1467	El Camino Ave	EB	F	Ione St	\$20,000	\$5,000	\$10,000	\$20,000	\$5,000	\$60,000	TBA
803	3001	La Riviera Dr	EB	F	Woodman Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
804	3042	Fair Oaks Blvd	NB	F	Sunrise E Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
805	3043	Fair Oaks Blvd	NB	N	Sunbonnet Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
806	3541	Andrea Blvd	WB	N	Hillsdale Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
807	3954	Don Julio Blvd	WB	F	Belva Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
808	565	Orange Grove Ave	WB	N	Pasadena Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
809	759	Elverta Rd	EB	F	28th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
810	771	Rio Linda Blvd	SB	N	Delano St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
811	779	10th St	SB	N	O St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
812	1270	Marconi Ave	EB	N	Ashbourne Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
813	1437	Pasadena Ave	NB	N	Winding Way	\$20,000	\$0	\$0	\$140,000	\$5,000	\$165,000	TBA
814	3280	Madison Ave	EB	N	Illinois Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
815	3454	Hazel Ave	SB	F	Central Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
816	3622	Manzanita Ave	NB	F	Maleville Ave	\$20,000	\$5,000	\$5,000	\$20,000	\$5,000	\$55,000	TBA
817	3634	Madison Ave	EB	F	Mariposa Ave	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
818	3867	Kiefer Blvd	WB	F	Arderly Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
819	3872	S Watt Ave	NB	F	Kiefer Blvd	\$25,000	\$0	\$0	\$0	\$5,000	\$30,000	TBA
820	4809	Antelope Rd	WB	F	Greywolf Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
821	5358	Temescal St	NB	N	California Ave	\$20,000	\$5,000	\$0	\$20,000	\$5,000	\$50,000	TBA
822	5364	San Juan Ave	NB	F	Santa Susanna Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
823	730	Elkhorn Blvd	WB	F	6th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
824	746	Q St	WB	N	Front St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
825	750	Rio Linda Blvd	NB	F	Delano St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
826	792	Elkhorn Blvd	EB	N	6th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
827	1296	Winding Way	EB	F	Jan Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
828	2956	La Riviera Dr	WB	F	Stansberry Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
829	454	Kilzer Ave	NB	F	Dudley Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
830	459	Northrop Ave	WB	F	Evelyn Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
831	512	Dudley Blvd	EB	F	Forcum Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
832	738	M St	EB	F	6th St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
833	751	Elverta Rd	EB	F	El Modena Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
834	1110	Arden Way	EB	F	La Playa Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
835	1149	Arden Way	WB	F	Mission Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
836	1154	Arden Way	WB	N	La Playa Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
837	1176	Fair Oaks Blvd	NB	F	Marione Dr	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
838	1210	Dewey Dr	SB	N	Winding Way	\$40,000	\$5,000	\$60,000	\$36,000	\$5,000	\$146,000	TBA
839	1230	California Ave	SB	N	Landis Ave	\$20,000	\$0	\$5,000	\$28,000	\$5,000	\$58,000	TBA
840	1299	Dewey DR	NB	F	Winding Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
841	1305	Dewey Dr	NB	F	Palm Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
842	1501	San Juan Ave	NB	F	Gail Way	\$20,000	\$0	\$0	\$8,000	\$5,000	\$33,000	TBA
843	2155	Center Pkwy	NB	F	H Pkwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
844	2172	Chandler Dr	EB	N	Lindale Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
845	3004	La Riviera Dr	EB	F	Rogue River Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
846	3470	Hazel Ave	NB	N	Winding Wy	\$20,000	\$5,000	\$5,000	\$60,000	\$5,000	\$95,000	TBA
847	3480	Hazel Ave	NB	F	Central Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
848	3902	Howe Ave	NB	N	Cottage Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
849	3990	Sierra Point Dr	NB	N	N Market Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
850	3993	Sierra Point Dr	SB	F	N Market Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
851	3996	National Dr	NB	N	N Market Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
852	4002	Antelope Rd	EB	F	Holbrook Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
853	5359	Hazel Ave	SB	F	Glori Dawn Dr	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	TBA
854	5362	San Juan Ave	SB	F	Winding Wy 2-S	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
855	725	Dry Creek Rd	NB	N	E St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
856	737	M St	EB	F	5th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
857	784	M St	WB	N	5th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
858	1112	Arden Way	EB	F	Esperanza Dr	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
859	1115	Arden Way	EB	N	Mission Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
860	1150	Arden Way	WB	F	Pluto Way	\$0	\$0	\$0	\$0	\$0	\$0	TBA
861	3283	Madison Ave	EB	F	Flyway Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
862	3452	Hazel Ave	SB	F	Oak Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
863	3459	Hazel Ave	SB	F	Phoenix Ave	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
864	3463	Hazel Ave	SB	F	Visage Cir	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
865	3781	Antelope Rd	WB	F	Antelope Hills Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
866	3871	Kiefer Blvd	WB	F	Autumnwood Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
867	5367	Antelope Rd	EB	N	Antelope Hills Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
868	95017	Calvine Rd	WB	F	Armand George Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
869	95020	Calvine Rd	WB	F	Grand Cru Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA
870	95293	Calvine Rd	EB	F	Power Inn Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
871	95333	Calvine Rd	WB	F	Elk Grove Florin Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
872	95338	Calvine Rd	WB	F	Auberry Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
873	95588	Bradshaw Rd	NB	F	Vintage Park Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA
874	95592	Bradshaw Rd	SB	F	Vintage Park Dr	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
875	95690	Calvine Rd	WB	F	Bradshaw Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
876	95040	Calvine Rd	WB	F	Power Inn Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
877	95043	Power Inn Rd	SB	F	Calvine Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
878	599	Forcum Ave	NB	N	Dudley Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
879	1177	Fair Oaks Blvd	NB	F	Ainsley Ct	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
880	1290	California Ave	NB	N	Sutter Ave	\$20,000	\$5,000	\$27,000	\$20,000	\$5,000	\$77,000	TBA
881	1301	Dewey Dr	NB	F	Will Rogers Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
882	3453	Hazel Ave	SB	F	Almond View Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
883	3473	Hazel Ave	NB	F	Bedford Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
884	3548	Hillsdale Blvd	SB	F	Walerga Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
885	3179	Howe Ave	SB	F	Marconi Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
886	3181	Howe Ave	SB	F	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
887	1648	Morse Ave	SB	F	Gerald Ave	\$20,000	\$5,000	\$15,000	\$20,000	\$5,000	\$65,000	TBA
888	3550	Hillsdale Blvd	SB	F	Palm Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
889	2910	Elkhorn Blvd	WB	F	Walerga Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
890	3526	Hillsdale Blvd	EB	N	Walerga Rd	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
891	1649	Morse Ave	SB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
892	2694	44th Ave	NB	F	47th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
893	3027	Sunrise Blvd	NB	F	Gold Express Dr	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
894	3187	Howe Ave	SB	F	Hallmark Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
895	1389	El Camino Ave	WB	F	Howe Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
896	4003	Walerga Rd	SB	F	Antelope Rd	\$20,000	\$5,000	\$30,000	\$80,000	\$5,000	\$140,000	TBA
897	762	Elverta Rd	WB	F	28thSt	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
898	783	M St	WB	F	6th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
899	1178	Fair Oaks Blvd	NB	F	Garfield Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
900	1180	Fair Oaks Blvd	NB	F	Claremont Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
901	1233	California Ave	SB	N	Palm Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
902	1234	Palm Dr	WB	F	Panama	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
903	2605	M L King Blvd	NB	N	41st Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
904	2799	Sunrise Blvd	NB	F	Cavalli Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
905	2846	Goethe Rd	EB	F	Branch Ctr Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
906	3587	Madison Ave	WB	F	Lincoln Oaks Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
907	3630	Madison Ave	EB	N	Lincoln Oaks Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
908	3976	N Market Blvd	WB	F	North Freeway Blvd	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
909	749	Rio Linda Blvd	NB	F	U St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
910	772	Rio Linda Blvd	SB	N	U St (mid block)	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
911	1152	Arden Way	WB	F	Cathay Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
912	1265	Marconi Ave	EB	N	Bell St	\$20,000	\$10,000	\$40,000	\$5,000	\$5,000	\$80,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
913	3275	Madison Ave	EB	N	Kahala Ct	\$20,000	\$0	\$20,000	\$40,000	\$5,000	\$85,000	TBA
914	3279	Madison Ave	EB	F	Valonia St	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
915	3455	Hazel Ave	SB	N	Greenhalgh Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
916	3474	Hazel Ave	NB	F	Phoenix Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
917	3479	Hazel Ave	NB	F	Calvert Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
918	3631	Madison Ave	EB	N	Linda Sue Way	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
919	2402	Scottsdale Dr	EB	N	Power Inn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
920	2382	Scottsdale Dr	WB	F	Power Inn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
921	3599	Manzanita Ave	SB	F	Verde Cruz Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
922	3951	Don Julio Blvd	WB	F	Pearson Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
923	2403	Power Inn Rd	SB	F	Macfadden Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
924	3635	Madison Ave	EB	F	Palmyra Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
925	1093	Ethan Way	NB	F	Delma Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
926	3523	Hillsdale Blvd	NB	F	Stockbridge Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
927	2685	Steiner Rd	SB	F	53rd St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
928	6512	Franklin Blvd	SB	F	Calvine Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
929	3482	Elm Ave	WB	N	Beech Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
930	3966	Elkhorn Blvd	EB	F	Sprig Dr	\$20,000	\$5,000	\$0	\$0	\$10,000	\$35,000	TBA
931	1675	Morse Ave	NB	F	Via Grande	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
932	2965	Watt Ave	NB	F	San Ysidro Way	\$20,000	\$0	\$20,000	\$0	\$5,000	\$45,000	TBA
933	680	Whitney Ave	WB	F	Montclair St	\$20,000	\$5,000	\$15,000	\$40,000	\$5,000	\$85,000	TBA
934	2028	Steiner Rd	NB	N	Austin Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
935	2959	La Riviera Dr	WB	F	Lanier Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
936	3696	Elverta Rd	WB	F	Big Sky Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
937	9942	Eastern Ave	SB	N	Engle Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
938	789	2nd St	SB	F	Monticello Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
939	3522	Hillsdale Blvd	NB	F	Chapel Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
940	3532	Hillsdale Blvd	NB	F	Tacomic Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
941	3543	Hillsdale Blvd	SB	N	Tacomic Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
942	9937	Edison Ave	WB	N	Terra Vista Wy	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
943	732	2nd St	NB	F	Jubilee Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
944	786	2nd St	SB	F	M St	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
945	2154	Center Pkwy	NB	F	East Pkwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
946	3457	Hazel Ave	SB	F	Pershing Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
947	3542	Hillsdale Blvd	SB	F	Andrea Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
948	3780	Walerga Rd	NB	F	Holworthy Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
949	4001	Antelope Rd	EB	F	Mountain Oak Wy	\$25,000	\$0	\$0	\$0	\$5,000	\$30,000	TBA
950	4097	Power Inn Rd	NB	F	Haystack Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
951	4099	Gerber Rd	EB	F	Fernridge Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
952	4107	Power Inn Rd	SB	N	Skywoods Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
953	6515	Franklin Blvd	SB	F	Lake Pleasant Dr	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
954	9938	Edison Ave	EB	N	Mira Vista Wy	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
955	735	2nd St	NB	N	M St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
956	788	2nd St	SB	F	Withington Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
957	1427	Whitney Ave	EB	F	Concetta Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
958	3002	La Riviera Dr	EB	F	Lanier Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
959	3593	Coyle Ave	WB	F	Woodleigh Dr	\$25,000	\$0	\$0	\$0	\$5,000	\$30,000	TBA
960	3624	Coyle Ave	EB	F	Woodleigh Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
961	4004	Walerga Rd	SB	F	Bainbridge Dr	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
962	9943	Eastern Ave	NB	F	Engle Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
963	728	Elverta Rd	WB	F	Bellingrath Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
964	733	2nd St	NB	F	Withington Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
965	744	Q St	WB	N	8th St	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
966	745	Q St	WB	N	7th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
967	777	Q St	EB	F	8th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
968	1220	Winding Way	WB	F	Barrett Rd	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
969	1229	California Ave	SB	N	Grant Ave	\$20,000	\$0	\$8,000	\$28,000	\$5,000	\$61,000	TBA
970	2389	Lindale Dr	WB	F	Fort Pitt Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
971	2396	Lindale Dr	EB	F	Del Coronado Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
972	3284	Madison Ave	EB	F	Winding Oak Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
973	3285	Madison Ave	EB	N	Blue Oak Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
974	3774	Don Julio Blvd	EB	F	Larchmont Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
975	9935	Edison Ave	SB	F	Gibbons Dr	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
976	676	Whitney Ave	WB	F	Concetta Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
977	748	Rio Linda Blvd	NB	F	U St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
978	756	Elverta Rd	EB	F	Cherry Brook Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
979	775	Q St	EB	F	Front St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
980	787	2nd St	SB	F	Dolphin Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
981	1148	Arden Way	WB	F	Park Place Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
982	1289	California Ave	NB	F	Grant Ave	\$20,000	\$5,000	\$27,000	\$40,000	\$5,000	\$97,000	TBA
983	3476	Hazel Ave	NB	F	Leedy Ln	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
984	4808	Rio Linda Blvd	NB	N	Savoy Ave	\$20,000	\$5,000	\$10,000	\$20,000	\$5,000	\$60,000	TBA
985	9936	Edison Ave	NB	N	Amberley Wy	\$20,000	\$5,000	\$5,000	\$60,000	\$5,000	\$95,000	TBA
986	660	Rio Linda Blvd	SB	F	Savoy Ave	\$20,000	\$5,000	\$10,000	\$10,000	\$5,000	\$50,000	TBA
987	734	2nd St	NB	F	Dolphin Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
988	755	Elverta Rd	EB	F	16th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
989	757	Elverta Rd	EB	N	Bellingrath Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
990	764	Elverta Rd	WB	F	Rivergreen Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
991	766	Elverta Rd	WB	F	16th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
992	769	Elverta Rd	WB	F	El Modeno Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
993	773	Rio Linda Blvd	SB	N	U St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
994	776	Q St	EB	F	7th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
995	1111	Arden Way	EB	F	La Sierra Dr	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
996	1114	Arden Way	EB	F	Surita St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
997	1153	Arden Way	WB	N	Maple Glen Rd	\$20,000	\$0	\$30,000	\$40,000	\$5,000	\$95,000	TBA
998	1225	California Ave	SB	N	Lincoln Ave	\$30,000	\$5,000	\$25,000	\$28,000	\$5,000	\$93,000	TBA
999	1231	California Ave	SB	N	Stanley Ave	\$40,000	\$5,000	\$0	\$0	\$5,000	\$50,000	TBA
1000	1232	California Ave	SB	N	Homesweet Way	\$40,000	\$5,000	\$0	\$0	\$5,000	\$50,000	TBA
1001	1286	California Ave	NB	N	Merry Ln	\$20,000	\$0	\$62,000	\$60,000	\$5,000	\$147,000	TBA
1002	1287	California Ave	NB	F	Stanley Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
1003	1288	California Ave	NB	N	Landis Ave	\$20,000	\$5,000	\$0	\$20,000	\$5,000	\$50,000	TBA
1004	1293	California Ave	NB	N	Lincoln Ave	\$20,000	\$5,000	\$20,000	\$40,000	\$5,000	\$90,000	TBA
1005	1298	Winding Way	EB	N	Barrett Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
1006	1302	Dewey Dr	NB	F	Hillhurst Dr	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
1007	1668	Northrop Ave	EB	F	Jonas Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
1008	2955	La Riviera Dr	WB	F	Linda Rio Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
1009	3465	Hazel Ave	NB	N	Aksarben Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
1010	3468	Hazel Ave	NB	F	Curragh Downs Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
1011	4775	Fair Oaks Blvd	NB	F	Niessen Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
1012	6514	Franklin Blvd	SB	F	Adalis Dr	\$20,000	\$0	\$20,000	\$40,000	\$5,000	\$85,000	TBA
1013	95015	Armand George Way	NB	F	Calvine Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
1014	95016	Turquoise Way	SB	N	Calvine Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
1015	95019	Calvine Rd	WB	F	Waterman Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
1016	95335	Calvine Rd	WB	N	Barrymore	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
1017	95336	Calvine Rd	WB	F	New Point Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
1018	1116	Arden Way	EB	N	Paloma Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
1019	723	Dry Creek Rd	NB	F	Ascot Ave	\$40,000	\$5,000	\$0	\$0	\$5,000	\$50,000	TBA
1020	799	Dry Creek Rd	SB	N	Ascot Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
1021	752	Elverta Rd	EB	N	El Verano Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
1022	754	Elverta Rd	EB	F	Palladay Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
1023	765	Elverta Rd	WB	N	16th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
1024	767	Elverta Rd	WB	F	Paladay Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
1025	768	Elverta Rd	WB	N	El Verano Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
1026	5235	Elverta Rd	EB	F	16th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA

BSTP Appendix 2 - Bus Stop Overall Priority Ranking and Improvement Costs

Overall Priority Rank	Bus Stop #	On Street	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
1027	3469	Hazel Ave	NB	F	Degas Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
1028	5243	Hazel Ave	EB	N	Lake Nimbus Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
1029	3551	Hillsdale Blvd	SB	F	Hamilton St	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
Grand Total											\$36,566,150	

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
AC Street	709	495	EB	N	Patrol Road	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Airbase Dr	243	3503	EB	F	Watt Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
Andrea Blvd	497	3540	WB	F	Elkhorn Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Andrea Blvd	591	5346	NB	N	Elkhorn Blvd	\$20,000	\$5,000	\$19,500	\$0	\$5,000	\$49,500	TBA
Andrea Blvd	545	3533	EB	F	Hillsdale Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Andrea Blvd	806	3541	WB	N	Hillsdale Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Antelope Rd	865	3781	WB	F	Antelope Hills Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Antelope Rd	867	5367	EB	N	Antelope Hills Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Antelope Rd	657	3783	WB	F	Black Bear Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Antelope Rd	820	4809	WB	F	Greywolf Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Antelope Rd	679	3782	WB	N	Holbrook Way	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Antelope Rd	852	4002	EB	F	Holbrook Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Antelope Rd	949	4001	EB	F	Mountain Oak Wy	\$25,000	\$0	\$0	\$0	\$5,000	\$30,000	TBA
Antelope Rd	463	4743	SB	F	Roseville Rd/Daly Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Antelope Rd	801	5270	WB	N	Walerga Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Antelope Rd	245	4000	EB	F	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Arden Way	772	1158	WB	F	Avondale Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Arden Way	600	1103	EB	F	Bell St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Arden Way	623	1161	WB	F	Bell St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Arden Way	771	1106	EB	F	Berkshire Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Arden Way	911	1152	WB	F	Cathay Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Arden Way	382	1104	EB	F	Clinton Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Arden Way	646	1151	WB	F	Eastern Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Arden Way	672	1113	EB	F	Eastern Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Arden Way	858	1112	EB	F	Esperanza Dr	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Arden Way	718	1101	EB	F	Ethan Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Arden Way	100	1147	WB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2018/19
Arden Way	223	1159	WB	F	Fulton Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Arden Way	446	1105	EB	F	Fulton Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Arden Way	361	1102	EB	F	Howe Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Arden Way	659	1162	WB	F	Howe Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Arden Way	834	1110	EB	F	La Playa Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Arden Way	836	1154	WB	N	La Playa Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Arden Way	995	1111	EB	F	La Sierra Dr	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Arden Way	997	1153	WB	N	Maple Glen Rd	\$20,000	\$0	\$30,000	\$40,000	\$5,000	\$95,000	TBA
Arden Way	835	1149	WB	F	Mission Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Arden Way	859	1115	EB	N	Mission Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Arden Way	605	1157	WB	F	Morse Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Arden Way	148	1107	EB	F	Morse Avey	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
Arden Way	1018	1116	EB	N	Paloma Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Arden Way	981	1148	WB	F	Park Place Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Arden Way	860	1150	WB	F	Pluto Way	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Arden Way	145	1108	EB	F	Professional Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
Arden Way	219	1156	WB	F	Professional Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Arden Way	996	1114	EB	F	Surita St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Arden Way	63	1155	WB	F	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
Arden Way	102	1109	EB	F	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2018/19
Arden Way	647	1160	WB	N	Wright St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Armand George Way	1013	95015	NB	F	Calvine Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Auburn Blvd	46	631	EB	F	Annadale Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Auburn Blvd	258	633	EB	F	Beresford Way	\$20,000	\$5,000	\$5,000	\$8,000	\$5,000	\$43,000	TBA
Auburn Blvd	261	1596	EB	F	Fulton BLvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Auburn Blvd	44	3352	WB	F	Garfield Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Auburn Blvd	157	3353	WB	N	Hemlock St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
Auburn Blvd	201	567	WB	F	Jones Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Auburn Blvd	89	3354	WB	F	Kohler Rd	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	2017/18
Auburn Blvd	78	3355	WB	F	Madison Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
Auburn Blvd	132	3350	WB	F	Manzanita Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
Auburn Blvd	147	1519	WB	F	Morse Ave	\$20,000	\$0	\$10,000	\$20,000	\$5,000	\$55,000	2021/22
Auburn Blvd	206	1597	EB	N	Morse Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Auburn Blvd	302	634	EB	F	Norris Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Auburn Blvd	231	566	WB	F	Orange Grove Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Auburn Blvd	91	632	EB	F	Pasadena Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
Auburn Blvd	284	3351	WB	F	Rosebud Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Auburn Blvd	13	630	EB	F	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
Auburn Blvd	295	1598	EB	N	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Auburn Blvd	246	646	EB	F	Garfield Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Auburn Blvd	217	644	EB	F	Hemlock St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Auburn Blvd	21	643	EB	N	Kohler Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
Auburn Blvd	69	642	EB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
Auburn Blvd	144	645	EB	N	Palm Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
Auburn Blvd	133	647	EB	N	Rosebud Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
Beech Ave	414	3483	NB	F	Mavis Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Bradshaw Rd	622	2824	SB	F	Country Roads Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Bradshaw Rd	488	2825	SB	F	Old Placer Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Bradshaw Rd	873	95588	NB	F	Vintage Park Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Bradshaw Rd	874	95592	SB	F	Vintage Park Dr	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
Branch Ctr	353	2889	NB	F	Agriculture Ln	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Branch Ctr	378	3864	SB	N	Agriculture Ln	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Branch Ctr	194	3860	NB	N	Goethe Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Branch Ctr	405	3861	SB	N	Preservation Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Branch Ctr	413	2891	NB	N	Preservation Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Branch Ctr	422	2892	NB	F	Preservation Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Branch Ctr	424	3862	SB	F	Preservation Way	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
Butano Dr	186	1131	EB	F	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Butano Dr	352	1129	NB	F	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Butano Dr	372	1076	SB	N	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Butano Dr	671	1075	SB	N	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$10,000	\$35,000	TBA
Butano Dr	699	1130	NB	F	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Butano Dr	340	1074	WB	F	Park Townr Cir	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
California Ave	476	1227	SB	F	Casita Ave	\$20,000	\$0	\$50,000	\$20,000	\$5,000	\$95,000	TBA
California Ave	286	1226	SB	F	Fair Oaks Blvd	\$30,000	\$0	\$0	\$0	\$5,000	\$35,000	TBA
California Ave	317	1292	NB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$75,000	\$80,000	\$5,000	\$185,000	TBA
California Ave	969	1229	SB	N	Grant Ave	\$20,000	\$0	\$8,000	\$28,000	\$5,000	\$61,000	TBA
California Ave	982	1289	NB	F	Grant Ave	\$20,000	\$5,000	\$27,000	\$40,000	\$5,000	\$97,000	TBA
California Ave	1000	1232	SB	N	Homesweet Way	\$40,000	\$5,000	\$0	\$0	\$5,000	\$50,000	TBA
California Ave	839	1230	SB	N	Landis Ave	\$20,000	\$0	\$5,000	\$28,000	\$5,000	\$58,000	TBA
California Ave	1003	1288	NB	N	Landis Ave	\$20,000	\$5,000	\$0	\$20,000	\$5,000	\$50,000	TBA
California Ave	998	1225	SB	N	Lincoln Ave	\$30,000	\$5,000	\$25,000	\$28,000	\$5,000	\$93,000	TBA
California Ave	1004	1293	NB	N	Lincoln Ave	\$20,000	\$5,000	\$20,000	\$40,000	\$5,000	\$90,000	TBA
California Ave	1001	1286	NB	N	Merry Ln	\$20,000	\$0	\$62,000	\$60,000	\$5,000	\$147,000	TBA
California Ave	901	1233	SB	N	Palm Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
California Ave	999	1231	SB	N	Stanley Ave	\$40,000	\$5,000	\$0	\$0	\$5,000	\$50,000	TBA
California Ave	1002	1287	NB	F	Stanley Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
California Ave	477	1228	SB	N	Sutter Ave	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	TBA
California Ave	880	1290	NB	N	Sutter Ave	\$20,000	\$5,000	\$27,000	\$20,000	\$5,000	\$77,000	TBA
Calvine Rd	868	95017	WB	F	Armand George Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Calvine Rd	872	95338	WB	F	Auberry Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Calvine Rd	1016	95335	WB	N	Barrymore	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Calvine Rd	875	95690	WB	F	Bradshaw Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Calvine Rd	427	95337	WB	F	Cliffcrest Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Calvine Rd	871	95333	WB	F	Elk Grove Florin Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Calvine Rd	869	95020	WB	F	Grand Cru Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Calvine Rd	572	95018	WB	F	Kingsbridge Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Calvine Rd	1017	95336	WB	F	New Point Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Calvine Rd	870	95293	EB	F	Power Inn Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Calvine Rd	876	95040	WB	F	Power Inn Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Calvine Rd	426	95334	WB	F	Vintage Park Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Calvine Rd	1015	95019	WB	F	Waterman Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Center Pkwy	945	2154	NB	F	East Pkwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Center Pkwy	541	2037	SB	F	Goya Pkwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Center Pkwy	843	2155	NB	F	H Pkwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Center Pkwy	540	2038	SB	N	South Pkwy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Chandler Dr	844	2172	EB	N	Lindale Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
College Oak Dr	122	641	NB	N	Auburn Blvd	\$20,000	\$5,000	\$20,000	\$10,000	\$5,000	\$60,000	2020/21
College Oak Dr	441	3356	SB	F	Auburn Blvd	\$25,000	\$5,000	\$21,750	\$20,000	\$5,000	\$76,750	TBA
College Oak Dr	7	640	NB	F	Myrtle Ave	\$20,000	\$5,000	\$30,000	\$20,000	\$5,000	\$80,000	2013/14
College Oak Dr	434	3357	SB	N	Myrtle Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
College Oak Dr	9	665	SB	F	Orange Grove Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2013/14
College Oak Dr	749	639	NB	F	Orange Grove Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Cottage Way	759	1128	EB	F	Kincaid Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Cottage Way	779	1077	WB	F	Kincaid Way	\$20,000	\$0	\$15,000	\$20,000	\$5,000	\$60,000	TBA
Cottage Way	45	1127	EB	F	Morse Ave	\$0	\$0	\$0	\$0	\$0	\$0	2015/16
Cottonwood Ln	697	4089	NB	N	Elsie Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Cottonwood Ln	783	4086	SB	F	Elsie Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Cottonwood Ln	187	4088	NB	F	Ivy Creek Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Cottonwood Ln	369	4087	SB	N	Ivy Creek Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Coyle Ave	299	3594	WB	F	Casa Alegre	\$25,000	\$0	\$0	\$0	\$5,000	\$30,000	TBA
Coyle Ave	310	3589	WB	F	Dewey Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Coyle Ave	110	3625	EB	F	Hesper Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
Coyle Ave	323	3592	WB	F	Hesper Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Coyle Ave	266	3623	EB	F	Manzanita Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Coyle Ave	373	3626	EB	N	Milburn St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Coyle Ave	388	3591	WB	F	Park Oaks Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Coyle Ave	225	3590	WB	F	Southbrook Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Coyle Ave	229	3627	EB	N	St Charles Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Coyle Ave	300	3628	EB	F	St Charles Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Coyle Ave	959	3593	WB	F	Woodleigh Dr	\$25,000	\$0	\$0	\$0	\$5,000	\$30,000	TBA
Coyle Ave	960	3624	EB	F	Woodleigh Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Dewey Dr	293	1213	SB	F	Coyle Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Dewey Dr	1006	1302	NB	F	Hillhurst Dr	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
Dewey Dr	673	1304	NB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Dewey Dr	752	1215	SB	F	Madison Ave	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
Dewey Dr	560	1218	SB	F	Moraga DR	\$20,000	\$0	\$12,000	\$0	\$5,000	\$37,000	TBA
Dewey Dr	559	1216	SB	F	Oakcrest Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Dewey Dr	841	1305	NB	F	Palm Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Dewey Dr	568	1300	NB	F	Papaya Dr	\$30,000	\$0	\$0	\$0	\$5,000	\$35,000	TBA
Dewey Dr	719	1214	SB	F	Templeton Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Dewey Dr	425	1217	SB	F	Will Rogers Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Dewey Dr	881	1301	NB	F	Will Rogers Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Dewey Dr	838	1210	SB	N	Winding Way	\$40,000	\$5,000	\$60,000	\$36,000	\$5,000	\$146,000	TBA
Dewey DR	840	1299	NB	F	Winding Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Don Julio Blvd	362	3772	EB	N	Belva Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	807	3954	WB	F	Belva Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	755	3770	EB	F	Bolivar Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	799	3956	WB	F	Bolivar Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	359	3775	EB	F	Craighurst Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	548	3950	WB	N	Craighurst Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	537	3773	EB	F	Garland Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	974	3774	EB	F	Larchmont Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	763	3771	EB	F	Larry Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	764	3955	WB	N	Larry Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	922	3951	WB	F	Pearson Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	285	3957	WB	F	San Martin St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	354	3953	WB	N	Scotia Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Don Julio Blvd	324	3769	EB	F	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Dry Creek Rd	1019	723	NB	F	Ascot Ave	\$40,000	\$5,000	\$0	\$0	\$5,000	\$50,000	TBA
Dry Creek Rd	1020	799	SB	N	Ascot Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Dry Creek Rd	770	798	SB	N	C St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Dry Creek Rd	786	724	NB	F	C St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Dry Creek Rd	670	797	SB	N	E St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Dry Creek Rd	855	725	NB	N	E St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Dry Creek Rd	253	726	NB	N	G St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Dry Creek Rd	259	796	SB	N	G St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Dudley Blvd	831	512	EB	F	Forcum Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Dudley Blvd	178	651	SB	F	Palm Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Dudley Blvd	409	544	SB	N	Peacekeeper Way	\$30,000	\$0	\$0	\$0	\$5,000	\$35,000	TBA
Eastern Ave	937	9942	SB	N	Engle Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Eastern Ave	962	9943	NB	F	Engle Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Edison Ave	985	9936	NB	N	Amberley Wy	\$20,000	\$5,000	\$5,000	\$60,000	\$5,000	\$95,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Edison Ave	975	9935	SB	F	Gibbons Dr	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
Edison Ave	954	9938	EB	N	Mira Vista Wy	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
Edison Ave	942	9937	WB	N	Terra Vista Wy	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
El Camino Ave	631	1377	WB	F	Andrade Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
El Camino Ave	662	1466	EB	F	Andrade Way	\$20,000	\$5,000	\$20,000	\$40,000	\$5,000	\$90,000	TBA
El Camino Ave	680	1468	EB	F	Avalon Dr	\$20,000	\$0	\$40,000	\$60,000	\$5,000	\$125,000	TBA
El Camino Ave	791	1375	WB	F	Avalon Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	438	1388	WB	F	Bell St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	601	1456	EB	F	Bell St	\$20,000	\$0	\$0	\$120,000	\$5,000	\$145,000	TBA
El Camino Ave	722	1384	WB	F	Belport Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	42	1378	WB	F	Butano Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2015/16
El Camino Ave	683	1465	EB	F	Butano Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	745	1462	EB	F	Drayton Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	190	1469	EB	F	Eastern Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	199	1374	WB	F	Eastern Ave	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	TBA
El Camino Ave	620	1366	WB	F	El Tonas Way	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
El Camino Ave	711	1478	EB	F	El Tonas Way	\$20,000	\$5,000	\$20,000	\$60,000	\$5,000	\$110,000	TBA
El Camino Ave	576	1094	EB	F	Ethan Way	\$20,000	\$0	\$5,000	\$5,000	\$5,000	\$35,000	TBA
El Camino Ave	304	1365	WB	F	Fair Oaks Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	6	1458	EB	F	Fulton Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2013/14
El Camino Ave	272	1386	WB	F	Fulton Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	632	1477	EB	F	Garfield Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
El Camino Ave	649	1367	WB	F	Garfield Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
El Camino Ave	364	1373	WB	F	Greenwood Ave	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	TBA
El Camino Ave	398	1470	EB	N	Greenwood Ave	\$20,000	\$0	\$10,000	\$40,000	\$5,000	\$75,000	TBA
El Camino Ave	188	1368	WB	N	Gunn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
El Camino Ave	612	1476	EB	F	Gunn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
El Camino Ave	420	1457	EB	F	Gwen Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	386	1455	EB	F	Howe Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	895	1389	WB	F	Howe Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
El Camino Ave	792	1376	WB	F	Ione St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	802	1467	EB	F	Ione St	\$20,000	\$5,000	\$10,000	\$20,000	\$5,000	\$60,000	TBA
El Camino Ave	485	1371	WB	F	Landwood Way	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
El Camino Ave	724	1472	EB	N	Landwood way	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	TBA
El Camino Ave	710	1370	WB	N	Laurence Ave	\$20,000	\$0	\$15,000	\$80,000	\$5,000	\$120,000	TBA
El Camino Ave	593	1460	EB	F	Meadowbrook Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	487	1471	EB	F	Mission Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
El Camino Ave	615	1372	WB	F	Mission Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
El Camino Ave	598	1383	WB	F	Morse Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	694	1382	WB	F	Morse Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	730	1461	EB	F	Morse Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	484	1385	WB	F	Parkwood Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	753	1459	EB	F	Parkwood Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
El Camino Ave	695	1474	EB	F	Upham Ct	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
El Camino Ave	746	1473	EB	F	Via Camino Ave	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
El Camino Ave	514	1475	EB	F	Walnut Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
El Camino Ave	776	1369	WB	F	Walnut Ave	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
El Camino Ave	35	1464	EB	F	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
El Camino Ave	36	1379	WB	N	Watt Ave	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	2015/16
El Camino Ave	127	1380	WB	F	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2020/21
El Camino Ave	358	1387	WB	F	Wright St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
El Camino Ave	38	1463	EB	F	Yorktown Ave	\$0	\$0	\$0	\$0	\$0	\$0	2015/16
El Camino Ave	277	1381	WB	F	Yorktown Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elkhorn Blvd	823	730	WB	F	6th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elkhorn Blvd	826	792	EB	N	6th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elkhorn Blvd	349	3969	EB	F	Andrea Blvd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Elkhorn Blvd	393	2905	WB	F	Andrea Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elkhorn Blvd	590	2909	WB	F	Butterball Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elkhorn Blvd	669	3965	EB	F	Butterball Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elkhorn Blvd	250	2913	WB	F	Cantel Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elkhorn Blvd	450	3961	EB	F	Cantel Way	\$20,000	\$5,000	\$10,000	\$80,000	\$5,000	\$120,000	TBA
Elkhorn Blvd	396	3970	EB	F	Crater Way	\$20,000	\$0	\$0	\$0	\$10,000	\$30,000	TBA
Elkhorn Blvd	706	2904	WB	F	Crater Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elkhorn Blvd	502	2903	WB	F	Diablo Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elkhorn Blvd	377	2907	WB	F	Don Julio Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elkhorn Blvd	394	3967	EB	F	Don Julio Blvd	\$20,000	\$0	\$0	\$0	\$10,000	\$30,000	TBA
Elkhorn Blvd	346	794	EB	N	Dry Creek Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elkhorn Blvd	447	727	WB	F	Dry Creek Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elkhorn Blvd	667	3968	EB	F	Hillsdale Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elkhorn Blvd	795	2906	WB	N	Hillsdale Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elkhorn Blvd	439	3963	EB	F	Larchmont Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elkhorn Blvd	523	2911	WB	F	Larchmont Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elkhorn Blvd	636	791	EB	F	Rio Linda Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elkhorn Blvd	645	731	WB	F	Rio Linda Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elkhorn Blvd	714	2908	WB	F	Sprig Dr	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
Elkhorn Blvd	930	3966	EB	F	Sprig Dr	\$20,000	\$5,000	\$0	\$0	\$10,000	\$35,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Elkhorn Blvd	368	2912	WB	F	Thomas Dr	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
Elkhorn Blvd	534	3962	EB	F	Thomas Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elkhorn Blvd	693	3964	EB	F	Walerga Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elkhorn Blvd	889	2910	WB	F	Walerga Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elkhorn Blvd	244	3960	EB	F	Watt Ave	\$20,000	\$5,000	\$10,000	\$80,000	\$5,000	\$120,000	TBA
Elm Ave	929	3482	WB	N	Beech Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
Elm Ave	402	3481	WB	F	Hazel Ave	\$25,000	\$5,000	\$5,000	\$40,000	\$5,000	\$80,000	TBA
Elsie Ave	419	4084	EB	F	Halbrite Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elsie Ave	411	4091	WB	N	Hemingway Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elsie Ave	644	4092	WB	N	Iona Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elsie Ave	716	4083	EB	F	Iona Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elsie Ave	301	4415	EB	F	La Fiesta Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elsie Ave	473	4414	WB	F	La Fiesta Wy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elsie Ave	462	4413	WB	F	Robinette Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elsie Ave	275	4777	WB	N	Stockton Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elsie Ave	276	4778	EB	F	Stockton Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elsie Ave	538	4090	WB	N	Wilbur Wy	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
Elsie Ave	544	4085	EB	F	Wilbur Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elverta Rd	988	755	EB	F	16th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	991	766	WB	F	16th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	1023	765	WB	N	16th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	1026	5235	EB	F	16th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	809	759	EB	F	28th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	897	762	WB	F	28thSt	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	963	728	WB	F	Bellingrath Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elverta Rd	989	757	EB	N	Bellingrath Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elverta Rd	936	3696	WB	F	Big Sky Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elverta Rd	978	756	EB	F	Cherry Brook Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elverta Rd	750	758	EB	F	Dutch Haven Blvd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	751	763	WB	F	Dutch Haven Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Elverta Rd	833	751	EB	F	El Modena Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	992	769	WB	F	El Modeno Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	1021	752	EB	N	El Verano Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	1025	768	WB	N	El Verano Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	566	760	EB	F	Electric Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	1024	767	WB	F	Paladay Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	1022	754	EB	F	Palladay Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Elverta Rd	567	770	WB	F	Rio Linda Blvd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Elverta Rd	990	764	WB	F	Rivergreen Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Elverta Rd	152	761	WB	F	Watt Ave	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	2021/22
Elverta Rd	203	721	EB	N	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Engle Rd	562	9940	WB	F	Miradera St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Engle Rd	570	9941	EB	F	Miradera St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Ethan Way	195	1091	NB	N	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Ethan Way	925	1093	NB	F	Delma Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Ethan Way	728	1092	NB	F	Keith Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	879	1177	NB	F	Ainsley Ct	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fair Oaks Blvd	71	1360	SB	N	Angelina Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
Fair Oaks Blvd	356	1175	NB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	96	1354	WB	F	California Ave	\$20,000	\$0	\$25,000	\$160,000	\$5,000	\$210,000	2018/19
Fair Oaks BLvd	226	2796	NB	N	California Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Fair Oaks Blvd	242	2934	SB	N	California Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Fair Oaks Blvd	455	1489	EB	F	California Ave	\$20,000	\$5,000	\$100,000	\$120,000	\$5,000	\$250,000	TBA
Fair Oaks Blvd	95	1492	EB	N	Champlain Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2018/19
Fair Oaks Blvd	900	1180	NB	F	Claremont Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Fair Oaks Blvd	547	1494	EB	F	Dell Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fair Oaks Blvd	247	1141	SB	F	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fair Oaks Blvd	66	1357	SB	F	Engle Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17
Fair Oaks Blvd	303	1183	NB	F	Frontier Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fair Oaks Blvd	428	1144	SB	F	Garfield Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Fair Oaks Blvd	899	1178	NB	F	Garfield Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fair Oaks Blvd	136	1356	SB	N	Gibbons Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
Fair Oaks Blvd	27	1485	NB	F	Grant Ave	\$0	\$0	\$0	\$0	\$0	\$0	2014/15
Fair Oaks Blvd	92	1358	SB	F	Grant Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
Fair Oaks Blvd	522	1349	WB	F	Grover Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	586	1493	EB	F	Hollister Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	614	1350	WB	F	Hollister Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	50	1182	NB	F	Kenneth Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Fair Oaks Blvd	17	1359	SB	N	Landis Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
Fair Oaks Blvd	128	1484	NB	F	Landis Ave	\$0	\$0	\$0	\$0	\$0	\$0	2020/21
Fair Oaks Blvd	239	2923	SB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	252	3040	NB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	172	1363	SB	F	Marconi Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
Fair Oaks Blvd	837	1176	NB	F	Marione Dr	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Fair Oaks Blvd	230	453	EB	N	Marshall Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	248	1353	WB	F	Marshall Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Fair Oaks Blvd	316	4774	NB	F	McMillan Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	103	1351	WB	N	Miles Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
Fair Oaks Blvd	329	3036	NB	N	Monte Park Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fair Oaks Blvd	1011	4775	NB	F	Niessen Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	292	5275	WB	N	Old Winding Wy	\$25,000	\$0	\$10,000	\$0	\$5,000	\$40,000	TBA
Fair Oaks Blvd	459	3035	NB	N	Orange Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	470	2925	SB	N	Orangetree Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	207	1481	NB	F	Palm Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Fair Oaks Blvd	278	1490	EB	F	Queenston Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	164	1361	SB	F	Robertson Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
Fair Oaks Blvd	175	1482	NB	N	Robertson Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
Fair Oaks Blvd	775	1348	WB	F	San Juan Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	325	1143	SB	F	Shelfield Dr	\$20,000	\$5,000	\$0	\$20,000	\$5,000	\$50,000	TBA
Fair Oaks Blvd	119	1486	NB	F	Shirley Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
Fair Oaks Blvd	53	1483	NB	F	Stanley Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Fair Oaks Blvd	805	3043	NB	N	Sunbonnet Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	251	2935	WB	N	Sunrise Blvd	\$20,000	\$0	\$80,000	\$5,000	\$5,000	\$110,000	TBA
Fair Oaks Blvd	255	2795	EB	F	Sunrise Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	804	3042	NB	F	Sunrise E Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	307	3037	NB	F	Sunset Ave	\$20,000	\$0	\$0	\$150,000	\$5,000	\$175,000	TBA
Fair Oaks Blvd	12	1139	SB	N	Topp Ct	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
Fair Oaks Blvd	26	1181	NB	F	Van Alstine Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
Fair Oaks Blvd	765	5360	SB	N	Walnut Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	283	3033	EB	N	Watkins Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fair Oaks Blvd	160	1488	EB	F	Wayside Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
Fair Oaks Blvd	161	1355	WB	F	Wayside Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
Fair Oaks Blvd	74	3038	NB	N	Westcamp Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
Fair Oaks Blvd	326	1146	SB	F	Westminster Ct	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	TBA
Fair Oaks Blvd	112	3034	NB	N	Winding Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2019/20
Fair Oaks Blvd	433	2931	SB	N	Winding Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Fair Oaks Blvd	474	3032	NB	F	Winding Way	\$20,000	\$5,000	\$20,000	\$120,000	\$5,000	\$170,000	TBA
Fair Oaks Blvd	288	3039	NB	F	Woodleaf Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fair Oaks Blvd	319	2924	SB	F	Woodleaf Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fair Oaks Blvd	313	2928	SB	F	Monte Park Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Fair Oaks Blvd	287	2927	SB	F	Sunset Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fair Oaks Blvd	458	2929	SB	N	Winding Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Florin Mall	28	2169	SB	F	66th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
Florin Mall	339	2168	SB	F	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Florin Mall Dr	111	2018	NB	F	66th Ave	\$20,000	\$0	\$0	\$80,000	\$5,000	\$105,000	2019/20
Florin Mall Dr	268	2019	NB	N	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Florin Rd	114	2311	WB	F	65th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2019/20
Florin Rd	37	2313	WB	F	Bowling Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Florin Rd	85	2373	EB	F	Bowling Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
Florin Rd	56	2312	WB	F	East Pkwy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17
Florin Rd	31	2374	EB	F	Easy Pkwy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Florin Rd	40	2165	EB	F	Florin Mall Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Florin Rd	124	2021	WB	N	Florin Mall Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
Florin Rd	23	2372	EB	F	Franklin Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
Florin Rd	39	2020	WB	N	Stockton Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Folsom Blvd	264	2756	EB	F	La Duinta Dr	\$20,000	\$5,000	\$0	\$5,000	\$5,000	\$35,000	TBA
Folsom Blvd	155	2751	WB	F	La Quinta Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
Folsom Blvd	131	2749	WB	F	La Riviera Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2020/21
Folsom Blvd	5	2755	EB	F	Manlove Rd	\$0	\$10,000	\$10,000	\$0	\$5,000	\$25,000	2013/14
Folsom Blvd	15	2752	WB	N	Manlove Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
Folsom Blvd	254	2750	WB	N	Norcade Cir	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Folsom Blvd	158	2757	EB	F	Starfire Dr	\$0	\$0	\$0	\$0	\$0	\$0	2022/23
Forcum Ave	412	479	SB	F	Dudley Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Forcum Ave	878	599	NB	N	Dudley Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Forest Pkwy	542	2162	WB	F	Center Pkwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Forest Pkwy	445	2163	WB	N	Franklin Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Forest Pkwy	448	2161	EB	F	Franklin Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Franklin Blvd	167	2592	SB	F	47th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
Franklin Blvd	297	2602	NB	N	47th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Franklin Blvd	232	2594	SB	F	52nd Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Franklin Blvd	281	2174	NB	N	A Parkway	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Franklin Blvd	1012	6514	SB	F	Adalis Dr	\$20,000	\$0	\$20,000	\$40,000	\$5,000	\$85,000	TBA
Franklin Blvd	928	6512	SB	F	Calvine Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Franklin Blvd	234	2600	NB	F	Cuny Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Franklin Blvd	47	2597	NB	F	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Franklin Blvd	282	2598	NB	F	Green Tree Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Franklin Blvd	953	6515	SB	F	Lake Pleasant Dr	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
Franklin Blvd	208	2601	NB	F	M L King Blvd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Franklin Blvd	305	2593	SB	F	M L King Blvd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Franklin Blvd	134	2160	NB	F	Meadowgate Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
Franklin Blvd	233	2599	NB	F	Turnbridge Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Franklin Blvd	263	2595	SB	F	Turnbridge Dr	\$0	\$0	\$0	\$0	\$0	\$0	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Franklin Blvd	658	6505	NB	N	Village Star Dr	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
Fruitridge Rd	688	2294	EB	F	42nd St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fruitridge Rd	582	2217	WB	F	44th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fruitridge Rd	608	2295	EB	F	44th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fruitridge Rd	754	2296	EB	N	Bonniemae Way	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
Fruitridge Rd	616	2218	WB	F	Enrico Blvd	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
Fruitridge Rd	625	2291	EB	F	Franklin Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fruitridge Rd	587	2293	EB	F	Mendocino Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fruitridge Rd	607	2292	EB	F	ML King Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fruitridge Rd	651	2297	EB	N	Sampson Blvd	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
Fulton Ave	170	1530	SB	F	Alta Arden Expwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
Fulton Ave	168	1556	NB	F	Alta Arden Expy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
Fulton Ave	149	1531	SB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
Fulton Ave	162	1555	NB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
Fulton Ave	237	1520	SB	F	Auburn Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fulton Ave	279	1523	SB	F	Carson Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fulton Ave	327	1564	NB	F	Carson Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fulton Ave	165	1529	SB	F	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
Fulton Ave	205	1557	NB	F	Cottage Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fulton Ave	138	1521	SB	F	Edison Ave	\$20,000	\$0	\$20,000	\$0	\$5,000	\$45,000	2020/21
Fulton Ave	139	1566	NB	F	Edison Ave	\$20,000	\$0	\$10,000	\$0	\$5,000	\$35,000	2021/22
Fulton Ave	141	1559	NB	F	El Camino Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
Fulton Ave	213	1526	SB	N	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fulton Ave	341	1527	SB	F	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fulton Ave	249	1560	NB	F	Elvyra Way	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	TBA
Fulton Ave	465	1528	SB	F	Hernando Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fulton Ave	120	1533	SB	F	Hurley Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
Fulton Ave	123	1553	NB	F	Hurley Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
Fulton Ave	18	1534	SB	F	Larkspur Ln	\$0	\$0	\$0	\$0	\$0	\$0	2014/15
Fulton Ave	93	1552	NB	F	Larkspur Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
Fulton Ave	466	1558	NB	F	Loma Vista Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Fulton Ave	214	1554	NB	F	Maison Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fulton Ave	16	1562	NB	F	Marconi Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
Fulton Ave	55	1524	SB	F	Marconi Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
Fulton Ave	215	1561	NB	N	Marconi Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fulton Ave	444	1525	SB	F	Marconi Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fulton Ave	52	1551	NB	F	Northrop Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2015/16
Fulton Ave	345	1535	SB	F	Northrop Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Fulton Ave	260	1522	SB	F	Pope Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fulton Ave	273	1565	NB	N	Pope Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Fulton Ave	312	1550	NB	F	Sierra Blvd	\$20,000	\$5,000	\$20,000	\$15,000	\$5,000	\$65,000	TBA
Fulton Ave	154	1532	SB	F	Wittkop Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
Gerber Rd	468	2406	WB	F	Gold Rod Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Gerber Rd	221	2379	EB	F	Palmer House Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Gerber Rd	451	2405	WB	F	Palmer House Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Gerber Rd	460	2378	EB	F	Par Parkway	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Gerber Rd	222	2404	WB	F	Power Inn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Gerber Rd	181	2377	EB	F	Stockton Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Gerber Rd	452	2407	WB	F	Whisper Willow Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Gerber Rd	784	4105	WB	N	Fernridge Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Gerber Rd	951	4099	EB	F	Fernridge Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Gerber Rd	461	4098	EB	F	Power Inn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Gerber Rd	406	4100	WB	N	Reese Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Goethe Rd	583	2826	WB	F	Branch Center Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Goethe Rd	905	2846	EB	F	Branch Ctr Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Greenback Ln	105	3295	WB	F	Almond Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
Greenback Ln	314	3290	WB	F	Chestnut Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Greenback Ln	315	3291	WB	F	Filbert Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Greenback Ln	236	3293	WB	F	Hazel Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Greenback Ln	290	3294	WB	N	Hickory Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Greenback Ln	185	3298	WB	F	Hoffman Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Greenback Ln	472	3296	WB	F	Kenneth Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Greenback Ln	101	3288	WB	F	Main Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2018/19
Greenback Ln	309	3292	WB	N	Pecan Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Greenback Ln	184	3297	WB	F	Trajan Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Greenback Ln	75	3289	WB	F	Walnut Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
Hazel Ave	1009	3465	NB	N	Aksarben Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	882	3453	SB	F	Almond View Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	883	3473	NB	F	Bedford Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	917	3479	NB	F	Calvert Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
Hazel Ave	575	3464	NB	F	Central Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Hazel Ave	815	3454	SB	F	Central Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	847	3480	NB	F	Central Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
Hazel Ave	1010	3468	NB	F	Curragh Downs Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Hazel Ave	1027	3469	NB	F	Degas Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	853	5359	SB	F	Glori Dawn Dr	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Hazel Ave	99	3456	SB	F	Greenback Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2018/19
Hazel Ave	129	3478	NB	F	Greenback Ln	\$0	\$0	\$0	\$0	\$0	\$0	2020/21
Hazel Ave	915	3455	SB	N	Greenhalgh Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	798	3477	NB	N	Hazel Hill Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	423	3471	NB	F	La Serena Dr	\$20,000	\$5,000	\$5,000	\$60,000	\$5,000	\$95,000	TBA
Hazel Ave	1028	5243	EB	N	Lake Nimbus Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Hazel Ave	983	3476	NB	F	Leedy Ln	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
Hazel Ave	633	3458	SB	F	Madison Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Hazel Ave	777	3475	NB	F	Madison Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Hazel Ave	418	3461	SB	F	N Winding Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	573	447	NB	F	Nimbus Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Hazel Ave	862	3452	SB	F	Oak Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	946	3457	SB	F	Pershing Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	863	3459	SB	F	Phoenix Ave	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
Hazel Ave	916	3474	NB	F	Phoenix Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	574	3451	SB	N	Pitman Lane Ln	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
Hazel Ave	399	3460	SB	F	Sunset Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	410	3472	NB	F	Sunset Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Hazel Ave	864	3463	SB	F	Visage Cir	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	797	3462	SB	F	Winding Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hazel Ave	846	3470	NB	N	Winding Wy	\$20,000	\$5,000	\$5,000	\$60,000	\$5,000	\$95,000	TBA
Hillsdale Blvd	947	3542	SB	F	Andrea Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	530	3547	SB	F	Brett Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	533	3528	NB	F	Brett Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	939	3522	NB	F	Chapel Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	551	3529	NB	F	Diablo Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Hillsdale Blvd	536	3552	SB	F	Frizell Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	782	3521	NB	F	Frizell Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	481	3549	SB	N	Greenholme Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	596	3525	NB	F	Greenholme Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Hillsdale Blvd	1029	3551	SB	F	Hamilton St	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
Hillsdale Blvd	107	3520	NB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
Hillsdale Blvd	520	3544	SB	F	McCloud Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	529	3531	NB	F	McCloud Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	437	3524	NB	F	Palm Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	888	3550	SB	F	Palm Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	385	3545	SB	F	Robert Frost Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	415	3530	NB	N	Robert Frost Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Hillsdale Blvd	926	3523	NB	F	Stockbridge Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	940	3532	NB	F	Tacomic Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	941	3543	SB	N	Tacomic Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	665	3527	NB	F	Walerga Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	884	3548	SB	F	Walerga Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Hillsdale Blvd	890	3526	EB	N	Walerga Rd	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Howe Ave	24	3900	NB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
Howe Ave	435	3185	SB	F	Arden way	\$20,000	\$0	\$0	\$50,000	\$0	\$70,000	TBA
Howe Ave	197	3183	SB	F	Cottage Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Howe Ave	848	3902	NB	N	Cottage Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Howe Ave	407	3182	NB	F	Delma St	\$20,000	\$10,000	\$20,000	\$60,000	\$25,000	\$135,000	TBA
Howe Ave	200	3903	NB	F	Delma Way	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	TBA
Howe Ave	492	3904	NB	F	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Howe Ave	886	3181	SB	F	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Howe Ave	4	3894	NB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2013/14
Howe Ave	894	3187	SB	F	Hallmark Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Howe Ave	79	3898	NB	F	Hurley Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
Howe Ave	115	3188	SB	F	Hurley Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2019/20
Howe Ave	885	3179	SB	F	Marconi Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Howe Ave	493	3190	SB	F	Northrop Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Howe Ave	507	3896	NB	F	Northrop Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Howe Ave	610	3180	SB	F	Rassy Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Howe Ave	48	3191	SB	F	Sierra Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2015/16
Howe Ave	49	3895	NB	F	Sierra Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Howe Ave	643	3905	NB	F	Tallac Street	\$15,000	\$2,400	\$6,000	\$60,000	\$3,000	\$86,400	TBA
Howe Ave	611	3899	NB	N	Villa Font Way	\$20,000	\$5,000	\$0	\$0	\$15,000	\$40,000	TBA
Howe Ave	490	3189	SB	N	Village Dr	\$20,000	\$5,000	\$0	\$0	\$45,000	\$70,000	TBA
Howe Ave	496	3897	NB	N	Village Dr	\$20,000	\$0	\$0	\$10,000	\$5,000	\$35,000	TBA
Howe Ave	495	3184	SB	F	Wyda Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Howe Ave	500	3901	NB	F	Wyda Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
James Way	508	650	WB	F	Dudley Blvd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
James Way	685	629	WB	F	Skvarla Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Jan Dr	571	1294	NB	N	Jaspar Ct	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
Jan Dr	564	1224	SB	N	Otis Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Jan Dr	563	1223	SB	N	Rampart Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Jan Dr	565	1295	NB	N	Rampart Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Jan Dr	780	1222	SB	F	Winding Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Kiefer Blvd	818	3867	WB	F	Arderly Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Kiefer Blvd	866	3871	WB	F	Autumnwood Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Kiefer Blvd	41	3865	WB	F	Branch Ctr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Kiefer Blvd	676	2874	EB	F	Brydon Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Kiefer Blvd	486	3868	WB	F	Huntsman Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Kiefer Blvd	595	2886	EB	F	Huntsman Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Kiefer Blvd	360	496	WB	F	Mayhew Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Kiefer Blvd	366	2888	EB	F	Mayhew Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Kiefer Blvd	666	3866	WB	N	N Kiefer Blvd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Kiefer Blvd	762	2873	EB	F	S Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Kiefer Blvd	335	2884	EB	F	Southport Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Kiefer Blvd	494	3870	WB	N	Southport Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Kiefer Blvd	479	2885	EB	F	Tallyho Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Kiefer Blvd	498	2887	EB	F	Tallyho Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Kiefer Blvd	513	3869	WB	F	Tallyho Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Kilzer Ave	829	454	NB	F	Dudley Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
La Riviera Dr	174	2954	NB	F	Folsom Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
La Riviera Dr	306	3007	EB	N	Folsom Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
La Riviera Dr	935	2959	WB	F	Lanier Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	958	3002	EB	F	Lanier Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	1008	2955	WB	F	Linda Rio Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	641	3005	EB	F	Payette Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	738	2957	WB	F	Rogue River Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	845	3004	EB	F	Rogue River Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	828	2956	WB	F	Stansberry Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	691	3006	EB	N	Tuolumne Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	557	2958	WB	F	Waterman Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	552	3003	EB	F	Waterton Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	603	2961	WB	N	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	677	3000	EB	F	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
La Riviera Dr	668	2960	WB	F	Woodman Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
La Riviera Dr	803	3001	EB	F	Woodman Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Lindale Dr	971	2396	EB	F	Del Coronado Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Lindale Dr	555	2397	EB	F	Ewing Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Lindale Dr	970	2389	WB	F	Fort Pitt Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Lindale Dr	532	2398	EB	F	Palmer House Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Lindale Dr	543	2387	WB	F	Palmer House Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Lindale Dr	558	2388	WB	F	Rimrock Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Lindale Dr	793	2390	WB	F	Sahara Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Lindale Dr	794	2395	EB	F	Sahara Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Luce Ave	727	9996	NB	N	Palm St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Luce Ave	436	2945	NB	F	Peacekeeper Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M L King Blvd	588	2609	NB	N	23rd St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M L King Blvd	663	2606	NB	N	32nd Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M L King Blvd	903	2605	NB	N	41st Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M L King Blvd	499	2604	NB	F	44th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M L King Blvd	509	2603	NB	F	47th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M L King Jr Blvd	504	2588	SB	F	32nd Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M L King Jr Blvd	512	2589	SB	F	41st Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M L King Jr Blvd	505	2590	SB	F	44th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M L King Jr Blvd	703	2591	SB	N	47th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M L King Jr Blvd	681	2587	SB	F	Fruitridge Rd	\$20,000	\$0	\$0	\$160,000	\$5,000	\$185,000	TBA
M St	856	737	EB	F	5th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M St	857	784	WB	N	5th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M St	832	738	EB	F	6th St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
M St	898	783	WB	F	6th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M St	391	781	WB	N	8th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M St	397	740	EB	F	8th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M St	743	739	EB	N	Front St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M St	744	782	WB	F	Front St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
M St	733	736	EB	F	Rio Linda Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
M St	769	785	WB	N	Rio Linda Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Madison Ave	973	3285	EB	N	Blue Oak Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Madison Ave	561	3277	EB	F	Buena Vista Ave	\$20,000	\$5,000	\$20,000	\$80,000	\$5,000	\$130,000	TBA
Madison Ave	550	3276	EB	F	Chicago Ave	\$20,000	\$5,000	\$20,000	\$60,000	\$5,000	\$110,000	TBA
Madison Ave	395	3588	WB	N	Dewey Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Madison Ave	656	3629	EB	F	Dewey Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Madison Ave	308	3274	EB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Madison Ave	861	3283	EB	F	Flyway Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Madison Ave	94	3505	EB	F	Harrison St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2018/19
Madison Ave	179	3509	WB	F	Harrison St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Madison Ave	664	3281	EB	N	Hazel Ave	\$20,000	\$0	\$5,000	\$0	\$5,000	\$30,000	TBA
Madison Ave	678	3282	EB	F	Hazel Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Madison Ave	126	3508	WB	F	Hillsdale Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2020/21
Madison Ave	814	3280	EB	N	Illinois Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Madison Ave	480	3504	EB	F	Jackson St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Madison Ave	491	3510	WB	F	Jackson St	\$0	\$0	\$0	\$0	\$0	\$0	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Madison Ave	913	3275	EB	N	Kahala Ct	\$20,000	\$0	\$20,000	\$40,000	\$5,000	\$85,000	TBA
Madison Ave	401	3278	EB	F	Kenneth Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Madison Ave	906	3587	WB	F	Lincoln Oaks Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Madison Ave	907	3630	EB	N	Lincoln Oaks Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Madison Ave	918	3631	EB	N	Linda Sue Way	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Madison Ave	333	11145	SB	N	Main Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Madison Ave	817	3634	EB	F	Mariposa Ave	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
Madison Ave	924	3635	EB	F	Palmyra Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Madison Ave	322	3286	EB	N	Pershing Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Madison Ave	741	3632	EB	N	San Juan Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Madison Ave	748	3273	EB	F	Sunrise Blvd	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Madison Ave	914	3279	EB	F	Valonia St	\$20,000	\$5,000	\$5,000	\$40,000	\$5,000	\$75,000	TBA
Madison Ave	972	3284	EB	F	Winding Oak Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Main Ave	332	11069	NB	N	Greenback Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Main Ave	159	3287	NB	F	Madison Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
Main Ave	334	11160	NB	F	Madison Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Main St	328	2932	SB	F	Sacramento St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Manzanita Ave	390	3618	NB	F	Bourbon Dr	\$20,000	\$5,000	\$22,500	\$40,000	\$5,000	\$92,500	TBA
Manzanita Ave	97	3603	SB	F	Cypress Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2018/19
Manzanita Ave	404	3621	NB	N	Ellerslee Dr	\$20,000	\$5,000	\$0	\$40,000	\$5,000	\$70,000	TBA
Manzanita Ave	90	3614	NB	F	Fair Oaks Blvd	\$0	\$0	\$0	\$0	\$0	\$0	2017/18
Manzanita Ave	109	3616	NB	F	Jan Dr	\$20,000	\$0	\$80,000	\$0	\$5,000	\$105,000	2019/20
Manzanita Ave	274	3615	NB	F	Lincoln Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Manzanita Ave	311	3602	SB	F	Locust Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Manzanita Ave	67	3597	SB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
Manzanita Ave	597	3620	NB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Manzanita Ave	816	3622	NB	F	Maleville Ave	\$20,000	\$5,000	\$5,000	\$20,000	\$5,000	\$55,000	TBA
Manzanita Ave	692	3619	NB	N	Muldrow Rd	\$20,000	\$5,000	\$22,500	\$40,000	\$5,000	\$92,500	TBA
Manzanita Ave	707	3598	SB	F	Muldrow Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Manzanita Ave	740	3595	SB	N	Nonnie Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Manzanita Ave	403	3596	SB	N	Peppermill Ct	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Manzanita Ave	921	3599	SB	F	Verde Cruz Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Manzanita Ave	64	3600	SB	F	Winding Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
Manzanita Ave	68	3617	NB	F	Winding Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17
Manzanita Ave	106	3601	SB	F	Windmill Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
Marconi Ave	789	1248	WB	F	Ashbourne Dr	\$20,000	\$0	\$30,000	\$60,000	\$5,000	\$115,000	TBA
Marconi Ave	812	1270	EB	N	Ashbourne Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	371	1273	EB	N	Becerra way	\$20,000	\$0	\$5,000	\$40,000	\$5,000	\$70,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Marconi Ave	389	1245	WB	F	Becerra Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	729	1253	WB	F	Bell St	\$20,000	\$5,000	\$80,000	\$60,000	\$5,000	\$170,000	TBA
Marconi Ave	912	1265	EB	N	Bell St	\$20,000	\$10,000	\$40,000	\$5,000	\$5,000	\$80,000	TBA
Marconi Ave	577	1250	WB	F	Calderwood Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	585	1268	EB	N	Corabel Ln	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	483	1263	EB	F	Darwin St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	584	1243	WB	F	Eastern Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	790	1275	EB	F	Eastern Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Marconi Ave	86	1251	WB	F	Fulton Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
Marconi Ave	87	1267	EB	F	Fulton Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
Marconi Ave	400	1281	EB	F	Garfield Ave	\$20,000	\$0	\$18,000	\$80,000	\$5,000	\$123,000	TBA
Marconi Ave	734	1237	WB	F	Garfield Ave	\$20,000	\$0	\$29,000	\$106,000	\$5,000	\$160,000	TBA
Marconi Ave	735	1242	WB	F	Greenwood Ave	\$20,000	\$0	\$10,000	\$16,000	\$5,000	\$51,000	TBA
Marconi Ave	773	1276	EB	N	Greenwood Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	606	1280	EB	F	Gunn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	350	1236	WB	F	La Vista Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Marconi Ave	375	1282	EB	N	La Vista Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Marconi Ave	721	1249	WB	F	Lacy Lane	\$20,000	\$5,000	\$85,000	\$60,000	\$5,000	\$175,000	TBA
Marconi Ave	613	1238	WB	F	Linden Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Marconi Ave	357	1277	EB	F	Mission Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Marconi Ave	363	1241	WB	F	Mission Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Marconi Ave	720	1246	WB	N	Montclair St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	736	1272	EB	N	Montclair St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	383	1274	EB	N	Norris Ave	\$20,000	\$0	\$18,000	\$92,000	\$5,000	\$135,000	TBA
Marconi Ave	478	1244	WB	F	Norris Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	637	1264	EB	F	Pyramid Way	\$20,000	\$20,000	\$55,000	\$0	\$5,000	\$100,000	TBA
Marconi Ave	648	1240	WB	F	Root Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Marconi Ave	661	1278	EB	F	Root Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Marconi Ave	700	1269	EB	F	Santa Paula Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Marconi Ave	503	1239	WB	F	Walnut Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Marconi Ave	515	1279	EB	F	Walnut Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Marconi Ave	58	1247	WB	F	Watt Ave	\$20,000	\$5,000	\$20,000	\$0	\$5,000	\$50,000	2016/17
Marconi Ave	83	1271	EB	F	Watt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
Marconi Ave	592	1266	EB	F	Wright St	\$20,000	\$10,000	\$15,000	\$0	\$5,000	\$50,000	TBA
Marconi Ave	642	1252	WB	F	Wright St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Mission Ave	177	674	SB	F	Engle Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Mission Ave	521	9939	NB	N	Engle Rd	\$25,000	\$0	\$0	\$80,000	\$5,000	\$110,000	TBA
Mission Ave	189	675	SB	N	Whitney Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Mission Ave	193	1428	NB	F	Whitney Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Morse Ave	535	1670	NB	F	Amberwood Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Morse Ave	130	1674	NB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2020/21
Morse Ave	634	1672	NB	F	Berkshire Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Morse Ave	108	1677	NB	F	Cottage Way	\$0	\$0	\$0	\$0	\$0	\$0	2019/20
Morse Ave	180	1646	SB	F	Cottage Way	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Morse Ave	198	1671	NB	F	Hurley Way	\$20,000	\$0	\$20,000	\$160,000	\$5,000	\$205,000	TBA
Morse Ave	731	1669	NB	F	Northrup Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Morse Ave	628	1673	NB	F	Trussel Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Morse Ave	931	1675	NB	F	Via Grande	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Morse Ave	891	1649	SB	F	Arden Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Morse Ave	712	1651	SB	F	Berkshire Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Morse Ave	887	1648	SB	F	Gerald Ave	\$20,000	\$5,000	\$15,000	\$20,000	\$5,000	\$65,000	TBA
Morse Ave	554	1653	SB	F	Hampton Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Morse Ave	192	1652	SB	N	Hurley Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Morse Ave	384	1654	SB	N	Northrop Ave	\$20,000	\$0	\$20,000	\$60,000	\$5,000	\$105,000	TBA
Morse Ave	640	1650	SB	F	Trussel Way	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Munroe St	173	1549	NB	F	Fair Oaks Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
Munroe St	342	1537	SB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Munroe St	72	1536	SB	F	Sierra Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17
Myrtle Ave	294	1438	EB	F	Auburn Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Myrtle Ave	687	1439	EB	N	College Oak Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
N Market Blvd	708	4712	EB	F	National Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
N Market Blvd	908	3976	WB	F	North Freeway Blvd	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
N Market Blvd	639	4754	WB	F	Northgate Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
N Market Blvd	800	4702	EB	F	S Market Ct	\$20,000	\$5,000	\$42,000	\$0	\$5,000	\$72,000	TBA
N Market Blvd	619	4708	EB	N	Sierra Point Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
N Market Blvd	519	4753	WB	F	Sports Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
N Parkway	98	2026	NB	F	Sky Pkwy	\$20,000	\$5,000	\$60,000	\$0	\$5,000	\$90,000	2018/19
National Dr	191	3994	EB	N	Lennane Dr	\$20,000	\$0	\$0	\$60,000	\$5,000	\$85,000	TBA
National Dr	196	3989	WB	F	Lennane Dr	\$20,000	\$0	\$0	\$60,000	\$5,000	\$85,000	TBA
National Dr	742	3987	SB	F	N Market Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
National Dr	851	3996	NB	N	N Market Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
National Dr	756	3988	SB	N	National Dr	\$20,000	\$0	\$0	\$20,000	\$10,000	\$50,000	TBA
National Dr	757	3995	NB	F	National Dr	\$20,000	\$0	\$0	\$20,000	\$5,000	\$45,000	TBA
Northrop Ave	506	1663	EB	F	Bell St	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
Northrop Ave	579	1659	WB	F	Bell St	\$30,000	\$0	\$0	\$0	\$5,000	\$35,000	TBA
Northrop Ave	617	1665	EB	F	Carro Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Northrop Ave	621	1657	WB	F	Carro Dr	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
Northrop Ave	650	1658	WB	F	Dornajo Way	\$20,000	\$5,000	\$0	\$20,000	\$5,000	\$50,000	TBA
Northrop Ave	830	459	WB	F	Evelyn Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Northrop Ave	337	1666	EB	F	Fulton Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Northrop Ave	338	1656	WB	F	Fulton Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Northrop Ave	62	1662	EB	F	Howe Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
Northrop Ave	348	1660	WB	N	Howe Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Northrop Ave	421	1655	WB	F	Jonas Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Northrop Ave	1007	1668	EB	F	Jonas Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Northrop Ave	624	1664	EB	F	Park	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Oak Lane	767	741	NB	F	9th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Oak Ln	787	780	SB	N	9th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Old Placerville Rd	416	9926	EB	F	Macready Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
On I-80 Service Dr	117	9903	SB	Mid	Watt Lower Level	\$0	\$0	\$0	\$0	\$0	\$0	2019/20
Orange Ave	296	2171	EB	N	Citrus Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Orange Ave	432	2392	EB	N	Citrus Ave	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Orange Ave	280	2170	EB	F	Florin Mall Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Orange Ave	467	2393	EB	N	Florin Mall Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Orange Grove Ave	240	636	EB	F	Auburn Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Orange Grove Ave	1	563	WB	F	College Oak Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2013/14
Orange Grove Ave	511	637	EB	F	Pasadena Ave	\$20,000	\$5,000	\$7,500	\$20,000	\$5,000	\$57,500	TBA
Orange Grove Ave	808	565	WB	N	Pasadena Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Orange Grove Ave	698	638	EB	F	Sycamore Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Palm Dr	902	1234	WB	F	Panama	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Palm Dr	774	1284	EB	N	Panama Ave	\$20,000	\$5,000	\$0	\$48,000	\$5,000	\$78,000	TBA
Palm St	454	9997	EB	N	Dudley Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Palmer House Dr	524	2399	SB	N	Pickoi Cir	\$20,000	\$0	\$0	\$60,000	\$5,000	\$85,000	TBA
Palmer House Dr	527	2385	NB	F	Pickoi Cir	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Palmer House Dr	725	2400	SB	F	Pickoi Cir	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Palmer House Dr	689	2383	NB	F	Scottsdale Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Pasadena Ave	553	667	SB	F	Cypress Ave	\$20,000	\$5,000	\$5,000	\$10,000	\$5,000	\$45,000	TBA
Pasadena Ave	785	666	SB	F	Pasadena Ave	\$20,000	\$5,000	\$10,000	\$10,000	\$5,000	\$50,000	TBA
Pasadena Ave	813	1437	NB	N	Winding Way	\$20,000	\$0	\$0	\$140,000	\$5,000	\$165,000	TBA
Peacekeeper Way	408	520	WB	F	Dudley Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Power Inn Rd	877	95043	SB	F	Calvine Rd	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Power Inn Rd	443	2381	NB	F	Gerber Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Power Inn Rd	923	2403	SB	F	Macfadden Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Power Inn Rd	778	4096	NB	F	Elsie Ave	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Power Inn Rd	291	4106	SB	F	Gerber Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Power Inn Rd	950	4097	NB	F	Haystack Dr	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Power Inn Rd	952	4107	SB	N	Skywoods Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Q St	966	745	WB	N	7th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Q St	994	776	EB	F	7th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Q St	965	744	WB	N	8th St	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
Q St	967	777	EB	F	8th St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Q St	824	746	WB	N	Front St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Q St	979	775	EB	F	Front St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Q St	768	774	EB	F	Rio Linda Blvd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Rio Linda Blvd	810	771	SB	N	Delano St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Rio Linda Blvd	825	750	NB	F	Delano St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Rio Linda Blvd	717	747	NB	F	Q St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Rio Linda Blvd	984	4808	NB	N	Savoy Ave	\$20,000	\$5,000	\$10,000	\$20,000	\$5,000	\$60,000	TBA
Rio Linda Blvd	986	660	SB	F	Savoy Ave	\$20,000	\$5,000	\$10,000	\$10,000	\$5,000	\$50,000	TBA
Rio Linda Blvd	909	749	NB	F	U St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Rio Linda Blvd	977	748	NB	F	U St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Rio Linda Blvd	993	773	SB	N	U St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
Rio Linda Blvd	910	772	SB	N	U St (mid block)	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
S Watt Ave	819	3872	NB	F	Kiefer Blvd	\$25,000	\$0	\$0	\$0	\$5,000	\$30,000	TBA
S Watt/Manlove LRS	80	9817	SB	F	Bay 3	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
San Juan Ave	702	1495	NB	F	Fair Oaks Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
San Juan Ave	267	1500	NB	F	Fairway Two Ave	\$20,000	\$0	\$0	\$40,000	\$5,000	\$65,000	TBA
San Juan Ave	686	1342	SB	N	Gail Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
San Juan Ave	842	1501	NB	F	Gail Way	\$20,000	\$0	\$0	\$8,000	\$5,000	\$33,000	TBA
San Juan Ave	701	1346	SB	F	Heather Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
San Juan Ave	758	5361	SB	F	Lincoln Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
San Juan Ave	331	1341	SB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
San Juan Ave	204	1343	SB	N	Pheasant Rd	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
San Juan Ave	822	5364	NB	F	Santa Susanna Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
San Juan Ave	594	1499	NB	F	Sunset Ave	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	TBA
San Juan Ave	682	1344	SB	N	Sunset Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
San Juan Ave	684	1497	NB	F	Winding Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
San Juan Ave	854	5362	SB	F	Winding Wy 2-S	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Scottsdale Dr	761	2401	EB	F	Palmer House Dr	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
Scottsdale Dr	919	2402	EB	N	Power Inn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Scottsdale Dr	920	2382	WB	F	Power Inn Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Sierra Point Dr	849	3990	NB	N	N Market Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Sierra Point Dr	850	3993	SB	F	N Market Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Sky Pkwy	10	2690	SB	N	65th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
Sky Pkwy	11	2024	NB	F	65th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
Sky Pkwy	262	2023	NB	F	65th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Sky Pkwy	182	2688	EB	F	6th Pkwy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Sky Pkwy	65	2689	WB	N	Klingon Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
Sky Pkwy	104	2025	WB	N	Klingon Ct	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2019/20
Steiner Rd	367	2683	SB	F	47th Ave	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
Steiner Rd	392	2684	SB	F	51st Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Steiner Rd	347	2029	NB	N	51st St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Steiner Rd	525	2027	NB	N	53rd Ave	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
Steiner Rd	539	2686	SB	F	53rd Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Steiner Rd	927	2685	SB	F	53rd St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Steiner Rd	934	2028	NB	N	Austin Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Steiner Rd	374	2030	NB	F	Sitton Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Stockton Blvd	212	1844	SB	F	15th Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Stockton Blvd	171	1845	SB	F	17th Ave	\$0	\$0	\$0	\$0	\$0	\$0	2022/23
Stockton Blvd	32	1860	SB	F	65th St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
Stockton Blvd	118	1884	NB	F	65th St	\$0	\$0	\$0	\$0	\$0	\$0	2019/20
Stockton Blvd	457	2391	SB	F	66th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Stockton Blvd	431	2375	NB	F	Elsie Ave	\$0	\$0	\$0	\$0	\$0	\$0	TBA
Stockton Blvd	430	2166	NB	F	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Stockton Blvd	183	2408	SB	F	Gerber Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Stockton Blvd	318	1853	SB	F	Gordon DR	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Stockton Blvd	209	1861	SB	F	Lindale Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Stockton Blvd	238	2409	SB	F	Massie Ct	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Stockton Blvd	81	1854	SB	F	McMahon Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
Stockton Blvd	166	1846	SB	F	Parker Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
Stockton Blvd	218	1847	SB	N	Roosevelt Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Stockton Blvd	169	1852	SB	N	Southwest Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2022/23
Stockton Blvd	456	2376	NB	F	Suncountry Ln	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Sunrise Blvd	904	2799	NB	F	Cavalli Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Sunrise Blvd	747	3026	NB	F	Coloma Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Sunrise Blvd	696	3028	NB	F	Gold Express Blvd	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
Sunrise Blvd	893	3027	NB	F	Gold Express Dr	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
Sunrise Blvd	501	1205	SB	F	Madison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Sunrise Blvd	655	2798	NB	F	Sunset Ave	\$20,000	\$5,000	\$0	\$20,000	\$5,000	\$50,000	TBA
Sunrise Blvd	660	1207	SB	F	Sunset Ave	\$50,000	\$5,000	\$0	\$0	\$5,000	\$60,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Sunrise Blvd	737	2800	NB	F	Wildridge Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Sunrise Blvd	788	1206	SB	F	Wildridge Dr	\$50,000	\$5,000	\$0	\$0	\$5,000	\$60,000	TBA
Sunrise Blvd	654	2797	NB	F	Winding Way	\$20,000	\$0	\$0	\$0	\$10,000	\$30,000	TBA
Temescal St	821	5358	NB	N	California Ave	\$20,000	\$5,000	\$0	\$20,000	\$5,000	\$50,000	TBA
Turquoise Way	1014	95016	SB	N	Calvine Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Walerga Rd	896	4003	SB	F	Antelope Rd	\$20,000	\$5,000	\$30,000	\$80,000	\$5,000	\$140,000	TBA
Walerga Rd	961	4004	SB	F	Bainbridge Dr	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	TBA
Walerga Rd	440	3776	NB	F	Don Julio Blvd	\$25,000	\$5,000	\$0	\$0	\$5,000	\$35,000	TBA
Walerga Rd	531	4007	SB	N	Don Julio Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Walerga Rd	599	4006	SB	F	Elkhorn Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Walerga Rd	630	3777	NB	F	Elkhorn Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Walerga Rd	635	3778	NB	F	Galbrath Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Walerga Rd	627	4005	SB	F	Galbrath Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Walerga Rd	948	3780	NB	F	Holworthy Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	125	3067	SB	F	A St	\$0	\$0	\$0	\$0	\$0	\$0	2020/21
Watt Ave	202	3107	NB	F	A St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	270	3104	NB	F	Air Base Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Watt Ave	449	3121	NB	F	Antelope Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	113	3081	SB	F	Arden Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2019/20
Watt Ave	140	3074	SB	F	Auburn Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
Watt Ave	475	3082	SB	F	Barrington Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	380	3122	NB	F	Blackfoot Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	796	2964	NB	F	Bodega Ct	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	271	3112	NB	F	Bolivar Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Watt Ave	57	3089	NB	F	Butano Dr	\$0	\$0	\$0	\$0	\$0	\$0	2016/17
Watt Ave	343	3113	NB	F	Channing Dr	\$20,000	\$0	\$0	\$0	\$15,000	\$35,000	TBA
Watt Ave	77	3079	SB	F	Chenu Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
Watt Ave	344	3087	SB	F	Cosmos Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	379	3055	SB	F	Davidson Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	330	3063	SB	F	Don Julio Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Watt Ave	51	3110	NB	F	E St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Watt Ave	59	3096	NB	F	Edison Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2016/17
Watt Ave	60	3075	SB	N	Edison Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17
Watt Ave	33	3090	NB	F	El Camino Ave	\$0	\$0	\$0	\$0	\$0	\$0	2015/16
Watt Ave	34	3080	SB	F	El Camino Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2015/16
Watt Ave	569	2966	NB	F	El Ricon Way	\$20,000	\$0	\$20,000	\$5,000	\$5,000	\$50,000	TBA
Watt Ave	137	3060	SB	F	Elkhorn Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
Watt Ave	211	3115	NB	F	Elkhorn Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Watt Ave	489	3052	SB	F	Elverta Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	739	3125	NB	F	Elverta Rd	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
Watt Ave	220	2962	NB	N	Fair Oaks Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Watt Ave	143	3064	SB	F	Freedom Park Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
Watt Ave	469	3084	SB	F	Hurley Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	453	3083	SB	F	Hyde Way	\$20,000	\$5,000	\$15,000	\$60,000	\$5,000	\$105,000	TBA
Watt Ave	256	3062	SB	F	I St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Watt Ave	2	9801	SB	Mid	I-80 LRT	\$0	\$0	\$0	\$0	\$0	\$0	2013/14
Watt Ave	3	9800	NB	Mid	I-80 LRT	\$0	\$0	\$0	\$0	\$0	\$0	2013/14
Watt Ave	163	3065	SB	F	Karl Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23
Watt Ave	442	3109	NB	F	Karl Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Watt Ave	151	3091	NB	F	Kings Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
Watt Ave	269	3057	SB	F	Larchmont Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	22	3073	SB	F	Longview Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
Watt Ave	336	3098	NB	F	Longview Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	228	3094	NB	F	Lynn Way 1	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Watt Ave	298	3093	NB	F	Lynn Way 1	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Watt Ave	54	3078	SB	F	Marconi Ave	\$0	\$0	\$0	\$0	\$0	\$0	2015/16
Watt Ave	19	3092	NB	F	Marconi Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
Watt Ave	76	3099	NB	F	Margaret Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2017/18
Watt Ave	224	3106	NB	F	McClellan Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	20	3101	NB	F	Myrtle Ave	\$20,000	\$5,000	\$10,000	\$0	\$5,000	\$40,000	2014/15
Watt Ave	135	3070	SB	F	Myrtle Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2020/21
Watt Ave	146	3066	SB	F	N Haven Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
Watt Ave	176	3108	NB	F	N Haven Dr	\$20,000	\$0	\$0	\$120,000	\$5,000	\$145,000	TBA
Watt Ave	370	3123	NB	F	Navaho Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	732	3054	SB	F	Navaho Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	715	2963	NB	F	Northrop Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	227	3086	SB	N	Northup Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Watt Ave	210	3072	SB	F	Orange Grove Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Watt Ave	320	3059	SB	F	Owens Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	471	3116	NB	F	Plymouth Dr	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	121	3077	SB	F	Pope Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2020/21
Watt Ave	43	3069	SB	N	Poplar Blvd	\$0	\$0	\$0	\$0	\$0	\$0	2015/16
Watt Ave	61	3103	NB	F	Poplar Blvd	\$0	\$0	\$0	\$0	\$0	\$0	2016/17
Watt Ave	289	3058	SB	F	Q St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	257	3119	NB	F	Quinn Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	464	3118	NB	F	Quinn Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
Watt Ave	25	3102	NB	F	Roseville Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
Watt Ave	142	3049	SB	F	Roseville Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
Watt Ave	932	2965	NB	F	San Ysidro Way	\$20,000	\$0	\$20,000	\$0	\$5,000	\$45,000	TBA
Watt Ave	581	3124	NB	F	Timberlane Pl	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	321	3117	NB	F	Turner Dr	\$20,000	\$5,000	\$0	\$0	\$15,000	\$40,000	TBA
Watt Ave	216	3056	SB	F	U St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	235	3061	SB	F	Van Owen St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Watt Ave	88	3076	SB	F	Whitney Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2017/18
Watt Ave	84	3095	NB	F	Whitney Way	\$0	\$0	\$0	\$0	\$0	\$0	2017/18
Watt Ave	265	3085	SB	N	Windsor Way	\$20,000	\$5,000	\$20,000	\$60,000	\$5,000	\$110,000	TBA
Watt Ave	82	3068	SB	F	Wings way	\$0	\$0	\$0	\$0	\$0	\$0	2017/18
Watt Ave	116	3100	NB	F	Winona Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2019/20
Watt Ave	150	3071	SB	F	Winona Way	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2021/22
Watt/Manlove	73	378	EB	F	Lt Rail Sta Bay 1	\$20,000	\$5,000	\$5,000	\$0	\$5,000	\$35,000	2016/17
Watt/Manlove	14	377	SB	F	Lt Rail Sta Bay 2	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2014/15
Whitney Ave	723	1423	EB	F	Becerra Way	\$20,000	\$0	\$15,000	\$0	\$5,000	\$40,000	TBA
Whitney Ave	766	679	WB	F	Becerra Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Whitney Ave	957	1427	EB	F	Concetta Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Whitney Ave	976	676	WB	F	Concetta Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Whitney Ave	351	1425	EB	F	Eastern Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Whitney Ave	760	1422	EB	F	Montclair St	\$20,000	\$0	\$25,000	\$80,000	\$5,000	\$130,000	TBA
Whitney Ave	933	680	WB	F	Montclair St	\$20,000	\$5,000	\$15,000	\$40,000	\$5,000	\$85,000	TBA
Whitney Ave	417	677	WB	N	Mulholland Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Whitney Ave	376	1424	EB	F	Norris Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Whitney Ave	381	678	WB	F	Norris Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Whitney Ave	153	1421	EB	F	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	2021/22
Whitney Ave	241	681	WB	N	Watt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Winding Way	968	1220	WB	F	Barrett Rd	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
Winding Way	1005	1298	EB	N	Barrett Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
Winding Way	827	1296	EB	F	Jan Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
Winding Way	675	2719	EB	F	Sunrise Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
10th St	811	779	SB	N	O St	\$20,000	\$0	\$0	\$5,000	\$5,000	\$30,000	TBA
10th St	546	743	NB	N	Q St	\$20,000	\$0	\$15,000	\$40,000	\$5,000	\$80,000	TBA
10th St	549	778	SB	F	Q St	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
23rd Ave	526	2666	WB	F	44th St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
2nd St	987	734	NB	F	Dolphin Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
2nd St	980	787	SB	F	Dolphin Wy	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
2nd St	943	732	NB	F	Jubilee Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA

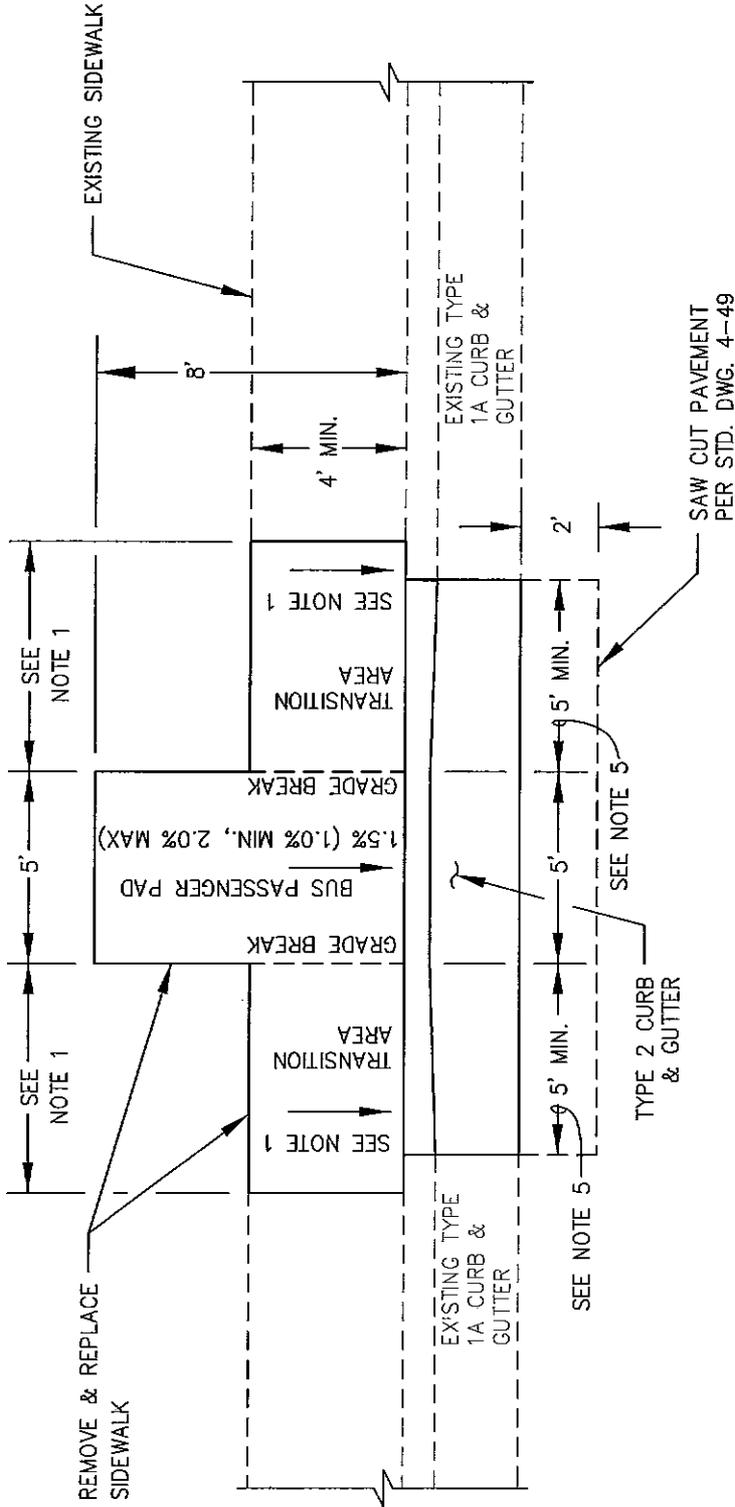
BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/ Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
2nd St	944	786	SB	F	M St	\$30,000	\$5,000	\$0	\$0	\$5,000	\$40,000	TBA
2nd St	955	735	NB	N	M St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
2nd St	938	789	SB	F	Monticello Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
2nd St	956	788	SB	F	Withington Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
2nd St	964	733	NB	F	Withington Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th Ave	892	2694	NB	F	47th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	604	2662	SB	F	14th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	629	2711	NB	N	14th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	580	2664	SB	F	18th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	626	2709	NB	N	18th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	516	2707	NB	N	23rd Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	781	2667	SB	N	25th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	355	2700	NB	F	37th Ave	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
44th St	618	2675	SB	F	37th Ave	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
44th St	602	2699	NB	N	39th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	704	2676	SB	F	39th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	713	2679	SB	F	41st St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	589	2680	SB	F	44th Ave	\$20,000	\$5,000	\$0	\$80,000	\$5,000	\$110,000	TBA
44th St	690	2695	NB	F	44th Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	578	2681	SB	F	45th Ave	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
44th St	510	2704	NB	F	Fruitridge Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	652	2670	SB	N	Fruitridge Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	674	2671	SB	F	Fruitridge Rd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	518	2672	SB	F	Iowa Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	528	2703	NB	F	Iowa Ave	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
44th St	387	2696	NB	N	Lemon Hill Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	705	2710	NB	N	Orinda Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	517	2665	SB	F	Roosevelt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	556	2708	NB	N	Roosevelt Ave	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
44th St	653	2706	NB	F	Sierra Vista Ave	\$20,000	\$5,000	\$0	\$60,000	\$5,000	\$90,000	TBA
47th Ave	726	2678	EB	F	46th St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
47th Ave	609	2682	EB	F	47th St	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
47th Ave	638	2693	WB	F	Laurine Way	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
47th Ave	482	2692	WB	F	Sampson Blvd	\$20,000	\$5,000	\$0	\$0	\$5,000	\$30,000	TBA
47th Ave	365	2691	WB	F	Steiner Dr	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
65th St	429	1611	WB	F	53rd Ave	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	TBA
65th St	8	3130	NB	F	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2013/14
65th St	70	1613	WB	N	Florin Rd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2016/17

BSTP Appendix 3 - Alphabetical Bus Stop Priority Ranking and Improvement Costs

On Street	Overall Priority Rank	Bus Stop #	Direct.	Far/Near	At Street	96"x60" Pad	Vertical Curb	Access. Path	Curb Ramps	Other	Total	Anticipated Fiscal Year Scheduled
65th St	29	3131	NB	F	Sky Pkwy	\$0	\$0	\$0	\$0	\$0	\$0	2014/15
65th St	30	1612	WB	N	Sky Pkwy	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2014/15
65th St	156	3132	NB	F	Stockton Blvd	\$20,000	\$0	\$0	\$0	\$5,000	\$25,000	2022/23

Grand Total: \$36,561,150



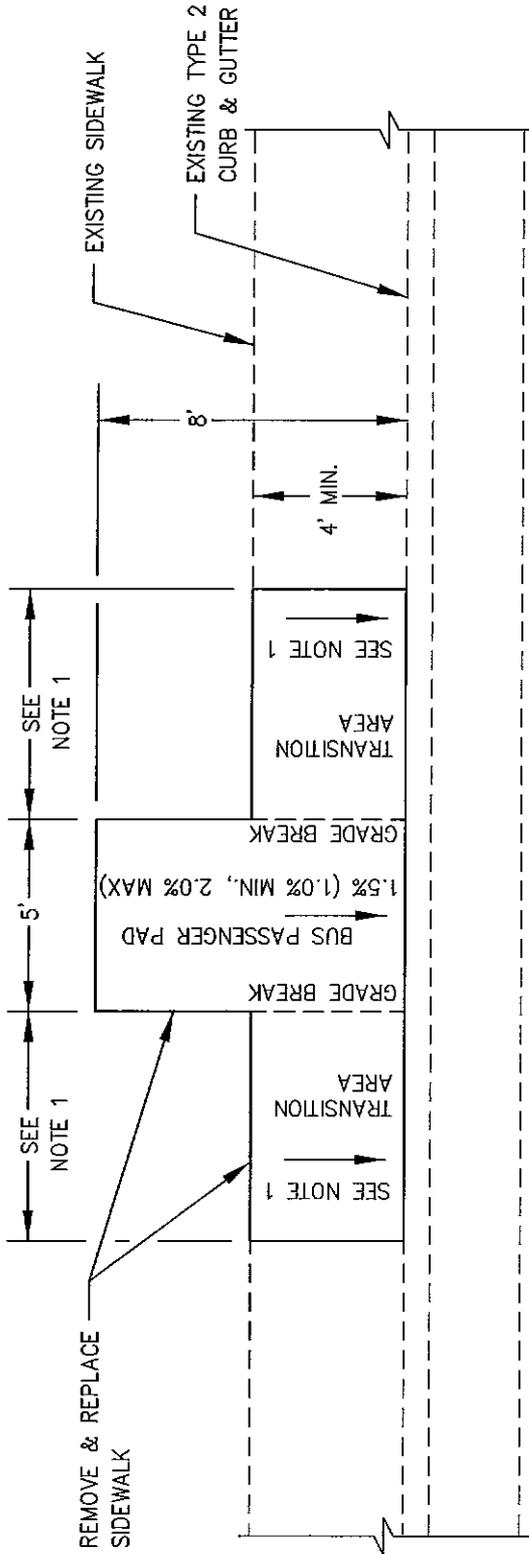
NOTES:

1. SIDEWALK TRANSITION AREA NEEDED TO CONFORM EXISTING SIDEWALK CROSS SLOPE TO 1.5% CROSS SLOPE AT BUS PASSENGER PAD AND TO CONFORM TO THE NEW ELEVATION OF THE NEW CURBING. SIDEWALK TRANSITION LENGTH SHALL EXTEND TO NEAREST SIDEWALK SCORE MARK OR JOINT AT OR BEYOND THE LIMIT OF CURB TRANSITION (SEE NOTE 5).
2. INSTALL SIDEWALK CONTACT JOINT AT CONNECTION TO EXISTING CURBING & EXISTING SIDEWALK PER STANDARD DRAWING 4-32.
3. PRIOR TO INSTALLATION, CONFIRM THERE IS SUFFICIENT AREA OF RIGHT-OF-WAY.
4. REGRADE EXISTING GROUND IN VICINITY OF BUS PAD TO CONFORM. REPAIR LANDSCAPING & IRRIGATION AS NEEDED.
5. TRANSITION CURB AND GUTTER FROM TYPE 1A CURB AND GUTTER TO TYPE 2 CURB AND GUTTER IN 5' MIN. EXTEND LENGTH TO NEAREST SCORE MARK/JOINT AS NEEDED - 8' MAX.

**COUNTY OF SACRAMENTO
MUNICIPAL SERVICES AGENCY**

**BUS STOP DETAIL-CLASS 'A'
STREET WITH ATTACHED
SIDEWALK AND TYPE 1A CURB**

SCALE: NONE
DATE: 02/2013
DRAWN BY: N.S./B.R.



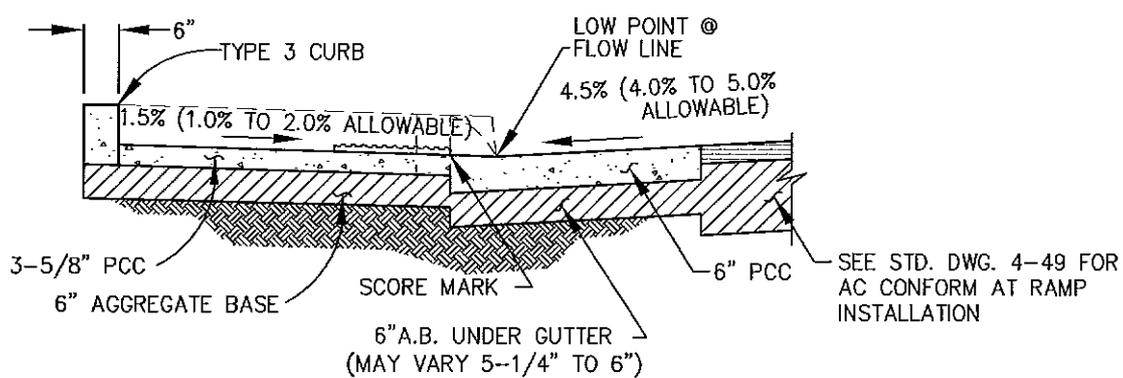
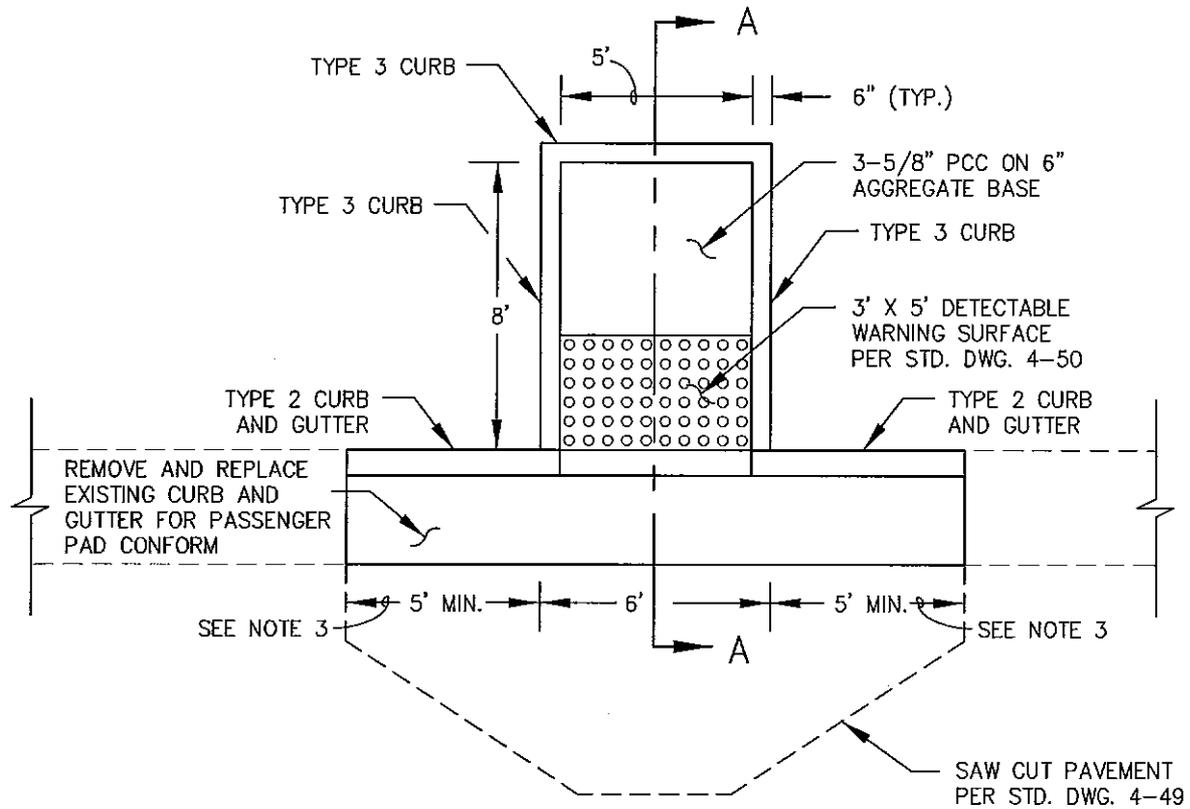
NOTES:

1. SIDEWALK TRANSITION AREA NEEDED TO CONFORM EXISTING SIDEWALK CROSS SLOPE TO 1.5% CROSS SLOPE AT BUS PASSENGER PAD AND TO CONFORM TO THE NEW ELEVATION OF THE NEW CURBING. SIDEWALK TRANSITION LENGTH SHALL EXTEND TO NEAREST SIDEWALK SCORE MARK OR JOINT AT OR BEYOND THE LIMIT OF CURB TRANSITION (SEE NOTE 5).
2. INSTALL SIDEWALK CONTACT JOINT AT CONNECTION TO EXISTING CURBING & EXISTING SIDEWALK PER STANDARD DRAWING 4-32.
3. PRIOR TO INSTALLATION, CONFIRM THERE IS SUFFICIENT AREA OF RIGHT-OF-WAY.
4. REGRADE EXISTING GROUND IN VICINITY OF BUS PAD TO CONFORM. REPAIR LANDSCAPING & IRRIGATION AS NEEDED.

**COUNTY OF SACRAMENTO
MUNICIPAL SERVICES AGENCY**

**BUS STOP DETAIL-CLASS 'A'
STREET WITH ATTACHED
SIDEWALK AND TYPE 2 CURB**

SCALE: NONE
DATE: 2/2013
DRAWN BY: N.S.



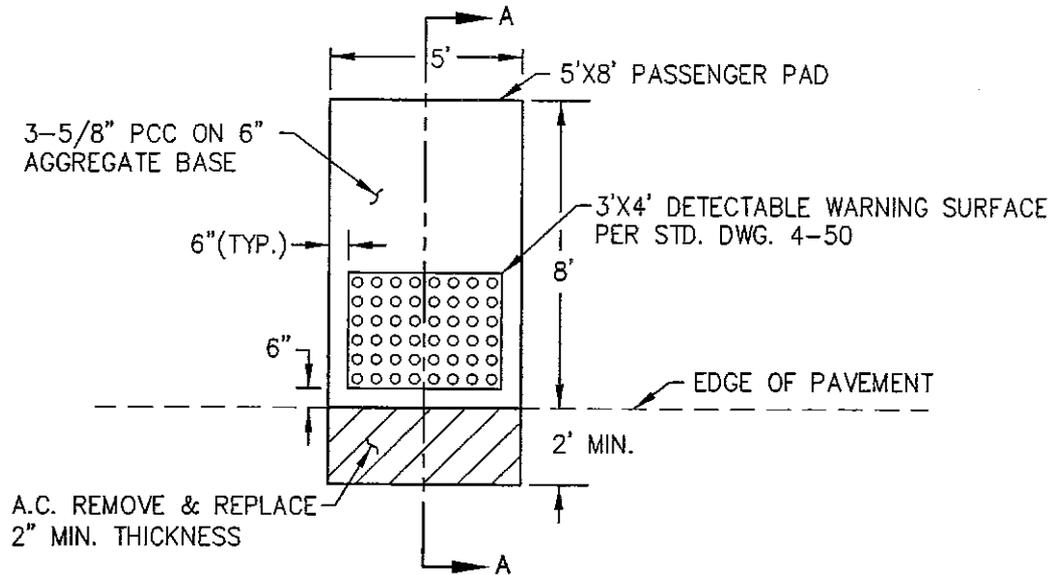
NOTES:

1. PRIOR TO INSTALLATION, CONFIRM THERE IS SUFFICIENT AREA OF PUBLIC RIGHT-OF-WAY.
2. REGRADE EXISTING GROUND IN VICINITY OF BUS WAITING AREA TO CONFORM. REPAIR LANDSCAPING & IRRIGATION AS NEEDED.
3. TRANSITION CURB AND GUTTER FROM TYPE 1A CURB AND GUTTER TO TYPE 2 CURB AND GUTTER IN 5' MIN. EXTEND LENGTH TO NEAREST SCORE MARK/JOINT AS NEEDED - 8' MAX.

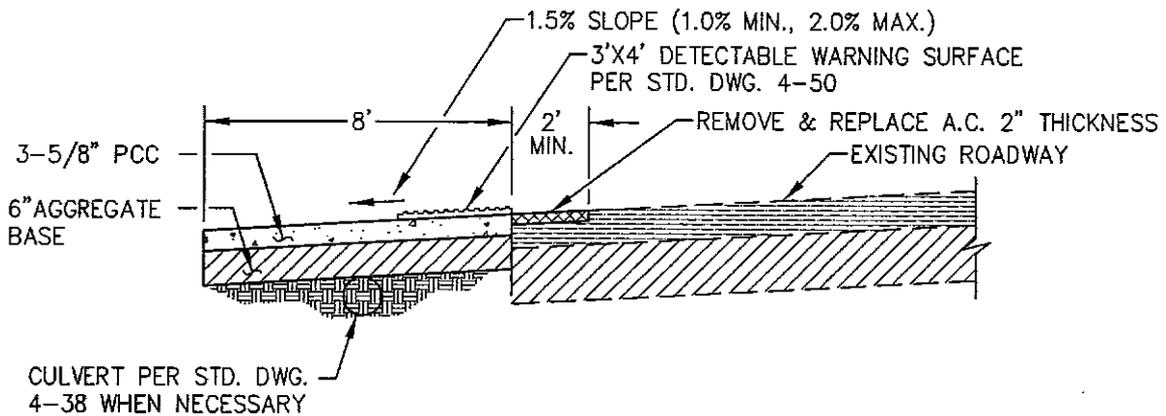
**COUNTY OF SACRAMENTO
MUNICIPAL SERVICES AGENCY**

**BUS STOP DETAIL
CLASS B STREET WITH
ATTACHED TYPE 1A CURB**

SCALE: NONE
DATE: 02/2013
DRAWN BY: N.S./B.R.



DETAIL - BUS PASSENGER PAD



SECTION A-A

NOTES:

1. PRIOR TO INSTALLATION, CONFIRM THERE IS SUFFICIENT AREA OF PUBLIC RIGHT-OF-WAY.
2. REGRADE EXISTING GROUND IN VICINITY OF BUS WAITING AREA TO CONFORM. REPAIR LANDSCAPING & IRRIGATION AS NEEDED.

**COUNTY OF SACRAMENTO
MUNICIPAL SERVICES AGENCY**

**BUS STOPS
CLASS "C" ROADS**

SCALE: NONE
DATE: 2/2013
DRAWN BY: NS/BR

BSTP Appendix 5 - Maintenance Agreement

BUS STOP INSTALLATION AND MAINTENANCE AGREEMENT

THIS AGREEMENT is made and entered into this 16th day of March 2011, ("Execution Date") by and between the **SACRAMENTO REGIONAL TRANSIT DISTRICT** ("RT"), a public corporation, and the **COUNTY OF SACRAMENTO** (COUNTY"), a political subdivision of the State of California.

RECITALS

WHEREAS, rerouting of existing RT bus service and establishment of new RT bus service in COUNTY requires installation and maintenance of sign poles, bus stop and No Parking signs; and

WHEREAS, in the past COUNTY assumed responsibility for installation and maintenance of sign poles, and No Parking signs, and RT assumed responsibility for installation and maintenance of RT bus stop signs; and

WHEREAS, repair of COUNTY curbs, gutters, and sidewalks at RT bus stops has been performed by COUNTY and RT has reimbursed COUNTY for a portion of the repair costs; and

WHEREAS, COUNTY's jurisdiction is within the activated portion of RT's jurisdiction, resulting in RT receiving COUNTY's allocation of Transportation Development Act revenues that COUNTY could otherwise use for street maintenance and repair; and

WHEREAS, the parties' desire to formalize their existing procedures for installation, maintenance and repair of COUNTY and RT improvements at RT bus stops and set out RT's obligation to pay COUNTY for a portion of certain COUNTY improvements and repairs.

NOW, THEREFORE, the parties agree as follows:

1. **BUS STOP IMPROVEMENTS, MAINTENANCE, AND COST ALLOCATION**

A. Sign Poles

(1) Installation. Within 45 days after RT submits written request, COUNTY will install a sign pole at an RT bus stop if there is no existing COUNTY sign pole or light standard, or if the existing COUNTY pole or light standard is unusable for placement of the signage necessary for establishment of a new bus stop.

(2) Unusable Poles. A COUNTY sign pole or light standard is unusable if COUNTY decides not to remove existing COUNTY signs, other than a No Parking sign, on a sign pole or light standard to create a No Parking Zone. Notwithstanding the foregoing, COUNTY is not obligated to install a sign pole at the proposed RT bus stop location if the pole would not be in compliance with the County's then existing

BSTP Appendix 5 - Maintenance Agreement

Standard Construction Specifications or Traffic Manual in addition to any applicable requirements in COUNTY codes, ordinances, improvement standards, or polices. In that event, the parties will meet to determine where a sign pole could be located within close proximity of the proposed bus stop location.

- (3) Sign Pole and No Parking Zone Sign Maintenance. COUNTY will be solely responsible to perform the maintenance, repair, and replacement required for joint use sign poles and COUNTY light standards and No Parking signs, at RT bus stops. RT will be solely responsible to perform the maintenance, repair, and replacement required on sign poles used solely by RT.
- (4) Sign Pole Installation Cost Allocation. RT will reimburse COUNTY for 100% of COUNTY's actual costs for installation of sign poles requested by RT if the scope of work and cost estimate were approved in advance by RT as set out in Section 5, below.
- (5) Sign Pole Maintenance Cost Allocation. COUNTY will be solely responsible for the costs of maintaining and replacing joint use sign poles, COUNTY light standards, and No Parking Signs. RT will be solely responsible for the costs of maintaining and replacing sign poles used solely by RT.

B. RT Bus Stop Signs

- (1) Installation. RT will be solely responsible for the installation and removal of RT bus stop signs located on COUNTY sign poles and light standards. RT may install an RT bus stop sign on a COUNTY pole or light standard within 15 days after RT submits written notice to the COUNTY identifying the proposed location, unless COUNTY issues a written rejection notice. Notwithstanding the foregoing, RT may not install an RT bus stop sign on any unusable COUNTY pole or light standard as defined in Section 1.A.(2), above.
- (2) Relocation. COUNTY may request RT to relocate its bus stop. Within 15 days after receipt of the COUNTY's written request, the parties will meet to determine whether and where to move the bus stop. If the parties agree to relocate the bus stop, RT will remove the RT bus stop sign from COUNTY's sign pole or light standard. COUNTY and RT will work together to determine the optimum location with respect to traffic needs, safety concerns and future maintenance. All installation, removal and relocation of bus stop poles and signs will be at RT's expense.

BSTP Appendix 5 - Maintenance Agreement

- (3) Bus Stop Sign Maintenance. RT will be solely responsible to perform the maintenance, repair, and replacement required for RT bus stop signs.
- (4) Bus Stop Sign Cost Allocation. RT will be solely responsible for the cost of installing, maintaining, and replacing RT bus stop signs.

C. Braille Signs

- (1) Installation. RT may place Braille signs on COUNTY sign poles and light standards to identify RT bus stops. The size, height, clearance, and type of sign and the method for affixing the sign to the pole or light standard must first be approved by COUNTY.
- (2) Braille Sign Maintenance. RT will be solely responsible to perform the maintenance, repair, and replacement required for RT Braille signs.
- (3) Braille Sign Cost Allocation. RT will be solely responsible for the cost of installing, maintaining, and replacing RT Braille signs.

D. No Parking Zones

- (1) Establishment. RT may request that COUNTY establish a No Parking Zone at RT bus stops. At RT's request, COUNTY will evaluate and where possible and practical may install a No Parking sign on a COUNTY pole or light standard unless it is unusable, as defined in subsection 1.A.(2), above. Where possible and practical County will remove any traffic signs, if necessary, to establish a No Parking Zone at the bus stop.
- (2) No Parking Zone Cost Allocation. RT will reimburse COUNTY for 100% of COUNTY's actual costs for installation of COUNTY No Parking signs at RT bus stops, if the scope of work and cost estimate were approved in advance by RT as set out in Section 5, below. COUNTY will be solely responsible for the costs of maintaining and replacing joint use No Parking signs at RT bus stops. RT will be solely responsible for the costs of maintaining and the costs of replacing No Parking poles and signs used solely by RT.

E. Curb, Gutter, and Sidewalk Repairs

- (1) Damage. RT shall report to COUNTY any known damage to COUNTY's curbs, gutters and sidewalks located immediately adjacent to an RT bus stop. Upon the receipt by COUNTY of such notice, COUNTY will temporarily repair damage to COUNTY's curbs, gutters and sidewalks in compliance with the COUNTY's most current curb, gutter and sidewalk repair and replacement policy. The COUNTY will schedule permanent repairs to COUNTY's curb, gutters, and sidewalks in compliance with the COUNTY's most current curb, gutter and

BSTP Appendix 5 - Maintenance Agreement

sidewalk repair and replacement policy. To the extent known to COUNTY, COUNTY will notify RT in writing to discontinue use of an RT bus stop if the curb, gutter or sidewalk immediately adjacent to the bus stop is damaged or deteriorated to the extent that it creates a risk of injury to pedestrians using the bus stop or immediately adjacent sidewalk with due care, until such time as the COUNTY makes repairs thereto.

(2) Curb, Gutter, and Sidewalk Maintenance. COUNTY will be solely responsible to perform the maintenance, repair, and replacement required for COUNTY curbs, gutters, and sidewalks in compliance with the COUNTY's most current curb, gutter and sidewalk repair and replacement policy.

(3) Cost allocation for Curb, Gutter and Sidewalk

(a) Permanent Repairs. RT will reimburse COUNTY for 50% of COUNTY's actual costs of permanent curb, gutter and sidewalk repairs if the scope of work and cost estimate were approved in advance by RT as set out in Section 5, below.

(b) Temporary Repairs. RT will reimburse COUNTY for 100% of COUNTY'S actual costs of temporary curb, gutter and sidewalk repairs No prior provision of scope and estimate in accordance with Section 5, below, shall be required.

F. Concrete Pads

(1) Passenger Waiting Areas

(a) County Installation. RT may request that COUNTY install concrete pads of an approximate size of 10' x 15' within COUNTY right of way at RT bus stops for use as passenger waiting areas. In the case where accessibility can be accomplished, COUNTY will install a concrete pad unless there is not sufficient right of way and if the pad cannot be installed in compliance with the COUNTY's then existing Standard Construction Specifications in addition to any applicable requirements in COUNTY codes, ordinances, improvement standards, or policies.

(b) Developer Installation. At locations requested by RT and subject to COUNTY's approval, COUNTY will require the owner of a new development project, as a condition of approval of the project or issuance of the building permit, to install a concrete pad of an approximate size of 10' x 20' within COUNTY right of way at locations that are existing bus stops or designated as future bus stops without passenger waiting areas. COUNTY's imposition of this condition is contingent upon the existence of sufficient nexus to require the developer of a given project to construct and/or to dedicate right-of-way for such a facility. COUNTY's imposition of

BSTP Appendix 5 - Maintenance Agreement

this condition is also contingent on whether there is sufficient right of way, including whether COUNTY may require owner to dedicate to COUNTY additional right of way, to construct this concrete pad and whether the pad can be installed in compliance with the COUNTY's then existing Standard Construction Specifications in addition to any applicable requirements in COUNTY codes, ordinances, improvement standards, or policies.

(c) Benches and Shelters. The concrete pads may be used as the location for placement of bus benches and shelters. RT or its contractors must obtain a COUNTY encroachment permit prior to installation of a bench or shelter at concrete pad locations within COUNTY right of way.

(2) Bus Loading Areas

(a) County Installation. RT may request that COUNTY install or caused to be installed concrete pads of a sufficient size to accommodate the number of buses expected to be loading at the same RT bus stop at the same time within COUNTY street right of way. COUNTY will confer with RT, and seek RT's approval, regarding the design and construction of the pad, including, but not limited to, the materials to be used. Installation of the pads is subject to COUNTY's approval and compliance with the COUNTY's then existing Standard Construction Specifications in addition to any applicable requirements in COUNTY codes, ordinances, improvement standards, or policies.

(b) Developer Installation. At locations requested by RT and subject to COUNTY's approval, COUNTY will design new streets and improvement of existing streets to include bus-loading areas with concrete pads as described in subsection F.1, if there is sufficient right of way. COUNTY will require the owner of a new development project as a condition of approval of the project to construct the bus loading areas at locations requested by RT and subject to COUNTY's approval and to install a concrete pad of an approximate size of 10' x 20' within COUNTY right of way as part of the COUNTY street as described in subsection F.2 at locations that are either existing bus stops or designated as future bus stops. COUNTY's imposition of this condition is contingent upon the existence of sufficient nexus to require the developer of a given project to construct and/or to dedicate right-of-way for such a facility. COUNTY's imposition of this condition is also contingent on whether there is sufficient right of way, including whether COUNTY may require owner to dedicate to COUNTY additional right of way, to construct this concrete pad and whether the pad can be installed in compliance with the COUNTY's then existing Standard

BSTP Appendix 5 - Maintenance Agreement

Construction Specifications in addition to any applicable requirements in COUNTY codes, ordinances, improvement standards, improvement standards, or policies.

- (3) Concrete Pad Maintenance. COUNTY will be solely responsible to perform the maintenance, repair, and replacement required for concrete pads.
- (4) Cost Allocation for Concrete Pads. RT will reimburse COUNTY for 50% of COUNTY's actual costs for installation of concrete pads in sidewalks for passenger waiting areas and in COUNTY streets for bus loading areas if the scope of work and cost estimate were approved in advance by RT as set out in Section 5, below. COUNTY will be solely responsible for the costs of maintenance of the concrete pads at an RT bus stop, including sealing cracks, application of adhesive or epoxy to areas of spalling, and application of asphalt where settling has occurred. RT will reimburse COUNTY for 50% of COUNTY's actual costs for repair of the concrete pads at RT bus stops if COUNTY obtained RT's approval regarding the design and construction of the pad. If the pad was installed without RT's approval of the design and construction, COUNTY will be solely responsible for the costs of repair. RT will reimburse COUNTY for 50% of COUNTY's actual costs for reconstruction of concrete pads, as necessary. In cases where COUNTY does not obtain RT's prior approval of design, COUNTY will be 100% responsible for all costs of needed repairs and reconstruction for the expected useful life of 20 years.

G. Curb Cuts

- (1) Procedure. RT may request COUNTY to modify the curbs at intersections adjacent to RT bus stops to facilitate access to RT's service by persons with disabilities. COUNTY will evaluate and may place requested locations on COUNTY's priority list of intersections scheduled for curb cut modifications after review and prioritization by the COUNTY's Department of Transportation and/or the Disability Compliance Office.
- (2) Cost Allocation for Curb Cuts. COUNTY will be solely responsible for the cost of modifying curbs at COUNTY street intersections to improve pedestrian access to RT bus stops. If there is an adjacent development requiring ramp upgrades, the developer will be responsible for the associated costs. COUNTY's imposition of this condition is contingent upon the existence of sufficient nexus to require the developer of a given project to construct and/or to dedicate right-of-way for such a facility.

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2. NOTIFICATION OF DEFICIENCIES

- A. RT may notify COUNTY in writing if RT determines that a COUNTY curb, gutter, sidewalk, street, pole or sign ("COUNTY Facilities") at an RT bus stop is damaged or its condition has deteriorated and should be maintained, repaired, or replaced.
- B. COUNTY may notify RT in writing if COUNTY determines that an RT sign or pole is damaged or its condition has deteriorated and should be maintained, repaired, or replaced or if maintenance, repair, or replacement of RT's signs is needed.
- C. Notwithstanding anything to the contrary set out herein, including the indemnification provisions of Section 9.A, below, RT has no duty to act as an inspector for COUNTY nor is RT obligated to indemnify COUNTY for Claims arising out of or resulting from the condition of COUNTY's Facilities; and COUNTY likewise has no duty to act as an inspector for RT nor is COUNTY obligated to indemnify RT for Claims arising out of or resulting from the condition of RT's signs.

3. ABANDONMENT OF EXISTING BUS STOPS

- A. If RT decides to discontinue use of an existing bus stop, RT will notify the COUNTY in writing. Within 30 days after the receipt of RT's written request, or within such later time thereafter that the parties may mutually establish, the parties will meet to determine whether the existing bus stop may be abandoned. If the parties agree to abandon the bus stop, COUNTY will remove all signs and poles unless explicit written consent is given to RT. However, if the parties cannot agree on the abandonment of the bus stop, the COUNTY's decision shall prevail and be final.
- B. COUNTY and RT shall equally share the cost of sign and pole removal for signs and poles jointly shared by COUNTY and RT. RT shall be 100% responsible for the cost of sign and pole removal for signs and poles used solely by RT.

4. REVIEW OF BUS STOPS AND STREET IMPROVEMENT PLANS

- A. Review of Plans. RT will provide COUNTY with a list of RT's existing bus stops annually. COUNTY will provide RT with an opportunity to review COUNTY street improvement plans that would affect an existing RT bus stop, or if any bus stop or related facilities are required within a given project's conditions of approval. RT will provide a contact name and mailing address for routing of plans. RT will contact COUNTY within 14 days of RT receipt of such notification with any comments or concerns.
- B. Street Design. In order to facilitate pedestrian access to RT bus stops, COUNTY will design its street improvements at existing or proposed RT bus stops to place the sidewalk immediately adjacent to the curb rather

BSTP Appendix 5 - Maintenance Agreement

than placing a landscaped strip between the curb and the sidewalk or to provide a concrete path between the bus stop and the sidewalk. COUNTY will require the owner of a new development project, as a condition of approval of the project, to construct its street improvements at existing or proposed RT bus stops by placing the sidewalk immediately adjacent to the curb, rather than to place a landscaped strip between the curb and the sidewalk or by providing a concrete path between the bus stop and the sidewalk.

C. COUNTY Street Improvements

Notwithstanding the provisions of Sections 1.E, 1.F and 1.G, above, RT shall not be responsible for COUNTY's costs to undertake improvements to COUNTY streets, curbs, gutters and/or sidewalks that are: (1) part of a scheduled COUNTY capital improvement project; or (2) required to facilitate access by persons with disabilities as part of COUNTY's obligations under the Americans with Disabilities Act.

5. PROCEDURES FOR COST REIMBURSEMENT

- A. At least 15 days prior to COUNTY's commencement of permanent repairs or improvements of COUNTY Facilities at an RT bus stop, for which RT is to reimburse costs to COUNTY, COUNTY must send RT a description of the scope of work and a cost estimate.
- B. The cost estimate will be based on COUNTY's actual costs, which means hourly labor costs, the actual materials costs, and the actual equipment costs for work performed by COUNTY's forces, and the COUNTY approved invoice amount for work performed by COUNTY's contractors. The scope of work and cost estimate must delineate the extent of the work at RT's bus stop and the work within the area adjacent to RT's bus stop.
- C. RT will approve or disapprove the scope and cost estimate for work to be funded by RT within 10 days after receipt. If the parties disagree as to the scope or cost estimate for work to be funded by RT, the parties will meet to resolve the dispute. If RT does not have sufficient funds budgeted to pay COUNTY for the work during the subject fiscal year, RT may condition its approval by specifying when RT would be able to fund its share of the costs.

6. METHOD OF PAYMENT

Unless otherwise specifically provided herein, RT shall make any payment to COUNTY that is required under this Agreement not later than 30 calendar days after receiving an invoice for such payment. The COUNTY invoice must specify the labor materials, and equipment costs required to perform the repair work. If RT disputes any items on an invoice for a reasonable cause, RT may deduct that disputed item from the payment, but shall not delay payment for the undisputed portions. The amounts and reasons for such deductions shall be documented to COUNTY within 10 days from the date of RT's receipt of COUNTY's invoice. The

BSTP Appendix 5 - Maintenance Agreement

parties shall meet no later than 10 days after COUNTY's receipt of RT's notice regarding the reasons for any deductions in an attempt to resolve any disputes and allow COUNTY to follow up with necessary billing changes prior to the next billing cycle.

7. TERM

- A. This Agreement will be effective as of July 1, 2011 and shall remain in effect for a term of five years unless terminated. The Agreement may be extended by the Director of Transportation of COUNTY and the Facilities Manager of RT when made in writing and signed by both parties. Each extension of time shall not exceed a period of five years.
- B. Either party may terminate this Agreement by providing the other party written notice of termination no less than 30 days prior to the effective date of such termination. Should RT terminate, RT still has the obligation to pay for outstanding repairs that were approved pursuant to Section 5.

8. NOTICES

All notices and other communications under this Agreement shall be in writing and shall be deemed to have been duly given (1) on the date of delivery, if delivered personally to the party to whom notice is given, or if made by telecopy directed to the party to whom notice is to be given at the telecopy number listed below, or (2) at the earlier of actual receipt or the second business day following deposit in the United States mail, postage prepaid. Notices and other communications shall be directed to the parties at the addresses shown below. A party may change its person designated to receive notice, its telecopy number, or its address from time to time by giving notice to the other party in accordance with the procedures set forth in this Article.

To COUNTY: Michael Penrose, Director
Department of Transportation
COUNTY of Sacramento
906 G Street, Suite 510
Sacramento, CA 95814
Phone: (916) 874-6291
Fax: (916) 874-7831

To RT: Sacramento Regional Transit District
Attn: Facilities Manager
PO Box 2110
Sacramento CA 95812-2110
Phone: (916) 321-3830
Fax: (916) 454-6016

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9. INDEMNIFICATION

- A. Pursuant to California Government Code Section 895.4: (1) each party as Indemnitor, shall defend, hold harmless and indemnify the other party, as Indemnitee, against any claim, obligation, loss, penalty, fine, demand, damage, cost, expense or liability, including attorneys' fees, (hereafter collectively referred to as "Claim(s)") caused by the negligent or wrongful act or omission of the Indemnitor (including, without limitation, Indemnitor's officers, agents or employees) arising out of or resulting from Indemnitor's performance of this Agreement; and (2) if a party is held liable upon any judgment for damages caused by a negligent or wrongful act or omission occurring in the performance of this Agreement and that party pays in excess of its share based on principles of comparative fault, that party is entitled to a contribution from the other party to the extent of the other party's comparative fault. If any excess amount remains outstanding after the foregoing contribution is made, the other party will contribute a fraction of that excess equal to: its percentage of fault divided by the total percentage of fault of the parties to this Agreement.
- B. Indemnitor shall, upon Indemnitee's request, defend at its sole cost any suit asserting a Claim covered by this indemnity. The parties shall cooperate in the defense of such actions brought by others with respect to the matters covered in this indemnity. In addition, Indemnitor shall reimburse Indemnitee for all costs, including reasonable attorney's fees, associated with efforts to enforce this indemnification provision.
- C. The foregoing right to indemnity shall be in addition to, and not exclusive of, any other legal, equitable or statutory right(s) of indemnification or insurance to which a party may be entitled.

10. INSURANCE

RT agrees that its obligation to indemnify COUNTY pursuant to the provisions of Section 9 shall be supported by its self-insurance program. COUNTY agrees that its obligation to indemnify RT pursuant to the provisions of Section 9 shall be supported by its self-insurance program.

11. INDEPENDENT CONTRACTOR

Nothing herein will be construed as or deemed to create the relationship of employer-employee or principal-agent between RT and COUNTY. Neither party will be responsible for providing workers compensation insurance or any other protective insurance coverage or employment benefit payable to the other party's employees. Each party will assume all responsibility for payment of wages to its employees and for federal and state income tax withholding. Each party agrees to indemnify and hold the other harmless from any and all claims made against the other based upon any contention by any employee or by any third party, including but not limited to any federal or state agency, that an employer-

BSTP Appendix 5 - Maintenance Agreement

employee relationship or a substitute therefore exists between COUNTY and employees of RT or between RT and employees of COUNTY.

12. NO AGENCY

This Agreement will not create among the parties a joint venture, partnership, or any other relationship of association or agency. Neither RT, nor any of RT's employees, contractors or subcontractors are or will be considered to be agents of COUNTY in connection with the performance of any of RT's obligations under this Agreement. Neither COUNTY, nor any of COUNTY's employees, contractors or subcontractors are or will be considered to be agents of RT in connection with the performance of any of COUNTY's obligations under this Agreement.

13. THIRD PARTY BENEFICIARY

Nothing herein will create and nothing herein will establish a standard of care for, or create any rights in, any person not a party to this Agreement.

14. MODIFICATIONS

Except as provided herein, no alteration, amendment, variation, or waiver of the terms of this Agreement shall be valid unless made in writing and signed by both parties. Waiver by either party of any default, breach or condition precedent shall not be construed as a waiver of any other default, breach or condition precedent, or any other right hereunder. No interpretation of any provision of this Agreement shall be binding upon COUNTY unless agreed in writing by the Director of Transportation and counsel for COUNTY. No interpretation of any provision of this Agreement shall be binding upon RT unless agreed in writing by the Facilities Manager and counsel for RT.

15. SEVERABILITY

If any term, covenant, or condition of this Agreement is held by a court of competent jurisdiction to be invalid, the remainder of this Agreement shall remain in effect.

16. SURVIVORSHIP

The responsibility for indemnification shall not be invalidated due to the termination of this Agreement. Termination of this Agreement will not affect the parties' respective responsibilities for the payment of costs incurred prior to the effective date of termination.

17. ATTORNEYS' FEES

If any legal proceeding should be instituted by either of the parties to enforce the terms of this Agreement or to determine the rights of the parties, the prevailing party in said proceeding shall recover all reasonable court costs and expenses, including attorneys' fees.

BSTP Appendix 5 - Maintenance Agreement

18. BINDING ON SUCCESSORS

This Agreement shall inure to the benefit of and be binding upon the successors and assigns of the parties hereto; provided that assignment shall require approval of the non-assigning party, and any purported assignment without such approval shall be void.

19. AMBIGUITIES & DISPUTES

A. The parties have each carefully reviewed this Agreement and have agreed to each term of this Agreement. No ambiguity shall be presumed to be construed against either party.

B. In the event of any dispute arising out of or relating to this Agreement, the parties shall attempt, in good faith, to promptly resolve the dispute mutually between themselves. If the dispute cannot be resolved by mutual agreement, nothing herein shall preclude either party's right to pursue remedy or relief by civil litigation, pursuant to the laws of the State of California.

20. COMPLIANCE WITH LAWS

RT and COUNTY shall observe and comply with all applicable Federal, State, and County laws, regulations, and ordinances.

21. GOVERNING LAWS AND JURISDICTION

This Agreement shall be deemed to have been executed and to be performed within the State of California and shall be construed and governed by the internal laws of the State of California. Any legal proceedings arising out of or relating to this Agreement shall be brought in Sacramento County, California.

22. ENTIRE AGREEMENT

This Agreement embodies the entire agreement of the parties in relation to the scope of services herein described, and no other understanding whether verbal, written or otherwise exists between the parties.

23. COUNTERPARTS

This Agreement may be executed in counterparts. The Agreement shall be deemed executed when it has been signed by both parties.

(Signature Page Follows)

BSTP Appendix 5 - Maintenance Agreement

Executed as of the day first above stated.

**SACRAMENTO REGIONAL
TRANSIT DISTRICT**

By: 
DON NOTTOLI, Chair

By: 
MICHAEL R. WILEY
General Manager/CEO

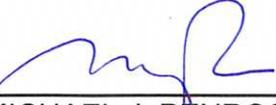
Approved as to Content:

By: 
MIKE MATTOS
Chief of Facilities and Business
Support Services

Approved as to Legal Form:

By: 
BRUCE A. BEHRENS
Chief Legal Counsel

COUNTY OF SACRAMENTO

By: 
MICHAEL J. PENROSE, Director,
Department of Transportation

Agreement approved by the Board of
Supervisors with authority delegated to
the Director to sign.

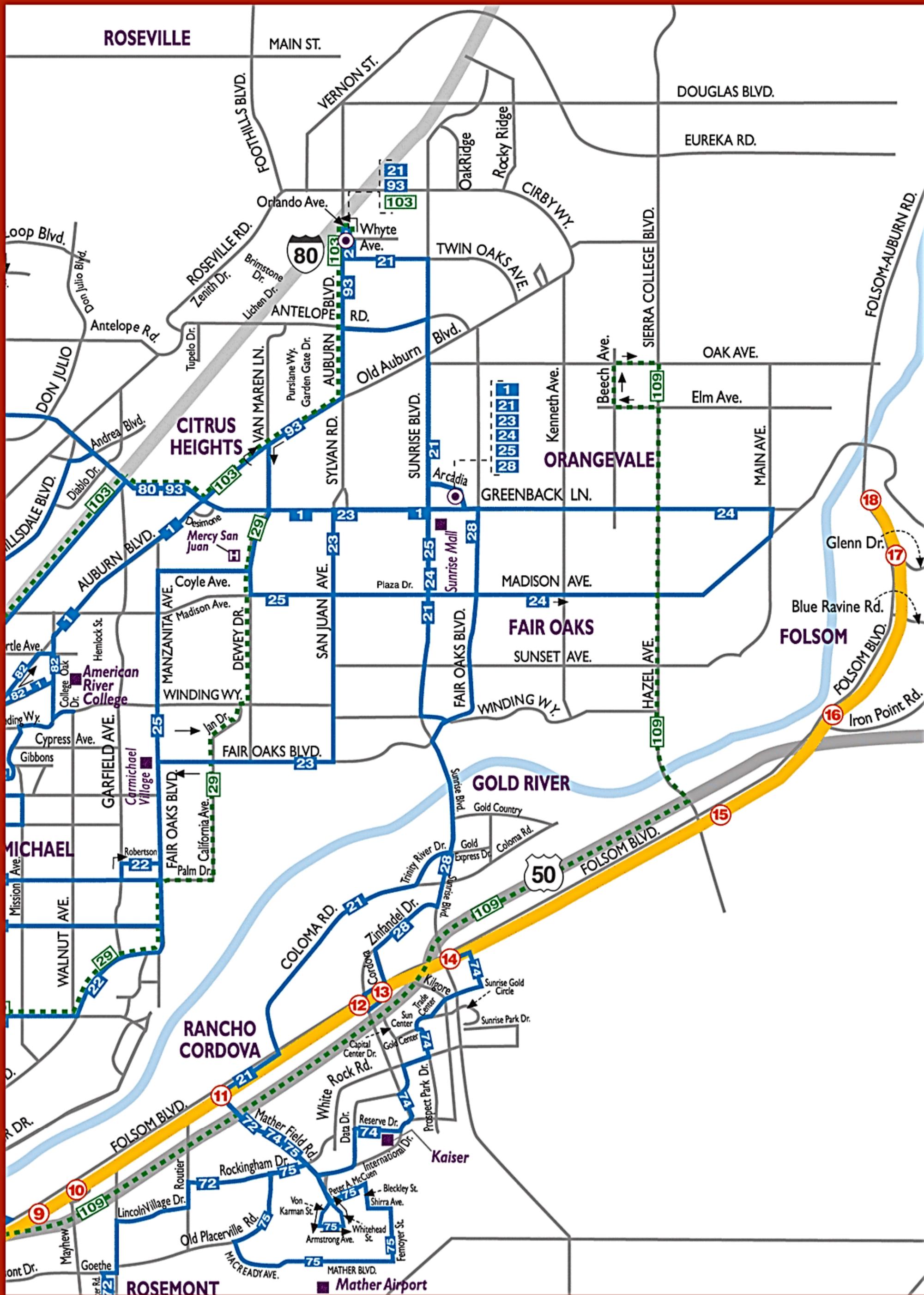
Agenda Date: 2/1/2011
Item Number: 15
Resolution Number: 2011-0089

Reviewed and Approved by County
Counsel:

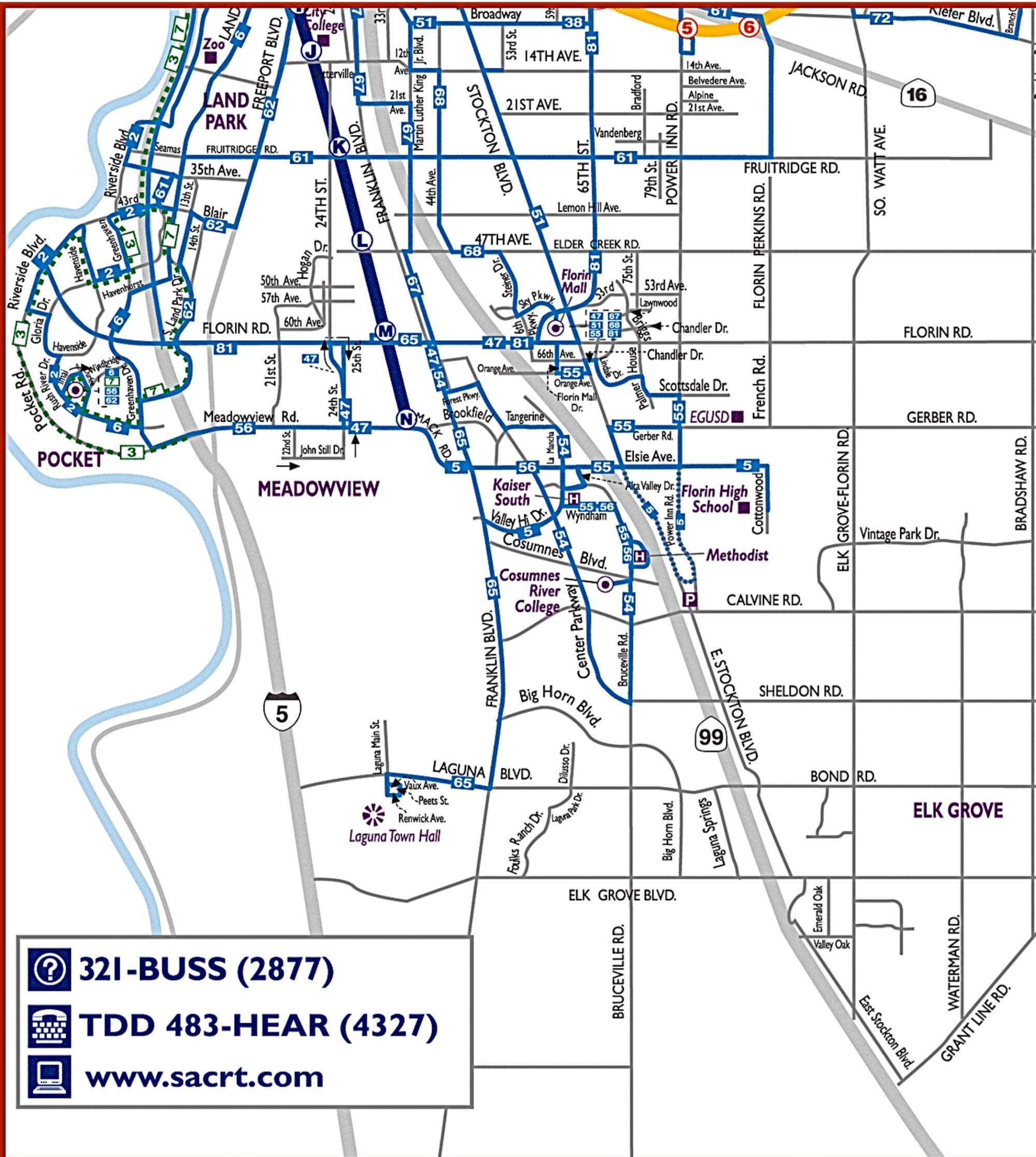
By: 
ROBERT P. PARRISH
Deputy County Counsel

County Ref. Contract #52312

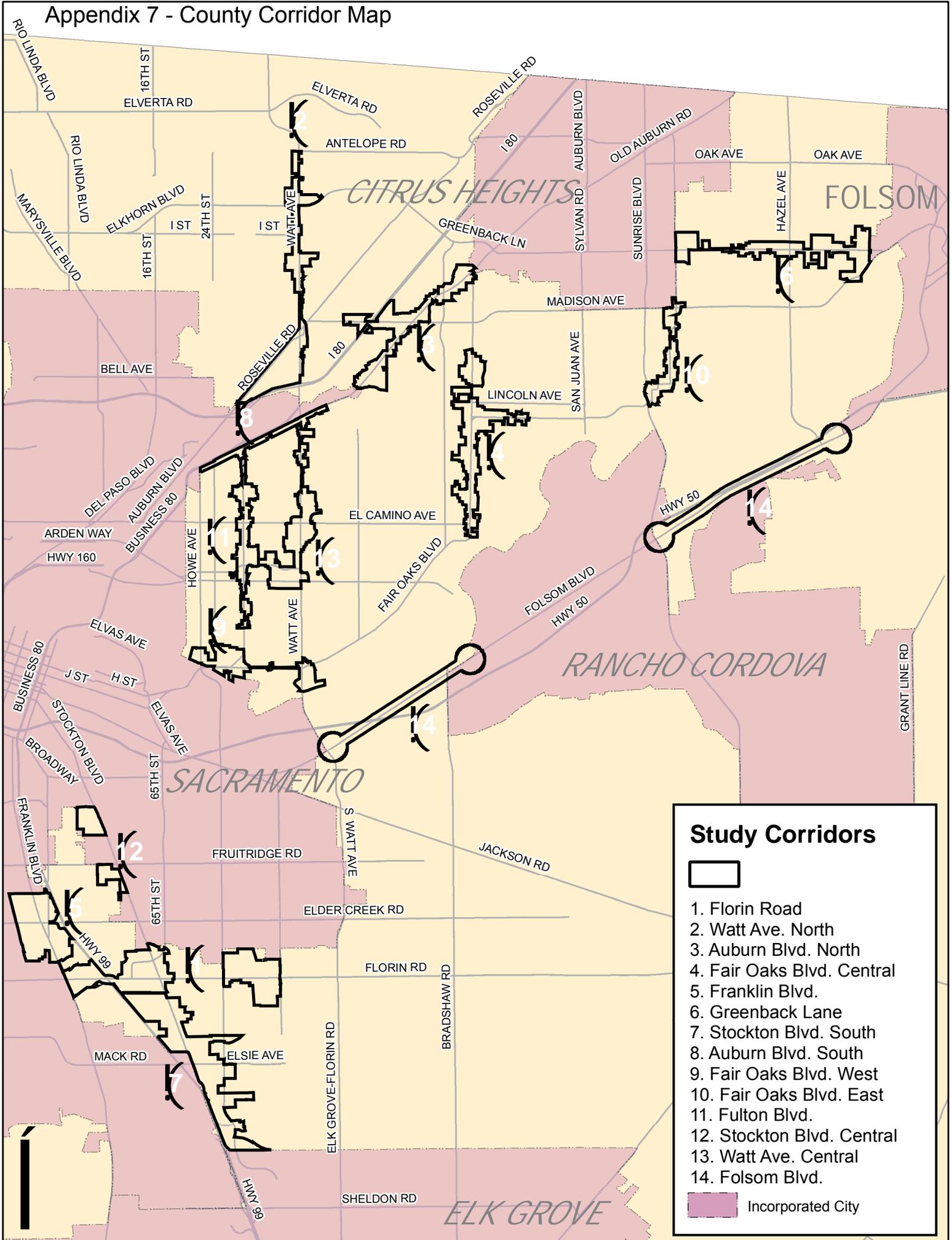




Appendix 6 - RT Route Maps



Appendix 7 - County Corridor Map



Study Corridors



1. Florin Road
2. Watt Ave. North
3. Auburn Blvd. North
4. Fair Oaks Blvd. Central
5. Franklin Blvd.
6. Greenback Lane
7. Stockton Blvd. South
8. Auburn Blvd. South
9. Fair Oaks Blvd. West
10. Fair Oaks Blvd. East
11. Fulton Blvd.
12. Stockton Blvd. Central
13. Watt Ave. Central
14. Folsom Blvd.

 Incorporated City



Bus Stop Site Survey - Legend

Bus Stop #:	On Street:	Near /Far	Side Street:	NB/SB/EB or WB	Date of Survey
1127	Cottage Way	F	Morse Ave	EB	2/06/12

Bus Stop #: A unique identifying number assigned to each bus stop location. This is a “clickable link” that leads to the Regional Transit webpage for that specific bus stop, showing additional information about the bus routes and schedules that serve that stop.

On Street: This is the street that the bus stop is physically located on.

Near/Far: This denotes which side of the nearest intersecting street (“Side Street”) that the bus stop is on relative to the direction of travel of the bus using that stop. For example, if a bus stop for a north-bound bus was on the north side of the closest intersecting “side street”, then it is considered to be on the “far” side of that intersection. Likewise, if a bus stop for a north-bound bus was on the south side of the closest intersecting “side street” then it is considered to be on the “near” side of that intersecting “side street”.

Side Street: This is generally the closest intersecting street to the bus stop location.

NB/SB/EB/ or WB: This denotes the direction of travel for the bus using that specific stop (NB: NorthBound, SB: SouthBound, EB: EastBound, WB: WestBound)

Date of Survey: When the site survey was conducted.

The “Needs” table below identifies a summary of any improvements and associated costs that were identified at the time of the site survey that will be needed at that bus stop.

Needs:

	Yes	No	Cost	Notes:
96”x60” Pad		x	\$0	Existing 8’x5’ pad and slopes are compliant.
Vertical Curb		x	\$0	
Sidewalk Infill To Nearest Intersect.		x	\$0	
ADA Curb ramps		x	\$0	
Other (See Notes)		x	\$0	
Total Costs			\$0	



Bus Stop Accessibility Survey

Bus Stop #:	On Street:	Near /Far	Side Street:	NB/SB/EB or WB	Date of Survey
<u>377</u>	Watt/Manlove LRS	F	Bay 2	WB	1/25/12

Needs:

	Yes	No	Cost	Notes:
96"x60" Pad	x		\$20,000	Exist pad, excessive slopes
Vertical Curb	x		\$5,000	
Sidewalk Infill To Nearest Intersect.		x	\$0	
ADA Curb ramps		x	\$0	
Other (See Notes)	x		\$5,000	
Total Costs			\$30,000	

Misc Notes:





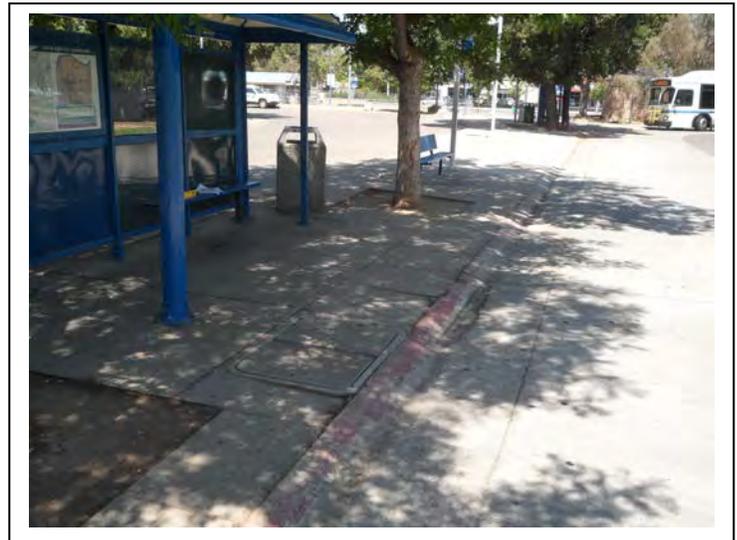
Bus Stop Accessibility Survey

Bus Stop #:	On Street:	Near /Far	Side Street:	NB/SB/EB or WB	Date of Survey
378	Watt/Manlove LRS	F	Bay 1	EB	1/25/12

Needs:

	Yes	No	Cost	Notes:
96"x60" Pad	x		\$20,000	Exist pad, excessive slopes
Vertical Curb	x		\$5,000	
Sidewalk Infill To Nearest Intersect.	x		\$5,000	Rework access between stop cover & Tree well
ADA Curb ramps		X	\$0	
Other (See Notes)		x	\$5,000	
Total Costs			\$35,000	

Misc Notes:





Bus Stop Accessibility Survey

Bus Stop #:	On Street:	Near /Far	Side Street:	NB/SB/EB or WB	Date of Survey
0447	Hazel Ave	F	Nimbus Rd	NB	1/25/12

Needs:

	Yes	No	Cost	Notes:
96"x60" Pad	x		\$20,000	Exist pad, excessive slopes
Vertical Curb		x	\$0	
Sidewalk Infill To Nearest Intersect.		x	\$0	
ADA Curb ramps		x	\$0	
Other (See Notes)	x		\$5,000	
Total Costs			\$25,000	

Misc Notes:





Bus Stop Accessibility Survey

Bus Stop #:	On Street:	Near /Far	Side Street:	NB/SB/EB or WB	Date of Survey
<u>0453</u>	Fair Oaks Blvd	N	Marshall Ave	EB	1/25/12

Needs:

	Yes	No	Cost	Notes:
96"x60" Pad	x		\$20,000	
Vertical Curb	x		\$5,000	
Sidewalk Infill To Nearest Intersect.		x	\$0	
ADA Curb ramps		x	\$0	
Other (See Notes)	X		\$5,000	
Total Costs			\$30,000	

Misc Notes:





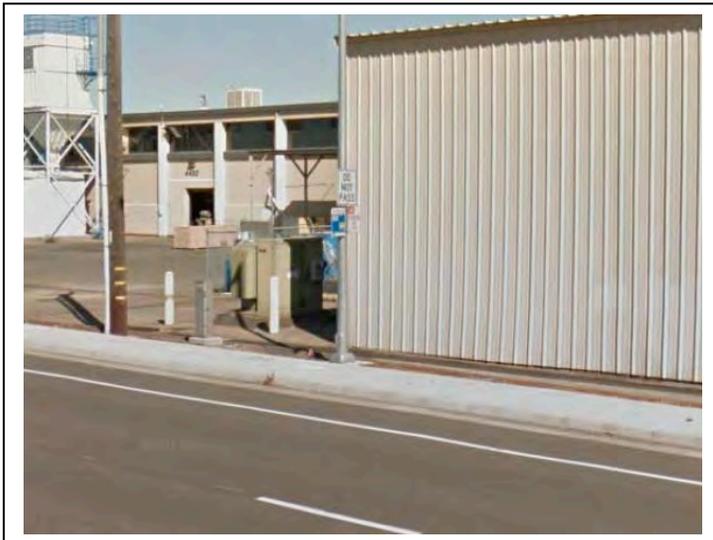
Bus Stop Accessibility Survey

Bus Stop #:	On Street:	Near /Far	Side Street:	NB/SB/EB or WB	Date of Survey
<u>0454</u>	Kilzer Ave	F	Dudley Blvd	NB	1/25/12

Needs:

	Yes	No	Cost	Notes:
96"x60" Pad	x		\$20,000	
Vertical Curb		x	\$0	
Sidewalk Infill To Nearest Intersect.		x	\$0	
ADA Curb ramps		x	\$0	
Other (See Notes)	X		\$5,000	
Total Costs			\$25,000	

Misc Notes:





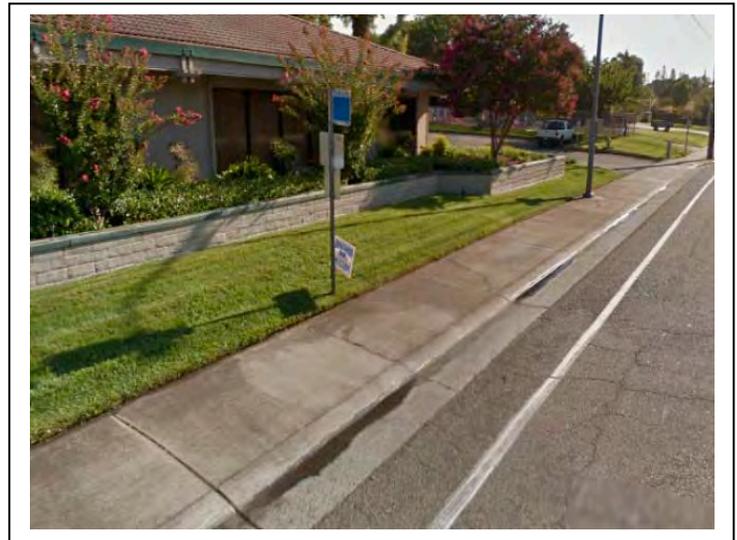
Bus Stop Accessibility Survey

Bus Stop #:	On Street:	Near /Far	Side Street:	NB/SB/EB or WB	Date of Survey
<u>0459</u>	Northrop Ave	F	Evelyn Ln	WB	1/25/12

Needs:

	Yes	No	Cost	Notes:
96"x60" Pad	x		\$20,000	
Vertical Curb		x	\$0	
Sidewalk Infill To Nearest Intersect.		x	\$0	
ADA Curb ramps		x	\$0	
Other (See Notes)	X		\$5,000	
Total Costs			\$25,000	

Misc Notes:





Bus Stop Accessibility Survey

Bus Stop #:	On Street:	Near /Far	Side Street:	NB/SB/EB or WB	Date of Survey
0479	Forcum Ave	F	Dudley Blvd	SB	1/25/12

Needs:

	Yes	No	Cost	Notes:
96"x60" Pad	x		\$20,000	
Vertical Curb		x	\$0	
Sidewalk Infill To Nearest Intersect.		x	\$0	
ADA Curb ramps		x	\$0	
Other (See Notes)	X		\$5,000	
Total Costs			\$25,000	

Misc Notes:





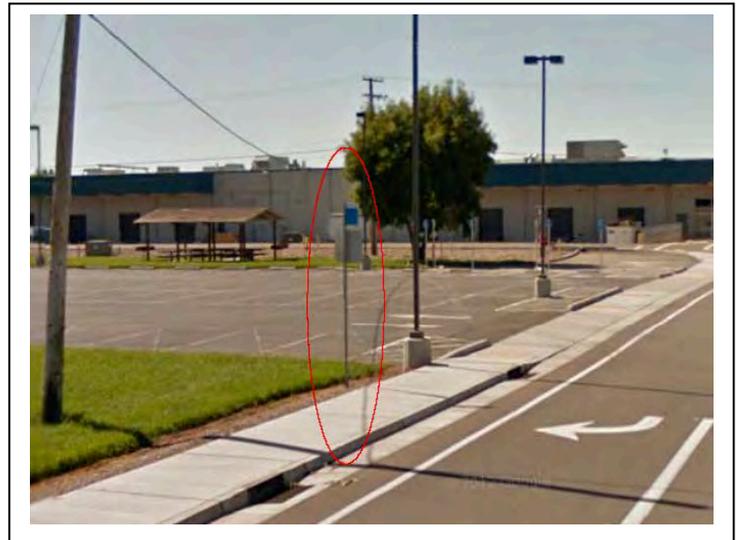
Bus Stop Accessibility Survey

Bus Stop #:	On Street:	Near /Far	Side Street:	NB/SB/EB or WB	Date of Survey
<u>0495</u>	AC Street	N	Patrol Road	EB	1/25/12

Needs:

	Yes	No	Cost	Notes:
96"x60" Pad	x		\$20,000	
Vertical Curb		x	\$0	
Sidewalk Infill To Nearest Intersect.		x	\$0	
ADA Curb ramps		x	\$0	
Other (See Notes)	X		\$5,000	
Total Costs			\$25,000	

Misc Notes:





Bus Stop Accessibility Survey

Bus Stop #:	On Street:	Near /Far	Side Street:	NB/SB/EB or WB	Date of Survey
0496	Kiefer Blvd	F	Mayhew Rd	WB	1/25/12

Needs: NONE!!

	Yes	No	Cost	Notes:
96"x60" Pad		X	\$0	
Vertical Curb		X	\$0	
Sidewalk Infill To Nearest Intersect.		X	\$0	
ADA Curb ramps		X	\$0	
Other (See Notes)		x	\$0	
Total Costs			\$0	

Misc Notes:

Field verified

Transverse slopes are < 2%





Bus Stop Accessibility Survey

Bus Stop #:	On Street:	Near /Far	Side Street:	NB/SB/EB or WB	Date of Survey
<u>0512</u>	Dudley Blvd	F	Forcum Rd	EB	1/25/12

Needs:

	Yes	No	Cost	Notes:
96"x60" Pad		x	\$0	Existing 8'x5' pad adjacent to vertical curb, meets 2% transverse slope requirement.
Vertical Curb		x	\$0	
Sidewalk Infill To Nearest Intersect.		x	\$0	
ADA Curb ramps		x	\$0	
Other (See Notes)		x	\$0	
Total Costs			\$0	

Misc Notes:

Field verified

Transverse slopes are < 2%

13' pad depth from back of curb





County of Sacramento

News Release
December 12, 2011

Contact: Dan Regan
916-874-7056

Bus Stop ADA Accessibility Study (& Survey Results) *On-line Survey Participants Invited to Help Identify Needs*

Sacramento CA – The Sacramento County Department of Transportation (SACDOT) was recently awarded a Caltrans Planning Grant to evaluate the accessibility of Regional Transit bus stops within the unincorporated county.

Providing accessible bus stops is a high priority for Sacramento County and Regional Transit, to help ensure that everyone can physically access bus stops and properly load and unload from buses. As part of this study, SACDOT staff will visit existing unincorporated area bus stops (approximately 1,100) and make note of any ADA accessibility improvements to enhance access. This information will enable SACDOT and Regional Transit to better manage, prioritize and schedule improvement projects as funding becomes available in the years ahead.

We would like to encourage residents to assist by voluntarily filling out a survey if they are having difficulty accessing bus stops in the unincorporated area. Interested residents can visit www.sacdot.com/survey to answer a brief and anonymous “Ridership Accessibility Survey” to help identify specific bus stop access issues. This feedback will supplement the SACDOT database for consideration in future projects. Examples of physical obstructions that could prevent access to a bus stop may include the following:

1. Lack of sidewalks leading to the bus stop.
2. Lack of curb ramps at street intersections in route to the bus stop.
3. Inadequate loading/unloading area at the bus stop to accommodate a wheelchair or scooter.
4. Physical barriers on a sidewalk such as a power pole restricting wheelchair or scooter passage.

Accessibility Study Schedule:

- Start-Up of Study: 2/2011 to 4/2011

Appendix 8 - Bus Stop Site Survey Samples & Website Ridership Survey Summary

- Ongoing Community & Stakeholder Outreach: 2/2011 to 7/2012
- Bus Stop Site Inspections: 10/2011 to 3/2012
- Develop Draft Plan: 4/2011 to 4/2012
- Final Plan Preparation: 5/2012 to 7/2012

This Accessibility Study is being funded by a \$250,000 “Environmental Justice and Community-Based Transportation Planning” grant made available through the California Department of Transportation.

Stay connected! Get County News as it happens by signing up for [E-SacCountyInfo](#)

Ridership Accessibility Survey Results

The following six pages are a summary of responses from questions posted the public “Ridership Accessibility Survey” (Posted on the “Survey Monkey” website) referenced above in the Press Release above and located at www.sacdot.com/survey.

Transit Ridership Survey



1. How often do you ride the bus?

		Response Percent	Response Count
1 to 3 days a month		20.0%	3
1 to 2 days a week		20.0%	3
3 to 5 days a week		40.0%	6
6 to 7 days a week		20.0%	3
answered question			15
skipped question			1

2. What is the purpose of your trip?

		Response Percent	Response Count
Work		80.0%	12
Training		0.0%	0
School		13.3%	2
Doctor/Dentist		40.0%	6
Shopping/Errands		53.3%	8
Government		13.3%	2
Visiting/Recreation		33.3%	5
Other		13.3%	2
answered question			15
skipped question			1

3. How do you reach your transit stop?

		Response Percent	Response Count
Walk 0-3 blocks		66.7%	10
Wheel chair or Scooter		20.0%	3
Walk more than 3 blocks		33.3%	5
Guide dog		0.0%	0
Transfer from another bus		20.0%	3
Crutches		6.7%	1
Drove or was dropped off		6.7%	1
Walker		13.3%	2
Bicycle		6.7%	1
White cane		20.0%	3
Other		0.0%	0
answered question			15
skipped question			1

4. Did you experience any sidewalk barriers when getting to your bus stop?

		Response Percent	Response Count
No		46.7%	7
Yes		53.3%	8

If yes, please explain including the bus route number, and the location (nearest cross street) and nature of the problem. 7

answered question			15
skipped question			1

5. Were there any bus stop related barriers you experienced while waiting for a bus, or when getting on or off of the bus?

		Response Percent	Response Count
No		42.9%	6
Yes		57.1%	8

If yes, please explain including the bus route number, and the location (nearest cross street) and nature of the problem. 8

answered question	14
skipped question	2

6. Do you have any other comments or suggestions regarding the ability to access any of the bus stops, or the bus stop amenities such as shelters, signage, waiting or boarding area, etc.? Please explain and give specific locations.

	Response Count
	9
answered question	9
skipped question	7

Appendix 8 - Bus Stop Site Survey Samples & Website Ridership Survey Summary

Q4. Did you experience any sidewalk barriers when getting to your bus stop?

1	Route 1; Auburn Blvd. and Beresford Way	May 14, 2012 5:15 PM
2	#1 to Sunrise Orange Grove and Sycamore About 80% of the walk there are no sidewalks - where bus stop is there are sidewalks	Feb 8, 2012 12:53 PM
3	#1 Myrtle & College Oak - no sidewalks present	Dec 20, 2011 10:32 AM
4	no sidewalks	Dec 19, 2011 9:06 PM
5	Various stops/routes -- signs are not accessible, oftentimes missing or broken sidewalks, lack of benches, lack of covering.	Dec 19, 2011 8:51 PM
6	Overgrown bushes requires me to go into the street to get to my stop.	Dec 19, 2011 3:32 PM
7	Bus #xx, No sidewalks on Main St north of North Street	Nov 21, 2011 8:34 AM

Appendix 8 - Bus Stop Site Survey Samples & Website Ridership Survey Summary

Q5. Were there any bus stop related barriers you experienced while waiting for a bus, or when getting on or off the bus?

1	I was dropped at a bus stop at Florin & 21st Street and there was no way for me to get off the median to the sidewalk.	Jun 10, 2012 7:39 PM
2	This stop should have a sidewalk because it is currently unsafe and difficult to traverse, especially if you are handicapped. Please work with the county to have this section improved. Thank you.	May 14, 2012 5:15 PM
3	Many of the bus stops were set up then city planning planted trees which are in the way of safely disembarking from the bus.	Feb 8, 2012 3:54 PM
4	At Bus Stop Number 3989, in front of the North Natomas Offices of PRIDE Industries, I recommend that it be moved several feet to the East to resolve complaints of both wheelchair users and the RT Bus Drivers so that the loading and the unloading of all passengers, wheelchairs in particular can enter and exit the bus at the beginning and/or end of the pathway to the parking lot of PRIDE Industries.	Dec 25, 2011 7:44 PM
5	no bus benches to sit on while waiting....no over head to protect from the weather	Dec 19, 2011 9:06 PM
6	Again, various -- buses not pulling close enough to curb so I would end up missing the steps (getting on) or missing the curb (getting off), and end up scraping my shins as I sank into the street.	Dec 19, 2011 8:51 PM
7	Bus 23. Fair Oaks and Marshall. The sprinklers behind the bus bench get the bench wet.	Dec 19, 2011 2:54 PM
8	no boarding pad, and a pole was in the way Bus #xx, No sidewalks on Main St north of North Street	Nov 21, 2011 8:34 AM

Appendix 8 - Bus Stop Site Survey Samples & Website Ridership Survey Summary

Q6. Do you have any other comments or suggestions regarding the ability to access any of the bus stops, or the bus stop amenities such as shelters, signage, waiting or boarding area, etc.? Please explain and give specific locations.

1	I would recommend that the bus stop signs be a different shape than the street signs. That way a blind or low vision person could identify if they were standing at a bus stop or not.	Feb 8, 2012 3:54 PM
2	The bus stop does not meet ADA or Title 24 requirements for bus stop access.	Feb 8, 2012 12:53 PM
3	Due to the long headways, and the lack of shade in the incorporated area, I would suggest retrofitting bus stops with some kind of protection from the sun.	Jan 9, 2012 3:29 PM
4	On West National Drive at Lennane Avenue in North Natomas across the street from PRIDE Industries, I recommend that a shelter be built so that when individuals leaving PRIDE Industries heading back towards Sacramento's Light Rail System, would have access to shelter that would either eliminate their necessity to get rained on in the Winter or a sunburn in the Summer. Trees are a good start, but in this case, a bus shelter and/or multiple shelters would resolve much of the problem while waiting for a bus in extreme weather conditions of rain in the winter and the hot sun in the summer. Thank you for your cooperation to the details in this matter.	Dec 25, 2011 7:44 PM
5	no side walks and no benches to side on....nothing to protect from the weather!!! Anywhere in Rio Linda this is true.	Dec 19, 2011 9:06 PM
6	As noted above, the signage is not accessible for a blind rider -- I don't even know if the stop is the correct one. I cannot even get the stop # off the back of the sign to call the automated phone service to see when the next bus will arrive.	Dec 19, 2011 8:51 PM
7	There are seats in the downtown area, but the seats are rare from mid-town through other areas. It's very painful to stand for more than a few minutes, and covered seating would be appreciated.	Dec 19, 2011 2:54 PM
8	I don't ride the bus currently, but as a baby boomer senior I would like the options to be available. I miss the neighborhood ride program. It was nice to know it was there, if I had to leave the car in the shop for maintenance or if I needed to use it for temporary medical reasons. A special needs young man needed to get from Rio Linda to our neighborhood near Hemlock and Madison and access was tough.	Dec 19, 2011 2:28 PM
9	I'd like a bench and shelter, and my driver is always cranky	Nov 21, 2011 8:34 AM



**ADA Complaint / Grievance Form
Sacramento County Department of Transportation**

Complainant: _____

Person Preparing Complaint (if different from Complainant): _____

Relationship to Complainant (if different from Complainant): _____

Street Address & Apt. No.: _____

City: _____ State: _____ Zip: _____

Phone: (____) _____ E-mail: _____

Please provide a complete description of the specific complaint or grievance:

Please specify any location(s) related to the complaint or grievance (if applicable):

Please state what you think should be done to resolve the complaint or grievance:

Please attach additional pages as needed.

Please do not contact me personally.

Signature: _____ **Date:** _____

Return to:
**Sacramento County Department of Transportation
Program Access Coordinator
906 G Street, Suite 510
Sacramento, CA 95814**

Upon request, reasonable accommodation will be provided in completing this form, or copies of the form will be provided in alternative formats. Contact the Program Access Coordinator at the address listed above, via telephone (916) 874-6291 or via TTY (916) 875-7105

