

Cold Spring Canyon Bridge Suicide Barrier

On State Route 154 at Cold Spring Canyon Bridge

05-SB-154-PM 22.9/23.1

05-0P9100

SCH# 2008011060

Final Supplemental Environmental Impact Report

Volume 3 of 3

Appendix I Comments and Responses

April 2011





Appendix I Comments and Responses

This Final Supplemental Environmental Impact Report has been prepared in accordance with the Judgment of the Superior Court of California for the County of Santa Barbara. In its Judgment, the court ruled that the Draft Environmental Impact Report impermissibly deferred the development of measures mitigating impacts to cultural and visual/aesthetic resources to the Final Environmental Impact Report, thereby effectively precluding any public comment about or public participation in the development of such mitigation measures. The Supplemental Environmental Impact Report has been prepared and publicly circulated to comply with the court's Judgment and Writ issued thereon.

Therefore, as stated in the Draft Supplemental Environmental Impact Report:

Pursuant to California Environmental Quality Act Guidelines Section 15088.5(f)(2), Caltrans, as lead agency, requests reviewers to limit their comments to the content of the Draft Supplemental Environmental Impact Report.

In accordance with the above, Appendix I addresses the comments received on the Draft Supplemental Environmental Impact Report for the Cold Spring Canyon Bridge Suicide Barrier project on State Route 154.

The Draft Supplemental Environmental Impact Report was circulated for public review and comment from December 9, 2010, to January 24, 2011; more than 200 copies of the document or web links were mailed/mailed to interested individuals, associations, and agencies. A copy of the Draft Supplemental Environmental Impact Report and 2009 Final Environmental Impact Report was available on the Caltrans website.

An open forum-style public hearing was held to further solicit public comment on the draft supplemental environmental document. The public hearing was held in the City of Santa Barbara on January 5, 2011.

The Draft Supplemental Environmental Impact Report was also available for public review at the Santa Barbara Central Library, Solvang Branch Library, Goleta Branch Library, Montecito Branch Library, at the Caltrans District Office in San Luis Obispo, and on the Caltrans website.

This appendix presents all of the written comments received on the draft document during the public review period. Responses to those comments are also provided.

Comments received on the Draft Supplemental Environmental Impact Report were in favor of or against the project; expressed suggestions and concerns; or were of a regulatory nature, including:

- Compliance with CEQA
- Bridge barriers may or may not save lives
- Preference for another alternative
- Effectiveness of physical suicide barriers
- Visual and aesthetic impacts
- Impacts to Historical Resources
- Using funding for mental health/community outreach or at another location

This appendix is organized according to the parties commenting on the Draft Supplemental Environmental Impact Report:

- Section 1.0 State Agencies
- Section 2.0 Local Government and Commissions
- Section 3.0 Associations
- Section 4.0 Individuals
- Section 5.0 Comment Cards from Public Hearings
- Section 6.0 Transcripts from Public Hearings

For Sections 1.0 through 5.0, responses are provided after each letter or email, or in groups if the response is the same. Corresponding numbers assigned to the comments are in the right-hand margin.

For Section 6.0, responses are withheld until the end of the transcripts and then provided, and in groups if the response is the same. Responses are identified by the surname of the person making the comment and by using the corresponding number assigned to the comments in the right-hand margin.

Several approaches have been used to respond to comments. Some comments were statements of information or opinion; these comments have been acknowledged for the public record. Other comments asked for additional information or for clarification of information in the 2009 Final Environmental Impact Report or 2010

Draft Supplemental Environmental Impact Report. Where appropriate, responses to these comments are provided in this appendix.

List of Abbreviated Terms for Appendix I

ADAC	Aesthetics Design Advisory Committee
Caltrans	California Department of Transportation
DEIR	Draft Environmental Impact Report
DSEIR	Draft Supplemental Environmental Impact Report
EIR	Environmental Impact Report
FEIR	Final Environmental Impact Report
HRER	Historical Resources Evaluation Report
SEIR	Supplemental Environmental Impact Report
SHPO	State Historic Preservation Officer
UBIT	Under Bridge Inspection Truck
VIA	Visual Impact Assessment

Section 1.0 State Agencies



JERRY BROWN
GOVERNOR

STATE OF CALIFORNIA
GOVERNOR'S OFFICE of PLANNING AND RESEARCH
STATE CLEARINGHOUSE AND PLANNING UNIT



January 24, 2011

Matt Fowler
California Department of Transportation, District 5
50 Higuera Street
San Luis Obispo, CA 93401

Subject: Cold Spring Canyon Bridge Suicide Barrier
SCH#: 2008011060

Dear Matt Fowler:

The State Clearinghouse submitted the above named Supplemental EIR to selected state agencies for review. On the enclosed Document Details Report please note that the Clearinghouse has listed the state agencies that reviewed your document. The review period closed on January 21, 2011, and the comments from the responding agency (ies) is (are) enclosed. If this comment package is not in order, please notify the State Clearinghouse immediately. Please refer to the project's ten-digit State Clearinghouse number in future correspondence so that we may respond promptly.

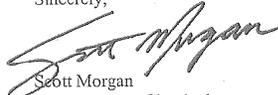
Please note that Section 21104(c) of the California Public Resources Code states that:

"A responsible or other public agency shall only make substantive comments regarding those activities involved in a project which are within an area of expertise of the agency or which are required to be carried out or approved by the agency. Those comments shall be supported by specific documentation."

These comments are forwarded for use in preparing your final environmental document. Should you need more information or clarification of the enclosed comments, we recommend that you contact the commenting agency directly.

This letter acknowledges that you have complied with the State Clearinghouse review requirements for draft environmental documents, pursuant to the California Environmental Quality Act. Please contact the State Clearinghouse at (916) 445-0613 if you have any questions regarding the environmental review process.

Sincerely,



Scott Morgan
Director, State Clearinghouse

Enclosures
cc: Resources Agency

1400 10th Street P.O. Box 3044 Sacramento, California 95812-3044
(916) 445-0613 FAX (916) 323-3018 www.opr.ca.gov

**Document Details Report
State Clearinghouse Data Base**

SCH# 2008011060
Project Title Cold Spring Canyon Bridge Suicide Barrier
Lead Agency Caltrans #5

Type SIR Supplemental EIR
Description The California Department of Transportation proposes to install a physical suicide barrier on the Cold Spring Canyon Bridge, near San Marcos Pass in Santa Barbara County.

Lead Agency Contact

Name Matt Fowler
Agency California Department of Transportation, District 5
Phone (805) 542-4603 **Fax**
email
Address 50 Higuera Street
City San Luis Obispo **State** CA **Zip** 93401

Project Location

County Santa Barbara
City Santa Barbara
Region
Lat / Long 34° 31' 35.90" N / 119° 50' 7.53" W
Cross Streets State Route 154 and Stagecoach Road
Parcel No.
Township 5N **Range** 28W **Section** 8 **Base** SBB&M

Proximity to:

Highways
Airports
Railways
Waterways Cold Spring Creek, Santa Ynez River
Schools
Land Use Scenic Corridor and State Scenic Highway, rural public facility

Project Issues Aesthetic/Visual; Archaeologic-Historic

Reviewing Agencies Resources Agency; Department of Conservation; Department of Fish and Game, Region 5; Cal Fire; Office of Historic Preservation; Department of Parks and Recreation; Department of Water Resources; California Highway Patrol; Air Resources Board, Transportation Projects; Regional Water Quality Control Board, Region 3; Native American Heritage Commission; State Lands Commission

Date Received 12/08/2010 **Start of Review** 12/08/2010 **End of Review** 01/21/2011

Note: Blanks in data fields result from insufficient information provided by lead agency.



DEPARTMENT OF FORESTRY AND FIRE PROTECTION

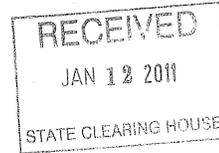
P.O. Box 944246
SACRAMENTO, CA 94244-2460
Website: www.fire.ca.gov
(916) 657-0300

clear : 1/21/2011
e



December 14, 2010

Contract County Chief
Attn: Robert Tanner, Environmental Coordinator
Santa Barbara County Fire
P.O. Box 843
Buellton, CA 93427



The following environmental document was submitted to CAL FIRE Headquarters, Environmental Protection for review under the California Environmental Quality Act (CEQA) or the National Environmental Policy Act (NEPA). The proposed project, located within your Unit/Program Area, may have an impact upon CAL FIRE's fire protection and/or natural resource protection and management responsibilities or require a CAL FIRE permit or approval. Your determination of the appropriate level of CAL FIRE involvement with this project is needed. Please review the attached document and address your comments, if any, to the lead agency prior to the due date. Your input at this time can be of great value in shaping the project. If your Department's Environmental Coordinator is not available, please pass on to another staff member in order to meet the mandated deadline.

Please submit comments directly to the lead agency before the mandated due date with a copy to the State Clearing House (P.O. Box 3044, Sacramento, CA 95812-3044).

Project name: Cold Spring Canyon Bridge Suicide Barrier
SCH #: 2008011060
Document Type: Supplemental/Subsequent EIR
Potential Area(s) of Concern: Fire Protection?

MANDATED DUE DATE: 1/21/2011

No Comment – Explain briefly on the lines below:

No impact on Fire Protection or Natural Resource Protection.

Name and Title of Reviewer: BOB TANNER FIRE CAPTAIN

Phone (805) 686-5068 Email: robert.tanner@sbafire.com

Note: Please complete this form and return it, with a copy of any comments, for CAL FIRE's records to Ken Nehoda or Allen Robertson, Environmental Protection, P.O. Box 944246, Sacramento, CA 94244-2460. If you have already reviewed and/or commented on this project, please send a copy to the address above.

CONSERVATION IS WISE-KEEP CALIFORNIA GREEN AND GOLDEN

PLEASE REMEMBER TO CONSERVE ENERGY. FOR TIPS AND INFORMATION, VISIT "FLEX YOUR POWER" AT WWW.CA.GOV.



Fire Department

"Serving the community since 1926"

HEADQUARTERS
4410 Cathedral Oaks Road
Santa Barbara, CA 93110-1042
(805) 681-5500 FAX: (805) 681-5563

Michael W. Dyer
Fire Chief
County Fire Warden

Christian J. Hahn
Deputy Fire Chief

clear: 1/21/2011
e

January 4, 2011

To: Matt Fowler
California Department of Transportation-Caltrans District 5
50 Higuera Street
San Luis Obispo, CA 93401



Re: Cold Spring Canyon Bridge Suicide Barrier; SCH#2008011060

The Santa Barbara County Fire Department has had the opportunity to review the draft supplemental environmental impact report for the proposed Cold Spring Canyon Bridge Suicide Barrier and offers the following:

Santa Barbara County Fire as an agent of CAL Fire feels that this project will have negligible impact on fire protection, but the installation of a suicide barrier is greatly needed to protect the public as well as the safety of emergency responders. Recently one of our personnel happened to be driving on HWY 154 and came across an individual sitting on the railing of the Cold Spring Canyon Bridge with legs hanging over the side of the Bridge. The individual was contemplating suicide; ultimately the individual decided not to go through with it. The Fire Department employee had to assist the individual off the railing with the aid of a passerby, at great risk of bodily harm or even death. Luckily no one was hurt during this incident; if a suicide barrier had been installed this would have never happened.

Thank you for the opportunity to review your proposed project.

Respectfully,

Bob Tanner
Fire Captain
Santa Barbara County Fire Department
(805)686-5068

cc: State Clearinghouse, PO Box 3044, Sacramento, CA 95812-3044
dd: Ken Nehoda, CAL Fire, PO Box 944246, Sacramento, CA 94244-2460

Serving the cities of Buellton and Goleta, and the Communities of Casmalia, Cuyama, Gaviota, Hope Ranch, Los Alamos, Los Olivos, Mission Canyon, Mission Hills, Orcutt, Santa Maria, Sisquoc, Solvang, Vandenberg Village

Response to letters from:

- Scott Morgan, Director, State Clearinghouse, Governor's Office of Planning and Research, letter and three-page enclosure

Enclosure 1: Document Details Report, State Clearinghouse Data Base

Enclosure 2: Ken Nehoda/Allen Robertson, Environmental Protection, Cal Fire Department of Forestry Protection

Enclosure 3: Bob Tanner, Fire Captain, Santa Barbara County Fire Department

Thank you for your letter, which acknowledges that Caltrans has complied with the State Clearinghouse review process.

- Ken Nehoda/Allen Robertson, Environmental Protection, Cal Fire Department of Forestry Protection

Your comments on the project have been noted.

Section 2.0 Santa Barbara County Government and Commission

Matt C
Fowler/D05/Caltrans/CAGov
01/24/2011 05:57 PM

To Mike Jacob/D05/Caltrans/CAGov@DOT
cc
bcc
Subject Fw: Cold Spring Canyon Bridge Suicide Barrier SEIR

----- Forwarded by Matt C Fowler/D05/Caltrans/CAGov on 01/24/2011 05:57 PM -----



"Wallar, Chandra"
<cwallar@co.santa-barbara.ca.us>
Sent by: "Van Wingerden, Cam"
<cvanwingerden@co.santa-barbara.ca.us>

01/24/2011 04:50 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>, "Todd, Rick" <Rick.Todd@sbcfire.com>, "Brown, Bill" <wfb4029@sbsheriff.org>, "McGolpin, Scott" <McGolpin@cosbpw.net>, "Stewart, Bret" <Bstewart@cosbpw.net>
cc "Russell, Glenn" <grussell@co.santa-barbara.ca.us>
Subject Cold Spring Canyon Bridge Suicide Barrier SEIR

Sent on behalf of Chandra L. Wallar

Mr. Fowler - Attached please find the subject correspondence and all the necessary attachments.

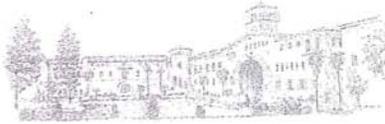
Thank you,
Cam

Cam Van Wingerden
Executive Secretary to Chandra L. Wallar
County Executive Office
County of Santa Barbara
105 East Anapamu Street
Santa Barbara, CA 93101
805-568-3404

 Cold Spring Canyon Bridge Suicide Barrier Supplemental Environmental Impact Report.pdf  PW Ltr. Cold Springs Bridge SEIR.pdf
 Sheriff Cmtk Ltr DEIR Cold Spring Bridge.pdf  Fire Cmtk Ltr Cold Springs.pdf

COUNTY OF SANTA BARBARA

Chandra L. Wallar
County Executive Officer



105 East Anapamu Street, Suite 406
Santa Barbara, California 93101
805/568-3400 • Fax 805/568-3414
cwallar@countyofsb.org
www.countyofsb.org

January 24, 2011

EXECUTIVE OFFICE

Mr. Matt Fowler
Senior Environmental Planner
Caltrans
50 Higuera St.
San Luis Obispo, CA 93401

Fax: 805-549-3620
Email: matt_c_fowler@dot.ca.gov

Re: Cold Spring Canyon Bridge Suicide Barrier Supplemental Environmental Impact Report

Dear Mr. Fowler:

Thank you for the opportunity to comment on the Supplemental Environmental Impact Report for the Cold Spring Canyon Bridge Suicide Barrier project. At this time, the County submits comments from the Fire Department, the Public Works Department and the Sheriff's Department for your consideration.

The County looks forward to continued dialogue on future projects. If you should have further questions, please do not hesitate to contact my office directly, or Jeff Hunt, Director of Long Range Planning Division, at (805) 568-2072.

Sincerely,

A handwritten signature in cursive script that reads "Chandra L. Wallar".

Chandra L. Wallar
County Executive Officer

cc: Glenn Russell, Director, Planning and Development Department
Richard Todd, Division Chief/Fire Marshal, Fire Department
Bill Brown, Sheriff
Scott McGolpin, Director, Public Works Department
Bret A. Stewart, Senior Development Engineering Manger, Public Works Department

Enclosures:
Fire Department letter, January 5, 2011
Public Works Department letter, January 10, 2011
Sheriff's Department letter, January 18, 2011

Terri Maus-Nisich
Assistant County Executive Officer
tmaus@countyofsb.org

Jason Stilwell
Assistant County Executive Officer
jstil@countyofsb.org

Sharon Friedrichsen
Assistant to the County Executive Officer
sfried@countyofsb.org



Office of the Sheriff
SANTA BARBARA COUNTY

HEADQUARTERS
P.O. Box 6427 • 4434 Calle Real • Santa Barbara, California 93160
Phone (805) 681-4100 • Fax: (805) 681-4322

BILL BROWN
Sheriff-Coroner

KENNETH R. SHEMWELL
Undersheriff

January 18, 2011

STATIONS

Buellton
140 W. Highway 246
Buellton, CA 93427
Phone (805) 686-8150

Carpinteria
5775 Carpinteria Avenue
Carpinteria, CA 93013
Phone (805) 684-4561

Isla Vista
6546 Pardall Road
Isla Vista, CA 93117
Phone (805) 681-4179

Lompoc
751 Burton Mesa Road
Lompoc, CA 93436
Phone (805) 737-7737

New Cuyama
70 Newsome Street
New Cuyama, CA 93254
Phone (661) 766-2310

Santa Maria
812-A W. Foster Road
Santa Maria, CA 93455
Phone (805) 934-6150

Solvang
1745 Mission Drive
Solvang, CA 93463
Phone (805) 686-5000

Sheriff - Coroner Office
66 South San Antonio Road
Santa Barbara, CA 93110
Phone (805) 681-4145

Main Jail
4436 Calle Real
Santa Barbara, CA 93110
Phone (805) 681-4260

**COURT SERVICES
CIVIL OFFICES**

Santa Barbara
1105 Santa Barbara Street
P.O. Box 690
Santa Barbara, CA 93102
Phone (805) 568-2900

Santa Maria
312 E. Cook Street "O"
Santa Maria, CA 93455
Phone (805) 346-7430

Lompoc
401 E. Cypress #105
Lompoc, CA 93436
Phone (805) 737-7710

Matt Fowler, Senior Environmental Planner
Central Coast Environmental Analysis
California Department of Transportation
50 Higuera Street
San Luis Obispo, California 93401

RE: Draft Environmental Impact Report (DEIR) for the Cold Spring Canyon Bridge
Suicide Barrier, Santa Barbara County, California
[05-SB-154-PM22.9/23.1, 05-OP9100]

Dear Mr. Fowler,

Since my last letter of support dated June 9, 2008, my position on the suicide barrier has not changed. The stated purpose and need – to prevent suicides at the Cold Spring Canyon Bridge and to reduce risks to emergency personnel – is still a paramount priority. The Grid/Mesh Alternative design as presented in the DEIR is the best possible alternative to meet this goal.

The remoteness of the bridge and the lack of equipment such as a “snooper truck,” which could be used to assist in the recovery, make the netting or lowered platform option an impractical alternative. Several individuals have parked their vehicles on the bridge and jumped to their deaths in front of witnesses without hesitation. The alternative designs, not visible from the roadway, offer no deterrence from this type of spontaneous action and could result in a subject either injured or trapped, requiring emergency personnel to respond to a difficult and technical type of rescue. This would place both the citizen and the emergency responders in a hazardous situation. Additionally, complex rescue operations could create the potential for major traffic problems along this narrow highway. For these reasons, I recommend that such alternative designs not be used.

Your agency has done extensive work on researching and preparing possible solutions to this problem. The recommended Grid/Mesh Alternative should be implemented as quickly as possible. It has been five years since the idea of a barrier was brought forward, and in that time, 14 people have jumped to their deaths and several others have been saved by emergency personnel and/or passing motorists. The continued delay will only add to those growing numbers and place emergency personnel at risk.

Mr. Matt Fowler, Senior Environmental Planner
California Department of Transportation
January 18, 2011
Page 2

In conclusion, Cal Trans has my full support for the Grid/Mesh Alternative as the appropriate solution to reducing the number of suicides at the bridge while minimizing risks to emergency responders. The pedestrian barrier will not only save lives, but will protect personnel responding to these critical situations.

Sincerely,



BILL BROWN
Sheriff – Coroner



Fire Department

"Serving the community since 1926"

HEADQUARTERS
4410 Cathedral Oaks Road
Santa Barbara, CA 93110-1042
(805) 681-5500 FAX: (805) 681-5563

Michael W. Dyer
Fire Chief
County Fire Warden

Christian J. Hahn
Deputy Fire Chief

January 4, 2011

To: Matt Fowler
California Department of Transportation-Caltrans District 5
50 Higuera Street
San Luis Obispo, CA 93401

Re: Cold Spring Canyon Bridge Suicide Barrier; SCH#2008011060

The Santa Barbara County Fire Department has had the opportunity to review the draft supplemental environmental impact report for the proposed Cold Spring Canyon Bridge Suicide Barrier and offers the following:

Santa Barbara County Fire as an agent of CAL Fire feels that this project will have negligible impact on fire protection, but the installation of a suicide barrier is greatly needed to protect the public as well as the safety of emergency responders. Recently one of our personnel happened to be driving on HWY 154 and came across an individual sitting on the railing of the Cold Spring Canyon Bridge with legs hanging over the side of the Bridge. The individual was contemplating suicide; ultimately the individual decided not to go through with it. The Fire Department employee had to assist the individual off the railing with the aid of a passerby, at great risk of bodily harm or even death. Luckily no one was hurt during this incident; if a suicide barrier had been installed this would have never happened.

Thank you for the opportunity to review your proposed project.

Respectfully,

Bob Tanner
Fire Captain
Santa Barbara County Fire Department
(805)686-5068

cc: State Clearinghouse, PO Box 3044, Sacramento, CA 95812-3044
dd: Ken Nehoda, CAL Fire, PO Box 944246, Sacramento, CA 94244-2460

Serving the cities of Buellton and Goleta, and the Communities of Casmalia, Cuyama, Gaviota, Hope Ranch, Los Alamos, Los Olivos, Mission Canyon, Mission Hills, Orcutt, Santa Maria, Sisquoc, Solvang, Vandenberg Village



Fire Department

"Serving the community since 1926"

HEADQUARTERS

4410 Cathedral Oaks Road
Santa Barbara, CA 93110-1042
(805) 681-5500 FAX: (805) 681-5563

Michael W. Dryer
Fire Chief
County Fire Warden

Christian J. Hahn
Deputy Fire Chief

January 5, 2011

Mr. Matt Fowler
Senior Environmental Planner
Caltrans
50 Higuera Street
San Luis Obispo, CA 93401

Dear Mr. Fowler:

SUBJECT: Caltrans Cold Spring Canyon Bridge Suicide Barrier SEIR

Fire Department staff has reviewed the above referenced project and has no comments on the project as presented at this time.

Please notify the Fire Prevention Division of any changes to the project proposal. Further intensification of use or change in the project description may require additional review.

As always, if you have any questions or require further information, please call 805-681-5523 or 805-681-5500.

In the interest of life and fire safety,

Richard Todd
Division Chief/Fire Marshal

RJ: mkb

COUNTY OF SANTA BARBARA
PUBLIC WORKS DEPARTMENT
123 East Anapamu Street
Santa Barbara, CA 93101
805\568-3000 FAX 805\568-3019



SCOTT D. MCGOLPIN
Director

January 10, 2011

Mr. Matt Fowler, Senior Environmental Planner
Central Coast Environmental Analysis
California Department of Transportation
50 Higuera Street
San Luis Obispo, CA 93401

Re: Draft Supplemental EIR, Cold Spring Canyon Bridge Barrier

Dear Mr. Fowler,

We have reviewed the draft supplemental environmental document regarding the proposed Cold Spring Bridge Suicide Barrier project and offer the following comments:

The project as proposed falls entirely outside of the jurisdiction of the Santa Barbara County Roads Division, as no detours are proposed that would send any traffic to county operated roadways. Secondary impacts to the County road system should also be negligible, since traffic delays during construction hours are limited to 5 minutes at a time. Our understanding is that the project site is planned to be fully open to normal traffic flow during the night.

Should any change occur in the planned construction operations which would make it necessary to detour traffic onto any County roadway, we would request that the Caltrans Resident Engineer contact Gary Smart of the Public Works Engineering Section at (805) 568-3308 so that we can make any necessary preparations or provisions.

Thank you for the opportunity to comment on this document.

Sincerely,

A handwritten signature in black ink, appearing to read "Bret A. Stewart".

Bret A. Stewart, P.E.
Senior Development Engineering Manager

Responses to comments from:

- Chandra L. Wallar, County Executive Officer, County of Santa Barbara

Enclosure 1: Richard Todd, Division Chief/Fire Marshall, Santa Barbara County Fire Department

Enclosure 2: Bret A. Stewart, P.E., Senior Development Engineering Manager, County of Santa Barbara Public Works Department

Enclosure 3: Bill Brown, Sheriff-Coroner, Santa Barbara County Office of the Sheriff

Your comments on the project have been noted.

- Bill Brown, Sheriff-Coroner, Santa Barbara County Office of the Sheriff
- Bob Tanner, Fire Captain, Santa Barbara County Fire Department

Thank you for your comments. Your support for the project has been noted.

- Richard Todd, Division Chief/Fire Marshall, Santa Barbara County Fire Department
- Bret A. Stewart, P.E., Senior Development Engineering Manager, County of Santa Barbara Public Works Department

Your comments on the project have been noted.

COUNTY OF SANTA BARBARA
HISTORIC LANDMARKS
ADVISORY COMMISSION



January 10, 2011

Mr. Matt C. Fowler
Senior Environmental Planner
Department of Transportation
50 Higuera Street
San Luis Obispo, CA 93401

Re: Comments on Draft Supplemental EIR for the
Cold Spring Canyon Bridge Suicide Barrier

Dear Mr. Fowler:

The Santa Barbara County Historic Landmarks Advisory Commission (HLAC) submits the following comments on the Draft Supplemental Environmental Impact Report for the Cold Spring Canyon Bridge Suicide Barrier proposed by the State of California Department of Transportation (Caltrans).

During 2007 and 2008 HLAC thoroughly studied the grid/mesh and the vertical picket suicide barriers proposed by Caltrans on the historic Cold Spring Canyon Bridge and voted unanimously on June 9, 2008 that "there was no acceptable mitigation that would offset the harm resulting from construction of a suicide barrier on the Bridge," and forwarded these findings in a letter to Caltrans dated June 18, 2008.

Your agency has recognized the historical significance of the Cold Spring Canyon Bridge, has acknowledged that it is eligible for listing on the National Register of Historic Places and, like HLAC, has also concluded that the proposed grid/mesh barriers would significantly affect the bridge's historic character, appearance and scenic views. It is therefore puzzling and disturbing to us that

1

Caltrans continues to promote a vertical barrier when there appears to be other, more environmentally superior design alternatives available.

Highway 154 is an officially designated California Scenic Highway, and the views from the deck of the Cold Spring Canyon Bridge are spectacular and ever changing with the seasons. These views are a critical element of what makes the bridge so important as a scenic resource. The proposed vertical barriers will block as much as 70 percent of the existing scenic views from the bridge deck, thereby drastically degrading this signature Santa Barbara County visual resource enjoyed by the people who make the nearly six million trips across it each year.

New information, not available to the public or government agencies when the vertical barrier proposals were previously circulated, demonstrates that a horizontal barrier is as effective as a vertical barrier in preventing suicide, and can be installed on the Cold Spring Bridge without compromising the historic views from the bridge deck. There are several different variations of horizontal barriers, including one that has proven to be effective in deterring would-be jumpers, and another developed by the State Historic Preservation Officer working with Caltrans engineers in Sacramento, which when combined with raising the height of the existing substandard railing will meet all of the stated goals for the project.

2

Moreover, the Golden Gate Bridge Authority has recently approved a horizontal wire net as an acceptable suicide prevention device for installation on the historic Golden Gate Bridge in San Francisco. We believe that if a horizontal barrier solution is appropriate for this iconic symbol of America, it certainly is appropriate for the Cold Spring Canyon Bridge.

The Santa Barbara County Historic Landmarks Advisory Commission continues to find that the proposed grid/mesh or vertical picket barriers would seriously damage the scenic quality and historic integrity of the Cold Spring Canyon Bridge. We also find that an appropriately designed horizontal barrier, either a Safety Net, a Cantilever Net, or some alternative design, combined with increasing the height of the existing substandard railing is a preferable alternative to the proposed grid/mesh project.

3

For all the above reasons, we strongly urge Caltrans to reject the proposed vertical barrier and instead adopt an alternative horizontal barrier design.

At a public hearing on January 10, 2011 the Santa Barbara County Historic Landmarks Advisory Commission discussed this matter in depth and voted unanimously (11-0) to approve the transmittal of this letter to you.

Sincerely,

COUNTY OF SANTA BARBARA
HISTORIC LANDMARKS ADVISORY COMMISSION



John C. Woodward, Chair

cc:

Santa Barbara Board of Supervisors
SBCAG Executive Director Jim Kemp, 260 N. San Antonio Road, Suite B, Santa Barbara CA 93110
City of San Luis Obispo
City of Solvang
City of Santa Ynez
City of Guadalupe
City of Santa Maria
City of Goleta
City of Santa Barbara
City of Carpinteria
City of Buellton
SHPO Milford Wayne Donaldson, 1725 23rd Street, Suite 100, Sacramento, CA 95816
Caltrans Director Rich Krumholtz, Caltrans District 5, 50 Higuera Street, San Luis Obispo, CA 93401

Response to Comments from John C Woodward, Chair, County of Santa Barbara Historic Landmarks Advisory Commission:

Your opposition to the project has been noted.

Response to comment #1: As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

The Updated VIA and 2010 DSEIR identify Significant and Unavoidable Class I visual impacts resulting from the project, the most adverse finding of impact allowed under CEQA [2010 DSEIR pages 21, 25; Updated VIA page 12]. The full extent and nature of the Significant *and* Unavoidable visual impact is fully discussed and disclosed in the Updated VIA and 2010 DSEIR.

Response to comment #2: A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. Caltrans fully identified and assessed impacts to views from the Cold Spring Canyon Bridge in its analysis of project effects to the structure as a historical resource. The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

A horizontal net barrier would have greater impacts on the National Register-eligible Cold Spring Canyon Bridge than the preferred alternative would. The bridge is eligible for its engineering qualities, and not for the views it happens to provide the

traveling public. The design and engineering of the substructure of the bridge are the most important character-defining features of the Cold Spring Canyon Bridge and the reason it has won engineering awards for its beauty. The design of the deck and railings are character-defining, but are less important than the substructure in conveying the engineering significance of the bridge.

The horizontal net barrier that has been chosen for the Golden Gate Bridge will still cause adverse impacts to that historic structure. Because of fundamental structural differences between the Golden Gate Bridge (a bulky, riveted suspension bridge hung from towers) and the Cold Spring Canyon Bridge (a slender, welded arch bridge supported by columns), a horizontal net barrier is feasible for the Golden Gate Bridge but not feasible for the Cold Spring Canyon Bridge without structural retrofit. In addition, the view from the Golden Gate Bridge is considered a character-defining feature -- whereas it has been accepted by both the State Historic Preservation Officer and the Advisory Council on Historic Preservation that there are no “historic views” from the Cold Spring Canyon Bridge deck.

A horizontal net barrier will result in fewer adverse impacts to the Golden Gate Bridge than the alternatives that have been proposed for that structure, but it is incorrect to assume that a horizontal net would have no impacts on the Cold Spring Canyon Bridge or fewer impacts than the preferred alternative would. On the contrary, the installation of a horizontal net barrier would have adverse impacts to the Cold Spring Canyon Bridge by obscuring portions of the substructure and by requiring a type of retrofit that would change the fundamental engineering of the structure – the engineering that makes the bridge eligible.

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance

6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

The current railing along the bridge is considered standard for the construction period of the structure.

Response to comment #3: The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge’s historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge’s columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit (physical modifications) and would further diminish the bridge’s integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge’s suicide barrier. The comment, however, does not account for two aspects of

the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its “Alternative 3” (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Cold Spring Canyon Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge’s main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge’s prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck’s roadway. As noted in the HRER and Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting the structure’s character-defining features is not necessary for the safety net’s installation. The safety net will diminish the Golden Gate Bridge’s character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge’s character-defining features at the substructure more than a vertical barrier would. As discussed above, views from the Cold Spring Canyon Bridge are not a character-defining element of the historical resource, and the impacts of the vertical barrier are therefore limited to diminishing the bridge’s integrity of design, feeling, and association, as compared to impacts of a safety net barrier.

The current railing along the bridge is considered standard for the construction period of the structure. In addition, the suggestion of raising the existing bridge railings in

combination with constructing a horizontal net barrier would likely cause an additional adverse effect by introducing a visual element that diminishes the property's historic integrity of design, feeling, and association. Increasing the height of the bridge rail would also not comply with the Secretary of Interior's Standards for Rehabilitation, Standards 2 and 9. Thus, inclusion of this aspect of the suggested alternative would further decrease the ability of the horizontal net barrier alternative to reduce impacts to historic resources.

Section 3.0 Associations



Bill Batty <Bill@fsacares.org>

01/11/2011 02:05 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Cold Spring Arch Bridge

Matt

Family Service Agency endorses the vertical grid barrier supported by Glendon Association. You may add our organization to the list of supporters of the project.

Sincerely,

Bill Batty MSW
Executive Director



Trevor Summerfield
<tsummerfield@afsp.org>
01/04/2011 09:02 AM

To <matt_c_fowler@dot.ca.gov>
cc John Madigan <jmadigan@afsp.org>
Subject Cold Springs Canyon Bridge Barriers - Comments from the American Foundation for Suicide Prevention (AFSP)

Mr. Fowler,

Please accept these comments on behalf of John Madigan, AFSP's Senior Director of Public Policy, for your upcoming public comment meeting on suicide barriers on the Cold Springs Canyon Bridge. A hard copy of the comments will be mailed out today.

Regards,

Trevor Summerfield
Manager of Public Policy
American Foundation for Suicide Prevention
1010 Vermont Avenue, NW, Suite 408
Washington, DC 20005
(202) 449-3600 ext. 102
www.afsp.org
www.spanusa.org



AFSP Comments on Cold Springs Canyon Bridge Barriers.pdf



January 4, 2011

Matt C. Fowler, Senior Environmental Planner
Caltrans
50 Higuera Street
San Luis Obispo, CA 93401

Dear Mr. Fowler,

When people die by suicide at the Cold Springs Canyon Bridge, their deaths take an enormous toll. The impact is felt beyond families, friends and loved ones. The community, including rescue workers, hospital staff, coroners and police officers, is affected too. Since 1964, more than 53 people have lost their lives after a 420-foot fall from the bridge. It is time to build suicide prevention barriers on the Cold Springs Canyon Bridge.

Barriers work by giving individuals and those who care for them something they desperately need – time: time to change their minds, time for someone to intervene and time to seek help. Despite many myths and misconceptions, suicide is often an ambivalent act, and many who attempt suicide don't want to die. Rather, they are seeking an end to psychological pain and mental distress. Most have a definable and diagnosable psychiatric illness, substance use disorder, or both, which are often unrecognized and untreated.

Another common misconception is that those averted from a suicide attempt at a bridge will simply find another method to die – called "means substitution." However, scientific research continues to raise doubt about this premise. One study, which followed up on 515 individuals who were thwarted from attempting suicide at the Golden Gate Bridge, found that only six percent of these individuals later died by suicide. Further studies have shown that once barriers have been built at one location, deaths do not increase from other nearby bridges and overall suicide deaths often decline throughout the area after barriers are in place. Furthermore, bridge barriers have been shown to be very effective at reducing suicides among young men, a group for whom prevention efforts are very challenging.

A side benefit of the reduction in suicides once a barrier is in place is the reduced media coverage. This helps remove the allure of locations like the Cold Springs Canyon Bridge as "suicide magnets" and also contributes to reducing copycat suicides. (It is clear from research that media coverage of suicides from a specific location can contribute to copycat suicides).

The American Foundation for Suicide Prevention (AFSP) is the leading national not-for-profit organization exclusively dedicated to understanding and preventing suicide through research, education and advocacy, and to reaching out to people with mental disorders and those impacted by suicide. Mr. Fowler, please know that AFSP fully supports erecting bridge barriers at the Cold Springs Canyon Bridge. Not only will this save time and money for state and local municipalities in the long run, but more importantly, it will save lives.

Sincerely,

A handwritten signature in blue ink, appearing to read "John Madigan".

John Madigan
Senior Director of Public Policy

SPAN USA, a division of AFSP • 1010 Vermont Avenue, NW, Suite 408 • Washington, DC 20005
(202) 449-3600 phone • (202) 449-3601 fax



Mike Bossenberry
<MBossenberry@t-mha.org>
01/20/2011 02:16 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc

Subject Cold Springs Canyon Bridge: Barriers

Mr. Fowler,

I support the effort to build barriers on the Cold Springs Canyon Bridge, in order to prevent suicides.

As one who has been active in suicide prevention efforts for 25 years, I know the research shows that with extra time for consideration, a suicidal individual may become conscious of their own ambivalence about taking action to end his or her life. As a result of this heightened awareness of the ambivalence, she or he may start to think about reasons to continue living, and/or seek therapy or other helping resources. Additionally, any delay of a suicide attempt allows more opportunities for intervention by witnesses and/or mental health and public safety professionals.

It is well documented that a high percentage of people who have been suicidal, but didn't act on their intentions, either because of the time factor, ambivalence or an intervention, have gone on to lead healthier lives. Many others, who attempted suicide but survived, recount how they regretted their actions almost immediately after they took them. With the assistance of community resources and professional help, they can find meaning and purpose to life.

The costs of suicide attempts/deaths to society in lost productivity and health care are enormous. The emotional cost to loved ones, neighbors and colleagues is immeasurable. Any objections to erecting the barriers must be weighed against the immense benefits which the preservation of lives brings to our society.

The erection of barriers will result in delayed suicidal acts, and for many, that extra time for deliberation will give them an opportunity to think of reasons to live, resources and options for resolving their despair. For others, it will allow witnesses, and/or mental health and public safety professionals to intervene to preserve life. Most of the individuals who are saved will go on to productive and meaningful lives.

Thank you for your consideration,

Mike Bossenberry
Program Coordinator

SLO Hotline
Transitions-Mental Health Association
805-540-6541
FAX 805-540-6501

Helping children and adults live, work and grow in our community.

www.t-mha.org
www.SLOtheStigma.org



Tom Sodergren
<TSodergren@casapacifica.org>

01/24/2011 04:10 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>

cc

Subject: Suicide Barriers

I work with children and families in Santa Barbara County at a non profit that works specifically with depression, suicide ideation and suicide prevention. The Barriers for the Cold Springs Bridge are necessary and long overdue. If the barriers could save but one life it would be worth it. It is a no brainer. Not worth the discussion and time and money it has taken to get this thing done. PLEASE PUT THE SUICIDE BARRIERS ON THE COLD SPRINGS BRIDGE TO SAVE SOMEONES LIFE!!!!!!

Thomas M. Sodergren

Director of Community Services

Casa Pacifica, Centers for Children & Families

"Providing Hope and Help"

Santa Barbara County Division

(805) 928-8622 Ext. 5000

tsodergren@casapacifica.org

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Carol Hershey
<cjhershey@gmail.com>
01/23/2011 09:04 PM

To <matt_c_fowler@dot.ca.gov>
cc

Subject Suicide Barrier on the Cold Springs Bridge

Dear Mr. Fowler,

This message of support for the proposed suicide barrier on the Cold Springs Bridge comes from the Santa Barbara Coalition against Gun Violence. Our coalition includes over 50 local organizations who support our mission: to educate the public about the danger of guns and to advocate for sensible gun legislation. Clearly our immediate focus is on guns, but our broader concern is public safety. As such, we believe that the beautiful Cold Springs Bridge is an attractive nuisance that makes it quick and easy to end a life, much like a gun. This is not to say that suicides would be eliminated by a barrier on the bridge or removal of easy access to a gun, but it would slow down the process and give the suicidal person time to seek help or reevaluate.

We also believe that putting aesthetics before a person's life is shameful and an embarrassment to the community of Santa Barbara; our values are better than that.

Kind regards,

The Steering Committee of
Coalition against Gun Violence



Dennis Wadley
<DennisW@bridgesworldwide.org>

01/01/2011 04:14 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Cold Springs Bridge Barrier - YES

Please, for the sake of the residents in SB County, continue with the construction of the barrier on the CS Bridge. Studies have proven that if a barrier is put up, suicides go down. Please consider the many families who have lost their loved ones and erect the barrier.

Dennis Wadley

Bridges of Hope International

DennisW@bridgesworldwide.org

US Office 805-967-4068

South African office 27-21-867-0099

310-773-3038 (US VoiP line rings in SA 9 hours ahead of Pacific Time)

Bridges of Hope

PO Box 6947

Santa Barbara, CA 93160-6947

"Charity provides crumbs from the table; justice offers a place at the table."

Bill Moyers



"Valerino, Michelle"
<michelle_valerino@uhc.com
>

01/03/2011 06:26 AM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Building barriers on the Cold Springs Bridge

To Whom it may concern,

Building a barrier on the Cold Springs Bridge should be done without question. If it can prevent ONE person from taken their own LIFE it is worth it, If it can make it difficult for ONE person to take their own LIFE and giving them another day to think about it ,another tomorrow to change their mind it is worth it. I really hope that in the future this will NEVER be a question whether to build a Barrier on a Bridge that has assisted so many suicide's.

Best,

Michelle Valerino
Member Outreach Coordinator
[United Healthcare Community and State](#)
Right Fax 1-866-730-3644
Office 1-800-339-5380 X 5716

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Nelson Trichler
<nelson@sbfinancial.com>

01/06/2011 11:51 AM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Cold Spring Canyon Bridge Suicide Barrier

Matt,

In response to the Draft Supplemental Environmental Impact Report, I would like to reinforce the commitment of the Santa Barbara County Search and Rescue team in support of Caltrans's efforts to build a barrier to reduce the potential of individuals using Cold Springs Bridge to commit suicide. We applaud you and your organization for taking preventative measures in not only trying to remove this man-made structure as a vehicle for suicide but also for reducing the risk to emergency personnel in recovering bodies from underneath the bridge.

We continue to support any efforts that would minimize the risks to emergency personnel responding to any such suicide attempts. To this end, the alternative barrier proposed by the "Friends of the Cold Springs Bridge" is not acceptable as it would still require our team members to be put at risk in rescuing suicidal subjects that are injured and/or mentally impaired under the bridge. Furthermore, it would increase the threat to emergency personnel responding to the top of the bridge and cause even great traffic delays as that would be the location of activity.

Regards,

Nelson Trichler
Santa Barbara County Search & Rescue

928 Garden Street, Suite 4
Santa Barbara, CA 93101
805.963.5822 (work)
805.698.0752 (cell)
805.456.2090 (fax)

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"Strayer, Hillary D"
<hdstrayer@anthc.org>
01/04/2011 04:09 PM

To <matt_c_fowler@dot.ca.gov>
cc "Franks, Barbara Jean" <bjfranks@anthc.org>
Subject Cold Springs Canyon Bridge Barriers



Mr. Fowler,

Thank you for the information you supplied about the risks associated with the Cold Springs Canyon Bridge. I work up in Alaska in Injury Prevention, and one of our approaches to suicide prevention is restricting lethal means. This includes supporting the installation of locking medicine cabinets and, because of the predominance of subsistence living up here, installation of gun safes in homes. We have anecdotal evidence of the success of these gun safes preventing a suicide which, as you describe, came from a momentary impulse and after that moment passed, so did the risk of action. Suicide attempts using firearms and bridges both cause irreversible damage, most often fatalities, and the highest level of protection and prevention should be used for these lethal means. There are several other bridges I am familiar with in San Francisco, Central Oregon, and other locations that have similar risks, many of which have had barriers put in place after deaths have occurred.

Because of these risks, I feel strongly that bridges like the Cold Springs Canyon Bridge that are built in places where a fall or jump would cause serious injury or fatality, should be retrofitted with barriers (or initially designed) to prevent intentional or unintentional falls from happening. Retrofitting prioritization should be given to bridges that, like the Cold Springs Canyon Bridge, have a known history of falls fatalities and might be a focus of people with suicidal intent.

I will not have the time to read the entire *Cold Spring Canyon Bridge Suicide Barrier Draft Supplemental Environmental Impact Report* prior to sending this message, but I would like to direct your attention to a line in Section 2.1 that reads:

"The National Environmental Policy Act of 1969, as amended, establishes that the federal government use all practicable means to ensure all Americans safe, healthful, productive, and aesthetically (emphasis added) and culturally pleasing surroundings..."

I understand the emphasis on aesthetics of historical structures - it is a beautiful bridge - but would like to point out that first (and foremost) in the list of surroundings characteristics is safety. I would be surprised if the majority of Americans did not agree that a safe bridge, with some aesthetic qualities, is preferred to one designed with the focus on aesthetics over safety. I invite those who prioritize

aesthetics to justify their preference to the families of those who lost their lives on the bridge. I also think there must be a way to preserve much of the aesthetic qualities while ensuring safety, for this and many other historical bridges - a design contest might produce several new ideas of adequate safety measures with satisfactory aesthetics, with all the new materials that are available today.

Thanks again for providing the opportunity to comment on this issue.
Hillary

*Hillary Strayer, M.P.H., Senior Injury Prevention Specialist
Wellness and Prevention Department
Division of Community Health Services*

*Alaska Native Tribal Health Consortium
4000 Ambassador Drive, D-CHS
Anchorage, AK 99508
907-729-3513 office
907-306-8863 cell
hdstrayer@anthc.org*

<http://www.anthc.org/chs/wp/> - Visit our website!



Area Agency on Aging
<seniors@kcbx.net>

01/17/2011 01:56 PM

To <matt_c_fowler@dot.ca.gov>
cc "Charles Carlson " <charlescarlson1@aol.com>, "Cole
Lucas " <coleclucas@gmail.com>
Subject Cold Spring Canyon Bridge

Dear Mr. Fowler,

Good afternoon. You will find attached a letter from the Area Agency on Aging Advisory Council presenting support for the suicide barriers at Cold Spring Canyon Bridge. Thank you for the opportunity to present this support.

joyce ellen

joyce ellen lippman
Central Coast Commission for Senior Citizens
528 S. Broadway, Santa Maria, CA 93454
805.925.9554

With the uncertainty of email delivery, please confirm receipt. Thanks.



L 2 Caltrans r Cold Spring bridge 1 17 11.pdf



For San Luis Obispo and Santa Barbara Counties

Central Coast Commission for Senior Citizens

528 South Broadway
Santa Maria, CA 93454-5109
(805) 925-9554 - Telephone
(805) 925-9555 - Facsimile
e-mail - seniors@kcbx.net
Home page: www.centralcoastseniors.org

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Martin Tucker
Tom Urbanske
Supervisor Joe Centeno

January 17, 2011

Caltrans
50 Higuera Street
San Luis Obispo, CA 93401

Attention: Matt Fowler, Senior Environmental Planner

Dear Mr. Fowler,

Hi! The intent of message is to advise you that the Area Agency on Aging Advisory Council has voted to support the proposed safety improvements to the Cold Spring Canyon Bridge. Specifically, we support the recommendation from the local task force to install a physical barrier and the other recommendations. These improvements are designed to address the suicides that occur at this location.

Suicide among the elderly is a major public health issue. Senior citizens generally use other means to commit suicide, but that does not reduce our support of the need to create physical barriers on the Cold Spring Canyon Bridge. We strongly support all efforts to make this a safer community and to reduce suicide in our community.

Thank you for this opportunity to support these efforts.

Sincerely,

joyce ellen lippman
Executive Director

c: Charles Carlson, Ph. D., Chair

Response to comments from:

- Bill Batty, MSW, Executive Director, Family Service Agency
- John Madigan, Senior Director of Public Policy, American Foundation for Suicide Prevention
- Mike Bossenberry, Program Coordinator, Transitions-Mental Health Association
- Tom Sodergren, Director of Community Services, Casa Pacifica, Centers for Children & Families
- The Steering Committee of Coalition Against Gun Violence
- Dennis Wadley, Bridges of Hope International
- Michelle Valerino, Member Outreach Coordinator, United Healthcare Community and State
- Nelson Trichler, Santa Barbara County Search & Rescue
- Hillary Strayer, M.P.H., Senior Injury Prevention Specialist, Wellness and Prevention Dept., Division of Community Health Services, Alaska Native Tribal Health Consortium
- Joyce Ellen Lippman, Executive Director, Area Agency on Agency Advisory Council, Central Coast Commission for Senior Citizens

Thank you for your comments. Your support for the project has been noted.

January 15, 2011

Matt Fowler
Senior Environmental Planner
Central Coast Environmental Analysis Caltrans
50 Higuera Street
San Luis Obispo, CA 93401

The Pearl Chase Society is a non-profit organization committed to preserving sites and structures that are an integral part of Santa Barbara County's history. The Society wishes to express its support for the suggested alternative to the safety barrier currently proposed for Cold Spring Canyon Bridge. This alternative, a horizontal net, would preserve the bridge's historic, engineering and aesthetic values and achieve the goal of preventing suicides at that location.

1

In 2007, the Pearl Chase Society urged Caltrans that unless there was clear evidence of an overriding public benefit that cannot be provided in any other way, the visual beauty of this architectural and engineering landmark and the vistas from its motorway must be preserved. The State Office of Historic Preservation has recommended a horizontal net type barrier that is a viable alternative. It is similar to the suicide prevention solution proposed for San Francisco's Golden Gate Bridge. Much of the visual beauty and vistas that would be lost by a nine-foot seven-inch high vertical cage is retained with this alternative.

2

The Pearl Chase Society Board of Directors respectfully asks Caltrans and the members of the Santa Barbara County Association of Governments to proceed with the suicide prevention plan endorsed by the California State Office of Historic Preservation.

Sincerely,

Sue Adams, President
The Pearl Chase Society

Response to comments from Sue Adams, President, The Pearl Chase Society

Your opposition to the project has been noted.

Response to comment #1: As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Response to comment #2: A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The bridge is eligible for its engineering qualities, and not for the views it happens to provide the traveling public. The design and engineering of the substructure of the bridge are the most important character-defining features of the Cold Spring Canyon Bridge. The design of the deck and railings are character-defining, but are less important than the substructure in conveying the engineering significance of the bridge. The view from the bridge is not a character-defining feature, and it has been accepted by both the State Historic Preservation Officer and the Advisory Council on Historic Preservation that there are no “historic views” from the bridge deck. It is therefore incorrect to assume that a horizontal net would preserve “the bridge’s historic, engineering and aesthetic values.” On the contrary, the installation of a horizontal net barrier would have adverse impacts to the bridge by obscuring portions of the substructure and by requiring a type of retrofit (physical

modifications) that would change the fundamental engineering of the structure – the engineering that makes the bridge eligible.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the bridge substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects the historic integrity of the National Register-eligible Cold Spring Canyon Bridge. The Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

The horizontal net barrier that has been chosen for the Golden Gate Bridge will still cause adverse impacts to that historic structure. Because of fundamental structural differences between the Golden Gate Bridge (a bulky, riveted suspension bridge hung from towers) and the Cold Spring Canyon Bridge (a slender, welded arch bridge supported by columns), a horizontal net barrier is feasible for the Golden Gate Bridge but not feasible for the Cold Spring Canyon Bridge without structural retrofit. In addition, the view from the Golden Gate Bridge is considered a character-defining feature -- whereas it has been accepted by both the State Historic Preservation Officer and the Advisory Council on Historic Preservation that there are no “historic views” from the Cold Spring Canyon Bridge deck.

A horizontal net barrier will result in fewer adverse impacts to the Golden Gate Bridge than the alternatives that have been proposed for that structure, but it is incorrect to assume that a horizontal net would preserve the Cold Spring Canyon Bridge’s “historic and aesthetic qualities.” On the contrary, the installation of a horizontal net barrier would have adverse impacts to the bridge by obscuring portions of the substructure and by requiring a type of retrofit that would change the fundamental engineering of the structure – the engineering that makes the bridge eligible.



Jarrell Jackman
 <docjj@sbthp.org>
 01/24/2011 01:38 PM

To <matt_c_fowler@dot.ca.gov>
 cc

Subject Cold Spring Bridge

Mr. Fowler: the SBTHP board of directors at its December meeting voted to oppose the proposed steel-caged barriers on Cold Spring Bridge. The Trust is an organization whose projects extend around the county. in fact two of the State Parks we manage (El Presidio SHP and S Ines Mills State Property) are connected by Highway 154 and the Cold Spring Bridge. Trust staff and volunteers frequently cross over the bridge, which is by far the most beautiful and historic in the County.

The Trust Board in essence is asking CalTrans to reconsider installing the ugly barriers, which will have an impact on one of the most beautiful viewsheds in the county on a state scenic highway, not to mention the impact on the bridge itself. . Talking photos of the original bridge and the other measures proposed as mitigation cannot overcome the impact of the current proposed barriers. The board strongly recommends that if barriers are installed they should be of the type proposed by State Historic Preservation Officer Mailford Wayne Donaldson, FAIA. If the engineering doesn't work as proposed then a similar alternative should be sought. The bridge matters to Santa Barbarans--this should be obvious to CalTrans by the public outcry against the caged barriers. The proposed ugly barriers may seem to be the best solution to the Sheriff and his department, and a suicide prevention group, but we live in a world of compromise. This is not the place to challenge whether the barriers will even save lives, but it is important for CalTrans and others proposing these barriers to realize that we are at a critical juncture with this project. Many around the state have been alerted to the assault on our historic bridges. There must be more sensitivity to the heritage of this state, and this bridge is part of that heritage.

1

2

Please do not construct these awful barriers. There is a better way. Wayne Donaldson has shown us a new direction. Please follow it.

Sincerely,

Jarrell C. Jackman, Ph.D.
 Executive Director
 Santa Barbara Trust for Historic Preservation
 123 East Canon Perdido Street

Santa Barbara, CA 93101
 (805) 965-0093
 FAX (805) 568-1999
www.sbthp.org <<http://sbthp.org/>>



**Santa Barbara Trust
 for Historic Preservation**

El Presidio de Santa Bárbara SHP | Casa de la Guerra
 Santa Inés Mission Mills | Jimmy's Oriental Gardens

Preserving what we love takes an ongoing commitment. *Membership Matters!* <
<http://sbthp.org/membership.htm> >

Response to comments from Jarrell C Jackman, PhD, Executive Director, Santa Barbara Trust for Historic Preservation

Your opposition to the project has been noted.

Response to comment #1: A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The bridge is eligible for its engineering qualities, and not for the views it happens to provide the traveling public. The design and engineering of the substructure of the bridge are the most important character-defining features of the Cold Spring Canyon Bridge, and the reason it has won engineering awards for its beauty. The design of the deck and railings are character-defining, but are less important than the substructure in conveying the engineering significance of the bridge. The view from the bridge is not a character-defining feature, and it has been accepted by both the State Historic Preservation Officer and the Advisory Council on Historic Preservation that there are no “historic views” from the bridge deck.

Response to comment #2: In designing a suicide barrier, Caltrans’ efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge’s substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

Senior Environmental Planner
State of California Department of Transportation
50 Higuera Street
San Luis Obispo, CA 93401

Re: Cold Spring Canyon Bridge Suicide Barrier- comments on
Draft supplement ER

Dear Mr. Fowler,

W.E. Watch is a Santa Ynez Valley community organization established in 1992. Our current membership roster is in excess of 120. After careful consideration, our organization submits the following comments on the Draft Supplement Environmental Impact Report for the Cold Spring Canyon Bridge Suicide Barrier proposed by the State of California Department of Transportation (Caltrans).

W.E. Watch is opposed to the vertical barrier. It is our belief that the proposed barrier would forever unnecessarily and negatively impact the gateway to the Santa Ynez Valley. Highway 154 is designated as a California Scenic Highway. Under CEQA the Cold Spring Canyon Bridge is considered a “Scenic Resource...due to its sculptural quality in the landscape, and the memorable visual image it creates”. Additionally, your agency has acknowledged the bridge’s eligibility for listing on the National Register of Historic Places. The currently proposed vertical grid/mesh barrier is inconsistent with the historic character of the bridge. Such a barrier would drastically alter the protected and cherished views from the bridge.

1

We strongly urge Caltrans to consider other environmentally superior design alternatives available – namely a horizontal barrier. There are several variations – one of which was developed working with Caltrans engineers in Sacramento. Recently, such a barrier was chosen for the Golden Gate Bridge. Installation of a horizontal barrier

2

along with raising the height of the substandard railing would meet the stated goals for the project while protecting the irreplaceable historic and scenic value of the Cold Spring Canyon Bridge.

Sincerely,

Cathie McHenry
President
Cc:
Santa Barbara County Board of Supervisors
SBCAG Executive Director, Jim Kemp
Caltrans Director Fifth District, Rich Krumholtz
Caltrans Fifth District Project Manager, Sara von Schwind

Response to comments from Cathie McHenry, President, WE Watch, Women's Environmental Watch

Your opposition to the project has been noted.

Response to comment #1: A horizontal net barrier would have greater impacts on the National Register-eligible Cold Spring Canyon Bridge than the preferred alternative would. The bridge is eligible for its engineering qualities, and the design and engineering of the substructure of the bridge are its most important character-defining features. The design of the deck and railings are character-defining, but are less important than the substructure in conveying the engineering significance of the bridge.

The horizontal net barrier that has been chosen for the Golden Gate Bridge will still cause adverse impacts to that historic structure. Because of fundamental structural differences between the Golden Gate Bridge (a bulky, riveted suspension bridge hung from towers) and the Cold Spring Canyon Bridge (a slender, welded arch bridge supported by columns), a horizontal net barrier is feasible for the Golden Gate Bridge but not feasible for the Cold Spring Canyon Bridge without structural retrofit (physical modifications). In addition, the view from the Golden Gate Bridge is considered a character-defining feature -- whereas it has been accepted by both the State Historic Preservation Officer and the Advisory Council on Historic Preservation that there are no "historic views" from the Cold Spring Canyon Bridge deck.

A horizontal net barrier will result in fewer adverse impacts to the Golden Gate Bridge than the alternatives that have been proposed for that structure, but it is incorrect to assume that a horizontal net would have no significant impacts, or fewer significant impacts, on the Cold Spring Canyon Bridge than the preferred alternative would. On the contrary, the installation of a horizontal net barrier would have adverse impacts to the bridge by obscuring portions of the substructure and by requiring a type of retrofit that would change the fundamental engineering of the structure – the engineering that makes the bridge eligible.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the bridge substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities

seriously in proposing a barrier design that respects the historic integrity of the National Register-eligible Cold Spring Canyon Bridge. The Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

Response to comment #2: A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. Caltrans fully identified and assessed impacts to views from the Cold Spring Canyon Bridge in its analysis of project effects to the structure as a historical resource. The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure’s character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans’ efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge’s substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

The suggestion of raising the existing bridge railings in combination with constructing a horizontal net barrier would likely cause an additional adverse effect by introducing a visual element that diminishes the property’s historic integrity of

design, feeling, and association. Increasing the height of the bridge rail would also not comply with the Secretary of Interior's Standards for Rehabilitation, Standards 2 and 9. Thus, inclusion of this aspect of the suggested alternative would further decrease the ability of the horizontal net barrier alternative to reduce impacts to historic resources.

Statements that the installation of a horizontal barrier along with raising the height of the existing railing would protect the scenic value of the Cold Spring Canyon Bridge are speculative and unsubstantiated. Empirical evidence indicates that raising the height of the existing railing as suggested may also adversely affect views from the bridge. The current railing along the bridge is considered standard for the construction period of the structure.

Additional discussion on each of these points is provided on pages 6 and 7 of the 2010 DSEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to "Reduce the exposure to risks for emergency personnel..." In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff's office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.



KEYS TO NEW HOPE

Caltrans
Attn: Matt Fowler
Senior Environmental Planner
50 Higuera Street
San Luis Obispo, CA 93401

Dear Mr. Fowler:

BOARD OF DIRECTORS
Merilee Conley
Hi Edell
Ron Huber
Valerie Stevens
Debbie Ulrick

I write not as a "Friend of the Bridge," but as a "Friend of the Despairing," seeking alternatives to death by suicide and completely by-passing discussions "for" or "against" barriers on the Cold Springs Bridge.

I wonder if there is any definitive evidence that persons thwarted by suicide barriers on a bridge do not find alternate ways to commit suicide?

1

ADVISORY BOARD
Glen Adams
William Coulson
Arthur Clarke
Douglas Crawford
George Dumas
Carl Gans
Robert Heavner
Barbara LesStrang
Dr. Robert Ohman
Kay Robinson-Schofield
Reed Spangler
Rev. Alan Stones

According to U.S. Suicide Statistics (2001), the percentage of suicide methods are:

- Firearms = 60% of all suicides
- Suffocation = 22%
- Poisoning = 18%
- 14 "Other Methods" are listed (including jumping off a bridge or high building) all of which = only 1%

Lacking this evidence, might not the "\$5 million federal grant to complete the design" and "the estimated \$45 million that would be needed to build the net system"¹ be better spent on the following?

2

- A spray-painted message on the top surface of the existing vertical concrete barriers: "There is HOPE; there is HELP. Call Suicide Hotline—1-800-784-2433"
- A direct-suicide-line telephone at each end of the bridge
- An *obvious* listing of the Suicide Hotline in the front of EVERY TELEPHONE DIRECTORY
- Easier emergency access to mental health services.

3

With this in mind, I respectfully request you reconsider this project.

Sincerely,

Valerie Stevens

Cc: Friends of the Cold Spring Bridge
Santa Ynez Valley News
Santa Ynez Valley Journal

¹ Raiza Canelon, "Forum evaluates bridge suicide barrier options," *Santa Ynez Valley News*, 11 January, 2011, p. A4, cols. 2-3.

Response to comments from Valerie Stevens, Keys To New Hope

Your comments on the project have been noted.

Response to comment #1: As referenced in the 2009 Final Environmental Impact Report, on pages 18-22, studies by suicidologists and mental health professionals have shown that physical barriers help reduce suicides on bridges. Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

Response to comment #2: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #3: Signs and suicide hot lines have been analyzed and were eliminated for reasons discussed in the 2009 FEIR (see Alternatives Considered but Eliminated From Further Discussion, pages 13-15). However, a separate project sponsored by the Santa Barbara County Association of Governments, would install crisis phones at the two nearest call boxes to the bridge, the crisis phones cannot be located on the bridge deck for safety reasons. Signs would say, “In Crisis? We Care Please Call Us” (see the 2009 FEIR at page 128).



Deborah Brasket
<dbrasket@verizon.net>
01/23/2011 10:51 AM

To <Matt_C_Fowler@dot.ca.gov>
cc
Subject Cold Spring Bridge

The Santa Barbara County Network, a nonprofit advocating on social justice and environmental issues, supports the Friends of the Bridge effort to urge Caltrans to take the time needed to fully consider and select an appropriately designed horizontal barrier system for the Cold Spring Bridge, as proposed by the State Historic Preservation Officer, which is similar to the system chosen for installation on the Golden Gate Bridge.

A horizontal barrier system is shown to be a superior means of meeting all of the purposes of the Caltrans suicide barriers proposal without needlessly destroying the historic and aesthetic qualities for which it is widely renowned and treasured.

1

Caltrans should not cut corners in its haste to resume work on its deeply flawed and destructive tall vertical cage fencing barriers proposal.

Let's save lives and preserve the integrity and beauty of this historic landmark at the same time.

Please let me know when you receive this message. Thank you.

Deborah Brasket, Executive Director
Santa Barbara County Action Network (SB CAN)
P.O. Box 23453, Santa Barbara, CA 93121
805-722-5094 / deborah@sbcn.org
www.sbcn.org

***Response to comments from Deborah Brasket, Executive Director,
Santa Barbara County Action Network***

Your opposition to the project has been noted.

Response to comment #1: The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge.

The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity. For the Cold Spring Canyon Bridge, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

Caltrans fully identified and assessed impacts to views from the Cold Spring Canyon Bridge in its analysis of project effects to the structure as a historical resource. The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design

of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.



January 3, 2011

*Santa Ynez
Valley Alliance*

Matt Fowler, Senior Environmental Planner
Central Coast Environmental Analysis
California Department of Transportation
50 Higuera Street
San Luis Obispo, CA 93401

DIRECTORS

DOUGLAS BRADLEY
JOE DUGAN
LANSING DUNCAN
GAIL MARSHALL
SHELLEY LANE
MARK OLIVER
WARD RAFFERTY, JR.
DONNA WILL

Dear Mr. Fowler,

Thank you for the opportunity to comment on the Draft Supplemental Environmental Impact Report (DSEIR) for the Suicide Barrier proposed for Cold Spring Canyon Bridge on State Route 154.

The Cold Spring Canyon Bridge is an historic and remarkable structure beloved by Valley residents. The views of the San Rafael Mountains and the Santa Ynez Valley seen from the bridge are breathtaking and inspiring for both long-term residents and first-time visitors.

The Santa Ynez Valley Alliance is concerned that "The construction of the Grid/Mesh Alternative on Cold Spring Canyon Bridge would introduce a new structure that would significantly impact the bridge's historic character, appearance, and scenic views..." (3.1.2 Unavoidable Significant Environmental Effects, pg. 36)

Despite the proposed mitigation measures, the DSEIR concludes, "It is not possible, however, to reduce the unavoidable visual, aesthetic, and cultural impacts to the bridge to a less than significant level."

This finding obligates CalTrans and the community to consider alternative measures that will result in reduced impacts. Although the DSEIR includes analysis of a Vertical Picket Alternative, this alternative is in most respects identical to the Grid/ Mesh Alternative. CalTrans appears to have settled on a vertical physical barrier as the sole solution to addressing suicide in this location.

1

As noted in the DSEIR, additional alternatives incorporating different types of physical barriers were "eliminated from further discussion." Although it is helpful to see the "rationale" used to eliminate these alternatives incorporated in the DSEIR text (1.5.1 Alternatives Considered but Eliminated from Further Discussion, pg. 5), the document does not facilitate ready public access to more information. Drawings and correspondence regarding the eliminated alternatives are buried in the appendices of a 507 page pdf. found on a compact disc.

Members of the public seeking to compare these alternatives with that proposed for construction will find the task daunting. The mere existence of these partially developed alternatives suggests that there may be more creative solutions to be developed, even if CalTrans is fixed on a physical barrier.

If a physical barrier is truly necessary and ultimately constructed, members of the public will best be served by a complete analysis of impacts and potential mitigation. This is particularly true when the proposed project will result in such dramatically significant impacts.

2

The mission of the Santa Ynez Valley Alliance is to work collaboratively with individuals, groups and governments to protect the rural character of the Santa Ynez Valley and support good stewardship of natural and agricultural resources through education, comprehensive planning and public participation.

Again, thank you for this opportunity to comment.

POST OFFICE BOX 941
SANTA YNEZ, CA. 93460
info@santaynezvalleyalliance.org
www.santaynezvalleyalliance.org

Sincerely,

Mark Oliver, President

Response to comments from Mark Oliver, President, Santa Inez Valley Alliance

Your opposition to the project has been noted.

Response to comment #1: The 2008 DEIR (pages 6-14) and 2009 FEIR (pages 7-15), incorporated by reference in the Draft Supplemental Environmental Impact Report, contains analyses of Alternatives, the Environmentally Superior Alternative, Preferred Alternative, and Alternatives Considered but Eliminated From Further Discussion, etc.

The 2010 Draft Supplemental Environmental Impact Report (2010 DSEIR) discusses in depth several types of horizontal net barriers and their effects on the Cold Spring Canyon Bridge. Five categories of horizontal net barriers [2010 DSEIR, page 5] were considered as potential alternatives but eliminated from further discussion. Two refined designs were developed from these five categories of ideas: the Safety Net Alternative and the Cantilever Arc Barrier Alternative.

The Safety Net Alternative is developed in the Supplemental Report, which is included in the 2010 DSEIR, Attachment 20. The Supplemental Report discusses in depth the impacts to the historic integrity of the structure as well as the evaluation of the adverse effects of the proposed Safety Net Alternative.

The Cantilever Arc Barrier Alternative is developed in the Feasibility Study included in the 2010 DSEIR, Attachment 39. This design, along with the constraints, was developed in conjunction with the State Historic Preservation Officer. The Feasibility Study discusses the impacts to the structure, including the impacts to the historic integrity of the structure.

In summary, horizontal net barriers have been rejected for the following reasons:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance

6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Response to comment #2: The 2010 DSEIR was prepared in accordance with the Judgment of the Superior Court of California for the County of Santa Barbara. In its Judgment, the court ruled that the DEIR impermissibly deferred the development of measures mitigating impacts to cultural and visual/aesthetic resources to the FEIR, thereby effectively precluding any public comment about or public participation in the development of such mitigation measures. This SEIR (which incorporates the 2008 DEIR and 2009 FEIR) was prepared and publicly circulated to comply with the court's Judgment and Writ issued thereon, and contains a complete analysis of impacts and mitigation.

Section 4.0 Individuals



Alan Strout
<PastorAlan@fumcsb.org>
01/05/2011 08:43 AM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc
Subject Install Barriers

Dear Sir:

I encourage the building of barriers on the Cold Springs Bridge as a safety project that will save lives at one of the most lethal spots in five counties. Those who jump from this bridge have no chance to survive. Putting time between the person's suicidal impulse and the action to take their lives is crucial. Restricting access to means for suicide is important and is the 5th Goal of the US National Strategy for Suicide Prevention.

Thank you for your consideration.

Rev. Alan R. Strout
First United Methodist Church of Santa Barbara

21 December 2010

Cal-Trans

50 Higuera Street

San Luis Obispo CA 93401

Attn. Matt C. Fowler, Senior Environmental Planner

Dear Mr. Fowler,

I am writing to express my strongest support for the grid/mesh suicide prevention barrier being installed as soon as possible on Cold Springs Bridge to prevent more suicides.

I am a Licensed Marriage and Family Counselor, Santa Barbara native and mother of a brilliant son who committed suicide at 23 in a crisis moment with no support. Bridges are quick and easy outs and as such, magnets for suicide, as Malcolm Gladwell in his book, The Tipping Point, specifically discusses. Without such instant, easy, 100% successful dramatic final exits, most people in crisis do NOT go on to commit suicide anyway. They get essential time to calm down, connect with others and think or talk things through. That belief is a myth propounded by a few biased individuals such as the predominantly environmental lawyers led by Mark McGinnes, aka "Friends of the Bridge". The more they say it, the more people may buy in but repeating a myth never increases its veracity.

I had an interesting occasion to speak to Mr. McGinnes personally, after he spotted me walking into Trader Joe's followed after and introduced himself. He said he'd seen me in court and my sense was he wanted to know who I was. Once it became clear we, as he put it, "...were on opposite sides of an issue," he began bringing serial points up to me against the barrier while we stood face to face in front of TJ's free sample concession. Because I am quite familiar with the issues as a trained clinician with firsthand experience in suicide, I easily countered each point. Finally he said, "Well, the bottom line is that beauty trumps life." I couldn't believe my ears. I responded, "Mark, are you saying that human life isn't beautiful, isn't precious, isn't irreplaceable?" He repeated his last statement. I said, "Absolutely, Mark, I don't and never will agree on that one." The whole time, people were walking by and/or listening in. He mumbled something and swiftly walked off down the aisle and out the door. The man handling the fast food concession offered me all the appetizers he'd made while overhearing us

Each human life is a treasure. At birth, most mothers recognize the beautiful miracle they have been given. If Mark McGinnes' own mother hadn't thought him precious, beautiful and worth everything to conserve and protect, he very probably wouldn't be here today.

It is vital to note that many people who became suicidal and jumped from Cold Springs Bridge were not "losers", but valuable, brilliant and caring professionals: lawyers, doctors, psychologists—exactly the people we need to help this species survive. My son, for example, was a theoretical mathematician at 21, receiving both his B.S. and M.S. from Carnegie Mellon on the same day after only four years' study. Our super-achievers are typically sensitive and brilliant, and possibly more prone to demoralized moments—yet, at one time or another, aren't we all? But their early demise is such a profound loss for humanity many ways, including so many foreclosed on contributions.

If we turn our heads and do nothing, there will only be more and more tragedies and loss for all of us up at the bridge and that's what Cold Springs Bridge already is and will become even more famous for, as a "Suicide Bridge". And that outcome is most assuredly NOT beautiful, will only magnetize more suicides there, and should demoralize us because it would stand as a terrible, very predictive omen for mankind.

Please support the aesthetics and intrinsic worth of human life and its continuity by putting up the barrier on Cold Springs Bridge. Let Santa Barbara be known for preserving our human landscape as much as those views we are so richly endowed with and can see everywhere.

Enclosed please find petitions and other material in support of the suicide barrier.

Very sincerely,


Sally Hughes, LMFT

Enclosures

20 August 2010

TO: THE HONORABLE THOMAS ANDERLE
JUDGE OF THE SUPERIOR COURT
DEPARTMENT 3
COUNTY OF SANTA BARBARA
STATE OF CALIFORNIA

IN RE: "FRIENDS OF THE BRIDGE" VS. CAL-TRANS
SUICIDE PREVENTION BARRIER
HEARING AUGUST 24, 2010
CASE NO: 3369834

I am a long-term Marriage and Family Therapist in Santa Barbara. Please consider the following in your judicial consideration of the specifics in this case.

The "Friends of the Bridge" have "stayed" a suicide prevention barrier legitimately being constructed, which could be nearly up by now. Days after the stay, a 52-year old man jumped, and days after that, an 18-year old girl was found and stopped from jumping by police happening to drive by. Without intervention, this long-festering, famous, 100% lethal bridge will never be known for just its beauty.

As a therapist, mother and public citizen, I find this irresponsible. There are between 100-200 attempts per suicide fatality. Most people in suicidal crisis who survive do NOT go on to kill themselves. I personally have saved four people in the act of suicide, all still alive, well, happy and successful now many years later. Most other means for suicide are not as lethal as the bridge. Why give hurting people such a well-known, easily-accessed, instant and certain death?

For over forty years, McGinnes' "Friends'.." have used courtroom tactics to prioritize a 10-second visual over life--a view that will still be seen by drivers on Cold Springs Bridge with the mesh barrier in place. Their tactics delay a humane intervention that would save money, unnecessary crises, trauma and lives, all ensuring everyone's safe passage across a narrow and dangerous bridge. This bridge was never designed for pedestrians or bicyclists. So why is it?

"The Friends'.." proposed "human barrier" solution is no solution. Even suicidal people carry cell phones these days. A callbox is nice but impractical. People in a suicidal moment are impulsive, irrational and desperate. If they even see a call box, would they stand there talking on it to an anonymous voice amid wind, noise and curious passersby? The "human barrier" label is simply linguistic misdirection; a more accurate term would be "remote policing": Neither "humane" nor effective.

The Bridge is a dangerous place. Everyone up there feels that. Pedestrians walk already in jeopardy, with less than two feet between the car lane and a 27" high wall with that 400' dropoff beyond. And, too dangerous for drivers who go into crisis seeing someone walking knowing what's probably going to happen next, yet still must drive safely, avoid hitting anyone, and either call 911 or make another choice.

By the time official personnel finally get there it will likely be too late. Impulsive, desperate people in a crisis moment usually are psychically prepared to step up and over. On the bridge with cars either coming or whizzing past, there's no safe place, space or room to sit, think, or talk intimately on a phone or to rescuers.

Life trumps a pretty, 10-second visual. Drivers enjoy nine other panoramic vistas driving 154 from the top of the pass just to Paradise Road alone, not counting the bridge. There's hardly a place on 154 (or in Santa Barbara proper) that isn't scenic. So what do we really need to preserve here?

The barrier will ensure everyone's safe passage over the bridge with minimal visual impediment and send the message that Santa Barbara citizens' do value and protect human life.

We respectfully ask you to review the enclosed articles in support of the barrier, including point-by-point rebuttals of "The Friends..." allegations. Enclosed find two petitions: one available online, and one newly being circulated by Mr. Sam Alfano et al., aka "The Friends of the Bridge for Life".

Thank you for your time and attention to a most serious but very preventable matter that will not go away without effective intercession.

Very respectfully submitted to the Court,



Sally Hughes, M.A.
Licensed Marriage and Family Therapist
PO Box 31054
Santa Barbara CA 93105
sbsal@cox.net

Enclosures

FRIENDS OF THE BRIDGE FOR LIFE

PETITION:

WHEREAS -- 55 PEOPLE HAVE JUMPED TO THEIR DEATHS SINCE THE COLD SPRINGS BRIDGE WAS BUILT IN 1963

WHEREAS --THE "FRIENDS OF THE BRIDGE" ARE OPPOSED TO RESTRAINING PEOPLE FROM JUMPING OFF THE BRIDGE

WHEREAS-- THE "FRIENDS OF THE BRIDGE" ARE PUTTING A HIGHER VALUE ON THE POSSIBLE LOSS OF AESTHETIC VALUE OVER THE LOSS OF HUMAN LIFE .

WHEREAS- WE AS A HUMANE SOCIETY OWE THOSE IN DISTRESS EVERY EFFORT TO CONTINUE ON WITH THEIR LIVES

WHEREAS-- IT IS INCONCEIVABLE THAT THERE ARE THOSE IN OUR SOCIETY WHO PLACE THE COST OF THE BARRIER OVER THE VALUE OF HUMAN LIFE

WHEREAS - CAL TRANS AND SBCAG HAVE THE FUNDING TO ERECT SUICIDE PREVENTION MEASURES

WHEREAS- THE LIVES OF CHP, SHERIFF, FOREST SERVICE, RESCUE PERSONNEL AND MOTORISTS ARE PUT AT RISK AS A RESULT OF SUICIDE ATTEMPTS AT THE BRIDGE

WHEREAS -AS EACH DAY PASSES BY WITHOUT THE PREVENTION MEASURES ANOTHER LIFE MAY BE LOST

NOW THEREFORE THE UNDERSIGNED HEREBY URGENTLY REQUEST THAT CAL TRANS AND SBCAG CONSTRUCT THE SUICIDE PREVENTION BARRIER WITHOUT FURTHER DELAY:

- | | NAME | DATE |
|----|---|---------|
| 1 | Sally Hughes, LMFT 11 Fasano Way SBACA | 8/20/10 |
| 2 | Catherine Atkin 224 Rametto Rd SB CA 93108 | 8/19/10 |
| 3 | AMANDA MARDON 901 MISSION CYN RD SB CA | 8/19/10 |
| 4 | Pamela Johnson 1747 Grand Ave # B SB CA 93103 | |
| 5 | Jane Siemonsma Po Box 3083 SB, CA 93130 | 8/19/10 |
| 6 | Paul A. (Tony) Tankin P.O. Box 30521 SB CA 93130 | 8/19/10 |
| 7 | CAROL SUCCO 2 W. CONSTANCE AVE # 3 | 8/19/10 |
| 8 | NANCY WOOD 4505 CARPINTERIA AVENUE, 93013 | 8/19/10 |
| 9 | Victoria Williamson 2675 Gibraltar Pkwy | 8/15/10 |
| 10 | Tamara LeBlanc 2800 Somerkent SB 93105 | 8/19/10 |
| 11 | Rebecca Wilson P.O. Box 87 Summerland, CA 93067 | 8/19/10 |
| 12 | Julius Wilson P.O. Box 87 Summerland, CA 93067 | 8/19/10 |
| 13 | Eldon Edwards 800 DeLaCuesta Terr SB 93103 | |
| 14 | Lynn Cederquist, 3375 Foothill Rd Carpinteria CA 93013 | |
| 15 | Christine Campos, 7210 Davenport Rd #203, Goleta CA 93111 | |
| 16 | Vijaya Jammalamadaka 1304 Ferrel Rd. Santa Barbara CA 93103 | |
| 17 | Emily H. Christel 10 Fasano Way, Santa Barbara, CA 93105 | |
| 18 | DUAHE E. AASTED 10 FASANO WAY, SANTA BARBARA, CA 93105 | |
| 19 | | |
| 20 | | |

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Support the Cold Spring Bridge Safety Barrier



signatures: 636

signature goal: 1,000

Target: California Residents, Friends and Families of Suicide Victims
Sponsored by: www.stophetragedy.org

We support the construction of a physical safety barrier on the Cold Spring Arch Bridge.

Since its construction, *more than 50 people have died at Cold Spring Arch Bridge*. Eight people died in 2009 alone. These deaths could have been prevented by the installation of a physical safety barrier. A proposal by Caltrans to construct a safety barrier on the bridge has been under discussion since 2005, but objections to the proposal have meant that no action has been taken to stop suicides at the bridge, resulting in further preventable deaths.

The facts in support of the barrier are these:

Construction of a physical barrier is the *only* proven method for the prevention of suicide by jumping.

Numerous research studies have demonstrated that the installation of barriers significantly reduces or eliminates deaths by suicide at a particular location.

No scientific evaluation of the effectiveness of alternative strategies such as human barriers, including the use of call boxes, video monitor... [more](#)

, You signed on July 14, 2010.
Your signature has been delivered to:
California Residents, Friends and Families of Suicide Victims



Fight Hunger in America this Holiday Season
signatures: 25,975

[sign petition](#)



Obama: Grant Permanent Protection to the Arctic National Wildlife Refuge
signatures: 137,930

[sign petition](#)



Tell Advertisers: Drop Fox!
signatures: 843

[sign petition](#)

[all petition categories](#) | [view more petitions](#)

we signed "Support the Cold Spring Bridge Safety Barrier"

- # 636 11:42, Dec 19, Don Adams, CA
- # 635 08:53, Dec 10, David N Moore, CT
- # 634 15:51, Nov 28, Sheri Collins, CA
- # 633 11:43, Nov 24, maria gonzalez, CA
- # 632 19:32, Nov 23, Kim Le, CA
- # 631 18:46, Nov 23, Donna Hamblin, CA
- # 630 18:36, Nov 23, Susan Adams, CA
- # 629 17:26, Nov 23, Juan gonzalez, CA

628 13:42, Nov 23, Name not displayed, AZ
627 13:25, Nov 23, Jean Miller, CA
626 13:07, Nov 23, Nicole Sattler, CA
625 12:18, Oct 18, Lynne Lowe, CA
624 19:07, Sep 25, Ingrid Brown, CA
623 07:11, Sep 21, Pedro Nava, CA
622 15:43, Sep 16, Peter Stone, CA
621 12:40, Sep 16, John Cloonan, CA
620 11:44, Sep 16, Patricia Mickelson, CA
619 07:00, Sep 16, stephen shugart, CA
618 03:45, Sep 16, Gunther Gottschalk, CA
617 23:04, Sep 15, Name not displayed, CA
616 22:23, Sep 15, Edward Ruiz, CA
615 21:16, Sep 15, Diane Lacey, CA
614 20:03, Sep 15, Laura Lynch, CA
613 20:03, Sep 15, Marcia Sherman, CA
612 08:47, Sep 10, Name not displayed, United Kingdom
611 13:03, Aug 25, Brian Melton, ID
610 19:59, Aug 17, Shannon Miller, CA
609 14:37, Aug 11, Sam & Lee Alfano, CA
608 16:09, Aug 09, Name not displayed, CA
607 12:10, Aug 02, kelsey brown, CA
606 19:21, Jul 31, Paul Tonkin, CA
605 20:41, Jul 29, E Onja brown-lawson, CA
604 10:35, Jul 28, Barbara Herring, CA

# 603	10:13, Jul 28, Negin Parvin, CA
# 602	20:00, Jul 27, Aubrey Ellsworth, CA
# 601	16:42, Jul 27, Name not displayed, CA
# 600	15:49, Jul 27, Name not displayed, CA
# 599	13:55, Jul 27, Name not displayed, CA
# 598	13:23, Jul 27, Name not displayed, CA
# 597	10:37, Jul 27, Lindsey Hyslop, CA
# 596	16:15, Jul 26, Katelyn Slosson, CA
# 595	16:03, Jul 26, Kevin Carpenter, CA
# 594	15:05, Jul 22, Jina Carvalho, CA
# 593	10:27, Jul 22, Name not displayed, CA
# 592	13:58, Jul 16, Steven Boehm, CA
# 591	17:53, Jul 15, MARY HUNTER, CA
# 590	17:28, Jul 15, Thomas Biesanz, CA
# 589	16:30, Jul 14, Sally Hughes, LMFT, CA
# 588	23:03, Jun 11, Christinia Green, IN
# 587	11:00, May 27, Borg Drone, United Kingdom
# 586	18:46, May 08, Rachel Viegas, CA
# 585	18:35, May 08, Lenette Viegas, CA
# 584	09:07, Apr 09, Amber Saucedo, United Arab Emirates

- # 602 20:00, Jul 27, Aubrey Ellsworth, CA
- # 601 16:42, Jul 27, Name not displayed, CA
- # 600 15:49, Jul 27, Name not displayed, CA
- # 599 18:55, Jul 27, Name not displayed, CA
- # 598 13:23, Jul 27, Name not displayed, CA
- # 597 10:37, Jul 27, Lindsey Hyslop, CA
- # 596 16:15, Jul 26, Kathryn Slosson, CA

- # 595 16:03, Jul 26, Kevin Carpenter, CA
- # 594 15:05, Jul 22, Jina Carvalho, CA

- # 593 10:27, Jul 22, Name not displayed, CA
- # 592 13:58, Jul 16, Steven Boehm, CA

- # 591 17:53, Jul 15, MARY HUNTER, CA
- # 590 17:28, Jul 15, Thomas Biesanz, CA
- # 589 16:30, Jul 14, Sally Hughes, LMFT, CA

- # 588 23:03, Jun 11, Christinia Green, JN
- # 587 11:08, May 27, Borg Drone, GB
- # 586 18:46, May 08, Rachel Viegas, CA
- # 585 18:35, May 08, Lenette Viegas, CA

- # 584 09:07, Apr 09, Amber Saucedo, AE
- # 583 00:46, Apr 04, Arleen Osborn, CA
- # 582 13:29, Mar 15, Sara Salvado, PT

stet

- # 581 20:53, Feb 03, Dale Weber, CA
- # 580 01:46, Jan 29, Martin Camp, CA

- # 579 23:50, Nov 29, Name not displayed, CA
- # 578 10:45, Nov 05, Melinda Huerta, CA
- # 577 21:28, Nov 04, Teresa Alcalá, CA

- # 576 11:46, Nov 04, ANGELICA RUVALCABA, CA
- # 575 11:13, Nov 04, JOSE MENDOZA, AZ

- # 574 23:09, Nov 03, Nazee nazari, CA

- # 573 22:14, Nov 03, Ray Dolen, CA

- # 572 11:37, Nov 03, Name not displayed, CA
- # 571 09:23, Nov 03, Susan Adams, CA

- # 570 08:55, Nov 03, Krista Adams, CA

- # 569 23:47, Nov 02, Name not displayed, CA

- # 568 19:14, Nov 02, Abel Rubalcava, CA
- # 567 18:58, Nov 02, shawn Kayvon, CA

- # 566 17:14, Nov 02, Anita Duplessis, CA
- # 565 16:10, Nov 02, Rosa Jorge, CA

- # 564 16:07, Nov 02, Julia Alpi-Kindred, CA

- # 563 15:24, Nov 02, Name not displayed, CA

- # 562 14:56, Nov 02, Juanita Jorge, CA

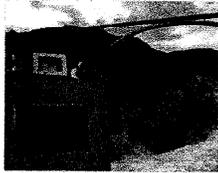
- # 561 14:51, Nov 02, Mariena Saldana, CA
- # 560 14:34, Nov 02, Name not displayed, CA
- # 559 14:20, Nov 02, Jesus Lara, CA
- # 558 13:54, Nov 02, Cindy Vargas, CA
- # 557 13:52, Nov 02, Raymundo Landavazo, CA
- # 556 13:51, Nov 02, Angle Garcia, CA
- # 555 13:31, Nov 02, Estela Hayward, CA
- # 554 12:59, Nov 02, Adriana Jorge-Zamora, CA
- # 553 12:42, Oct 31, Name not displayed, CA
- # 552 16:38, Oct 29, Name not displayed, CA
- # 551 00:35, Oct 28, Erin Morey, CA

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Support the Cold Spring Bridge Safety Barrier



signatures: 610

signature goal: 1,000

Target: California Residents, Friends and Families of Suicide Victims

Sponsored by: www.stopthetragedy.org

We support the construction of a physical safety barrier on the Cold Spring Arch Bridge.

Since its construction, *more than 50 people have died at Cold Spring Arch Bridge*. Seven people have died in 2009 alone. These deaths could have been prevented by the installation of a physical safety barrier. A proposal by Caltrans to construct a safety barrier on the bridge has been under discussion since 2005, but objections to the proposal have meant that no action has been taken to stop suicides at the bridge, resulting in further preventable deaths.

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No scientific evaluation of the effectiveness of alternative strategies such as human barriers, including the use of call boxes, video mon... more

Sally, You signed on July 14, 2010.
Your signature has been delivered to:
California Residents, Friends and
Families of Suicide Victims

we signed "Support the Cold Spring Bridge Safety Barrier"

- # 550 12:37, Oct 25, James Smith, CA
- # 549 13:57, Oct 24, Name not displayed, CA
- # 548 15:49, Oct 22, Sheana Green, CA
- # 547 21:45, Oct 21, Geneva de la Rouge, CA
- # 546 19:13, Oct 21, Name not displayed, CA
- # 545 10:17, Oct 19, Name not displayed, CA
- # 544 00:34, Oct 19, Lauren Franz, CA
- # 543 15:40, Oct 18, serena baird, CA
- # 542 11:11, Oct 18, Jen Willis, CA

541 20:26, Oct 17, Danell Knox, CA
540 13:42, Oct 16, Kelly Cole-Smith, CA
539 08:03, Oct 16, Name not displayed, CA
538 20:42, Oct 14, Name not displayed, CA
537 18:13, Oct 14, Jennifer Elkins, CA
536 17:30, Oct 14, Ginger Sandoval, CA
535 16:57, Oct 14, Name not displayed, NV
534 12:54, Oct 14, Monica Elias, CA
533 11:00, Oct 14, Vanessa Elias, CA
532 14:35, Oct 13, Name not displayed, CA
531 10:20, Oct 13, Lindsey LeBlanc, CA
530 13:03, Oct 12, Terri McDonald, CA
529 22:53, Oct 09, rene block, CA
528 21:14, Oct 09, Carrie Patterson, CA
527 20:09, Oct 09, Jessica Fox, CA
526 10:29, Oct 09, Joe Silva, CA
525 06:53, Oct 09, Anne Farrell, CA
524 10:35, Oct 08, Sergio Rodriguez, CA
523 08:29, Oct 08, Elizabeth Venegas, CA
522 00:42, Oct 08, April Baird, CA
521 11:27, Oct 06, LeAnn Ashton, CA
520 19:46, Oct 05, Nicole Swartz, CA

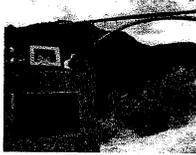
- # 519 19:27, Oct 05, Kev Gar, CA
- # 518 19:03, Oct 05, Tamara Blessing, CA
- # 517 23:04, Sep 27, Brian Kenney, MN
- # 516 23:05, Jul 24, Lisa Ouellette, CA
- # 515 16:32, Jun 10, Name not displayed, CA
- # 514 17:05, Jun 08, Harlina Thornton, CA
- # 513 17:03, Jun 05, Lethia Nall, NY
- # 512 10:58, Jun 05, Shannon murphy, CA
- # 511 03:14, Jun 05, Sabrina Glazbrook, CA
- # 510 19:06, Jun 04, Name not displayed, CA
- # 509 15:53, Jun 04, Gina Watkins, CO
- # 508 12:02, Jun 04, Cordelia Woodruff, CA
- # 507 11:42, Jun 04, Jody Taylor, CA
- # 506 11:25, Jun 04, Allison Taylor, CA
- # 505 09:43, May 29, Brenda Zuber, TX
- # 504 08:01, May 29, Catherine Barber, MA
- # 503 17:26, May 28, Mary Vriniotis, MA
- # 502 10:22, May 27, Janet Billiet, CA
- # 501 12:21, May 23, Name not displayed, CA

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we signed "Support the Cold Spring Bridge Safety Barrier"

- # 500 20:54, May 12, Shauna A, CA
- # 499 14:25, May 12, Michele Janee, CA
- # 498 12:30, May 10, aaron frederick, CA
- # 497 17:29, May 07, Jacqueline Vanderlaan, CA
- # 496 22:42, May 06, beth janss, CA
- # 495 18:02, May 03, Lynda Thompson, CA
- # 494 19:53, Apr 30, Sarah Kass, CA
- # 493 06:50, Apr 30, Teresa Mcmillian, MO
- # 492 06:37, Apr 30, Frederic Kass, CA
- # 491 05:27, Apr 30, Karen Adams, IL

<http://www.thepetitionsite.com/51/support-the-cold-spring-bridge-safety-barrier/>

Page 1 of 3

490 21:01, Apr 29, Laura Wilson, MO
489 18:58, Apr 29, Name not displayed, CO
488 00:02, Apr 29, Nancy Fields, CA
487 21:32, Apr 27, Stacy L. Christopher, CA

486 09:49, Apr 27, Name not displayed, CA
485 18:37, Apr 26, Leslie Holtzman, CA

484 19:44, Apr 24, Jamie Gumins, CA
483 14:37, Apr 24, Irene and John O'Brien, CA
482 09:25, Apr 24, Russell Abbey, CA

481 06:21, Apr 24, Michiel McHugh, CA

480 16:10, Apr 23, Lee Scheuermann, CA
479 16:04, Apr 23, Christine Sobenes, CA
478 15:37, Apr 23, Amy Locke, CA
477 14:50, Apr 23, Eric Sappenfield, NJ
476 14:27, Apr 23, Name not displayed, CA

475 14:00, Apr 23, Mark Childs, CA
474 13:59, Apr 23, Claudia Chotzen, CA

473 12:29, Apr 23, peggy and alan nakashima, CA
472 11:15, Apr 23, Derrick+Diane Venzor, CA

471 10:56, Apr 23, Name not displayed, CA
470 10:33, Apr 23, Name not displayed, CA

469 10:32, Apr 23, Ernesto Quiroz, CA

468 10:31, Apr 23, robert crane, CA
467 09:45, Apr 23, Suzi Serbin, CA

466 22:35, Apr 22, Name not displayed, CA
465 14:56, Apr 22, Don McDermott, CA

Support the Cold Spring Bridge Safety Barrier



signatures: 610

signature goal: 1,000

Target: California Residents, Friends and Families of Suicide Victims

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Sally, You signed on July 14, 2010.
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California Residents, Friends and Families of Suicide Victims

we signed "Support the Cold Spring Bridge Safety Barrier"

- # 450 21:22, Apr 16, Rhonda Reid, CA
- # 449 18:01, Apr 16, Cathy Fortunato, CA
- # 448 17:13, Apr 16, Michelle Hollander, NY
- # 447 16:35, Apr 16, Rita Barton, CA
- # 446 08:58, Apr 16, Karen Grandberry, CA
- # 445 22:55, Apr 15, Luz Rodriguez, CA
- # 444 19:54, Apr 15, Name not displayed, CA
- # 443 17:50, Apr 15, Jennifer Howard Kiclnski, WA

442 15:57, Apr 15, Anna Lincoln, CA
441 21:56, Apr 14, Kaitlyn Riddle, CA
440 20:54, Apr 14, Vanessa Marquez, CA
439 16:04, Apr 14, Alisa Togami, CA
438 15:14, Apr 14, Delois Thomas, CA
437 14:24, Apr 14, Name not displayed, CA
436 14:15, Apr 14, jennifer valadez, CA
435 09:56, Apr 14, Shelley Hession, CA
434 07:29, Apr 14, Linarosa Vendasco, CA
433 22:50, Apr 13, James Eddy, CA
432 21:56, Apr 13, Name not displayed, CA
431 20:15, Apr 13, Mary O', CA
430 20:14, Apr 13, Shirley Obermeyer, CA
429 19:42, Apr 13, Mary Johnston, CA
428 16:45, Apr 13, John Adams, CA
427 13:35, Apr 13, Julie Miller, CA
426 12:26, Apr 13, Mark Meinzer, CA
425 10:39, Apr 13, Abby Burnett, ME
424 09:47, Apr 13, William Henderson, CA
423 08:46, Apr 13, Lisa Martin, CA
422 22:49, Apr 12, Lisa Housef, CA
421 22:25, Apr 12, Name not displayed, CA
420 13:08, Apr 12, Erin O'Connell, CA

403 14:21, Jan 07, Cynthia Young, CA
402 16:09, Jan 02, Jennifer Gardner, FL
401 15:43, Jan 01, Kevin Purcoll, IL

450 21:22, Apr 16, Rhonda Reid, CA
449 18:01, Apr 16, Cathy Fortunato, CA

448 17:13, Apr 16, Michelle Hollander, NY
447 16:35, Apr 16, Rita Barton, CA
446 08:58, Apr 16, Karen Grandberry, CA
445 22:55, Apr 15, Luz Rodriguez, CA

444 19:54, Apr 15, Name not displayed, CA
443 17:50, Apr 15, Jennifer Howard Kicinski, WA
442 15:57, Apr 15, Anna Lincoln, CA
441 21:56, Apr 14, Kaitlyn Riddle, CA
440 20:54, Apr 14, Vanessa Marquez, CA

439 16:04, Apr 14, Ailisa Togami, CA

438 15:14, Apr 14, Delois Thomas, CA

437 14:24, Apr 14, Name not displayed, CA
436 14:15, Apr 14, Jennifer valadez, CA
435 09:56, Apr 14, Shelley Hession, CA
434 07:29, Apr 14, Linarosa Vendrasco, CA
433 22:50, Apr 13, James Eddy, CA

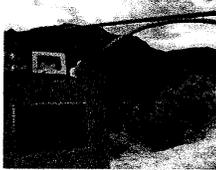
432 21:56, Apr 13, Name not displayed, CA
431 20:15, Apr 13, Mary O', CA

430 20:14, Apr 13, Shirley Obermeyer, CA
429 19:42, Apr 13, Mary Johnston, CA
428 16:45, Apr 13, John Adams, CA
427 13:35, Apr 13, Julie Miller, CA
426 12:26, Apr 13, Mark Meinzer, CA
425 10:39, Apr 13, Abby Burnett, ME
424 09:47, Apr 13, William Henderson, CA
423 08:46, Apr 13, Lisa Martin, CA
422 22:49, Apr 12, Lisa Housel, CA
421 22:25, Apr 12, Name not displayed, CA
420 13:08, Apr 12, Erin O'Connell, CA
419 12:37, Apr 12, Nancy DeCaro, CA
418 00:26, Apr 12, Sue H, CA
417 11:47, Apr 11, Theresa Vogel, CA
416 14:25, Apr 09, Lana Campbell, CA
415 13:27, Apr 09, Ronald Brown, CA
414 13:16, Apr 09, Name not displayed, CA
413 08:51, Apr 09, Terri Jackson, NC
412 08:04, Apr 09, Joy Brown, CA

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Support the Cold Spring Bridge Safety Barrier



signatures: 610

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California Residents, Friends and Families of Suicide Victims

we signed "Support the Cold Spring Bridge Safety Barrier"

- # 400 14:57, Dec 22, Jacob Mercer, CA
- # 399 21:20, Dec 12, Amber Mondae Bozman, OR
- # 398 07:55, Dec 05, clarissa nieves, OH
- # 397 07:33, Dec 05, Crystal Menicola, NJ
- # 396 14:39, Nov 29, William Etling, CA
- # 395 10:25, Nov 26, Miriam Lindbeck, CA
- # 394 10:50, Nov 20, Name not displayed, CA

393 09:42, Nov 20, PATRICIA TORRES, CA

392 23:11, Nov 18, Name not displayed, CA

391 14:34, Nov 18, Name not displayed, CA

390 21:58, Nov 17, Amanda Curnow, CA

389 11:52, Nov 17, Venessa Hurka, CA

388 19:04, Nov 16, Sarah Barkley, CA

387 15:20, Nov 16, Jo Ellen Brainin-Rodriguez, CA

386 12:36, Nov 16, Name not displayed, CA

385 09:45, Nov 16, Jessica Ray, CA

384 23:39, Nov 15, Michael Spiker, CA

383 23:34, Nov 15, Name not displayed, IL

382 23:06, Nov 15, Name not displayed, CA

381 22:44, Nov 15, Ben Tilbury, AU

380 21:37, Nov 15, Megan Gentle, CA

379 19:12, Nov 15, Name not displayed, CA

378 16:35, Nov 15, Krysta Falloon, CA

377 14:18, Nov 15, Verity Harvey, GB

376 13:09, Nov 15, Michelle Barnett, CA

375 09:54, Nov 15, Esperanza Daly, CA

374 07:12, Nov 15, Jamie Rowswell, CA

373 04:25, Nov 15, angie szily, AU

372 22:36, Nov 14, Sara Nickel, CA

371 21:27, Nov 14, Evan Janke, CA

370 21:22, Nov 14, Brenna Zimmerman, CA

369 21:12, Nov 14, Alycia Paulsen, CA

368 20:59, Nov 14, Elena Daly, CA

367 20:51, Nov 14, cory anglin, CA

366 20:33, Nov 14, Ali Head, CA

- # 365 20:29, Nov 14, Timothy Petter, CA
- # 364 20:28, Nov 14, Chase Fisher, CA
- # 363 20:26, Nov 14, Mindy Petter, CA
- # 362 20:15, Nov 14, Name not displayed, CT
- # 361 20:15, Nov 14, samantha curnow, CA
- # 360 20:12, Nov 14, Lindsay Gould, CA
- # 359 20:00, Nov 14, Thomas Burdette, MD
- # 358 14:14, Nov 14, Cody Brown, CA

- # 357 21:03, Nov 13, Dimitri Anthes, CA

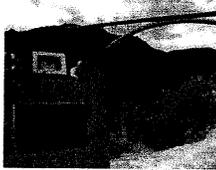
- # 356 18:00, Nov 13, Johnny Escalera, CA
- # 355 17:33, Nov 13, Brenda Cockrell, FL

- # 354 17:12, Nov 13, tegan rasmussen, UM
- # 353 13:57, Nov 13, Deborah Samson, CA
- # 352 07:03, Nov 13, RENE CASTANEDA, CA
- # 351 23:29, Nov 12, Name not displayed, CA

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Support the Cold Spring Bridge Safety Barrier



signatures: 610

signature goal: 1,000

Target: California Residents, Friends and Families of Suicide Victims

Sponsored by: www.stopthetragedy.org

We support the construction of a physical safety barrier on the Cold Spring Arch Bridge.

Since its construction, *more than 50 people have died at Cold Spring Arch Bridge*. Seven people have died in 2009 alone. These deaths could have been prevented by the installation of a physical safety barrier. A proposal by Caltrans to construct a safety barrier on the bridge has been under discussion since 2005, but objections to the proposal have meant that no action has been taken to stop suicides at the bridge, resulting in further preventable deaths.

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No scientific evaluation of the effectiveness of alternative strategies such as human barriers, including the use of call boxes, video mon... more

Sally, You signed on July 14, 2010.

Your signature has been delivered to:

California Residents, Friends and Families of Suicide Victims

we signed "Support the Cold Spring Bridge Safety Barrier"

390 23:19, Nov 12, Timothy Petter, CA

349 22:54, Nov 12, Steve Sharer, CA

348 22:44, Nov 12, Christopher Deem, CA

347 22:43, Nov 12, Tarielle Thomasco, CA

346 22:39, Nov 12, Name not displayed, CA

345 22:33, Nov 12, Steve Ponce, CA

344 22:31, Nov 12, Edward Peszewski, CA

343 22:22, Nov 12, Jodi Barrett, CA
342 22:21, Nov 12, Samantha Everson, CA
341 21:56, Nov 12, Emil Kraus, CA
340 21:49, Nov 12, Marc Ferguson, CA
339 21:35, Nov 12, richard rivera, CA
338 19:39, Nov 12, Paige McIlmoll, CA
337 19:37, Nov 12, Shawn Morey, CA
336 19:15, Nov 12, Sara Monteabaro, CA

335 12:21, Nov 12, Name not displayed, CA
334 12:16, Nov 12, Name not displayed, CA
333 22:13, Nov 10, Abbie Dawn, AU
332 10:51, Nov 08, Christie Tarman, CA
331 16:36, Nov 07, Jamila Gonzalez, CA
330 16:25, Nov 07, Susan Short, CA
329 16:00, Nov 07, Ruth Wilson, CA
328 15:35, Nov 07, Chaplain Fr. Jon-Stephen Hodges, CA

327 11:04, Nov 07, Jeff Shaffer, CA
326 10:48, Nov 07, Wistosis Parker, CA
326 10:07, Nov 07, Jason Tarman, CA

324 09:19, Nov 07, Jillian Marut, CA

323 08:50, Nov 07, Name not displayed, CA
322 21:53, Nov 06, Name not displayed, CA

321 10:29, Nov 04, Jane Oish, CA

320 16:09, Nov 03, Summer Frappier, CA

319 11:31, Oct 28, Robert McCain, CA
318 10:41, Oct 28, Gilles and Lissa Lalonde, CA

317 08:20, Oct 28, Braden Rhetts, NY

316 21:32, Oct 27, Mark Lytle, CA

315 14:43, Oct 27, Kelsey Kehoe, CA

314 19:39, Oct 26, Kim Elberg, CA
313 12:39, Oct 26, sandy simpkins, WA

312 17:59, Oct 24, Ryan McCain, CA

311 17:30, Oct 23, Katie McCain, CA
310 08:16, Oct 23, Name not displayed, MI

309 11:41, Oct 22, Dan Woods, MN
308 19:02, Oct 21, pamela reeves, m.d., CA

307 16:55, Oct 20, Doris Lew, CA

306 16:29, Oct 20, Brooke Winkler, CA
305 15:22, Oct 20, Peppe Rose, CA

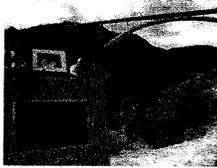
304 09:30, Oct 20, kayla unger, CA
303 05:06, Oct 20, Jan Poppe, MI

- # 302 18:24, Oct 19, Loretta Boyle, CA
- # 301 12:58, Oct 19, Lourdes Hernandez, CA

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we signed "Support the Cold Spring Bridge Safety Barrier"

- # 300 20:02, Oct 18, Dave Hull, CA
- # 299 19:07, Oct 18, lisa wollack, MN
- # 298 12:19, Oct 18, Noah Kaplan, CA
- # 297 05:39, Oct 18, DC Robbins, CA
- # 296 20:52, Oct 17, Seiko Niimi, CA
- # 295 14:05, Oct 17, Jenni Olson, CA
- # 294 10:28, Oct 17, Mary Teetor Zablotny, CA

293 06:55, Oct 17, anne greaney, CA

292 00:05, Oct 17, Lisa Rivera, CA

291 21:30, Oct 16, Nanette Orman, CA

290 15:13, Oct 16, James Costello, MA

289 14:21, Oct 16, Neile King, NY

288 13:28, Oct 16, Susan Lynch-Aumaier, CA

287 11:07, Oct 16, Jeremy McKittrick, VT

286 10:04, Oct 16, Doris Noyes, NH

285 09:45, Oct 16, Jennifer Bellucci, MA

284 09:10, Oct 16, Ingrid G., IL

283 09:09, Oct 16, Gary Lazarski, IL

282 08:44, Oct 16, Dan Braun, CA

281 08:37, Oct 16, Peggy Schramm, IL

280 08:28, Oct 16, Anna Campbell, CA

279 07:19, Oct 16, Matthew Monge, CA

278 06:27, Oct 16, Michael Olstad, TX

277 05:38, Oct 16, Kathleen Petty, CA

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- # 276 20:54, Oct 15, Name not displayed, CA
- # 275 20:46, Oct 15, Lara Mondae, IL
- # 274 20:45, Oct 15, Rebecca Sweeney, CA

- # 273 20:38, Oct 15, Mimi Muraoka, CA

- # 272 20:14, Oct 15, Meghan Talt, CA

- # 271 19:30, Oct 15, marc hyman, CA

- # 270 18:40, Oct 15, David Chen, OH
- # 269 18:32, Oct 15, Patricia Lobenberg, CA

- # 268 17:39, Oct 15, Michael Calawa, MA
- # 267 17:16, Oct 15, Mark Del Signore, NY

- # 266 17:08, Oct 15, Jordan Oathout, NY
- # 265 17:06, Oct 15, Father Stacy, CA

- # 264 16:58, Oct 15, Amber Del Signore, NY
- # 263 16:56, Oct 15, Julia Inobe, CA
- # 262 15:28, Oct 15, Zachary Thomas, MA
- # 261 15:24, Oct 15, Nissa Belanger, NH
- # 260 15:07, Oct 15, Jill Davies, CA
- # 259 14:56, Oct 15, Allison Andrade, CA

- # 258 14:37, Oct 15, Name not displayed, CA
- # 257 14:36, Oct 15, Marsha Peterson, CA

- # 256 13:48, Oct 15, Louise Mooney, OH

- # 255 13:48, Oct 15, Janice DeLoof, CA

- # 254 13:36, Oct 15, Gil Varon, CA
- # 253 13:22, Oct 15, Patricia Durna, CA

252 12:46, Oct 15, Name not displayed, MA

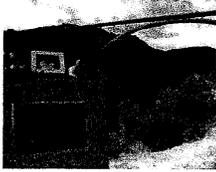
251 12:21, Oct 15, MIKE MONGE, CO

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Support the Cold Spring Bridge Safety Barrier



signatures: 610

signature goal: 1,000

Target: California Residents, Friends and Families of Suicide Victims

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we signed "Support the Cold Spring Bridge Safety Barrier"

# 250	11:49, Oct 15, Beth Monge, CO
# 249	11:16, Oct 15, Brian Bellucci, MA
# 248	10:56, Oct 15, Jenny Taylor, CA
# 247	10:14, Oct 15, Martin Aydelott, MN
# 246	09:30, Oct 15, Nathan Shadle, CA
# 245	09:22, Oct 15, Marilyn Doyle, MA
# 244	09:02, Oct 15, Christopher Lake, GB
# 243	08:55, Oct 15, Colleen Carpenter, IN

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242 08:33, Oct 15, heidi ortiz, CA

241 07:04, Oct 15, Name not displayed, CA

240 05:55, Oct 15, Jenai Wu, MA

239 22:43, Oct 14, Christina Hale+, CA

238 21:24, Oct 14, Lynn Newman, CA

237 20:57, Oct 14, jon slavet, CA

236 20:29, Oct 14, Alicia Mitchell, CA

235 18:04, Oct 14, Douglas Spellman, CA

234 17:52, Oct 14, Christy Frank, CA

233 16:15, Oct 14, sally stewart, CA

232 16:14, Oct 14, Meg Kinney, NY

231 16:05, Oct 14, Dan Bech, CA

230 16:04, Oct 14, Mary Malik, CA

229 15:51, Oct 14, Steve Feinberg, CA

228 15:04, Oct 14, Judith R. Brown, CA

227 14:49, Oct 14, Tami Smason, CA

226 14:31, Oct 14, john kevin hines, CA

225 14:21, Oct 14, Isabel Oliveira M.A., MFTI, CA

224 14:17, Oct 14, Dulcey Simpkins, CO

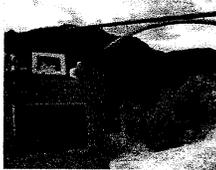
223 13:54, Oct 14, Cassandra Gaeto, CA

222 13:44, Oct 14, Name not displayed, CA

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Support the Cold Spring Bridge Safety Barrier



signatures: 610

signature goal: 1,000

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200 19:10, Oct 13, LeAnn Woodridge, CA

199 17:15, Oct 13, Benjamin Levin, WI

198 17:07, Oct 13, Sara Bartlett, CA

197 16:09, Oct 13, Nancy Del Signore, NY

196 15:44, Oct 13, Trish Lane, CA

- # 195 15:44, Oct 13, Ali Ironside, CA

- # 194 15:36, Oct 13, Joyce Catlett, CA
- # 193 15:04, Oct 13, Hugh O'Halloran, WI
- # 192 14:57, Oct 13, Paul Trent, CA
- # 191 14:19, Oct 13, Richard Tubis, CA

- # 190 13:50, Oct 13, Tamara Short, CA
- # 189 13:39, Oct 13, Gloria Cevallos, CA

- # 188 13:35, Oct 13, Kelly Kirscht, CA
- # 187 13:04, Oct 13, Heather Quisenberry, CA

- # 186 12:00, Oct 13, Kristina Jansen, CA
- # 185 11:47, Oct 13, BRUCE ARDEN, CA

- # 184 11:33, Oct 13, Geoffrey Parr, CA

- # 183 11:32, Oct 13, Jessica Firestone, CA

- # 182 11:30, Oct 13, Name not displayed, CA
- # 181 11:28, Oct 13, Sarah Adams, CA

- # 180 11:26, Oct 13, Arlene Moody, CA

- # 179 11:22, Oct 13, Irma Catlett, CA

- # 178 11:21, Oct 13, Nicki Frank, CA
- # 177 11:12, Oct 13, Andrew Cranmer, CA

- # 176 11:10, Oct 13, Robert Feinberg, CA
- # 175 11:07, Oct 13, Anna Zamir, CA

- # 174 11:06, Oct 13, LISA LAPLACA, CA
- # 173 11:04, Oct 13, bertie anderson, PA
- # 172 10:56, Oct 13, Matthew Tolander, CA
- # 171 10:55, Oct 13, John Brooks, CA
- # 170 10:07, Oct 13, Name not displayed, CA
- # 169 10:03, Oct 13, Dayna Whitmer, CA
- # 168 09:29, Oct 13, Name not displayed, CA
- # 167 09:01, Oct 13, Shirlyn Makar, TX
- # 166 08:54, Oct 13, Virginia Finn-Lenhardt, WI
- # 165 08:41, Oct 13, Harriet Chessman, CA
- # 164 08:35, Oct 13, Mary Ann Hudson, KY
- # 163 07:10, Oct 13, Tamar Gollan, CA
- # 162 07:06, Oct 13, Michael Hatch, WI
- # 161 06:28, Oct 13, Beryl Kreisel, CA
- # 160 05:28, Oct 13, Christina Cooper, CT
- # 159 22:38, Oct 12, Annie Merriam, MN
- # 158 20:23, Oct 12, Name not displayed, CA
- # 157 20:06, Oct 12, Andrea Price, DC
- # 156 19:35, Oct 12, H Thompson, WA
- # 155 18:43, Oct 12, Jill Eichner, VA

- # 154 16:48, Oct 12, Everett Clayton, CA
- # 153 16:07, Oct 12, Margaret Clayton, CA
- # 152 15:03, Oct 12, Name not displayed, NY
- # 151 12:54, Oct 12, Bill Jewell, VA

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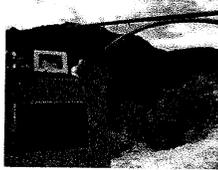
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Support the Cold Spring Bridge Safety Barrier



signatures: 610

signature goal: 1,000

Target: California Residents, Friends and Families of Suicide Victims

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we signed "Support the Cold Spring Bridge Safety Barrier"

50 21:00, Oct 08, Karin Robbins, CA

49 20:58, Oct 08, JANE Reitz, CA

48 20:49, Oct 08, Eilyn Cole, CA

47 20:15, Oct 08, Mickie Robbins, LMFT, CA

46 19:16, Oct 08, cAROL SHERMAN, CA

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45 18:41, Oct 08, Tish Osborn, MN

44 17:49, Oct 08, MARY BETH NOBS, CA

43 17:21, Oct 08, Frederick Findley, CA

42 16:41, Oct 08, Veronica Juarez, CA

41 16:39, Oct 08, Chris Sherman, CA

40 16:26, Oct 08, Michael Dzedzic, OR

39 16:10, Oct 08, Erika Stone, VA

38 16:02, Oct 08, Jillyanne Michelle Cape, MO

37 15:55, Oct 08, Laura Telep, CA

36 15:54, Oct 08, Joni Kelly, CA

35 15:49, Oct 08, Karen Aydelott Robbins, CA

34 15:49, Oct 08, Emily Dzedzic, OR

33 14:09, Oct 08, Peter Vantine, MN

32 13:46, Oct 08, Paul Burke Journey Home, VA

31 13:34, Oct 08, Anne Meijster, MN

30 13:32, Oct 08, Liam Monahan, MN

29 13:03, Oct 08, Matthew Stern, MN

28 12:24, Oct 08, Name not displayed, CA

27 12:17, Oct 08, Marion White, NZ

26 11:48, Oct 08, Jennifer Kellogg, IL

25 11:32, Oct 08, Lisa Firestone, CA

24 11:25, Oct 08, JOSSIE ROSS, CA

- # 23 11:12, Oct 08, Mike Kellogg, IL
- # 22 11:04, Oct 08, Nathan Aydelott, IL
- # 21 10:48, Oct 08, Franziska Eber, DE
- # 20 10:35, Oct 08, Deborah Del Signore, IL
- # 19 10:30, Oct 08, Jane Meister, IL
- # 18 10:28, Oct 08, James Sieck, IL
- # 17 10:27, Oct 08, Emmett McCann, GB
- # 16 10:26, Oct 08, Elizabeth Walsh, IL
- # 15 10:26, Oct 08, Randy Illian, ME
- # 14 09:56, Oct 08, betty greenwell, KY
- # 13 09:56, Oct 08, Adrienne Giske Aydelott, MA
- # 12 09:51, Oct 08, Alfred Aydelott, MA
- # 11 09:40, Oct 08, Lynn Hamilton, CA
- # 10 09:27, Oct 08, Freddie Geisler, FL
- # 9 08:42, Oct 08, Name not displayed, NY
- # 8 08:28, Oct 08, Staci Galvin, WV
- # 7 07:01, Oct 08, John Tucker, NY
- # 6 06:47, Oct 08, susan marden, ME

- # 5 06:30, Oct 08, Dan cappello, PA
- # 4 05:39, Oct 08, Mieke Bernaards, BE
- # 3 05:36, Oct 08, Filomena Pereira, GB
- # 2 02:53, Oct 08, Jenny Lake, MA
- # 1 02:49, Oct 08, Sigrid De Ruyck, BE

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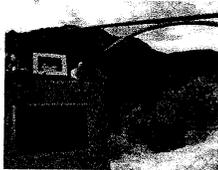
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- # 160 12:04, Oct 12, Jennifer Cosenza, MA
- # 149 11:36, Oct 12, Sarah Somers, NC
- # 148 11:00, Oct 12, Linda Jorn, MN
- # 147 08:50, Oct 12, Laurie Gilbert, CA
- # 146 08:49, Oct 12, Natasha Kirkham, GB
- # 145 08:37, Oct 12, Frederic Dick, CA
- # 144 08:11, Oct 12, gerald slavet, MA

<http://www.thepetitionsite.com/51/support-the-cold-spring-bridge-safety-barrier/>

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- # 143 07:56, Oct 12, Christina Powell, MT
- # 142 06:43, Oct 12, Majka Burhardt, CO
- # 141 21:39, Oct 11, Name not displayed, CA
- # 140 19:19, Oct 11, Ann Lippincott, CA
- # 139 18:23, Oct 11, Melissa Nemeth, CA
- # 138 17:04, Oct 11, JOHN ROBBINS, CA
- # 137 13:12, Oct 11, arnold slavet, MA
- # 136 12:39, Oct 11, kaye slavet, CA
- # 135 09:51, Oct 11, Name not displayed, IL
- # 134 09:19, Oct 11, Mary Villarreal, TX
- # 133 08:34, Oct 11, miida Bubiys, NY
- # 132 07:48, Oct 11, Lisa Sieck, IL
- # 131 07:36, Oct 11, Katie Osborn, NY
- # 130 07:14, Oct 11, joel rubinstein, MA
- # 129 05:23, Oct 11, Laura McGrath, MA
- # 128 23:12, Oct 10, Can Atik, TR
- # 127 18:16, Oct 10, Donna Murphy, CA
- # 126 18:15, Oct 10, Name not displayed, AW
- # 125 17:49, Oct 10, David K. Anderson, CA
- # 124 16:36, Oct 10, Mary B. Strauss, CA
- # 123 16:21, Oct 10, Marie DeSousa, CT

- # 122 16:09, Oct 10, JoAnn Kubick, CT

- # 121 16:00, Oct 10, Maggie Cox, CA

- # 120 15:33, Oct 10, Debra LOOMIS, CA

- # 119 15:20, Oct 10, Lance Dodes, MA
- # 118 15:18, Oct 10, Christopher Castaneda, IL
- # 117 14:50, Oct 10, Agnieszka Gal, IL
- # 116 14:49, Oct 10, Heather Murphy, WA
- # 115 13:35, Oct 10, Charles van der Horst, NC

- # 114 13:11, Oct 10, Dave Metz, MA
- # 113 12:50, Oct 10, Phyllis Pollack, MA

- # 112 09:47, Oct 10, Camille Rillorta, CA

- # 111 08:53, Oct 10, Marlo Cornejo, CA
- # 110 07:41, Oct 10, Michelle Nuehring, IA
- # 109 07:17, Oct 10, Amy Idzik, IL
- # 108 05:15, Oct 10, Lisa Rubinstein, MA

- # 107 02:29, Oct 10, Ralph Xx, DE
- # 105 02:18, Oct 10, Lindsay McCann, GB
- # 104 23:51, Oct 09, Panagiotis Rigopoulos, GR
- # 103 22:43, Oct 09, Nate Rogers, CA

- # 102 22:30, Oct 09, Pam Boland, GA
- # 101 21:58, Oct 09, diane bobich, CA

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**PUBLIC COMMENTARY,
EVIDENCE
AND
PETITIONS**

**IN SUPPORT OF
COLD SPRINGS BRIDGE
SUICIDE PREVENTION BARRIER**

**IN COMPLIANCE WITH
COURT-ORDERED CIRCULATION
OF THE SUPPLEMENTAL
ENVIRONMENTAL IMPACT REPORT**

Respectfully Compiled and Submitted by
Sally Hughes, LMFT and Sam Alfano to Cal Trans.

Friends of the bridge for life

Being a Knight of Columbus and a past Los Padres Forest Officer compels me to speak out on the tragic deaths that have occurred on Cold Springs bridge since 1964. Fifty five citizens have taken their lives at the bridge which has now gained notoriety through out the South Coast.

Some of the essential tenants of Knights of Columbus Founder Fr. MGivney dating back to 1882 were: giving aid to those in times of sickness or death, strengthening families and family life and being of service to the community to those most in need in society.

As a retired Outdoor Recreation Management Officer it is distressing to accept that the Forest setting surrounding the bridge has become a locale in which distraught individuals are taking their live.

It is sad commentary on our society when a solution is being proposed to prevent these deaths and the "Friends of the Bridge" represented by Mr. McGinnis are opposed to placement of the suicide barriers.

It is almost as though the "Friends" have a love affair with a steel and concrete structure. How can you compare the adulation of a man made structure to the loss of one life let alone the loss of 55 lives since the completion of bridge.?

The "Friends" say these suicidal individuals will take their lives elsewhere if the barriers are in place. This stance is extremely problematic. Taking the most callus view if individuals attempted to take their lives else where it would certainly minimize the impact on the lives of CHP, Sheriffs and Forest Service personnel who have to respond to suicide attempts at the Bridge with red light and siren at each and every call for assistance. In addition rescue personnel risk their lives as they climb in extremely rough terrain to remove the bodies of those who have taken their own lives. It is not a pleasant experience to remove a body that has fallen 154 feet.

One of the unbelievable assertions that the “Friends” make is that prevention measures would “waste tax payers money”. How can one equate any amount of money to human life? Are there those in our society whose mental processes have been so twisted by the dollar that all compassion for human life has been put aside?

The proposal is made by the “Friends” that better prevention measures would be by use of cameras and a voice box of some kind. In these dire emergencies seconds and minutes can make the difference in the attempt to save a precious life. Without the barriers this can only be accomplished with 24 hour personnel surveillance on the bridge. Recently an 18 year girl attempted to take her life by jumping off the bridge. It was a coincidence of fate that Deputy Sheriffs were driving by and noticed her on the bridge. They were able to convince her not to take the fatal jump.

The past loss of life and the potential for future deaths weights heavily on our society. It is imperative that the prevention measures be constructed as soon as possible

Sam S. Alfano
Retired Los Padres National Forest
Outdoor Recreation Management Officer
71 Lassen Drive
Santa Barbara, Ca. 93111

805-967-0595



August 19, 2010, 3:46 pm

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Courtesy Photo

Jon at the piano

Safe Passage over the Canyon

Cold Spring Bridge Suicide Barrier Will Save Lives

Friday, August 6, 2010

<http://www.independent.com/news/2010/aug/06/safe-passage-over-canyon/>

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By Sally Hughes

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I am a licensed therapist and the mom of two terrific Santa Barbara sons. My eldest, Jon, killed himself in a crisis moment, at the age of 23, at a perfect suicide spot along Cupertino's Highway 49 after his call for help wasn't heard. It was Cupertino's first and last suicide there. Immediately, the city took responsibility, redesigned the area, and eliminated a now obvious safety problem. Santa Barbara in contrast has done nothing for 40-plus years despite 55 suicides and many dangerous rescues from the Cold Spring Canyon Bridge.

As the yet-unpublished author of a book using Jon's life stories to teach readers how to save, sustain, and buoy life, I care about what Cold Spring Bridge represents and functions as.

At a [recent court hearing](#), part of a lawsuit brought by the group opposed to constructing a suicide barrier, I saw how these lawyers kept cherrypicking, citing the same few flawed and dated studies. They are legalistically misinforming the people and the courts on this issue, when so many recent and robust studies indicate just the opposite of what they are claiming. With this tactic, they are interrupting, as they have for 42 years, a vital and humane public safety project—one that preserves, defends, and implements our constitutionally given right to life, liberty, and the pursuit of happiness. Repeating the same error message over and over doesn't make it true, but does over time effectively make more people buy in and believe it. Now they have stopped construction of the suicide prevention barrier pending recirculation of the Environmental Impact Report (EIR) for full disclosure and public comment. The next hearing is scheduled for August 24, presided over by Judge Thomas Anderle, in Superior Court Department 3 at 9:30 a.m.

Though there are between 100 and 200 suicide attempts per actual suicide fatality, these lawyers continue to contend that suicidal people will kill themselves anyway, no matter what we do or how much money we spend to prevent it. That is absolutely untrue, as the majority of research on suicide barriers bears out. All well-known, easily accessed, quick and certain "final exits," such as our lovely bridge, have over the years become world famous for problems just like ours, exactly because they are "highly lethal."

Jumping from Cold Spring Bridge is to date 100 percent fatal. Most other means easily available to people are not as lethal. When people are depressed or in crisis, they scan for places just like our bridge which they identify and remember, for potential use, in a worst-case moment, to "end it all" simply, quickly, and surely. Research shows that most bridge "jumpers" are acting impulsively in a demoralized moment. Barriers either delay or prevent jumping, which gives them essential time to calm down, recalibrate, and get help.

We all live life in waveform, with alternating troughs and highs. When we're at a low point, that's precisely when things will in time begin to naturally rise, if we simply stop, breathe, and ride the wave. But people in crisis often stop thinking—are impulsive and panicky and lose all perspective.

That's why it's essential we don't continue to blithely and callously provide them Santa Barbara's well-publicized, dramatic, highly visible, and very lethal, simple and easy out. If between 99 and 199 people change their minds after an attempted suicide, why wouldn't Santa Barbarans support that? When shouldn't we as a community err on the side of compassion and humanity by doing what we can to preserve, cherish, and protect human life?

The one boy who was 18 and jumped after he got his first "B" could have in his life alone earned enough to cover the entire bill for the barrier, not to mention the other 54 people who have jumped to their deaths from the bridge. And the error each of them made is bound to repeat, and that number will only grow and grow *because* we continue to default. Just-released statistics showed that the number of suicides in Santa Barbara County practically doubled last year—from 34 suicides in 2008 to 60 suicides in 2009—a huge, nonrandom increase that the bridge's seven suicides definitely factored into.

Two days ago I spoke to a young waitress about this issue, and she sadly shared that her best friend's boyfriend was the 50th person to commit suicide from the bridge. She promised to buy every book I wrote on the subject to give to her friends.

The judge stopped work on the barrier on July 16. Days later, another person jumped to his death, and on July 24, a local news service reported deputies rescuing an 18-year-old Santa Ynez girl from perhaps becoming number 56.

What or who do you want to make sure you protect? There is no such thing as "doing nothing." In Australia, a man who lives across the street from a famous suicide drop has saved close to 200 people, simply by watching the bluff and, when he sees someone out near the edge, walking up and kindly asking, "Won't you come in and share a spot of tea with my wife and I?" Most of those he spoke to walked away with him to talk over tea and never returned to that edge. A man in China, I'm told, constantly walks a bridge doing the same thing with many rescues.

People in crisis are generally ambivalent: Most just need a compassionate connection and time takes care of the rest. One of the few to survive jumping off the Golden Gate, Gregory Hines, tells how the second he saw his hands leave the railing he fervently wanted to live; and realized that everyone who jumped felt the same way but it was too late. Now, he travels the country dedicating his life to speak on behalf of suicide prevention. I personally have intervened in four imminent suicide attempts, one happening in San Francisco while I was in Santa Barbara, and am happy to report that they are all living on well since, stably, with meaning, quality of life, and financial success.

Had I the chance, I would have done the same for my son.

Believe me, you'll be very clear on this answer if, God forbid, what happened to my son and my family happened to you. I remain, to stand in witness of what the world lost with Jon, which is why I'm writing my book. And Jon is just one person.

Bridges are constructed to get people safely across. Cold Spring Bridge was built with a safety design flaw, one that all of the fatalities there prove with their lives. About a foot separates the roadway from bridge edge—with its retaining rail just two feet and seven inches tall, and a 400-foot dropoff beyond. The bridge was not designed to accommodate pedestrians or bicyclists.

Still, they come and cross, in jeopardy whether suicidal or not, as is every driver who must avoid hitting them. The barrier will help prevent suicides in a desperate moment and protect pedestrians and bicyclists from a perilous passage, ensuring everyone's safe passage across.

Nietzsche says, "Whatever doesn't kill you makes you stronger." Bad moments are to survive, which does make us stronger but each of us will only know and appreciate how true Nietzsche's words are by living and looking back in retrospect. If we are to err, let's err on the side of protecting people's safe passage over dangerous chasms and life's low points.

That view from the bridge, which the "Friends of the Bridge" have for years obsessively focused on preserving, over life itself, is over in about 10 seconds, and will still be seen through and above the barrier's mesh. The EIR contains pictures that clearly show this, along with many research citations all proving that suicide prevention barriers save lives.

And there's a lovely place at the west end of the bridge to safely pull off and enjoy the view for as long as you like.

Comments

Discussion Guidelines

Thank you Sally.

It is quite simple: the barrier on the Cold Spring Bridge will save lives, at a bargain cost (yearly cost 1/400 of what is spent yearly on all County mental health services).

Many of the potential jumpers will not go on to kill themselves another way... your point that for every 1 successful suicides there 100 to 200 attempts is quite strong.

sevendolphins (anonymous profile)
August 6, 2010 at 7:16 a.m. [\(Suggest removal\)](#)

Thank you for this well written first hand explanation and account. I love this idea that people can be talked down with the offer of tea and conversation with a stranger, a simple social service. I think that although the barrier opponents are overly obsessed with the historical 10 second view, they also suggest that funds could be better spent with social services and perhaps a suicide prevention call box at the bridge. They could be right, I doubt it, because social services have a low ranking here in the U.S.A. Funding for hard-scape projects such as the barrier cannot be used for social services. Then there is this new pesky Toronto study that seemingly supports the barrier opponent opinion that barriers do not reduce suicide rates. I think the conclusion in the study could be wrong because as our local suicide rates went up, in the Toronto study the suicide rate was static. There could be other unaccountable reasons for the static number, including the barrier built there or perhaps someone new and unaccounted for in the study who is offering conversation and tea. Regardless I think considering all factors, including the minor view loss, the barrier should be constructed.

DonMcDermott (anonymous profile)
August 6, 2010 at 7:49 a.m. [\(Suggest removal\)](#)

FWIW, here is a link to the Australia guy:

<http://gawker.com/5563648/meet-the-au...>

stinkycatfish (anonymous profile)
August 6, 2010 at 9:19 a.m. [\(Suggest removal\)](#)

Beautifully, intelligently and calmly written, with a deep empathy for people in trouble.

I have read many comments by posters that persons wanting to commit suicide will find a way even if the bridge is not

Breakdown
 Total comments
 Suicide barrier 21
 Tea 13
 Con 4
 WA 3
 Break 2
 (McGarry et al.)

From: [REDACTED]
Subject: [REDACTED]
Date: [REDACTED]
To: [REDACTED]

----- Original Message -----

Sent: Monday, August 23, 2010 9:48 PM

The suicide barrier is not my personal pet project and I haven't really done anything for it except sign a couple of petitions...but I can't understand why anyone would spend energy working against it.

If he really wants to preserve nature, then I guess he wants to get rid of the road - that's right - a manmade object going through the middle of that beautiful nature. But he has no problem with the road, just the barrier. He wants his convenience without the responsibility.

It just reminds me of people screaming in St. Louis (my hometown) a few years ago because someone wanted to build a building downtown and it would block the view of our historic Arch from the highway. Some people were for it, some against, but no one realized that it's pretty darn pathetic when the best existing view of our city's crowning architectural achievement was from a moving vehicle.

If you love the view of nature so much get out of your darned car and the barrier won't block your view at all. But that's just my two cents. Sam Alfano (longtime Toastmaster) also had a letter in the Independent the week before you supporting the barrier. We're all with you.

-Shannon

[REDACTED]

[REDACTED]

Date: Tuesday, August 24, 2010, 12:12 AM

...Marc and I had a little back and forth on the Indy's website, check it out.

[REDACTED]



August 24, 2010, 11:56 am

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Criss-Cross

Wednesday, August 18, 2010

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<http://www.independent.com/news/2010/aug/18/criss-cross/>

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iPhone App!

I rode my bicycle over the bridge once, many years ago. But I realized that the wind from passing cars could easily force me sideways, and the railing was so low I could easily go over the side. I never rode across the bridge after that.

The bridge is about 1,100 feet long, and at speeds of 55 to 60 mph, that allows only about 11 to 14 seconds (or less) to look at the view. A driver on this narrow two-lane road would be unwise and unsafe to gaze at that view even momentarily. There is an off-road pullout near the bridge going west. A perfect and quiet and safe spot to stop and see the beautiful valley.

From reading the anti-barrier postings, I have come to the conclusion that what we have here is a bias against people who become depressed—and, as often happens, temporarily overwhelmed with a feeling of hopelessness and a monumental emotion of being worthless. The effort required to jump is minimal and the act is frequently impulsive. Yet depression is usually transitory and very treatable.

Someone said something about the effort being representative of a “nanny government.” I would remind them that before we had a “nanny government” we had no laws to protect workers, children, or the elderly, and most of the money was in very few hands. Perhaps the so-called Friends of the Bridge would volunteer to help the Sheriff’s Search and Rescue team retrieve the remains after the tragic suicides. It is not-so-pleasant duty, even traumatic.

It is imperative to finish the suicide-barrier project. This is about saving our humanity. The Friends of the Bridge could better spend their energy and time working for the environment and for life.—Tony Tonkin

Comments[Discussion Guidelines](#)

"most of the money was in very few hands"

That should not be in the past-tense.

[equus posteriori](#) (anonymous profile)

August 18, 2010 at 10:55 a.m. ([Suggest removal](#))

Thanks Tony. In a few days with luck the project will start up again, and we can look forward to preventing some suicides and tragedy.

[sevendolphins](#) (anonymous profile)

August 18, 2010 at 3:32 p.m. ([Suggest removal](#))

Whenever I'm driving across the Coldspring bridge, I'm too busy watching out for other drivers to gaze at the view!

Many northbound drivers often speed down the bridge due to the pitch and possibly impatience having just slowly climbed the pass.

So for me, that short stretch has always been about safety and not about enjoying the scenery.

[EastBeach](#) (anonymous profile)

August 18, 2010 at 9:25 p.m. ([Suggest removal](#))

Dear Tony,

The last sentence of your letter invites response, as it is a fact that most people who are affiliated with Friends of the Bridge do indeed spend their time and energy working for the environment and for life.

That is the reason we have been spending our time and energy over the past five years to prevent Caltrans from defacing the unique grace and beauty of the Cold Spring Bridge by constructing 10-foot-tall fencing barriers as an inferior means to saving the lives of suicidal people.

Caltrans has so far failed to give adequate consideration to live-saving suicide prevention plans that Friends and others have presented to it. The plan presented by Friends was designed by the former Director of the Office of Suicide Prevention in the New York State Department of Mental Health, and it has been working well since 2007 on five bridges in that state. Caltrans is trying to ignore it, and that is one of the reasons that it illegally attempted to move ahead with its more expensive and less effective fencing proposal.

BOTH saving lives AND preserving beauty are the goals of Friends of the Bridge, and we will keep on spending our time and energy to achieve this on the Cold Spring Bridge for as long as it takes.

Marc McGinnes
a Friend of the Bridge
and also
Founding President, Community Environmental Council;
Founding Exec. Dir., Environmental Defense Center;
Senior Faculty Member, UCSB Environmental Studies Program (1971-2005);
Founding President, Community Mediation Council;
etc.

marcmcginnes (anonymous profile)
August 19, 2010 at 8:37 a.m. ([Suggest removal](#))

For all your experience, Dr. McGinnes, you still shout at SBCAG:

<http://www.independent.com/news/2009/...>

and refer to the very serious and concerned citizens who want to save lives as the 'Nanny State in Drag'. It is just not credible that you have a deep concern about life or the suicidal; mainly you belittle serious people who have a serious concern.

I think you know very well that there is no evidence that the barrier is either more expensive or less effective than the New York system; certainly not the kind of evidence you and Garrett Glasgow have demanded for barriers. In particular, there is no evidence of a decline in suicide rates due to the New York system. Oddly enough, Toronto and Ontario suicide rates did decline after introduction of the Bloor Street Barrier; that is a simple fact. Yes, I understand that the decline could be due to other factors, but it could also be due to the publicity caused by the Bloor Street Barrier. No one will ever prove the case one way or the other.

It is hard to see your embrace of an unproven technique while trashing a better-proven technique as anything but duplicity, given your outbursts and name-calling.

sevendolphins (anonymous profile)
August 19, 2010 at 2:18 p.m. ([Suggest removal](#))

Dear Marc McGinnes and "Friends of the Bridge",

I am aware of your work on behalf of the environment. Work worthy of praise and support. In this situation I find myself on the "other side of the fence".

I am not familiar with the Director of the Office of Suicide Prevention in the New York State Department of Mental Health (do you have his/her name?)

I am convinced your proposals are not practical or efficacious.
Repeatedly retrieving bodies from under the bridge is not a positive for the environment.
The statistics that purport to show no change in suicide rates after barriers have been installed do show that suicides at places where barriers are installed drops very significantly in some cases to zero. How can you establish a cause effect for lowering suicides rates when the collection of data must be limited to a geographical area or population that is presumed to be the source of all or all the statistically significant demographic of potential suicides? Can you define

that population vis a vis the Cold Spring Bridge?...I cannot.
That population can change, expand and is at best difficult to define...not a very good predictor of saving lives.

Your statement about how effective the New York State suicide prevention projects have been, support the installation of a barrier on Cold Springs Bridge.

What is then in question is the view. It will be impaired but not eliminated. There are much safer places to view the valley and the bridge itself. This in places where the distraction of the view will not interfere with attention to driving, as others have mentioned.

Cold Springs Bridge is beautiful and the view breathtaking, however it is a 100% fatal place to end life. It is terribly unfortunate that such a gorgeous bridge and view is linked to suicides. The bridge has developed a notorious reputation and as I alluded to previously, those who might contemplate jumping can come from anywhere.

I have talked with a suicide survivor, someone who jumped off the Golden Gate Bridge, he is outspokenly supportive of barriers.

Often suicides are not planned but impulsive. A barrier will thwart that desperate impulse. Someone bent on ending their lives can choose another method. Something I have heard very often. That is true. Changing that is an important task for mental health and for all of us together. Suicide may be acceptable in limited cases where suffering is so great from terminal illness. That is a personal and philosophical issue. Even in cases of terrible depression, the prognosis for improvement is very good! Research in Neuroscience, Psychology, Psychiatry, and even in genetics and epigenetics suggest in the future there will be treatments much better than the ones available today.

Marc McGinnes Esq., I laud your accomplishments, but here we disagree.

Best wishes,

Tony Tonkin, Santa Barbara, California

ptt (anonymous profile)

August 20, 2010 at 12:50 p.m. ([Suggest removal](#))

Dear Tony,

The name of the former director is Gary Spielmann, and his letter to Caltrans is here:

<http://www.cscbfriends.com/pdfs/Spiel...>

I think his arguments are good... phones and human intervention *are* good things. But I don't really understand why he goes so far as opposing a barrier, particularly given that the Cold Spring bridge is so remote. The bridges in the burrows of NYC are much nearer to people who can physically go to those bridges.

Also, a jump off of Cold Spring is 100% fatal, while it is not as clear that jumps off of Spielmann's bridges are quite as fatal.

Finally, he doesn't really address the statistical efficacy of human barriers on the bridges where human barriers have been deployed. In particular, if there is a call box, how many jumpers ignore it? *When* people *do* pick up the phone on the call box, he says 53/54 did not jump. But how many jumpers did not even pick up the phone?

BTW, very interesting radio program on a major bridge with jumpers in Nanjing... human intervention certainly is a good thing!

<http://www.thisamericanlife.org/radio...>

sevendolphins (anonymous profile)

August 20, 2010 at 5:22 p.m. ([Suggest removal](#))

>>"Perhaps the so-called Friends of the Bridge would volunteer to help the Sheriff's Search and Rescue team retrieve the remains after the tragic suicides. It is not-so-pleasant duty, even traumatic."<<

I'll do that, if you volunteer to clean up after the fatal car accidents that will become more common after Caltrans diverts highway safety money to this boondoggle.

Pinatubo (anonymous profile)August 22, 2010 at 9:45 a.m. ([Suggest removal](#))

>>"No one will ever prove the case one way or the other."<<

LOL! So now what are we to make of the dozens of messages you've posted claiming that barriers are proven to work? Self-directed internet therapy?

Pinatubo (anonymous profile)August 22, 2010 at 9:57 a.m. ([Suggest removal](#))

You have snipped that comment out of its context, which was, the documented drop in total suicide rates in Toronto and Ontario after introduction of a barrier will never be proven to be due to publicity from the barrier or due to another cause. There is no statistically effective way to do that, which I have been arguing for quite some time; Glasgow tried and his technique was totally indecisive, which he tried to make into something significant, which his failure was not; his failure was entirely uninteresting.

If all suicides via bridge jumping per year stopped, that effect would not be big enough to explain the decline in total suicide rates in Toronto and Ontario; the rates from bridge jumping are just too low. However, publicity from the Bloor Street Barrier might have had an effect that was far larger than the direct saving of lives by the barrier itself.

Suicides from bridges in Toronto specifically *did* decline after introduction of the Bloor Street Barrier. The decline was not large enough to qualify as 'statistically significant', but the decline was more significant than an increase in building jumping; the increase in building jumping got a lot of publicity by anti-barrier folks. But the anti-barriers stayed silent on the more significant decline in bridge jumping after introduction of the barrier. An example of bias.

sevendolphins (anonymous profile)August 23, 2010 at 9:22 a.m. ([Suggest removal](#))

Dear Tony,

I and other Friends of the Bridge are guided in our efforts by expert opinion in the field of life-saving suicide prevention means.

The following statements are those of Gary Spielmann, former Director of the Office of Suicide Prevention in the New York State Department of Mental Health, and they were made specifically about the proposed fencing barriers on the Cold Spring Bridge:

"The message conveyed by a physical barrier (i.e. fence) on a bridge to a potential jumper is: don't jump here. The message that should be conveyed to a distraught person is: we want to help you now, so that you don't lose your life as a result of a temporary crisis. Advocates like the Glendon Association rely far too much on the efficacy of a structure to block the impulsive behavior of would-be suicidal individuals, and fail to appreciate its limitations and shortcomings."

"[Physical barriers] provide society with the impression that by installing a physical barrier, we have somehow addressed the needs of suicidal individuals, so we can continue to ignore the likely root problem - mental illness, which is probably treatable in a majority of cases. WE NEED TO UNDERSTAND THAT THE BRIDGE IS NOT THE PROBLEM. (emphasis added) The problem is the stigma, shame, and fear behind mental illness and the thoughts that surround suicide."

"[Physical barriers] do nothing to address the suicidal condition of the person who might be tempted to jump from the bridge. Unlike the live voice at the receiving end of a callbox, a physical barrier does not give a desperate person a reason to live or serve as a listening post for the real or imagined motives for being on the bridge at that point in time. By relying solely on an inanimate object to "save a life", an opportunity to identify and help a suicidal individual is lost."

"[Physical barriers] shift the risk of suicide, rather than manage it. A careful review of the literature on the efficacy of bridge barriers shows that their installation does not reduce the suicide rate in the surrounding geographic area."

It is very unfortunate that The Glendon Association was allowed by Caltrans to be in a primary advising role in this

case. Their limited expertise compares very unfavorably with the extensive experience of Mr. Spielmann, and Glendon's highly-partisan pro-barriers agenda should have disqualified it from acting in an advisory capacity.

In any event, it is not too late for Caltrans to turn away from its tall fencing barriers proposal and to adopt the superior human barriers alternative plan that we have proposed.

Thank you for your consideration,
Marc McGinnes

[marcmcginnes \(anonymous profile\)](#)
August 23, 2010 at 11:54 a.m. ([Suggest removal](#))

Mr. Spielmann seems not to care about the message that those Golden Gate Bridge jumpers, who, unlike himself, did actually try to commit suicide by jumping, but who survived give: the Golden Gate Bridge jump survivors say: build a barrier.

Nobody is relying *solely* on an inanimate object: we spend about \$40 million/year on County mental health services; the amortized cost of the barrier is \$0.1 million/year, a factor of 400 less.

Nothing stops us from building *both* a barrier and also putting outreach information/phones on the bridge *in addition*.

But leaving a loaded gun out under a sign that says 'don't use this gun to kill yourself, call the following phone number instead' would not be effective. Better not to leave the loaded gun out in the first place.

[sevendolphins \(anonymous profile\)](#)
August 23, 2010 at 1:47 p.m. ([Suggest removal](#))

LOL! Sevendolphins, you've conceded Glasgow's point without even realizing it.

More brilliant scientific analysis from the "PhD in statistics from Stanford." Thanks anyway, Annie.

[Pinatubo \(anonymous profile\)](#)
August 23, 2010 at 1:50 p.m. ([Suggest removal](#))

Yo-ho Sevendolphins,

Your comparison of the the spectacularly beautiful Cold Spring Bridge to a loaded gun left laying about in a public place makes an important point about mental health-- one that can be seen and considered carefully by you as you peer into your mirror.

Hoping that the getting-a-grip thing works out for you.

[southlander \(anonymous profile\)](#)
August 23, 2010 at 3:23 p.m. ([Suggest removal](#))

Yes, I agree that Glasgow made no point pertinent to any discussion of the efficacy of bridge barriers; he failed to achieve sufficient sensitivity to probe whether barriers are effective, ineffective, or irrelevant. He failed.

Many people find guns quite beautiful and artistic.

[sevendolphins \(anonymous profile\)](#)
August 23, 2010 at 5:17 p.m. ([Suggest removal](#))

Days after the "Friends.." got construction "stayed", a 52-year old man jumped, and days after that, an 18-year old girl was found and stopped from jumping by police happening to drive by. Without intervention, this long-festering, famous, 100% lethal bridge will never be known for just its beauty.

I find this irresponsible. There are between 100-200 attempts per suicide fatality. Most people in suicidal crisis who survive do NOT go on to kill themselves. Most other means for suicide are not as lethal as the bridge. Why give hurting people such a well-known, easily-accessed, instant and certain death?

For many years, Mr. McGinnes et al. have used courtroom tactics to prioritize a 10-second visual over life--a view that

will still be seen with the barrier in place. Their tactics delay a humane intervention that will save money, unnecessary crises, trauma and lives, all ensuring everyone's safe passage across a narrow and dangerous bridge, a bridge never designed for pedestrians or bicyclists.

"The Friends'..." proposed "human barrier" solution is no solution. Even suicidal people use cell phones. A callbox is nice but impractical. People in a suicidal moment are impulsive, irrational and desperate. If they even see a call box, would they stand there talking on it to an anonymous voice amid wind, noise and curious passersby? The "human barrier" is a misnomer; a more accurate term would be "remote policing": Neither "humane" nor effective.

The Bridge is a dangerous place. Pedestrians walk already in jeopardy, with less than two feet between car lane and low wall with 400' dropoff beyond. Too dangerous for drivers who go into crisis seeing someone walking knowing what's probably about to happen, yet still must drive safely and call 911 or make another choice.

By the time rescuers arrive it will likely be too late. Impulsive, desperate people in crisis usually are psychically prepared to step up and over. On the bridge with cars either coming or whizzing past, there's no safe place, space or room to sit, think, or talk intimately on a phone or to real rescuers.

Life trumps a pretty, 10-second visual. Drivers enjoy nine other panoramic vistas driving 154 from the top of the pass just to Paradise Road alone, not counting the bridge. Where on 154 or Santa Barbara isn't it scenic? So what do we really need to preserve here?

The barrier will ensure everyone's safe passage over the bridge with minimal visual impediment and send a vital message that Santa Barbara citizens' will act to protect human life.

Without effective intercession, this problem will not go away. Perhaps we should erect a plaque next to the one saying "Most Beautiful Bridge" with a Vietnam-type Memorial inscribed with the names of all lives lost there. Because BOTH are true. Except that the names will increase in number if McGinnes et al. have it their way, AND cost the county a FORTUNE in legal costs.

sbs (anonymous profile)

August 23, 2010 at 9:47 p.m. ([Suggest removal](#))

Sevendolphins (apologies to all dolphins):

I see that the get-a-grip thing is not going well for you. Your case shows that one can indeed overdose on one's own foolishness (a beautiful bridge is like a loaded gun).

sbs:

You state that opposition to this poorly conceived and badly bungled barriers proposal will "cost the county a fortune in legal costs," but you are just plain WRONG about this. The fact is that Caltrans is the one who is costing we taxpayers a fortune, including the lawyers fees and costs that have been incurred so far in the lawsuit in which the judge has ruled in favor of the Friends folks and against Caltrans.

Ask Caltrans how much taxpayer money has been poured into its misguided effort so far, or if you want the truth, add these numbers: \$2.6 million in staff time, plus \$1.5 million diverted from federal stimulus funds for construction costs, plus litigation expenses incurred by Caltrans in its losing effort to defend its decision-making process. What it adds up to is more than \$4 million and climbing, every cent of which is paid by we taxpayers.

It is said that while everyone is entitled to his own opinion, no one is entitled to his own facts. I sure wish that you two and other Caltrans groupies would stop trying to make up your own facts around which you can form your opinions,

southlander (anonymous profile)

August 23, 2010 at 11:19 p.m. ([Suggest removal](#))

sbs, the courts have already thrown out lawsuits brought by the families of jumpers, on the grounds that the jumper did not use due care on the bridge.

Pinatubo (anonymous profile)

August 24, 2010 at 12:41 a.m. ([Suggest removal](#))

>>"Yes, I agree that Glasgow made no point pertinent to any discussion of the efficacy of bridge barriers; he failed to achieve sufficient sensitivity to probe whether barriers are effective, ineffective, or irrelevant. He failed."<<

Oh, we're so close to the moment of revelation! You may, beyond all hope, almost understand the point that has been eluding you for three years now. Here's the next part in the tutorial:

Please name a study that *did* have "sufficient sensitivity to probe whether barriers are effective, ineffective, or irrelevant."

Pinatubo (anonymous profile)
August 24, 2010 at 12:48 a.m. ([Suggest removal](#))

Six surviving Golden Gate Bridge jumpers who support the Cold Spring barrier.

sevendolphins (anonymous profile)
August 24, 2010 at 7:10 a.m. ([Suggest removal](#))

Southlander:

Interesting response. That is not what I wrote, or even implied. You try to attack when you are really doing is projecting culpability. I am sure you read and understand English. Your response is so distorted and off-point, your comments prove why legal costs in America are astronomical and unjustified. It is our duty to protect human life in a 40+ year festering, clearly unsafe, dangerous and flawed bridge design problem. One that has already cost and will keep on costing this community incalculable millions and, if your legal likes have your way, on an on ad infinitum. And money is merely the collateral damage. What specious rationales and legal interference perpetuate are in this case unnecessary danger, crises and tragedy. Look down below the bridge at the graveyard there. It's as real as the view above. There, unseen, are the most egregious real costs and wages, all projecting far beyond each life lost to maim, jeopardize or destroy many, many living others. The true costs to the public and this community of continually having people with such special linguistic skills, tactics and agendas lobbying in judicial forums of power behind closed doors is what helps pathologize, corrupt and bankrupts us all.

I am sure you are a fine man, but in this cause and instance, consider me not impressed.

sbs (anonymous profile)
August 24, 2010 at 8:26 a.m. ([Suggest removal](#))

>>"Six surviving Golden Gate Bridge jumpers who support the Cold Spring barrier."<<

You're not even trying to be rational now.

Pinatubo (anonymous profile)
August 24, 2010 at 10:59 a.m. ([Suggest removal](#))

Post a comment

Username: **sbs** ([Log out](#))

Comment:

From: [REDACTED]
Subject: [REDACTED]
Date: [REDACTED]
To: [REDACTED]

----- Original Message -----

[REDACTED]
[REDACTED]

Subject: Letter to the INDEPENDENT

Tony,
I agree with all that you said in your letter regarding the safety barrier for the Cold Spring Bridge.

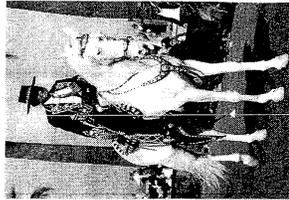
The next time there is a tragic death I propose that McGinnis and Chytilo be made to accompany the rescue team down to the bottom of the ravine to retrieve the body. Stop the nonsense and let CALTRANS complete its work.

[REDACTED]

Opinions
LETTERS

Who Goes There?

Being of a certain age, I usually resist the clichéd temptation to write letters to editors, but the oversight in your Old Spanish Days 2010 "Fiesta Horse Parade Lineup" photo caption cries out for redress. Identified only as "Parade participant (circa 1934)" is someone other than Don Adolfo Camarillo, family namesake of the city of the same name; the horse is Sultan, perhaps the most recognizable horse in the Fiesta parade for many, many years. This same horse and rider proudly represented southern California on opening day of the Oakland Bay Bridge two years later. —Charles Johnson, Ojai



The Love Economy

A time-proven economic theory is to encourage and facilitate many more people to fall in love! People in this blissful, euphoric state spend, on dates, every form of entertainment, travel, clothes, restaurants (and they tip more), exercise, motels and hotels, books. They give more gifts, give to charity, etc. The ripple effects and trickle-down effects come from these people exuding happiness, planet, and other beneficial traits, which impact remarkably everybody with whom they work, go to school, or recreate. This helps reduce societal stress and anxiety from the lingering effects

of the recession, which in turn spurs more hope, entrepreneurship, drive, ambition, and energy. Another byproduct of more people in love is that they make plans for a promising, bright future. They buy homes, furniture, cars, work harder, set goals, start new businesses. So assist anyone you can to fall in love, including yourselves. It's good for the soul, for health, and for the economy. —Alan Trattner, S.F.

Criss-Cross

I rode my bicycle over the Cold Spring Bridge once, many years ago. But I realized that the wind from passing cars could easily force

me sideways, and the railing was so low I could easily go over the side. I never rode across the bridge after that.

The bridge is about 1,000 feet long, and at speeds of 35 to 60 mph, that allows only about 11 to 14 seconds (or less) to look at the view. A driver on this narrow two-lane road would be unwise and unsafe to gaze at that view even momentarily. There is an off-road pullout near the bridge going west. A perfect and quiet and safe spot to stop and see the beautiful valley.

From reading the anti-barrier postings, I have come to the conclusion that what we have here is a bias against people who become depressed — and, as often happens, temporarily overwhelmed with a feeling of hopelessness and a monumental emotion of being worthless. The effort required to jump is minimal and the act is frequently impulsive. Yet depression is usually transitory and very treatable.

Someone said something about the effort being representative of a "nanny government." I would remind them that before we had a "nanny government," we had no laws to protect workers, children, or the elderly, and most of the money was in very few hands. Perhaps the so-called Friends of the Bridge would volunteer to help the Sheriff's Search and Rescue team retrieve the remains after the tragic suicides. It is a not-so-pleasant duty, even traumatic.

It is imperative to finish the suicide-barrier project. This is about saving our humanity. The Friends of the Bridge could better spend their energy and time working for the environment and for life. —Tony Tonkin, S.B.

Five Days of Pay

I got a rolling-stop ticket. It was early morning and no one was around for miles. The cop said he saw me look around and slow to 6.5 mph, not a complete stop. The ticket is \$380. I understand that cops give their family and friends [a break], also firefighters and people like that don't have to pay — but I have to work five days to pay this thing, probably to remodel the courthouse.

If the thieves don't get your money, the cops will. Stay in the house and watch TV. —Pam Rochell, S.B.

Tow-to-Tow

Congratulations to Russ Trenholme for coming up with the study showing that Santa Barbara is overrun with illegal alien drivers, and thanks to the Santa Barbara police for the enforcement of driving laws that help keep Santa Barbara safer. [News, "Towed Away," 8/12/10, independent.com/unlicensed.]

If we had a police force trained under 287(g) of the Immigration and Nationality Act [enabling local law enforcement to perform immigration law enforcement functions] or a law like the Arizona law, these people could be immediately arrested for deportation. Based on Trenholme's study, I have requested Rep. Lois Capps to see what she can do to get an immigration control and enforcement office established in this county to expedite federal enforcement. —Ted Solomon, S.B.

CONT'D PAGE 13

Opinions

LETTERS

Elders Are People, Too

I am deeply appreciative of *The Independent's* and particularly Nick Welsh's coverage of news concerning elders. Last week's article outlining problems with conservators [Voice, "Overseeing the Overseers," 8/5/10, independent.com/rosemped] gave examples of the need for transparency in this matter.

I have had years of experience in caring for elders in Santa Barbara. Even after an award as one of three caregivers honored by the State of California as "Caregiver of the Year," I was "fired" by a well-known fiduciary conservatorship outfit. I had spoken up on behalf of my hundred-year-old client.

Referring to the case of Patricia Rosen in last week's article: Why should she not have her own life back? She is now quite capable and has healed magnificently. Also, she has strong family support, especially from her son, Bryan. We do need citizen oversight on conservators.

—Judith Evered, Goleta

...

I agree with everything Bryan Rosen said in his article last week. These conservators of the elderly (and disabled) are not doing the job they are supposed to. They are taking advantage of their clients. Some of them are just like sharks, waiting for their prey.

—Veronica Birkland, S.E.

Compassionate by Nature

The same-sex marriage debate reminded me of the one between intelligent design and evolution. Gays want to feel natural but sometimes don't because they do not reproduce as

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Misuse of Forest

Being a Knight of Columbus compels me to speak out on the 55 tragic deaths, by suicide, at Cold Spring Bridge since 1964. One of the essential tenets of the Knights of Columbus is to be of service to those in the community who are most in need. In addition, as a retired recreation management officer for the Los Padres Forest, it distresses me that the forest has become a place where distraught individuals are taking their lives.

The Friends of the Bridge say these individuals will find other methods if suicide barriers are installed. This stance is extremely problematic. Taking the most callous view of it, if individuals attempted to take their lives elsewhere, it would certainly minimize the impact on the lives of CHP, sheriff's deputies, and Forest Service personnel who have to respond with red lights and sirens to each and every report of a possible suicide attempt at the bridge. In addition, rescue personnel risk their lives as they climb in extremely rough terrain to remove the bodies of those who have gone over the side. It is not a pleasant experience to remove a body that has fallen 154 feet.

Without barriers, suicide prevention at the bridge can only be accomplished with 24-hour surveillance. Recently an 18-year-old girl went there intending to take her life. It was a coincidence of fate that deputy sheriffs were driving by and able to convince her not to take the fatal jump.

It is imperative that the prevention measures be constructed as soon as possible.

—Sam Alfano, S.B.

[News Briefs, 8/5/10]. Why? Crisis is good for business.

Last week's crisis was the lack of taxpayer funding to pay for more research. This week is a three-year average decline in the spring population survey. Oh my!

The truth is that the survey method used to count the population is known to be off by 10 percent. This is because the otters may be underwater, in kelp, or affected by weather factors like wind, waves, and fog, which obscure visibility. This means that instead of the 2,711 animals reported, there would actually be approximately 2,982.

This "real" number is very close to the 3,090 needed to remove the animal from the Endangered Species Act list, and we can't have that, can we? That would be bad for the sea otter industry.

If the animals were delisted, would things change? Well, not much. In fact, things could improve for the animals. Delisting would increase management options for sea otters. Will this ever happen? Not likely in today's preservationist climate. Too many people who make a living off endangered species might lose their jobs. The majority of money raised to help sea otters really does little for the animals. Instead, it pays for all those who have carved out a comfortable living on the backs of the animals and on the backs of the coastal communities who used to supply fish and shellfish for our dinner tables.

Balance? What balance?

—Steve Reibick, San Luis Obispo

required by evolution: Biology recognizes sterility in nature, but only the fertile species survive.

The right to the religious tradition of bonding by marriage gives gays a sense of belonging to nature and the community. Their self-esteem is not supported by evolution, so they might look to intelligent design because it can alter nature, where evolution cannot.

Same-sex marriage is compassionate human nature, which is what intelligent design really is.

—Kenneth Moore, Montecito

New Improved Crisis

Just like clockwork, every year is a crisis for sea otters, and this year is no different

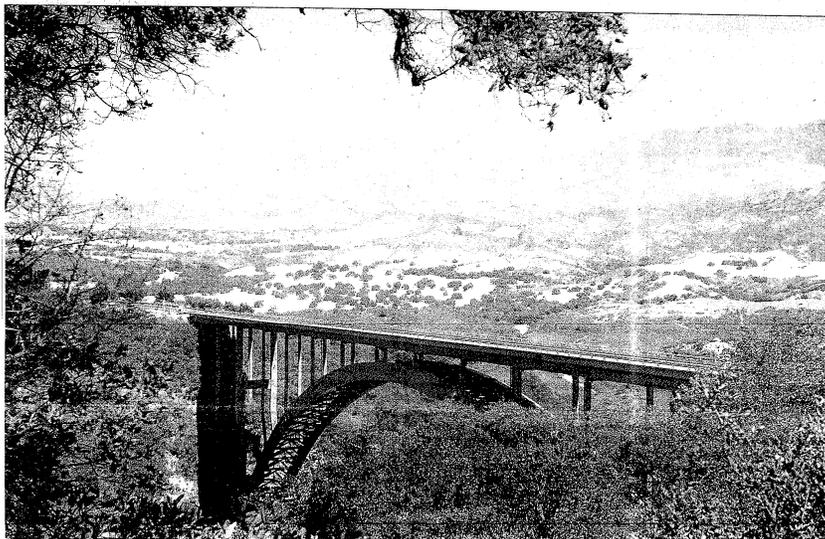
Voices

SUNDAY



**STAND UP,
BE COUNTED**

GUEST OPINION: Why it is important to have an accurate census.
Terry Tyler / **G2**



NEWS-PRESS FILE

Expanded bear hunts don't pass smell test

For too long, the California Department of Fish and Game has served the interests of the trophy-hunting lobby over the interests of the millions of California residents who do not hunt. Wildlife watchers in California outnumber bear hunters by a factor of 250 to 1. But nothing demonstrates just how far the agency will go to placate this narrow trophy-hunting constituency than new proposals to expand the range and the terms for killing black bears with packs of dogs in California.



Wayne Pacelle

The author is president and CEO of the Humane Society of the United States.

With these steps, the department is walking straight into an expensive and needless wrangle with voters — because rank-and-file citizens cannot, and will not, condone this kind of unsporting and inhumane mismanagement and may be left with no choice but another ballot initiative to bring the government to its senses.

Californians may remember that in the 1980s the department pressed to authorize a trophy hunting season for mountain lions, despite objections from the public. Wildlife protection and humane organizations eventually initiated a ballot measure, which voters approved in 1990 and then affirmed again in 1996. Unfortunately, this lesson appears lost on today's department regulators.

The department is calling for a significant expansion of the range open to hound hunting of bears, including starting a season in San Luis Obispo

Please see **PACELLE** on **G4**

TROUBLED BRIDGE

I am a Santa Barbara native and a mom. We lost our brilliant, 23-year-old son Jon in 1991, when he threaded his car into a perfect suicide slip inadvertently carved into a soundwall on Highway 49 by the Cupertino Corps of Engineers. It was the first suicide to occur here, and the last.

Cupertino earned my respect and attitude forever when its engineers immediately paved the area over so no other in a despairing moment could take a now obvious, easy, instant and irrevocable way out. Cupertino honored my son's death and prioritized life by correcting the problem.

It seems unbelievable that our beautiful city, my hometown, doesn't follow it.

Malcolm Gladwell, in his acclaimed book "The Tipping Point" addresses this exact issue. Even

one instance of suicide, by any means, he writes, can function as a "virus" that infects vulnerable others. Then he discusses its "stickiness" factor: how dramatic, memorable — indelible — the means is. Both precedent and its stickiness create a tipping point, and others follow.

Any publicized suicide and memorable means can merge to operate as a culture, according to Mr. Gladwell, a culture that lives to infect others, be remembered and used ever after.

Yes, the Cold Spring Bridge spans a beautiful view on the horizontal plane, but below lies a tragic, gruesome and growing ad hoc cemetery of souls who chose to end life at a desperate moment because others have gone before.

It's too easy, instant and certain. Suicide is called "a permanent solution to a temporary problem" for excellent reason. I can testify

that no suicide is a solitary act. Parents, families, friends and more get "infected" as well — pain continues, afflicting survivors — never the same again. At birth, each baby lives at the center of a surrounding, growing human galaxy. Losing their center, all orbits gone, the others implode, crash or spin out.

It's no accident that we have had the most suicides in Cold Spring Bridge history during the past year. Depression, crisis and hopelessness are common feelings in a jobless economy where health care is too expensive, terror is the media's favorite watchword, and war seems an endless, miserable opportunity to increase corporate coffers.

In America, murder is illegal. My son proved that suicide is murder — the wrongful, unnatural and premature taking of a life by anyone's hand. Americans are constitutionally entitled to life,

Please see **HUGHES** on **G4**



Sally Hughes

The author is a marriage and family therapist.

Voters declare independence, parties don't get it



Thomas D. Elias

The author is a longtime observer of California politics.

Something remarkable is happening in California politics, but the leaders of the two major parties don't seem to get it.

Voters are deserting the state's Republican Party in droves, but most are not going to the Democrats. Instead, they are declaring their independence and becoming an entirely unpredictable force capable of moving elections one way in a particular year and reversing their field the next.

The same thing is happening nationally, but the California secretary of state's voter registration report produced shortly before each statewide election demonstrates the phenomenon is at least as strong here as anywhere else.

Two recent election results reveal the enormous potential importance of what's happening: When the large majority of decline-to-state voters swung Democratic in 2008, they handed the presidency to

Decline-to-states have become so numerous that California might be back to unpredictable swing state status from this fall forward, as they move back and forth.

Barack Obama. But when the same category of voters went the other way in Massachusetts two months ago, Republican Scott Brown won the Senate seat held more than 50 years by John and Edward Kennedy.

Here are the new numbers that essentially render California a swing state, one that neither party can afford to take for granted even if some analysts persist in calling this a solidly "blue" Democratic state:

Democrats remain the largest party in this state, with about 7.5 million adherents, about 45 percent of all registered

voters. Republicans are second with 5.2 million registered members, or 31 percent. There has been some increase among Democrats since the last statewide primary election in 2008, but most of that bump came during the Obama voter registration drive of 2008 and no one knows how many of those new voters will turn out again.

But decline-to-states are up almost 600,000 over the last four years, rising from barely 18 percent of all voters to just over 20 percent. Anyone who doesn't think 600,000 voters can swing an election hasn't been paying attention.

And yet... the leaders of both major parties continue to oppose the kinds of changes independents usually like and vote for.

It was a joint effort of both major party organizations that killed the "blanket" primary California used for a couple of years after voters approved it in 1996 by a 59-41 percent margin. The two parties — which agree on few other things — also joined forces in 2004 to defeat an effort to set up a "top two" primary election system that would list all candidates together on the primary ballot, regardless of party, with the top two vote-getters advancing into the November runoff election.

A similar proposal will be on the ballot in June as Proposition 14, and once again the two major parties are working in tandem to defeat it.

They say they don't like this measure

Please see **ELIAS** on **G4**

LETTERS CONT'D

Thank you for this well-written, firsthand explanation and account [“Slate Passage over the Canyon, 8/6/10, independent.com/suicidebarriers/]. I love this idea that people can be talked down with the offer of tea and conversation with a stranger; a simple social service. I think that although the suicide-barrier opponents are overly obsessed with the historical 10-second view, they also suggest that funds could be better spent with social services and perhaps a suicide prevention call box at the bridge. They could be right. I doubt it, because social services have a low ranking here in the U.S.A. Funding for landscape projects such as the barrier cannot be used for social services.

Then there is this new pesky Toronto study that seemingly supports the barrier-opponent opinion that barriers do not reduce suicide rates. I think the conclusion in the study could be wrong because as our local suicide rates went up in the Toronto study, the suicide rate was static. There could be other unaccountable reasons for the static number, including the barrier built there — or perhaps someone new and unaccounted for in the study who is offering conversation and tea. Regardless, I think considering all factors, including the minor view loss, the barrier should be constructed.

—Don McDermott from online

Hoist This

Wow, Nick Welsh has outdone himself in his latest, politically directed “Hoist by El Perro’s Petard” [Angry Pooodle Barbecue, 8/5/10, independent.com/onenews/10/]. What a marvellously entertaining conjunction of irreverent double entendres, historical misatches, and mind-bending innuendos. Nick Welsh has obviously been “chewing” on the numerous, wildly improbable, but entirely possible novels by the all-time masters of same, Tim Dorsey (*Frammentary Ranch Motel*) and John Bardeen (*Ringside 8*). Or perhaps the hair of the fastest dog has merely served to “fertilize” his comedic creativity?

—Ron Hoys, S.B.

...

Nick Welsh’s article on the Santa Barbara City Council’s general plan amendments with regard to cars was certainly eye-opening. Dale Francisco’s statement that “You can’t make people behave the way you think they ought to behave, short of a totalitarian society,” and [Mitchell] Self’s

belief that the “emergence of the automobile is one of the top five reasons for American prosperity” are frightening in their lack of reasoning and foresight.

People in the South were “made to change their ‘behavior’ with the Civil Rights Act. Other examples of behavior changing would be the cigarette smoking laws, seat belt and helmet laws, and Americans with Disabilities Act laws. Locally, during drought years, we had water-use laws.

We used to get our fuel from whales, until they were hunted to extinction. We now use unrenewable fossil fuels to move people and products. We are not “prosperous” because we have cars; rather, we have cars, way too many, because we are prosperous and greedy and use way more than our share of finite natural resources.

Just as the 1935 earthquake gave Santa Barbara a chance to change its growth plan, the 2010 oil spill should make us all look at car dependence from a different perspective and adjust our general plan accordingly.

—Judy Keim, S.B.

...

These was reference to 30,000 daily commuters to Santa Barbara at a recent city council meeting, but that’s not correct. According to the staff of the Santa Barbara County Association of Governments, there are 29,000 commuters to the entire South Coast from outside it. Of those about 16,000 commute to the City of Santa Barbara.

Increased density in the City of Santa Barbara is not expected to take any existing commuters off the 101 freeway. It is expected to make it possible for a greater portion of workers filling new jobs in the city to be able to live in it.

—Shelia Lodge, S.B.

For the Record

¶ Due to an editing error, last week’s Voice, “Overseeing the Overseers,” said that a conservator/trustee was allowed to purchase shares in the conservatee’s property after the conservatee’s death. The conservative reports that she was, and is, alive and well.

¶ Santa Barbara Councilmember Michael Self has a collection of 10 cars (not 15, as reported in last week’s Angry Pooodle Barbecue, “Hoist by El Perro’s Petard”). Also, she is not a Republican. She “declines” to state a party affiliation on her voter registration form.

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d staffer earning that and many negations his own inside well as receive labs did earned a list in to fix in now. sed for one of his was in a staffers Mr. Deflecting accountability for her own office's violations of trust, Ms. Pelosi feigned sympathy for Mr. Massa, and attributed the transparen-

sexual assaults on his Navy underlings, Ms. Pelosi is pooh-poohing the scandal, "I have a job to do and not to be the receiver of rumors." Transition: Don't bother me with concerns about my members' indiscretions. I'm busy. How quickly we've accelerated from the "most ethical" House ever to "see no evil, hear no evil, speak no evil."

There was a time when Pelosi, the protector held House leaders to the highest standards and expectations in guarding young people working on Capitol Hill. During the GOP Mark Foley scandal, she investigated. The children who work as Pages in the Congress are Members' special trust. Statements by the Republican Leadership indicate that they violated this trust when they were made aware of the Internet stalking of an underage Page by Mr. Foley and covered it up for six months to a year."

Yet, she remains silent on the plight of the 20-somethings with whom Mr. Massa was keeping house under circumstances that rate an F- Factor of 10-plus. Mr. Massa's alleged targets are someone's children, too.

Deflecting accountability for her own office's violations of trust, Ms. Pelosi feigned sympathy for Mr. Massa, and attributed the transparen-

something to stop suicides

HUGHES
Continued from Page G1

liberty and the pursuit of happiness, not to aid and abet premature death, play God or treat people in pain as deserving to suffer or even die.

By doing nothing, we are culpable. Every time I drive over the bridge, I feel only pain and sorrow for what we haven't done that has by our default contributed to every life lost there. Instead of seeing a beautiful panorama, I see a hideous scene.

People assume that people committing suicide are mentally ill or lost causes. My son Jon was neither. He was a brilliant intellectual, temporarily depressed and in crisis. Like many people in personal crises, his call for help was not heard.

At the funeral, a distant, highly successful uncle came up to me and said: "You know, I only met Jon once, but after talking with him for five minutes, I realized that here really was someone who could save the world." His words stunned me because — they were true.

Had Jon only lived past that moment, he would have contributed to society at high levels in useful and powerful ways. Preventing just one suicide would more than pay the cost of an existing problem proven to Santa Barbara now 54 times over that is guaranteed to continue.

If Cupertino can see a problem and correct it, why can't Santa Barbara? We have the money, motivation and mandate to do so. God gave us life. It is not for us to take that away prematurely by passively standing by as another does the deed.

The "Friends of the Bridge" should err on the side of life itself. To conserve and preserve our inestimable, precious and diverse human landscape as much as those lovely vistas Santa Barbara is so famous for. I don't think it's an either/or situation. I think it's possible to do both, which Santa Barbara is known to do very well.

I implore my community to do something — something effective. The proposed call box is good, but not enough. Even suicidal people carry cell phones these days. If they just wanted to talk, they could do so at home and save themselves the ticket, towing and impound charges.

Stakeholders should have open mind about project

BEACH
Continued from Page G1

lot (nearest UCSB) and the underground sewer and utility lines that are at risk from coastal erosion. The parking would be replaced just off site with an interpretive trail and water taxi connecting to the park.

No one wants a sewage spill on Goleta Beach. The sewer and utility lines should be moved as far inland as possible to get them outside of the

kayak rentals, improved bike and pedestrian access and a larger beach.

The county's new approach promises to cost substantially less than the flawed groin project which would have required extensive monitoring, maintenance and frequent expensive sand replenishment to try to offset losses to down-coast beaches. With some relatively minor changes, the new plan for Goleta Beach — in concept — is acceptable

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A Plea About Helping Prevent Suicides

July 16, 11:49 AM Long Beach Christian History Examiner David Crammer



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Jon Hughes
Sally Hughes

Sally Hughes lives in Santa Barbara which is over 100 miles from Long Beach but suicide is a world wide problem. Sally's son Jon took his own life which devastated her and sent her on a mission to help prevent others from sustaining the same loss. Presently she is writing a book about what she has gone through as a result. Recently at a toastmaster's event she made the following plea.

Today at Unity Speakeasy Toastmasters, I spoke about how one or two environmental lawyers have skewed information (the alleged "Friends of the Bridge") to make people believe that prevention barriers are ineffective. I was at the hearing on Tuesday and witnessed what happened. As a licensed therapist and as yet unpublished author on the subject, I recognize how these lawyers keep cherrypicking and citing the same few flawed and dated studies to keep legalistically misinforming the people and the courts on this issue, when so many recent and robust studies indicate just the opposite. With this tactic, they have for 42 years and still are interrupting a vital and humane

public safety issue, one that preserves, defends and implements our constitutionally-given right to life, liberty and the pursuit of happiness. Now they have stopped construction of the barrier pending the people's full disclosure and comment. The next hearing is scheduled for August 24 presided by Judge Thomas Anderle in Superior Court Department 3 at 9:30 a.m..

Though there are between 100-200 suicide attempts per actual suicide fatality, these lawyers continue to contend that suicidal people will kill themselves anyway, no matter what we do, or how much money we spend to prevent it. That is absolutely untrue, and the preponderance of research bears this out. All easy, quick, known and certain "final exits", such as our lovely bridge has over the years become WORLD FAMOUS for are problems exactly because they are "highly lethal". Jumping from Cold Springs Bridge is to date 100% fatal. Most other means easily available to people are not as lethal. When people are depressed or in crisis, they scan for places just like the bridge which they keep in their "hip pocket" for a worse case moment to "end it all" simply and quickly. Yet we all live life in waveform, with alternating troughs and highs. When we're at a low

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Of Roses and the Path We All Travel



Wednesday, July 14, 2010
Going around to various churches to visit and talk to

Tragic suicides belie peaceful setting

Being a Knight of Columbus as well as a past Los Padres National Forest officer compels me to speak out on the tragic deaths that have occurred on the Cold Spring Bridge since 1964. To date, 55 people have taken their lives by jumping off the bridge, bringing it increased notoriety on the South Coast.

Dating back to 1882, some of the tenets of the Knights of Columbus, founded by Father Michael J. McGivney, are giving aid to those in times of sickness or death, strengthening families and family life, and being of service to those in need. As a retired outdoor recreation management officer, it's distressing to accept that the soothing forest setting surrounding the bridge has become a locale in which distraught individuals are taking their lives.

What a sad commentary on our society when a solution proposed to prevent these deaths is opposed. The Friends of the Bridge are opposed to the placement of suicide barriers for aesthetic reasons. It is almost as though the "Friends" have a love affair with a steel and concrete structure. How can one compare the adulation of a man-made structure to the loss of even one life, much less the atrocious loss of 55 lives, with eight in 2009 and one this month? The Friends argue that suicidal individuals still will take their lives, if not at the bridge then elsewhere, if the barriers are in place. However, it is precisely the isolated location of the bridge that serves to attract those desperate enough to take their lives. At other

locales, family, friends and even good samaritans are closer and have more context and immediate opportunity to intervene.

Another real cost of the bridge as it stands is the impact of each suicide on the lives of CHP, Sheriff's, and Forest Service personnel who must respond with red lights and siren at every call for assistance.

First, the emergency call increases the hazard on Highway 154, which is always a dangerous passage. Then, rescue personnel must risk their lives as they deal with the person in crisis, and/or must climb rough terrain to remove bodies. In performing these duties, the traumatic loss of each life weighs heavily on team members as well as on others.

One outrageous assertion the Friends make is that the proposed prevention measures would "waste more taxpayer money." How can anyone equate any amount of money to human life? Are there those in our society whose mental processes have been so twisted by the dollar that all compassion for human life has been put aside?

The proposal is made by the Friends that a better prevention measure would be the use of cameras and a call box. In an imminent life crisis, seconds and minutes are critical in saving a precious life.

The past loss of lives and the potential for future deaths dictate we as a caring community must correct this untenable situation.

Sam S. Alfano lives in Santa Barbara.

VOICE FROM SANTA BARBARA
Sam S. Alfano



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Report of Suicide

updated: Nov 13, 2010, 12:03 PM

By Roger the scanner guy

Possible jumper, who has possibly already jumped. Cold Spring Arch Bridge. When I hear you will too. Fire, medics, CHP on the bridge, Hwy. 154.

There is also an injury traffic accident at Valerio and State streets. Roger

I haven't heard anymore about this. On the CHP website it says attempted suicide. 11:53AM

I don't think this has to do with the Cold Springs incident but someone is being transported by chopper to Signature Air then to Cottage. * In the city there is a transformer on fire at 2217 Oak Park Lane.

Around 1:55 heard "Search and Rescue" say they were on top of the bridge, now they are going under it.

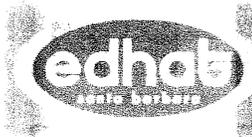
Haven't actually heard 1144 which would be a dead body, but could have missed it.

I haven't heard any more about this incident. But besides the bridge there were several overdoses of various substances over the weekend thus far, unknown if any of those people passed away. Many check the welfares for depressed suicidal people. The bridge is not the only way to end it.

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Edhat Fact: The fireworks on 4th of July lasted 19 minutes and 46 seconds. [\[more\]](#)

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Roger the Scanner Guy - 07/24/2010

Jumper on Bridge

updated: Jul 23, 2010, 8:14 PM

by Roger the scanner guy

UPDATE: There was another possible subject threatening to jump from the Cold Spring Arch Bridge an 18-year-old from Santa Ynez was also stopped at Vista Point. There was a mention of Bradford Dam, not sure if it had anything to do with this call or another. Have not heard whether or not the other suicidal person who left Goleta Valley Cottage Hospital has been picked up or not.

UPDATE: 1145 Subject was transported to Goleta Valley Cottage Hospital apprently she bolted 5'1 blue eyes, brown hair, blue jeans brown top last seen North from the parking lot toward Hollister, deputies are now searching for her and have notified CHP.

UPDATE: Sounds like the deputies got there in time, they are with the subject at Vista Point. All is code 4 (A-O.K.) Thank Dog for the police.

1145 Jumper- Cold Springs Bridge Sheriff's responding Code 3. Call came in from CHP they believe the woman is on the bridge at this time. Deputies there now no visual on subject will be checking. No vehicle associated with subject as I hear I will report. Roger

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RAFT OF SUICIDES ON COLD SPRING ARCH BRIDGE

By Jeremy Foster, Staff Writer

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Three suicides at Cold Spring Bridge have been reported by the Sheriff's Department within the last two weeks.

On Tuesday, Santa Barbara County Search & Rescue hoisted the body of an unnamed 26-year-old man from beneath the bridge, forcing law enforcement to confine traffic on the bridge to one lane. Witnesses contacted police at 4 a.m. after they saw the man's car stopped with a door open and lights on, Sheriff's Department spokesman Drew Sugars said. Rescuers had to wait until 8 a.m., when the weather got lighter and the fog lifted, before they went down, and they hoisted him up because of the wet, steep terrain.

The latest death marks the sixth suicide off the 400-foot-high bridge this year — a record number for any single year since the bridge opened — and the 50th since it was built in 1964.

The 1,200-foot-long bridge, which carries travelers from San Marcos Pass into the Valley, is currently the most lethal spot of road in five counties, according to the National Highway Traffic Safety Administration.

Caltrans plans to construct a suicide barrier on the bridge in the summer of 2010, a project that has spurred much debate in the community.

Before the latest suicide, a 28-year-old man from Santa Barbara jumped to his death mid-morning on Sept. 30. The coroner determined no drugs or alcohol were involved. Witnesses said the man

parked his car in the middle of the bridge before jumping.

A Ventura County woman was killed in the early morning hours Oct. 8 after jumping off the bridge. Just before 1 a.m., sheriff's deputies were notified that witnesses reported seeing a woman near the safety railing in the middle of the bridge.

When Deputies arrived, they found a car parked with its lights on in the northbound lanes in the center of the bridge. They also discovered a small footstool next to the railing.

Their investigation revealed a 59-year-old Santa Paula resident had climbed the stool with the assistance of two canes and jumped to her death.

Members of the Volunteer Sheriff's Search and Rescue team worked in the dark to recover the woman's body in the steep, treacherous terrain.

"We extend our heartfelt condolences to the victim's family," Sheriff Bill Brown said in a prepared statement. "This is a classic case where a suicide barrier would have saved a life and spared a family from immeasurable grief."

Barrier spurs more debate

Members of the Search and Rescue team, Sheriff Brown, the Santa Barbara County Association of Governments and The Glendon Association have spoken out in favor of the 9-foot high suicide prevention barrier.

Critics of the plan contend that it constitutes a waste of money, will ruin the scenic experience of driving across the historic bridge, and won't save lives because suicidal people will find other ways to end their lives.

Friends of the Bridge, which filed suit against Caltrans in July to stop the barrier, have offered an alternative "human barriers" plan to Caltrans that calls for call-boxes connecting to suicide prevention counselors; closed-circuit surveillance cameras to enable round-the-clock monitoring; and patrolling by law enforcement and transportation agency personnel as well as more training for law enforcement officers in dealing with suicidal behavior on bridges.

"We are saddened; nobody wants anyone to lose their life up there," said Marc McGinnes, a retired environment professor at the USCSB and head of the group. "The sooner that we come up with an effective program, we will have done the best that can be done. There is no guarantee."

Mel Blaustein, medical director of San Francisco's Saint Francis Hospital near the Golden Gate Bridge, said people often commit suicide impulsively and that a barrier can save people in extreme emotional pain.

A study Blaustein co-authored for the October issue of the American Journal of Psychiatry concluded that barriers at bridges in four cities — Washington, D.C.; Augusta, Maine; Bern, Switzerland; and Bristol, England — reduced suicides without an increase in suicides at other sites.

"These bridges and other places have a magic or a certain cache about them, so they become suicide magnets," he said. "In New York City, people aren't jumping off the Chrysler Building; they're jumping off the Empire State building."

The phenomenon doesn't allow for easy measurement, he noted.

"What we know is that the Golden Gate Bridge has a mythology that its less famous neighbor, the (San Francisco-Oakland) Bay Bridge, doesn't," he said. "And we see that in the number of suicide cases at each bridge."

Blaustein said the effectiveness of barriers is buttressed by a 1978 study by Richard H. Seiden of the University of Berkeley, which found that of 515 who attempted suicide from the Golden Gate Bridge between 1937 and 1971 and who were stopped, only about 10 percent of them



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Suicide bridge

*This article is about bridges used to commit suicide.
For Suicide Bridge in London, see A1 road (London).*

A **suicide bridge** is a bridge used frequently to commit suicide, most typically by jumping off and into the water below (because a fall from that height into the water is almost inevitably fatal).

The Golden Gate Bridge in San Francisco has had more suicides than any other in the world, "The Golden Gate Bridge is the world's No. 1 suicide magnet" the number currently being over 1,200. In 2005, documentary filmmaker Eric Steel set off controversy by revealing that he had tricked the bridge committee into allowing him to film the Golden Gate for months, and had captured 23 suicides on film for his documentary *The Bridge*. In March 2005, San Francisco supervisor Tom Ammiano proposed funding a study on erecting a suicide barrier on the bridge.



As a suicide prevention initiative, this sign on the Golden Gate Bridge promotes a special telephone that connects to a crisis hotline.

In Seattle, Washington, more than 230 people have committed suicide from the George Washington Memorial Bridge, making it the second deadliest suicide bridge in the United States. In the last decade, nearly 50 people jumped to their deaths, nine in 2006. , a nonprofit group dedicated to eliminating suicides from the bridge, is petitioning city and state officials to install a suicide barrier on the bridge.



Suicide hotline on the George Washington Memorial Bridge, Seattle, Washington.

The San Diego-Coronado Bridge is the third deadliest suicide bridge in the U.S., followed by the Sunshine Skyway Bridge in St. Petersburg, Florida.

The Cold Spring Canyon Bridge in Santa Barbara County has seen 54 jumps by suicide since opening in 1964, including 7 in 2009. A proposal to install a barrier on this bridge has brought intense debate.

More than 60 people have jumped to their death from the Van Stadens Bridge, near Port Elizabeth, Eastern Cape, South Africa.

A notable suicide bridge in London is the Hornsey Lane Bridge, which passes over Archway Road and connects the Highgate and Crouch End areas. The bridge provides views of notable landmarks such as St Paul's Cathedral and The Gherkin.

To reach such locations, those with the intention to commit suicide must often walk long distances to reach the point where they finally decide to jump. For example, some jumpers have traveled over the San Francisco - Oakland Bay Bridge by automobile to reach the Golden Gate., *The New Yorker*, October 13, 2003 p. 48

Suicide prevention advocates believe that suicide by bridge is more likely to be impulsive than other means, and that barriers can have a significant effect on reducing the incidence of suicides

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by bridge. One study showed that installing barriers on the Duke Ellington Bridge in Washington, D.C. – which has a high incidence of suicide – did not cause an increase of suicides at the nearby Taft Bridge. Families of victims and groups that help the mentally ill have lobbied governments to erect similar barriers. One such barrier is the Luminous Veil on the Prince Edward Viaduct in Toronto, Ontario, once considered the world's second most deadly bridge with over 400 jumps on record. The Sydney Harbour Bridge, The West Gate Bridge in Melbourne, the Jacques Cartier Bridge in Montreal, Quebec and the Colorado Street Bridge in Pasadena, California have also seen barriers erected.

Special telephones with connections to crisis hotlines are sometimes installed on bridges.

See also

- List of suicide sites
- Lover's Leap

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Edhat Fact: There were 15 car in the parking lot of Motel 6 on 6-6-6. [more]

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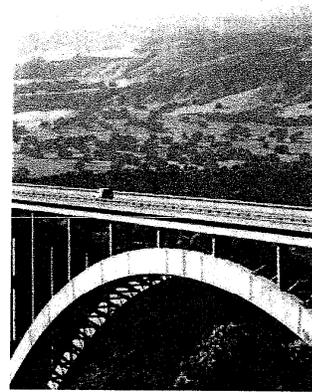
Fatal Attraction - Cold Spring Toll Hits (51)

by *William Etling*

An internet suicide this week outraged those who can't imagine watching as someone kills himself. Yet we all dink around for 44 years, while our neighbors throw themselves, one after another, into the Cold Spring abyss.

After a 29-year old homemaker from Carpinteria jumped from the new Cold Spring Bridge in May of 1964, it should have taken about ten seconds to drape enough razor wire on that baby to make San Quentin look like kiddie daycare. The bridge had opened that January.

Forty-seven suicides later, we're still discussing aesthetics. For me, forty-seven funerals trumps aesthetics. I wrote about the public push for this project over two and a half years ago, yet the 32 inch-high joke called a guard rail is still in place. We have laws that require fences around swimming pools because they're "attractive nuisances," while a bridge you could trip and fall off, into a 420-foot chasm, baffles our best minds.



The Cold Spring Arch Bridge's 32 inch-high joke of a guard rail is still in place.

Barriers work. [link] From a 2003 New Yorker article by Tad Friend: "The Empire State Building, the Duomo, St. Peter's Basilica, and Sydney Harbor Bridge were all suicide magnets before barriers were erected, as were Mt. Mihara, a volcano in Japan (more than six hundred people jumped into it in 1936 alone); the Arroyo Seco Bridge in Pasadena; and the Eiffel Tower. After barriers, the number of jumpers declined to a handful, or to zero."

People don't jump off secured bridges. And they don't necessarily move on to other means of self-destruction.

Dr. Richard Seiden of UC Berkeley's School of Public Health followed up on five hundred and fifteen people who were prevented from attempting suicide at the Golden Gate Bridge between 1937 and 1971. More than twenty-six years later, ninety-four percent of the would-be suicides were either still alive, or had died of natural causes.

Arthur Rubinstein, 20, attempted suicide in 1908 in Berlin, tying his robe belt to a clothes hook in the closet and jumping off a chair. He landed on the floor, as the hook pulled out of the ceiling. The world famous conductor died of natural causes in 1982 at age 95, after a brilliant career.

No one has ever survived a plunge from the Cold Spring Arch Bridge.

Commander Dominick Palera of the Santa Barbara Sheriff's Department never wants to see another suicide. Partly to save the life of the jumper, but also to protect his colleagues. "I don't want to see one of my people go off the bridge saving someone else," said Palera.

"All suicidal persons are ambivalent," said Joni Kelly of the Glendon Association, [\[link\]](#) where her mission is to save lives. "Suicide is a permanent solution to a temporary problem." Ms. Kelly noted that 1,200 people attended a memorial service for a teen who jumped. "Placing barriers will save lives, and it sends a message: This is a community that cares."

Psychologist Mary Ann Evans, PhD, points out that some "accidental" suicides are persons under the influence, who just need time to sober up.

Supervisor Brooks Firestone said that while initially skeptical, he has been swayed by "compelling" humanitarian and practical reasons to support a barrier. "It's a good investment," said Firestone, noting the long term financial and psychological costs for police and medical responders, rescue and recovery crews, and the effects of highway closures.

After decades of debate, and unknown numbers of lost lives, the board that controls the Golden Gate Bridge recently voted to erect a net under that deadly span. The estimated cost of the metal net is \$40 to \$50 million.

Writing in the New York Times in July, Scott Anderson told this story: [\[link\]](#)
"In September 2000, Kevin Hines, a 19-year-old college student suffering from bipolar disorder, leapt from the Golden Gate. Along with Ken Baldwin, he is one of only 29 known survivors of the fall. Today Hines controls his bipolar disorder with medication and a strictly controlled regimen of diet and exercise and sleep, even while maintaining a frenetic schedule. Having recently married, he is frequently on the road lecturing for a suicide-prevention network while simultaneously working toward a psychology degree. One of his most intense ambitions, though, is to finally see a suicide barrier erected on the Golden Gate.

"I'll tell you what I can't get out of my head. It's watching my hands come off that railing and thinking to myself, My God, what have I just done? Because I know that almost everyone else who's gone off that bridge, they had that exact same thought at

that moment. All of a sudden, they didn't want to die, but it was too late. Somehow I made it; they didn't; and now I feel it's my responsibility to speak for them."

On September 8, Matt Aydelott went over the thigh-high Cold Spring rail. A teacher at Cuesta College, he had developed educational and career opportunities for young people, and initiated a workplace readiness program for under-served high-risk students. A website honoring him lists an array of powerful arguments for the barrier. [\[link\]](#)

At 8:30 am on Thursday, November 6, the Santa Barbara County Sheriff's Department received a 911 call from a person who witnessed a man jumping, the fourth this year. Members of the Department's Search and Rescue Team inched through treacherous terrain beneath the bridge to reach the body of a 56-year old Santa Barbara man.

"We extend our sympathy to the family of the victim," said Santa Barbara County Sheriff Bill Brown. "This latest incident underscores once again the need for a barrier on Cold Spring Bridge to safeguard the lives of citizens and first responders alike."

A passerby talked down an 89-year-old Santa Barbara man who had one leg over the railing at 10:30 a.m. Nov. 18.

At this writing, CalTrans hopes to put up a proper fence in 2010. "This project remains on track at this time, with construction anticipated to begin in the summer of 2010. The estimated cost of the project so far, which includes environmental studies, design work and public meetings, is \$3.2 million dollars," said Caltrans public information officer Jim Shivers.

A few heartless, misguided dollar-worshippers are in opposition. Resistance is futile. This is a corporate budget; no one will get a refund check if the barrier is stopped. The money will just go elsewhere; perhaps a fast lane in Fresno, or some Bottdots in Barstow. Lawsuits won't help anyone.

Let's get this done, before someone else in pain appears on the list the Santa Barbara County Coroner has compiled of our fallen neighbors, friends, and children.

1. Homemaker, 29, Carpinteria, White Female, 5/5/1964
2. Homemaker, 38, Goleta, White Female, 4/14/1967
3. Realtor, 58, Goleta, White Male, 5/1/1967
4. Customer rep, 31, Santa Barbara, White Female, 5/30/1968
5. Secretary, 29, Santa Barbara, White Female 2/16/1969
6. Trucker, 48, Windsor, CA, White Male 4/13/1969
7. Pharmacist, 51, Santa Barbara, White Male, 6/23/1971
8. Secretary, 41, Santa Barbara, White Female, 2/8/1974
9. Bank Teller, 52, Santa Barbara, White Female, 5/25/1974
10. Dental Hygenist, 60, Santa Barbara, White Female, 3/15/1977
11. Shipping Clerk, 45, Santa Barbara, White Male, 8/1/1977
12. Technician, 41, Montecito, White Male, 9/9/1982
13. Farmer, 24, Goleta, White Male, 10/23/1982
14. Student, 20, Goleta, White Male, 12/5/1982
15. Student, 22, Pomona, CA, White Male, 5/12/1984
16. Farmer, 27, Arroyo Grande, Asian Male, 9/23/1984

17. Machinist, 26, Goleta, White Male, 9/2/1986
18. Homemaker, 39, Santa Barbara, White Female, 8/27/1990
19. Technician, 28, Goleta, White Male, 11/30/1992
20. Auto Mechanic, 41, Santa Barbara, White Male, 5/14/1993
21. Professor, 51, Santa Barbara, White Male, 10/29/1993
22. Forest Service, 22, Los Olivos, White Male, 12/9/1993
23. Restaurant worker, 19, Buellton, Black Male, 6/16/1994
24. Computer Programmer, 31, Santa Maria, White Male, 10/12/1994
25. Salesman, 71, Santa Barbara, White Male, 11/5/1995
26. Attorney, 66, Santa Ynez, White Male, 3/12/1997
27. Psychologist, 60, Santa Barbara, White Male, 3/17/1997
28. Farm Owner, 63, Goleta, White Male, 3/27/1997
29. Attorney, 32, Solvang, White Male, 8/23/1997
30. Teacher, 26, Santa Ynez, White Female, 7/8/1998
31. Water Conditioner, 32, Solvang, White Male, 8/28/1998
33. Consulting Engineer, 74, Santa Maria, White Male, 9/12/2000
33. Graphic Designer, 46, Santa Barbara, White Male, 3/16/2001
34. Construction worker, 35, Lake Forest, CA, White Male, 12/11/2001
35. Graphic Designer, 40, Los Osos, White Male, 12/31/2002
36. Program Director, 56, Santa Barbara, White Female, 4/21/2004
37. Carpenter, 37, Santa Barbara, White Male, 12/8/2004
38. Student, 20, Ojai, White Male, 12/30/2004
39. Disabled, 35, Los Angeles, White Male, 4/5/2005
40. Student, 18, Santa Barbara, White Male, 7/15/2005
41. Surgeon, 60, Santa Barbara, White Male, 2/17/2008 0715
42. Laborer, 44, Buellton, Hispanic Male, 8/18/2008
43. Teacher, 39, San Luis Obispo, White Male, 9/8/2008
44. Construction worker, 56, Santa Barbara, White Male, 11/6/2008
45. Jan 5, 2009 39-year-old Ventura man
46. April 4, 2009 45-year-old Santa Barbara woman
47. June 3, 2009 39-year-old Santa Barbara man
48. Sept. 30, 2009 28-year-old Santa Barbara man
49. October 8, 2009 59-year-old Santa Paula woman used two canes and a footstool to get over the edge.
50. October 13, 2009 26-year-old Santa Barbara man
51. October 27, 2009 58-year-old man from Toluca Lake, Ca.

Note: After this column was rerun on Edhat on Oct. 27, 2009, the Sheriff's Dept. contacted Mr. Etling and provided him with a revised list. The list that appears above reflects what they provided. Commander Dominick Palera stated that the previous list, which had 54 people on it, was not correct. "Most of the reports on these cases were old and had to be hand searched which showed we had misidentified 3 deaths as being the result of jumping from the bridge," he wrote.

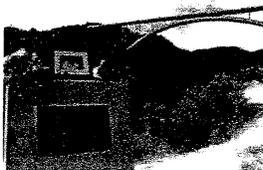
###

William Etling is a 42 year resident of Santa Ynez, and the author of Sideways in Neverland: Life in the Santa Ynez Valley.

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Stop the Tragedy at Cold Spring Arch Bridge



EIGHT DEATHS IN 2009.

*Support the construction of a safety barrier
to prevent suicides by jumping*

SIGN OUR ONLINE PETITION

1-800-273-TALK (8255)

more than 50 people have died at Cold Spring Arch Bridge.

Cold Spring Arch Bridge has the highest concentration of highway fatalities in Santa Barbara County, according to data from the National Highway Traffic Safety Administration's Fatality Analysis Reporting System.

On October 16, 2008, the Board of Directors of the Santa Barbara County Association of Governments voted unanimously in support of a proposed Caltrans project to install a safety barrier along the Cold Spring Bridge. This project has the support of the Glendon Association, the Santa Barbara County Sheriff's Department, the California Highway Patrol, Santa Barbara County Supervisor Brooks Firestone, Assemblymember Pedro Nava, and the Family Service Agency, as well as the leading national organizations dedicated to suicide prevention.

Details of this project, including the final environmental impact report, may be found at the Caltrans website: http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

Objections raised by opponents continue to delay construction of the barrier, resulting in further preventable deaths.

- **DO YOU NEED HELP?** If you are in crisis and contemplating suicide by any means, please call The National Suicide Prevention Lifeline is there to help. Your loved ones want you to live. Make the call.
- **Seven deaths this year** A man died at the bridge on October 27. Our thoughts go out to his loved ones during this difficult time. The tragic history of this bridge and its devastating effects on members of the community cannot be allowed to continue. Please help support the construction of a safety barrier to prevent suicides by jumping. Sign the online petition and write letters of support to Caltrans and local government representatives.
- **Three deaths in two weeks** A 26-year-old man died at the bridge on October 13. We extend our sympathy and condolences to his family and friends.
- **Fifth death in 2009** A 59-year-old woman died at the bridge on October 8. Her friends and loved ones are in our thoughts.
- **Another tragedy at the bridge** A 28-year-old man died at the bridge on September 30. Our thoughts are with his family and friends.

- **Third death this year** A 39-year-old man died at the bridge on June 3. We extend our sincerest sympathy to his family and friends.
- **Harvard School of Public Health campaigns for means restriction** The Means Matter campaign, established at the Harvard School of Public Health's Injury Research Center, aims to increase the proportion of suicide prevention groups who promote activities that reduce a suicidal person's access to lethal means of suicide. This includes endorsing the construction of suicide prevention barriers on bridges as well as limiting access other highly lethal suicide methods such as firearms. The Means Matter website provides detailed information about the effective implementation of means restriction for families and loved ones of potential suicide victims as well as clinicians and suicide prevention organizations. Please join us in supporting the Means Matter campaign in its efforts to save lives.
- **Another lost loved one** A 45-year-old woman died at the bridge on April 4. Those who were close to her are in our thoughts. Please help stop the tragic loss of life at Cold Spring Arch Bridge, and the suffering of friends and loved ones.
- **APA President advocates bridge barriers** In the April 3 issue of *Psychiatric News*, President of the American Psychiatric Association Nada Stotland, M.D., Ph.D., promotes the construction of barriers on bridges to prevent suicides by jumping. The APA chose not to use images of the Golden Gate Bridge on posters and programs for its annual meeting, to be held in San Francisco in May, because of the status of the bridge as a suicide hotspot. Stotland writes: "Let us use the largest psychiatric meeting in the world -- ours -- to advocate for suicide 'means restriction' at the Golden Gate Bridge and throughout the world."
- **2009 brings another death** A man jumped from Cold Spring Arch Bridge on January 5. Our thoughts are with his family and friends during this difficult time.
- **Another tragic death** On November 6, a 56-year-old man jumped to his death from Cold Spring Arch Bridge. We extend our deepest sympathy to his family and friends.
- **Triumph over tragedy!** At the public hearing in Santa Barbara on 16 October, the SBCAG Board of Directors voted *unanimously* to support the Cold Spring Bridge safety barrier project. The outstanding presentations by the Glendon Association, the Santa Barbara County Sheriff's Department, and 26 members of the public, as well as the many letters of support sent to board members and the signatures and comments on the online petition, helped to make this outcome a reality. Many thanks to all who contributed, and to Chairman Brooks Firestone and the SBCAG board for your support; your actions will make a real difference in the lives of others.
- **Petition:** A copy of the petition was given to all members of the SBCAG Board of Directors in advance of the public hearing. We have collected **over 500 signatures** so far, and the petition will remain online until the construction of the barrier has been completed for those who wish to show their support. Many thanks to those who have already signed. Please continue to circulate the details to others who want to help.
- The Glendon Association website has a comprehensive set of research articles on the effectiveness of safety barriers and other means restriction methods in preventing suicides, downloadable in PDF format. Please consult this excellent resource and read the experimental evidence in its original form. Special thanks to Dr. Lisa Firestone and the Glendon Association for all of their efforts in support of the Cold Spring Bridge barrier project.
- An article published in the *New York Times Magazine* reports interviews with survivors of attempted suicides by jumping, and reviews the evidence in favor of means restriction.
- **Triumph over tragedy at the Golden Gate Bridge: Managers vote to erect net to prevent suicides by jumping** (*New York Times*, October 10 2008; *Los Angeles Times*, October 11 2008)

Project
edit

The following facts support the construction of a physical safety barrier on the Cold Spring Arch Bridge:

- Numerous research studies have demonstrated that the installation of barriers significantly reduces or eliminates deaths by suicide at a particular location (Beautrais, 2007; Bennewith et al., 2007; O'Carroll & Silverman, 1994; Pelletier, 2007).

- No scientific evaluation of the effectiveness of alternative strategies such as human barriers – including the use of call boxes, video monitoring, or on-site patrols – has been carried out.
- The leading mental health organizations dedicated to suicide prevention support the construction of physical barriers on bridges to stop suicidal jumpers.
- Depression is a serious illness that results in irrational, impulsive behavior in the short term (Mann, 2003; Nock et al, 2008); suicide attempts are often immediately precipitated by stressful life events (Brent et al., 1993; Nock et al., 2008; Phillips et al., 2002; Vijayakumar & Rajkumar, 1999).
- The overwhelming majority (about 90%) of individuals who survive suicide attempts or are prevented from completing suicide do *not* commit suicide subsequently (Seiden, 1978).
- Individuals who are prevented from committing suicide by jumping have a *lower rate* of subsequent completed suicide than individuals who attempt suicide by other means (Seiden, 1978).
- A local historic landmark has become a monument to tragedy and grief.
- Law enforcement, safety, and search and rescue personnel are *unnecessarily* diverted from other emergencies and placed in potentially hazardous situations, at the expense of taxpayers, when responding to suicides and suicide attempts at this location.
- Individuals who have died at the bridge are not statistics, but people; the importance of these individuals to their families, friends, and communities, and their contribution to the quality of life in the local area, cannot be overestimated.
 - *Example:* Matt Aydelott, who died at the bridge on September 8, 2008, was a teacher at a local community college who developed educational and career opportunities for young people and initiated a workplace readiness program for underserved, high-risk students. His death represents an incalculable loss to the young people of California, as well as to those who knew and loved him.
- Continuing to allow preventable deaths to occur at this location is unethical and violates the public safety provision of the state constitution.
- Suicide prevention strategies, including the construction of a physical barrier, have been under discussion since at least November 2005. In the intervening period, debate about the barrier has brought additional media attention to the site as a highly lethal means of suicide.
- Since August 2008, as the debate has continued, no action has been taken to prevent suicides at Cold Spring Bridge, and ten further preventable deaths have occurred.

Opponents of the proposed project, including the so-called “Friends of the Bridge” organization, have raised a number of objections to the safety barrier based on false or misleading information. These objections and their

implications are addressed below.

X

- This type of claim is called *accepting the null hypothesis*, and it reflects flawed statistical reasoning; it is equally legitimate to argue that there is no evidence that suicide barriers *don't* save lives.
- Studies evaluating the effectiveness of safety barriers in the prevention of bridge suicides reveal no increase in suicides by jumping at other nearby locations following the installation of barriers (Beautrais, 2007; Bennewith et al., 2007; O'Carroll & Silverman, 1994; Pelletier, 2007),
- In a number of studies that have directly examined the overall suicide rate in a surrounding area following installation of a barrier at a particular location, the data show a *decrease* in suicides (by any method, including jumping at other locations), although this decrease does not reach statistical significance (e.g., Bennewith et al., 2007).
- As jumping as a method of suicide is relatively uncommon, failure to obtain statistical significance may simply be due to a lack of *power*, i.e., to the variance in the overall number of suicides over time (the change in number of suicides from one time interval to the next) being greater than the number of suicides by jumping in a given time interval.

X

- This claim implies that all suicide methods are equally lethal; in fact, jumps from the Cold Spring Bridge have a case fatality rate to date of 100% – far greater than other methods (average case fatality rate for all methods of suicide: 12%; Elnour & Harrison, 2008), meaning that lives could be saved, even if method substitution did occur.
- The method substitution argument ignores the selective nature of suicidal ideation in those who jump (many suicidal jumpers focus on a particular location, e.g., individuals who have driven across the Bay Bridge to commit suicide at the Golden Gate Bridge; Friend, 2003), and is not supported by statistical evidence.
- Most importantly, this argument ignores the human cost of suicide, and the ethical responsibility of a society to protect its citizens in the most effective way possible.

X

- “Choice” implies a reasoned decision made by a healthy person in a rational state of mind. Depression is an *illness* with a biological basis, which impairs rational decision-making and is associated with impulsive behavior.
- Evidence for the impulsivity of suicide by jumping comes from Seiden (1978), who measured subsequent attempts in a 25-year period following averted suicide attempts by jumping or other means. Suicidal jumpers were *less likely* to commit suicide at a later time than individuals who had attempted suicide by other means.
- As a society we have a responsibility to protect the safety of the individual, including (and perhaps especially) those made vulnerable by illness, using the most effective means at our disposal. The right of every individual to safety and the obligation of local government to offer such protection are clearly

identified in the state constitution of California:

- Article 1, Section 1: *All people* are by nature free and independent and have inalienable rights. Among these are enjoying and defending life and liberty, acquiring, possessing, and protecting property, and *pursuing and obtaining safety*, happiness, and privacy.
- Article 13, Section 35: The *protection of the public safety is the first responsibility of local government* and local officials have an obligation to give priority to the provision of adequate public safety services.

x

- The definition of a "suicide hotspot" provided by the National Initiative for Mental Health in England is as follows:
 - "A specific, usually public, site which is frequently used as a location for suicide and which provides either means or opportunity for suicide (e.g., a particular bridge from which individuals frequently jump to their deaths)."
- Cold Spring Bridge is a public site that has served as a *consistent* and *predictable* location for suicides for 46 years, resulting in a substantial loss of life in the local community. More deaths have occurred at Cold Spring Bridge than at any other single location in the Caltrans District 5 area during this time period (Caltrans DEIR, 2008). An analysis of traffic fatalities from 1994 to 2008 reveals that **no single mile of highway in all of Santa Barbara County is associated with a higher annual fatality rate than Cold Spring Bridge** (Fatality Analysis Reporting System, National Highway Traffic Safety Administration). The bridge has earned its reputation as a "suicide hotspot."
- Approximately 2.4% of all suicides in Santa Barbara County each year are the result of jumps from Cold Spring Bridge, which is consistent with the overall frequency of jumping as a means of suicide in the wider population (less than 4% of all suicides in the United States; Marzuk et al., 1992).
- Statistics citing a greater absolute number of suicides from other bridges in California are misleading, as they do not take into account the population of the surrounding area (e.g., six suicides per year at Coronado Bridge in San Diego County, population approx. 3.1 million, vs. one suicide per year at Cold Spring Arch Bridge in Santa Barbara County, population approx. 425,000).
- Defining "hotspot" in terms of the Golden Gate Bridge, a world-famous landmark and tourist attraction which is arguably the most popular location for suicide by jumping in the world, is disingenuous and invalid.

x

- In a joint statement, the American Association for Geriatric Psychiatry, the American Association of Suicidology, the American Foundation for Suicide Prevention, the American Psychiatric Association, Families for Depression Awareness, Mental Health America, Suicide Awareness Voices of Education, and Suicide Prevention Action Network USA, make the following declaration:
 - "As leading organizations dedicated to preventing suicide and ending the stigma of mental illness, we support *restricting the means of access* by which people die by suicide as a vital and effective approach for saving lives. This includes the *construction of suicide prevention barriers* on bridges such as those being considered for the Golden Gate Bridge. Numerous scientific

studies examining the effectiveness of bridge barriers and other means restrictions support this position.”

- The following organizations have explicit policies supporting the construction of barriers on bridges to prevent suicides:
 - *American Foundation for Suicide Prevention*: “According to AFSP-funded research and additional studies worldwide, prevention barriers on bridges have been effective at reducing suicide. Since *suicide by jumping tends to be more impulsive in nature* than some other methods of suicide, *barriers help prevent suicide* by providing suicidal individuals the time needed to change their minds, and to seek the treatment that might save their lives. AFSP *supports the construction of barriers.*”
 - *National Suicide Prevention Lifeline*: “The Lifeline Steering Committee position is that the use of bridge barriers is *the most effective means* of bridge suicide prevention.”
 - *American Association of Suicidology*
- The National Suicide Prevention Lifeline policy statement on bridge barriers may be found here.

X

- As noted above, a physical barrier is the *only* method that has been shown empirically to be effective for preventing suicides by jumping at a particular location.
- Restricting the availability of highly lethal means of suicide is the most effective method for reducing the overall suicide rate on a large scale (Hawton & Heeringen, 2000).
- Physical barriers restrict the availability of jumping as a means of suicide, either by deterring or thwarting suicide attempts by making these attempts physically difficult or impossible, or by increasing the amount of time and effort required to complete an attempt, thereby giving law enforcement and safety personnel greater opportunity to react.
 - *Example*: On at least one recent occasion, an individual was observed on Cold Spring Bridge by a passing motorist who reported the sighting to authorities. In the time it took law enforcement personnel to respond, the individual jumped from the bridge. In this instance, the delay introduced by a physical barrier might have been sufficient to save a life.
- No peer-reviewed study of the effectiveness of human barriers – including the use of call boxes, video monitoring, or on-site patrols – has been carried out.
- Human barriers have failed to reduce the number of suicides at other bridge locations, e.g., Coronado Bridge in San Diego (Caltrans DEIR, 2008).
- Human barriers do not restrict the available means of suicide, and place the entire burden of suicide prevention on law enforcement, safety, and support personnel.
- Exclusive use of human barriers presents an *unnecessary* increased risk to law enforcement and safety personnel (see below).
- Construction of a physical barrier does not preclude the use of human barrier strategies such as signs and call boxes. The National Suicide Prevention Lifeline policy report recommends the combined use of both approaches: “Rather than contrasting the effectiveness of these approaches, a strong case can be made for their complementary impact on suicide prevention if employed in tandem.”

X

- The actual cost-effectiveness of human barriers in the long term (e.g., the projected salary costs for individuals responsible for telephone and camera monitoring and foot patrols) must be comprehensively reviewed before a fair comparison can be made with the cost of installation and maintenance of a physical barrier.
- The full cost to the taxpayer of suicide attempts and completed suicides at this location, which make quantifiable time demands on law enforcement, safety, and medical personnel, must also be considered.

X

- This argument assumes the value of a human life can only be measured in taxpayer dollars.
- In fact, as a society we must also consider the ethical consequences of allowing preventable deaths to occur, and weigh our civic responsibility, as outlined in the state constitution, to protect the public safety of all citizens.

X

- To support this claim, the “Friends of the Bridge” organization cites data from a nationwide survey that was in fact intended to assess levels of *misinformation* and *incorrect assumptions* about the likelihood of substitution of suicide method and inevitability of suicide (Miller et al., 2006). This article in fact argues *in favor* of reducing method availability: “In light of the evidence that *the most dramatic reductions in suicide rates to date have resulted not from improvements in well-being but rather from reductions in the availability of certain lethal methods of suicide* (Hawton & Heeringen, 2000), efforts to inform public debate about suicide prevention efforts should include the *promotion of means restriction* as one among several approaches.”
- No formal opinion survey has been carried out in the Caltrans District 5 area to determine the extent of local public support for the project. Those who support the barrier must therefore make their views known to the Board of Supervisors.

X

- Risks to law enforcement, search and rescue, traffic management, and emergency personnel in responding to suicide-related incidents at Cold Spring Bridge include:
 - inherent dangers associated with crossing the bridge on foot in the absence of a safety barrier due to the narrow footpath, high speed of passing traffic, and heavy winds
 - inherent dangers associated with providing traffic control at the bridge during prevention and recovery operations
 - increased risk associated with approaching suicidal individuals whose behavior is unpredictable
 - posttraumatic stress associated with failure to prevent suicides
 - physical hazards encountered when locating and recovering bodies from the steep and heavily wooded terrain below the bridge, including risk from falling objects
- These risks are preventable by the installation of a physical barrier, and are therefore entirely unnecessary.

- The suggestion by “Friends of the Bridge” that minimizing unnecessary risk to those responsible for protecting the safety of the public is “coddling” is callous and offensive.
- The safety guidelines indicated in the alternative human barrier proposal suggested by “Friends of the Bridge” (e.g., “Do remain at all times in a safe position from which you can attempt to induce the person to move to you so that you can safely accept the person into protective custody. Don’t move out of a safe position in an effort to take the person into protective custody, as by grabbing, shoving, or similar means”) would minimize the effectiveness of the human barrier strategy.

X

- When viewed from the perspective of a motorist, the original bridge railing resembles a standard guard rail used on highway overpasses elsewhere in the state. The arch structure of the bridge is not visible to motorists on the highway.
- The bridge itself is over 400 feet tall; a six foot barrier therefore represents less than 1.5% of the overall height of the structure. The visual impact of the barrier would be minimal relative to the overall impact of the bridge from the perspective of an observer on the ground. (Please see DEIR photo simulations and architectural diagrammatic elevation sketches for illustration.)
- A motorist traveling at the speed limit traverses the full length of the bridge in a matter of seconds. The surrounding scenic view can be observed from other points on the route before and after the bridge.

X

- The only decisive facts available show that barriers *are* effective in preventing suicides at the location where they are constructed.
- No studies to date support claims of displacement or substitution of suicide method in response to the construction of a physical barrier, nor have any studies investigated the effectiveness of human barriers.
- The argument against the barrier is based on three considerations: (1) cost effectiveness; (2) possible disturbance to the “grace and beauty” of the bridge; and (3) possible obstruction of the scenic view from the bridge. Two of these three considerations are subjective and/or emotional in nature.
- The argument for the barrier favors taking the most effective steps toward protecting this location from continuing to serve as a monument to human tragedy.

■ **Sign the online petition at thepetitionsite.com**

■ **Post comments in support of the barrier on news media blog sites**

■ **Contact Caltrans representatives and declare your support for the proposed barrier:**

Sara von Schwind
 Caltrans Project Manager
 50 Higuera Street
 San Luis Obispo, CA 93041
 (805) 549-3198
 Sara.von.Schwind@dot.ca.gov

Colin Jones
Public Affairs Manager
Caltrans District 5
50 Higuera Street
San Luis Obispo, CA 93041
Colin_Jones@dot.ca.gov

■ **Contact members of the Santa Barbara County Board of Supervisors and declare your support for the proposed safety barrier (see sample letters):**

1st District: Salud Carbajal
Supervisor Salud Carbajal
First District Supervisor
105 E. Anapamu St.
Santa Barbara, CA 93101
Phone: (805) 568-2186
Fax: (805) 568-2534
E-mail: SupervisorCarbajal@sbcbos1.org
Website: <http://www.countyofsb.org/bos/carbajal/index.asp>

2nd District: Janet Wolf
Supervisor Janet Wolf
Second District Supervisor
105 E. Anapamu St.
Santa Barbara, CA 93101
Phone: (805) 568-2191
Fax: (805) 568-2283
E-mail: jwolf@sbcbos2.org
Website: <http://www.countyofsb.org/bos/wolf/default.aspx?id=3038>

3rd District: Brooks Firestone
Supervisor Brooks Firestone
Third District Supervisor
105 E. Anapamu St.
Santa Barbara, CA 93101
Phone: (805) 568-2192
Fax: (805) 568-2883
Solvang: (805) 686-5095
Fax: (805) 686-8133
E-mail: bfirestone@co.santa-barbara.ca.us
Website: <http://www.countyofsb.org/bos/firestone/index.asp>

4th District: Joni Gray
Supervisor Joni Gray
Fourth District Supervisor
511 E. Lakeside Parkway, Suite 126
Santa Maria, CA 93455
Lompoc: (805) 737-7700
Santa Maria: (805) 346-8407
E-mail: jgray@co.santa-barbara.ca.us
Website: <http://www.countyofsb.org/bos/gray/index.asp>



Michelle Arnerich
<marnerich07@yahoo.com>
01/02/2011 07:46 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject suicide prevention

Dear Matt,

Please support suicide barriers on the Cold Springs Canyon Bridge.

Thank you,
Michelle Arnerich



Aydelott and Associates
<alaarch@earthlink.net>

01/22/2011 02:57 PM

Please respond to
Aydelott and Associates
<alaarch@earthlink.net>

To <matt_c_fowler@dot.ca.gov>

cc <KarenAydelott@ymcala.org>

Subject Cold Spring Canyon Bridge

Dear Mr. Fowler,

I am attaching a copy of a letter that I have mailed in support of the proposed Cold Spring Canyon Bridge barrier. Thank you for considering my contribution to this important discussion.

Sincerely,

Alfred L. Aydelott

251 Boston Post Road



Weston, MA 02493 Cold+Spring+Bridge+Letter.pdf

Architects/Engineers
251 Boston Post Road
Weston, MA 02493
781.891.9837/Fax 781.899.3798

A. L. Aydelott & Associates Inc.

January 21, 2011

Matt Fowler, Senior Environmental Planner
Caltrans
50 Higuera Street
San Luis Obispo, California 93110

Re: Draft Supplemental Environmental Impact Report, Cold Spring Canyon Bridge

Dear Mr. Fowler:

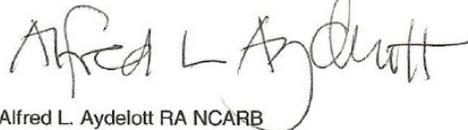
My nephew, Matthew Aydelott, was one victim of the fatal seduction of the Cold Spring Canyon Bridge. I hope that the modifications being considered for this structure will end its terrible legacy, and that other families will no longer have to endure the needless agony ours has.

I write as an architect, trained equally in aesthetics and in engineering. I am also chairman of my town's planning board and have been a member of its historical commission for over 25 years. I have read the Suicide Barrier Draft Supplemental Environmental Impact Report, and in my professional opinion, the Grid Mesh Preferred Alternative offers the best balance of the competing interests influencing the barrier design: Aesthetics, historical preservation, public safety, and financial viability. I believe that this alternative offers an effective deterrent at reasonable cost, while preserving the bridge's historical fabric and acceptably mitigating viewscape impacts.

I therefore urge adoption and implementation of this proposal. I am convinced that my nephew would be alive now had this barrier been in place, as would many of the others who have lost their lives at this tragically alluring place. Their families grieve for them as ours does for Matthew. He was a beautiful, remarkable, irreplaceable person whose loss was completely unnecessary. I appreciate the time, thoughtfulness, sensitivity and energy that has been devoted by so many to developing the preferred alternative, and now it is time that the issue finally be resolved.

Sincerely,

A.L. AYDELOTT AND ASSOCIATES
ARCHITECTS AND ENGINEERS, INC.



Alfred L. Aydelott RA NCARB
President

ALA/jpd



Karen Aydelott
<kaydelott@aol.com>
01/20/2011 11:45 AM

To <Matt_C_Fowler@dot.ca.gov>
cc
Subject Re: Cold Spring Canyon Bridge Suicide Barrier Draft
Supplemental Environmental Impact Report

Dear Mr. Fowler,

Please accept the attached letter regarding the Draft Supplemental Environmental Impact Report for the Cold Spring Canyon Bridge. This letter contains my comments related to a thorough reading of the the Environmental Impact Report and after having attended the January 5, 2011 public hearing in Santa Barbara as requested.

Thank you for your consideration and that of Caltrans of my comments.

Karen Aydelott



January 19 EIR letter.docx

January 19, 2011

Mr. Matt C. Fowler, Senior Environmental Planner
California Department of Transportation
Central Coast Environmental Analysis
50 Higuera Street
San Luis Obispo, CA 93401

Re: Cold Spring Canyon Bridge Suicide Barrier Project

Dear Mr. Fowler,

My name is Karen Aydelott and I am the mother of Matthew Aydelott who sadly took his life on September 8, 2008 from the Cold Spring Canyon Bridge. He was the 46th person to do so and at least 10 more have done so since that time. I became aware of the efforts to build a suicide barrier for the Cold Spring Canyon Bridge soon after Matthew's death.

Not only do I believe that such a barrier could well have prevented Matt's death, I believe that such a barrier could save many lives. Matthew was a successful and contributing member of the San Luis Obispo community through his work at Cuesta College where he changed many lives for the better and has left a wonderful legacy that continues today. From what we know Matthew was in a sudden crisis, as are many people who when in such a frightening and anxiety ridden state think that ending their life is the only way out. Multiple studies show that barriers such as the one proposed for the Cold Spring Canyon Bridge do work in diverting the individual and allowing that person to seek help. When considering the efficacy of such a barrier at a location now well known as a location for multiple suicides, we must also keep in mind that jumping from the Cold Spring Canyon Bridge is always fatal.

Given my bias in support of the suicide barrier for the Cold Spring Canyon Bridge, I still would like to address the Draft Supplemental Impact Report and the Court Ruling requiring additional study and opportunities for public input. I have read the Report and I attended the January 5, 2011 public hearing to be thoroughly familiar with the issues surrounding the visual, aesthetic and cultural impacts that could result from building the suicide barrier and what has been proposed to mitigate those impacts.

When I made the decision to move to San Luis Obispo in 1996, it was in great part due to the beauty of the Central Coast and the Santa Inez Valley. My son Matthew followed me shortly thereafter for many of the same reasons. Before recently relocating to San Luis Obispo, I worked Pasadena for several years and had reasons to make many trips on Highway 154 and to cross the Cold Spring Canyon Bridge. I appreciate the beauty of the view and as an art history graduate of Wellesley College I certainly appreciate the cultural and historical value of this bridge as an outstanding and historically important example of a steel arch bridge design.

However, I am convinced that Caltrans after the distribution of the Draft Environmental Impact Report/Environmental Assessment and with the able assistance of the Aesthetics Design Advisory Committee has chosen a design for the barrier which minimizes the visual impact by choosing the grid/mesh version and using a low-reflectivity finish to reduce glare. In addition, the barrier chosen creates minimal permanent impact to the bridge itself should the barrier ever be removed.

When considering the study and work that Caltrans has done to satisfy all environmental concerns related to this project, I am drawn to the statement regarding the establishment of the National Environmental Policy Act of 1969 (the year my son, Matthew, was born) that “the federal government use all practical means to ensure all Americans safe, healthful, productive and aesthetically (emphasis added) and culturally pleasing surroundings”. When deciding whether or not to construct this very vital barrier in a beautiful location on a culturally significant bridge shouldn't we also weigh the requirement to protect the safety and health of our citizens? My concern is not just for those who may consider ending life but also for those involved in the dangerous rescue and/or recovery operations.

How can it be that a slightly diminished view for 10 seconds of a spectacular 32 mile drive and a view from below the bridge that is visually no different with or without the barrier are not worth the saving of even one life?

It is my fervent hope that the environmental phase of the process required by CEQA and the courts will be declared complete and that the construction of the suicide barrier for the Cold Spring Canyon Bridge will begin before another life is lost.

Thank you for your consideration.

Respectively,

Karen Aydelott
524 Bluerock Drive
San Luis Obispo, CA 93401



Jo Barrington
<jobarr61@yahoo.com>
01/07/2011 01:13 PM

To <matt_c_fowler@dot.ca.gov>
cc

Subject Suicide Barriers on the Cold Springs Canyon Bridge.

To whom it may concern:

Providing suicide barriers on the Cold Springs Bridge is of the utmost importance. As someone whose life has been touched by suicide, I know first hand the importance, the life-saving importance to a person who feels hopeless, of putting time between the impulse to commit suicide and the ability to do so. Just that amount of time can turn a person around, can allow them to come to grips with the state of their own mind and possibly reach out for help. There are many among us who would not be here today, who would not be contributing to our community, who would not be sitting next to us at work offering a smile or a joke, who would not be offering their children love and support, if it had not been for just that kind of time, just that bit of time that made them stop and rethink their actions, their options. I think this is the only sane and humane decision to be made; to construct the barriers. Ironically these barriers will improve the view from the bridge, as the view will no longer be able to be marred by tragic, preventable deaths.

Sincerely,
Jo Barrington



Bonnie Beedles
<bonniebeedles@gmail.com>

To <matt_c_fowler@dot.ca.gov>

cc

12/17/2010 03:03 PM

Subject: Support for Suicide Barrier on Cold Springs Bridge

Dear Mr. Fowler:

I am writing to express my strong support for construction of a suicide barrier at Cold Springs Bridge.

As a lifelong Santa Barbara County resident who has worked with and known people who are or have been depressed and suicidal, I know that Cold Springs Bridge is viewed as a fairly easy option for people in the area considering suicide. Handguns are not readily available, pills are fallible, hanging and cutting are gruesome, but jumping from a high building or bridge seems easier to a lot of people who consider suicide. There are not many other structures that are high enough in our area to ensure death, and so Cold Springs Bridge is an important structure that should be safeguarded and removed as a suicide option. I personally know two people who jumped to their death from that bridge, and both of these people might be alive today if that option had not been available to them.

Please do what you can to ensure that the suicide barrier moves forward. Those opposing the barrier have been very loud, but they are wrong on the facts--that barriers do save lives.

Thank you,

Bonnie Beedles
589 Los Feliz Dr.
Santa Barbara, CA 93110



Ann Bennett
<annbennet@gmail.com>
01/04/2011 12:58 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Spring Bridge Barrier

Dear Mr. Fowler;

I support the Bridge Barrier. It will save lives.

As is, the bridge is a magnet for distressed, (likely temporarily) people.

I have read the related research and have been a local resident, living near the bridge, for 19 years, and have followed the trend toward increasing numbers of jumpers.

The trauma to safety and rescue workers, not to mention the families of victims, is horrible.

I hope reason will prevail against the small number of well-known attention getters, and the bridge barrier will proceed as planned.

thank you,
Ann Bennett



Alisa Goughnour-Benson
<alisa_benson@sbcglobal.net>
01/23/2011 05:11 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject comment on EIR for Cold Spring Bridge Suicide Barrier

Good morning Matt,

Please find attached a letter from me supporting the current EIR for the Cold Spring Canyon Suicide Barrier Project.

I am a friend of Karen Aydelott, but I have also known her son, Matthew, and one other friend of mine who took their lives on this bridge.

On top of these two tragedies, I also know someone who came very close to taking their life on the bridge. This person wants to remain anonymous. Upon asking, this person said that a barrier would have ruled out their contemplation of the bridge and it being a "desirable" location to take their life.

Any temporary loss of view (10 seconds?) must be weighed with the potential of saving lives and other risks.

Thank you,



Alisa Benson caltrans letter.docx

January 20, 2011

To: Matt Fowler, Senior Environmental Planner
Caltrans District 5
50 Higuera Street
San Luis Obispo, CA 93401

From: Alisa Benson
823 Bougainvillea St.
San Luis Obispo, CA 93401

Re: Cold Spring Canyon Bridge Suicide Barrier

Dear Mr. Fowler and all concerned,

This is a letter in support of the Draft Supplement Environmental Impact Report by Caltrans for the Cold Spring Bridge Suicide Barrier. Caltrans has completely studied and researched this project to best improve and prevent accidents at this site.

The pattern of suicides and accidents at the Cold Spring Canyon Bridge is the highest concentration of fatalities in District 5. The proposed barrier at this site would be the most effective safety improvement in order to prevent further accidents and fatalities, plus decrease the risk to rescue personnel and the public overall. I believe the visual/aesthetic impact to be minor. The proposed barrier will not detract from the beauty of the area. Crossing the bridge in a vehicle lasts only a few seconds and stopping on the bridge is not allowed.

The alternative catch net idea, as some are suggesting, will not prevent accidents and will continue to result in accidents, as well as search and rescue efforts that will elevate the associated risk and costs with a catch net barrier. I agree with the Caltrans research that the particular structure of the Cold Spring Canyon Bridge and installing the continuous and high barrier will be the safest and most effective project for all concerned.

Please support and move forward with Cold Spring Canyon Bridge Suicide Barrier Project under the currently proposed specifications. The safety of the public and the prevention of further tragedy depends on moving the project forward as designed. Caltrans has completed thorough research for the best solution.

Sincerely,

Alisa Benson



Jeff Bermant
<JB@bdcdevelopment.com>
12/30/2010 09:56 PM

To <matt_c_fowler@dot.ca.gov>
cc

Subject Support for suicide barrier

Mr. Fowler,

This email is in support of a suicide barriers to be erected on the Cold Springs Bridge . I have lived in SB since 1980 and have too often open the paper and found that a person used the bridge as a means of suicide. In fact in the mid-eighties a former tennis player I knew in took his life there.

I know many people are concern about the looks of the barrier . However, safety should always outweigh how something looks. But if this is a concern than at minimum a safety net should be erected below the bridge that hopefully would stop a fatal jump .

Thanks,

Sincerely

Jeff Bermant
5383 Hollister Ave.
Santa Barbara, CA. 93111



Nancy Black
<nancy@mercurypress.com>
Sent by:
<nancielena@gmail.com>

To <matt_c_fowler@dot.ca.gov>
cc
Subject Suicide barriers

12/30/2010 03:53 PM

Please respond to
<nancy@mercurypress.com>

I want to let it be known that I support adding suicide barriers to Cold Springs Bridge. I have a friend who threatened to drive off that bridge. I've lost five loved ones to suicide. That bridge invites suicide attempts. Please make it more difficult to satisfy that impulse.

Thank you,
Nancy



Nancy Black, Editor
Mercury Press International
405 Santa Anita Rd., Santa Barbara, CA 93105 ofc) 805-569-1559 www.mercurypress.com



Rene Block Baird
<rene@arnettgunson.com>
01/05/2011 07:21 AM

To <matt_c_fowler@dot.ca.gov>
cc
Subject barrier for cold springs

Please please enforce the cold springs barrier to be constructed if it is not already. I was told that e-mailing you my support will help in this project being in force. We need to do anything that will prevent harm to lost souls that is encouraged by what the bridge currently stands for.
Thank you so much!

Rene' Block



"Campos, Adriana"
<Acampos@co.santa-barbara.ca.us>
01/05/2011 01:47 PM

To ""matt_c_fowler@dot.ca.gov"" <matt_c_fowler@dot.ca.gov>
cc
Subject RE: Suicide Barriers on the Cold Springs Canyon Bridge

I am a life long resident of the County of Santa Barbara born and raised here since 1972 and as far back as I can remember, even crossing the bridge by vehicle, this is a very dangerous spot for suicidal people.

I would like to encourage and request that this project please continue for the safety of our community. This is a beautiful area but very dangerous.

Please listen to this request; we don't want anymore victims of suicide here; please! This is my petition for this request.

Thank you,

Adriana Campos



<GailGBoehm@aol.com>

01/24/2011 07:38 AM

To <matt_c_fowler@dot.ca.gov>

cc <gailgboehm@aol.com>

Subject Cold Springs Bridge-Yes to Suicide Barrier (resend)

Dear Mr. Fowler,

I hope I am not too late to express my extreme support for a suicide barrier for the Cold Springs Bridge in Santa Barbara County. My husband and I live in Goleta but were out of town for the meeting there and won't return until this Wednesday.

I am appalled at the number of people who put the "beauty of the bridge" above the saving of lives. In fact I find it hard to believe that it has taken this long to take this action. We moved to Goleta just a few years after the bridge was built and I have always found it very scary to drive across. Realizing that the current barrier is only thigh high was even more shocking. I believe that is less than the requirements for a deck railing in the city/county of Santa Barbara! That is even a convenient height to sit on and tumble over backward accidentally!

I agree that if a person really wants to commit suicide they will find a way to do so, but that doesn't mean that we shouldn't do everything in our power to discourage them. And the current non-barrier is a hazard to anyone walking across or trying to help or deter a "jumper," as well. I admit to a slight case of acrophobia, but I also remember my mother, who was the last person that I would describe as suicidal, not wanting to go near the sheer edge of a cliff at the Grand Canyon. Later she confided to me that looking over and watching the birds soaring below her gave her an urge to leap off and soar with them. Others may have this same feeling.

I don't believe that the barrier has to absolutely prevent some determined person from jumping, but I believe it should be at least high enough to make it difficult and certainly to prevent accidents--maybe 4-6 feet as a minimum. And I believe that the style of the design (which I have not seen) may be debated for esthetic reasons but not the fact of the barrier. I believe a barrier could be attractive as well as safe.

I have known several people who have considered or attempted suicide. All of them have gone on to live happy and successful lives. I also knew one of the teen age suicides from the bridge-the older brother of my young son's friend. That is a life that was never lived, and caused the family great anguish.

Please put my vote on the side of the barrier. Bless you all.

Sincerely,

Gail G. and Thomas K. Boehme



<LSBogatz@aol.com>

01/22/2011 06:18 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject ColdSpringsBridge

Very important project; please continue installation.
Larry Bogatz



Bruce
<brucejames@me.com>

01/22/2011 06:23 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>

cc

Subject Barriers on the 154 bridge.

I am in support of having barriers on the bridge as I live on 154 and it's difficult to explain to my three children every-time some decides to take there life with such ease.
Thank you,

Bruce



Melissa Jones Cantekin
<mjcantekin@juno.com>
01/05/2011 07:06 AM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Suicide Prevention--Barriers on Bridge

I am writing in support of putting the barriers on the bridge to prevent suicide. As a psychologist, I understand the implications of the time and energy it actually takes to commit suicide by the person doing it. Any obstacle to suicide that extends time and creates difficulty saves lives.

Melissa Jones Cantekin Ph. D., CA Psy 22622
mjcantekin@juno.com
1129 State St. Suite 3G
Santa Barbara, CA 93101
805-965-9508

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Walton Clark
<waltonclark3@hotmail.com>
01/24/2011 03:32 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc
Subject

I am writing in favor of the suicide barrier on the cold Springs Bridge. the fact that it will decrease the total suicides in this area has been proven by experts. also talk to the people who have had to retrieve the bodies. they are strongly in favor of the barrier because it is a grim, painful and dangerous task. the bridge itself is is a dangerous place for pedestrians and bikers. Please approve this project. Thank you.

Walton Clark

Sent from my iPhone



Jina Carvalho
<jina@glendon.org>
01/20/2011 04:11 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Support For Cold Springs Arch Bridge Suicide Barriers

Dear Mr. Fowler

I am in complete support of the suicide barriers on the cold springs arch bridge. These barriers will save lives as well as save distress for those who witness the suicides. I know the lives of members of law enforcement that have to respond as well as the search and rescue crews that have to respond after the suicide.

Thank you for receiving my comments.

Jina Carvalho

115 West Canon Perdido Street
Santa Barbara, CA 93101

jina@glendon.org

(805) 681-0415 x232

www.Glendon.org

www.PsychAlive.org



Irma Catlett
<irmacatlett@yahoo.com>
01/06/2011 08:45 AM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge Barrier

I support the building of the barrier on the Cold Spring Bridge. I drive Hwy 154 daily to and from Santa Barbara and the knowledge of people committing suicide on that bridge when it could be prevented is unthinkable. It has taken too long to start construction.
Stop the stalling!!!!!!
Irma Catlett



Joyce Catlett
<jcatlett@glendon.org>
01/05/2011 03:46 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject cold springs bridge barriers

The barriers on Cold Springs Bridge should be erected as soon as possible before there are more deaths. Research has consistently shown that when you remove the means for suicide, such as putting up bridge barriers or not using poisonous gas to heat your homes, as was eliminated in Great Britain in the 1950's, the suicide rate falls – many people fail to seek other means because they are always ambivalent about wanting to die anyway, so anything that delays their death or puts a “barrier” in their way helps prevent suicide. Joyce Catlett, M.A. Child Mental Health Specialist.



"Ralph R. Chase ARA/SED"
<rchase@ara.com>
12/14/2010 08:14 AM

To "matt_fowler@dot.ca.gov" <matt_fowler@dot.ca.gov>
cc
Subject Suicide Barrier Cold Springs Bridge Highway 154

I am strongly in favor of a suicide barrier on the Cold Springs Bridge on Highway 154.

I acknowledge that the view from the bridge is spectacular, but I don't think it is worth one life, much less the 54 lives that have been lost there.

I recommend that you highly publicize each suicide that has occurred since the "Friends of the Bridge" have mounted their insane campaign against the suicide barrier.

The "Friends of the Bridge" are certainly assisting anyone who is contemplating suicide.

Thanks very much.

Ralph Chase
3615 Capri Drive
Santa Barbara, CA
805-679-1778 Cell



Becka Chester
<becka.l.chester@gmail.com>
>
01/05/2011 02:46 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Barriers at Cold Springs Bridge

Matt,

I feel it is critical to put up these barriers...too many people have died. Please consider this an important element...it is more important than a "pretty bridge" to save lives.

Thank you.

--
Becka Chester



Ed Csapo
<Ed@eandmequipment.com>

To <matt_c_fowler@dot.ca.gov>

cc

12/30/2010 03:46 PM

Subject: Bridge...

Matt,

My back ground is construction as trade , I am 47 years old , stopped drinking at the age of 24 due to problems , and wanting to kill myself . Well my choice was to jump , but that moment I look over the edge of the roof , I thought that will not do the job completely so I didn't . I reached out for help... please note this is 2 years after not drinking... if you have never been that close to doing it , well you will never get the bridge barrier. I do ... I feel for people that don't have enough compassion for their fellow man life .

Please feel free to use my name and story ... least I can do for my fellow human .

Ed Csapo
ed@2000kw.com
www.2000kw.com
E&M Equipment, LLC
P 805 879 5270
C 805 689 6776
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Della Casberg Deats
<dellacasbergdeats@mac.com>
m>

01/22/2011 02:42 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc

Subject Cold Springs Bridge suicide barrier

Dear Mr. Fowler,

Though I understand that a vertical barrier may filter one's view for the brief drive over Cold Springs Bridge, there are still numerous opportunities for viewing that beautiful scene and one can pull over and park to do so!

I have lost one very dear YOUNG friend and another middle aged acquaintance in brief acts of despondency from atop that beautiful arch.

Frankly, I wonder what critics can be thinking when they consider the esthetics of the view from the bottom if an ORANGE "horizontal net structure" is placed. Is that the "more pleasing feature" to be observed from that historic place? Will unintended consequences include the title "nesting attractor"? Has litter or mischief maintenance budget been calculated?

I believe this started as an effort to stop the modification. I hope rational heads can talk this through and not leave us with an unintended monstrosity adorning that beautiful arch.

It is clearly all related to one's personal point of view.....unless one has lost a loved one there....

Sincerely,

Della Casberg Deats

PO Box 1349
Solvang CA 93464
Muffy: 805 452-5847



ALVIN DIAS
<alvinjd34@verizon.net>
01/03/2011 08:16 PM

To <Matt_C_Fowler@dot.ca.gov>
cc
Subject San Marcos Bridge

I firmly believe that the bridge should have a restraining barrier built on it. People are more important than the environment, I should think. Yes, they may find another way to commit suicide from the bridge, but the barrier will hinder them and maybe give them a few more minutes to think about what they are doing. One of these days the state will be sued for having not build the barrier when they had a chance. Or are those who are against the barrier willing to take on that cost if there is a lawsuit? Think you may want to poll them.

Sincerely, Laura Dias, Santa Maria resident



John Douglas
<johndog@cox.net>
01/03/2011 01:38 PM

To <Matt_Fowler@dot.ca.gov>
cc Gail Boehm <GailGBoehm@aol.com>
Subject Cold Springs suicide-prevention bridge

Mr. Fowler: I have been a resident of Goleta for nine years. I am working this coming Wednesday night, so I request that you please enter my comment on the Cold Springs bridge suicide prevention barrier into the record of public comments to be taken at the January 5 hearing at San Marcos High School on this issue.

Especially in this season of *It's a Wonderful Life* (whether you believe in angels or not), the local or state government should install a suicide prevention barrier on the bridge. The government is paternalistic in many other aspects of public safety, and should take all available steps within reason to impede people committing suicide at a public site well known for previous suicides. Regarding the question of aesthetics, people should be paying attention to the road, not the scenery, when driving on the bridge, and with all the accidents on that dangerous stretch of highway, particularly involving intoxicated drivers, passengers should be watching cars coming the other way too. There are plenty of spots on other roads where folks can park to catch a view of the scenery in question.

John Douglas
PO Box 8552
Goleta, CA 93118

■ John Enrico Douglas ■
johndog@cox.net
(805) 284-2082
<http://www.johnericodouglas.com>
<http://www.facebook.com/john.e.douglas>



Emily Dziezic
<emily@eggfoo.com>
01/05/2011 06:56 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Letter on the Draft Supplemental Environmental Impact Report

Dear Mr. Fowler,

Attached, please find my letter regarding the Draft Supplemental Environmental Impact Report on the subject of the Cold Spring Canyon Bridge Project.

Thank you for your time.

Regards,
Emily Dziezic



barrier_2-signed.pdf

January 5, 2011

Matt Fowler
Caltrans
50 Higuera Street
San Luis Obispo CA 93401

Dear Mr. Fowler:

I am writing to comment on the Draft Supplemental Environmental Impact Report on the Cold Spring Canyon Bridge Project. I have looked over the report and it seems that, when it comes to the accessibility of the views from the bridge, the sole concern has been for how passengers and drivers of automobiles will be effected by the barrier. As the bridge is equipped with two sidewalks I thought that it would be important to add the perspective of someone who has traversed the bridge on foot and I would say that, due to the inadequately high guardrail that currently spans the bridge, the lack of a barrier essentially makes this bridge, and therefore the views that are visible from it, inaccessible to anyone on foot.

In its current form, without a barrier, crossing the Cold Spring Arch Bridge on foot is not a pleasant experience. The present guardrail is far too short to enable any pedestrian to be at ease while they are on the bridge, so enjoying the view is not something that can easily be done by someone who is on either sidewalk. There are strong gusts of wind that buffet you as you walk across the span and one gets the sense that it would be possible to get blown over the side of the bridge if a large truck passed by or a strong blast of wind caused you to lose your footing. I certainly couldn't enjoy the view while I was crossing the bridge, my main concern was that I made it to the other side without tripping, and everyone I know who has tried to cross the bridge has experienced the same difficulties. I believe that the proposed barrier will actually make the view from the bridge more accessible not only to pedestrians but also to cyclists, as it will serve as a protective barrier that will give anyone on foot or on a bicycle a sense of security as they traverse the bridge. In this way, a barrier will essentially make enjoying the views from the Cold Spring Arch Bridge more accessible and more available to individuals who are not traveling over the bridge in an automobile.

I also believe that the mitigation efforts outlined in the report have sufficiently addressed the concerns of those individuals who were worried that the implementation of a barrier would negatively impact the aesthetic experience one has while crossing the bridge. It is my opinion that is only while on the bridge that the barrier will have any negative impact at all on the aesthetic experience of the bridge as all of the views of the bridge itself and its beautiful span are experienced from distances that are great enough that the barrier will barely be noticeable at all.

I hope that you will take my comments into consideration and I thank you for your time.

Sincerely,



Emily Dziedzic



Sarah Eichenbaum
<sarahe81@gmail.com>
01/05/2011 02:06 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge Barriers

I am writing to say that I strongly support barriers being put up on the Cold Springs Bridge.

Thank you,

Sarah Eichenbaum
4500 Highway 154
Santa Barbara, CA 93105



"Farro, Christine"
<C.Farro@sbcsocialserv.org
>
12/27/2010 02:36 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc
Subject: Suicide Barrier: Cold Springs Bridge

Recent research (Australian and New Zealand Journal of Psychiatry, 2009, Vol. 43, No. 6, Pages 498-502) validated previous research into the effect of restricting access to a suicide jumping site. A suicide jumping "hotspot" was studied for 10 years before access was restricted. In those 10 years there were 13 deaths at this hotspot (or open verdicts) before the road was closed for construction and none in the 2 years since the road opened after construction.

Suicide is an impulsive act, the success of which is partly dependent on the means the individual uses. If the means is removed, or restricted at the Cold Springs Bridge (our suicide jumping "hotspot") suicidal individuals will be less likely to choose jumping off a bridge as their means of attempting suicide.

Certainly, the overall beauty of the area will not be negatively impacted by such restrictions on the bridge. Even if it were, losing some "sights" is a small price to pay for saving lives.

Thank you for your consideration in this matter.

You must be the change you wish to see in the world.
~M. Gandhi

Christine Farro, MA, SSSII
Child Welfare Services~Voluntary Family Maintenance
c.farro@sbcsocialserv.org
2125 Centerpointe Parkway
Santa Maria, CA 93454
805.346.8357

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Steve Feinberg
<sfeinberg@tempesttelecom.com>

01/01/2011 06:30 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Building a barrier on the Cold Springs Bridge - YES Please ASAP!!!!!!

Hi Matt,

I'm a regular commuter on Highway 154 and I'm very much in favor building a barrier to prevent suicides from happening.

Please do whatever is necessary as quickly as possible.

One of my friends actually ran out to prevent someone from jumping but it was too late. A barrier would have saved that person's life, giving the precious few seconds necessary to delay the person from jumping.

Feel free to contact me at the information below.

Thank you,
Steve Feinberg

Steve Feinberg
Tempest Telecom Solutions
5383 Hollister Avenue #130 Santa Barbara CA 93111
Phone 805-879-4832 Fax 805-690-3345
Mobile 805-570-7410
Email sfeinberg@tempesttelecom.com
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Robert Feinberg
<robert.feinberg@globalpwr.com>

01/06/2011 02:50 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Cold Spring Canyon Bridge Barrier

Dear Mr. Fowler:

Unfortunately, I was unable to attend last night's public hearing regarding the pending Cold Spring Canyon Bridge barrier project. However, I would like to submit my comments in support of moving forward with the project and building the barriers.

I currently reside in the Santa Ynez Valley and have done so for the last 13 years. I commute to and from Goleta on S.R. 154 over the Cold Spring Bridge every work day. Sometimes, particularly on weekends, I travel this route two and even three times.

More than once I have witnessed emergency vehicles gathered at the end of the bridge and am saddened at the thought that yet another suicide has occurred that could possibly have been prevented. I fear for the safety of the law enforcement officers and other emergency response personnel and am loathe for the day when I might have to get out of my vehicle on the bridge to help a stranded motorist or, God forbid, intervene in a suicide attempt. From time to time, I also drive below the bridge on Stagecoach Road and see the growing collection of crosses and memorials to the unfortunate victims.

Although the view from the bridge is spectacular, there are plenty of other places around the valley from which to sightsee including the nearby Vista Point, both East and West Camino Cielo roads and numerous hiking trails. The view from below the bridge is also visually pleasing architecturally, but I doubt the barriers would have much of an impact on the view from that distance just as the earthquake reinforcement work of maybe ten years ago was not disputed and had no significant visual impact.

I have heard the arguments from opponents of the project and am frankly unimpressed. Contrary to the "Friends of the Bridge" website, I see no "overwhelming public opposition to them [the barriers]".

Please proceed with the construction of the barrier as quickly as possible. I believe more than enough time and money has been spent in consideration of those who oppose the project and it's imperative that we move forward to prevent more needless deaths.

Thank you for your consideration.

Sincerely,

Robert Feinberg
PMB 526
3463 State Street,

Santa Barbara, CA 93105
(805) 689-7221



Sue Firestone
<sfirestone@sfadesign.com>
01/05/2011 06:14 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Barriers on the Cold Springs Bridge

Dear Matt,

I am writing to express my strong support in regards to the addition of barriers along Cold Springs Bridge. The barriers are a very necessary addition to the bridge. It is most important to protect the lives and safety of the citizens of our county. I hope that you join me and the countless others who support this much required addition.

sue firestone
chairman / founder

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santa barbara, ca 93105
tel: 805.692.1948
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www.sfadesign.com

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Tamsen Firestone
<tamsenf1@gmail.com>

01/07/2011 12:19 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Please put up barriers on the Cold Springs Bridge!

I live down the highway from the Cold Springs Bridge and am aware of how many people jump to their deaths from it every year. One of my friends was driving across when someone jumped and he tried to stop him. He was unable to get to the man, and he was traumatized for months with the image of man disappearing over the side of the bridge.

You may argue that everyone has the right to take their own life; I happen to disagree. So if you aren't putting the barriers up for the people who are suiciding, at least do it for those who are traumatized by their act! If you aren't putting the barriers up because the disturb the scenic beauty of the bridge, then you are just plain insane!

Tamsen Firestone

--

psychalive.com



Carolyn Firestone
<carolynfirestone@gmail.com>

01/04/2011 03:19 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Cold Springs Bridge Barriers

I strongly support the bridge barriers on Cold Springs Bridge. I hope this project goes forward as soon as possible before more lives are lost.

--

Carolyn Firestone



Nina Firestone
<nfirestone@sfadesign.com>

01/04/2011 02:00 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Support for barriers on the Cold Springs Canyon Bridge

To Whom it May Concern,

I am in support of putting barriers up on the Cold Springs Canyon Bridge to prevent people from using this bridge as a tool to commit suicide.

Thank you!



Steve Firestone
<sfirestone@someracapital.com>

01/24/2011 01:54 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Suicide Barriers on the Cold Springs Bridge

Hello,

I am a Santa Barbara resident and reside close to the Cold Springs Bridge.

I am fully supportive of the construction of suicide barriers on the bridge.

Furthermore, I am alarmed that this project has not even begun and I would press everyone to expedite this project with the hope that we won't lose too many more lives. These are tragedies that can be prevented and it our responsibility to do whatever we can to get these barriers up.

Thank you,

Steve

Steve Firestone

Chairman

Somera Capital Management, LLC

115 West Canon Perdido Street

Santa Barbara, California 93101

Tel: (805) 681-0144 x207

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Fax: (805) 683-5791

E-Mail: sfirestone@someracapital.com

Visit us at: www.someracapital.com

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Michelle FitzGerald
<mlfitz805@yahoo.com>

01/04/2011 04:09 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject in support of bridge barrier

I am in favor of barricading the Cold Springs bridge. They did this to the arroyo bridge in Pasadena and they made it aesthetically nice. This bridge is used in many tv and film productions still, even with the barrier.

Thank you

Michelle FitzGerald
Goleta, CA



<frankoort@cox.net>

12/30/2010 04:35 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Coal Springs Bridge

I strongly support the barrier for the protection of the community and the police officers.



"Franks, Barbara Jean"
 <bjfranks@anthc.org>
 01/04/2011 10:12 AM

To <matt_c_fowler@dot.ca.gov>
 cc
 Subject Comment re: barriers

Building barriers on the Cold Springs Bridge is a safety project that will save lives at one of the most lethal spots in five counties. Those who jump from this bridge have no chance to survive. Putting time between the person's suicidal impulse and the action to take their lives is crucial and why restricting access to means for suicide is so important and is [5th Goal of the US National Strategy for Suicide Prevention](#).

Greetings from Alaska: My name is Barbara Franks and I am writing in support of the building barriers on the Cold Springs Bridge. Even though I may never make it there to see the bridge or hear if this project was given the thumbs up, I support it because my son completed his suicide 12/14/97 and I do not wish to share the chronic pain that you are left with the rest of your life. He added to the category of Alaska Native, between the ages of 15-24 (he was 23) using a firearm his father gave him for Christmas in 1996.

As parents we try to protect our children, we share some of the things that happened to us and keep them out of harm's way. No matter how much a parent tries or no matter how much we "think" we know our children, there is something so bad they think they cannot share with us. I always say, "A person's crisis is as individualistic as their fingerprint, no two are alike."

I believe if we all pull together that we can make the difference in making positive changes that may cause that person to change their mind. Putting a price tag on a person's life versus seeing how much it would cost for the response to an emergency seems unheard of to me, but I would venture to say, that money could be used to help finance programs that would help "before" the person becomes suicidal.

For me, I used to say, my son was "only" 23 when he died, and now I say, "I was blessed with 23 years of his life." *Change is in the wind*, and we need to begin to pull together, Alaska is #1 across the United States on suicide, and it has been my passion to help when I can, and the full effort to bring in comments that might help with the decision of moving forward, I'm all for that!

We may let others know we mean business by pulling together as a Nation, this is one movement

I would like to be a part of, thank you for asking for public comments.

Barbara Franks
 Mother of a Child Who Completed His Suicide
 Ron David, Born 5/13/74 – Completed Suicide 12/14/97 (two days before my husband of 25 years lost his battle to cancer).
 Email: gfeld4@gmail.com

Barbara J. Franks / Tlingit
 729-3751

The "Life" Model

- Listen to your friends when they need to talk about problems or thoughts of suicide.
 - Insist that they be honest with you.
 - Feelings, share them with each other
- Extend a helping hand and go with them to get a responsible adult involved to help.

Resource: The Jason Foundation <http://www.jasonfoundation.com/student>, 12/08



<nonky1@gmail.com>

01/04/2011 11:19 AM

To <matt_c_fowler@dot.ca.gov>

cc

Subject safety barriers for Cold Spring Bridge

Dear Matt:

I wholeheartedly agree with the many who are concerned about the prevention of suicides from the Cold Spring Bridge in Santa Barbara.

If only as a temporary or intermediary step, perhaps a camera can be installed to monitor the bridge, &/or an emergency phone can be installed nearby as well. However, a more permanent barrier should be top priority in the prevention of future attempts by distraught or suicidal persons contemplating a jump from the bridge.

Sincerely,

C. Fujimura

Santa Barbara Response Network volunteer



jzgiler <jzgiler@cox.net>

01/05/2011 10:12 AM

Please respond to
<jzgiler@cox.net>

To <matt_c_fowler@dot.ca.gov>

cc

Subject suicide barrier at Cold Springs Road

I totally support the building of a suicide barrier at Cold Springs Road.
I am a therapist who has been horrified at the number of deaths there.

Janet Giler

549 N. Hope Ave.

Santa Barbara, CA. 93110



alannah godwin
<alannahgodwin@yahoo.com>

01/04/2011 04:21 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject bridge

I surely support the barrier for the bridge. I was suicidal for many years and when I figured out how to do it with out surviving, it got very close. I did not follow through because I got help and my parrot stopped me. Please make sure this barrier goes up!! I would like to help those in pain. Life is wonderful and can be pain free.

Sincerely,
Alannah Godwin



Barbara Grimmitt
<bgrimmitt@verizon.net>

01/02/2011 10:29 AM

To <matt_c_fowler@dot.ca.gov>
cc
Subject In support of barriers on Cold Springs Bridge

Mr. Fowler,

Safety and Lives First!!!

This correspondence is in support of barriers on Cold Springs Bridge. We need these barriers NOW! If placing a barrier helps to save one life, it is worth it. Suicide has impacted my life in that I have lost a sister by suicide. In addition, my son has attempted suicide. Since these incidences I have become more aware of how prevalent this is in our society, and how many families it impacts. One life taken by suicide affects many families and friends forever. Our society needs to step up and assist in whatever ways we can. You can play a part in this by hearing and acting on this plea, and as a result may save lives and suffering. Please do all you can to ensure that these barriers are built soon.

Thank you,
Barbara Grimmitt
Santa Barbara County



Ken Hampian
<kchampian@charter.net>
01/09/2011 04:33 PM

To <matt_c_fowler@dot.ca.gov>
cc <sara_von_schwind@dot.ca.gov>
Subject Cold Springs Bridge Barrier: Supplemental EIR

Matt -

The attached letter is in response to the comment period for the Supplemental EIR for the Cold Springs Bridge Suicide Prevention project. Thank you for entering my letter into the record.

Kenneth C. Hampian
4143 Poinsettia Street
San Luis Obispo, CA 93401
Cell: 805-431-7158
Home: 805-546-0876



Cell: kchampian@charter.net Cold Springs Bridge SEIR.doc

January 9, 2011

Mr. Matt C. Fowler, Senior Environmental Planner
California Department of Transportation
Central Coast Environmental Analysis
50 Higuera Street
San Luis Obispo, CA 93401

RE: Cold Springs Canyon Bridge Suicide Barrier Project

Dear Mr. Fowler:

My name is Ken Hampian and I retired as the City Manager of San Luis Obispo one year ago. Throughout the course of my long public service career, I have been involved in dozens of environmental studies. I know the complexity involved in making public decisions that involve difficult trade-offs and I appreciate the process that has been followed in studying the Cold Springs Canyon Bridge Suicide Barrier Project.

While one might question the Court Ruling requiring added study and opportunities for public input, I also know that conscientious courts and public agencies will usually leave no stone unturned in evaluating project impacts, and involving the public, especially for projects located in sensitive settings, such as within the San Ynez Valley.

However, after reviewing the Draft Supplemental Environmental Impact Report, I strongly believe that the environmental work is more than complete and provides ample information upon which a decision can be made. Any further criticism of the overall completeness of the work or the process, I believe, is likely to be more about creating further delay. The process has gone on long enough – it is time to declare the EIR work as sufficiently responding to the core intent of CEQA which, as you know, is about disclosure of impacts – and not about whether or not a project should be approved.

It continues to be my strong belief that, even with some visual and historical impacts, that this project should be approved.

The core purpose of those charged with public leadership is to protect the public safety and promote other core public purposes. In other words, to promote "the greater good." As a result, we make visual and even historical trade-offs all of the time – e.g. radio towers to assure necessary public safety communication, signs and traffic signals to promote a safe flow of traffic, median barriers and rail guards to protect drivers, sound barriers to protect quality of life, massive wheel chair ramps to allow access for all citizens. The examples could go on and on.

There is no question that the Cold Springs Bridge has become an attraction for those contemplating suicide, any many sadly act on their impulse. The statistics are clear and just keep piling up. But this isn't really about statistics, either. It is about human beings.

I was a friend of Matt Aydelott, the 46th person in 45 years to take his life at the bridge in 2008. Matt was a wonderful, contributing member of the San Luis Obispo community, serving in several public service capacities, particularly working in service of at-risk youth. Little did we know that Matt himself had also become "at risk." Matt took his life on the second day of National Suicide Prevention Week.

No one can say that a barrier would have prevented Matt from taking his life, but a safer bridge would have discouraged Matt from making his surprising trip to the bridge on that fateful day. Suicide prevention experts know that "buying time" can be the difference between life and death. More lives have been lost at the bridge since Matt's death and more will follow, unless something is done. The cost in human life, and the risk and cost associated with dangerous rescue and/or recovery operations, is higher than the impacts outlined in the environmental studies -- concerns that are legitimate, but can be reasonably addressed, as set forth prior design processes and plans.

In summary, I believe that the supplemental environmental work fully meets the intent of CEQA and the courts. It is now time to declare the environmental phase of the process as complete and concluded, and move on to the decision-making phase -- which I hope will result in a very necessary public safety improvement on the Cold Springs Bridge.

Thank you for your consideration.

Sincerely yours,

Ken Hampian
4143 Poinsettia Street
San Luis Obispo, CA 93401



Thomas Heck
<insights2us@gmail.com>
01/04/2011 10:40 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Suicide preventers at Cold Spring bridge

Dear Mr Fowler and Committee,

I drive Hwy 154 often, and strongly disagree that some kind of fence to prevent the depressed from leaping to their deaths from the Cold Spring Bridge would "spoil the view." There is a great view point within a quarter mile of that bridge, if memory serves. Let people stop there, already! Safely.

Anything that encourages people in cars stopping on or near that bridge could be very hazardous in terms of setting up traffic for accidents. Cars on the downhill are usually going close to 60mph, and would find it hard to brake to avoid hitting a car entering the highway after a "view" stop.

A fence to prevent suicides is long overdue. The lethal consequences of keeping a suicide point, like that bridge, open to pedestrians and available for depressed citizens to use to leap to their deaths, are already well documented. It's an ugly story.

Please, let's make jumping from this bridge damned near impossible!

Thomas Heck

--

Thomas F. Heck
592 Rosa Linda Way
Santa Barbara, CA 93111-1523 USA
Tel. 805-692-1969



"Patrick K. Hines"
<pk057@yahoo.com>
12/30/2010 06:06 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge

Each person who dies, with your knowledge of the existing low rail, is your responsibility.

Stop the deaths now !

Patrick K. Hines
Principal
The Rainier Group, Inc.

Northern California
One Montgomery Street - Suite 2300
San Francisco, California 94104
Phone 415-407-0228

Southern California
1500 North Vermont - Suite 2000
Los Angeles, California 90027
Phone 323-798-5074

pkHines@rainiergroup.com

<http://www.rainiergroup.com/>

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Kevin Hines
<jkevin057@gmail.com>
01/23/2011 02:31 PM

To <matt_c_fowler@dot.ca.gov>
cc

Subject Kevin Hines in support for a bridge railing/barrier/net at the
Cold Springs Canyon Bridge

My name is Kevin Hines, a long time advocate for reducing access to lethal means of suicide. Putting up a railing at the CSC bridge will save lives...period. this has been proven by example all over the world. Bridges become icons for death by suicide, it is a myth that "they" will go some where else. the first 48 hours between suicidal thoughts and actions are the most crucial. This effort will save lives. I know I jumped off of the Golden Gate Bridge in the year 2000, i am the 26th survivor, over 2000 have died at the GGB. lets end the needless deaths at the CSC bridge today.

The opposition holds not enough evidence, nor knowledge to dispute the facts that these methods of means reduction work efficiently and effectively. They are misguided in their attempts to halt the building of a railing barrier or net. "What are the aesthetics of a bridge compared to just one human life" I ask you this in all candor, A mother a father a loved one. doesn't that outweigh the way a bridge appears.

Kevin Hines
International Public Speaker
National Mental Health Advocate
Author: Upcoming Memoir " The Wounded Healer: Chronicles of a Psychotic Mind"

www.kevinhinesstory.com

Phyllis Parsons: Manager/Publicist
www.parsonspanvinc.com
p_parsons2001@yahoo.com
925.934.5300
925.788.0010



dave hull
<davyhull@gmail.com>
 01/01/2011 06:15 PM

To <matt_c_fowler@dot.ca.gov>
 cc
 Subject Built the suicide barrier at Cold Springs Canyon

The consensus of scientifically controlled studies across the world over the past 30 years is that barriers on bridges work, and that they work better than any other alternatives such as hotlines, patrols, video cameras.

Dave Hull



Ali Ironside
<aliironside@gmail.com>
 01/06/2011 09:41 AM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
 cc
 Subject Cold Springs Bridge

To me, it seems like there can be no question regarding how necessary it is to put up barriers on the Cold Springs Bridge. The suicide numbers speak for themselves. My only concern is that too much time has gone by making the decision and lives have been lost.
 Thank you, Ali Ironside

Sent from my iPhone



<sjansen@vzw.blackberry.net
>
 01/05/2011 03:04 PM

Please respond to
 <sjansen@vzw.blackberry.net
 >

To <matt_c_fowler@dot.ca.gov>
 cc
 Subject Bridge

Please do what you can to put barriers up. I know it would deter people from jumping. Thank you Sandra Jansen
 Sent from my Verizon Wireless BlackBerry



Jean Johnson
<jhj4jesus@yahoo.com>
01/02/2011 05:46 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject re:suicide barrier

Dear Matt,

I support the suicide barrier on the Cold Springs Canyon bridge.
Please help prevent more suicides with this barrier.

Jean Johnson
6828 Shadowbrook Dr
Goleta, CA 93117



dana kiesel
<dkieselphd@aol.com>
12/30/2010 07:53 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Bridge

I support barriers on the fowler bridge. Studies show that people who attempted suicide and lived are glad they did and wish that something or somebody would have stopped them from their attempt. Barriers would be just the sort of thing that would not make it easy for the person considering suicide and their ability to easily carry out an attempt.

Sincerely,

Dana Kiesel, Ph.D.
Clinical Psychologist



J Lake
<admin@stopthetragedy.org>

01/12/2011 05:09 AM

To: Matt C Fowler <matt_c_fowler@dot.ca.gov>
cc: Colin Jones <colin_jones@dot.ca.gov>
Subject: Cold Spring Bridge suicide barrier

January 12, 2011

Caltrans

Attn: Matt C. Fowler

50 Higuera St.

San Luis Obispo, CA 93401

Dear Mr. Fowler,

I am writing in enthusiastic support of the proposed vertical grid-mesh suicide barrier on Cold Spring Canyon Arch Bridge.

My cousin, Matt Aydelott, died at the bridge on September 8, 2008. He was a teacher at Cuesta College in San Luis Obispo, where he developed a program to help high-risk students prepare for and find employment.

In a moment of crisis, Matt acted on impulse to end his pain. The bridge provided an immediate and certain solution that did not give him time to reflect on the alternatives, or to reach out to those of us who loved him. His death is an immeasurable loss to me and my family, to his many friends, and to the young people who stood to benefit from his commitment to their success.

For Matt, and for the dozens of others who have lost their lives at the bridge, a physical barrier might have meant the difference between taking this final step and seeking help.

I have detailed the arguments in favor of the construction of a physical barrier to prevent suicides by jumping on my website, www.stopthetragedy.org, which I created to honor Matt's memory. I am certainly not alone in endorsing Caltrans's efforts: the online petition linked to my website demonstrates that the barrier project has considerable popular support. I have attached a copy of this petition, with the signatures and comments of more than 600 individuals. Most of these are California residents, although the signatures of non-residents are also relevant, as these demonstrate the far-reaching impact of this project for the families and friends of those who have died at the bridge.

The urgent need for a physical barrier is no longer in dispute, as even the most vehement opponents of the vertical barrier solution have now conceded.

The only remaining issue is what form the barrier will take. An alternative has been suggested that would preserve the view from the bridge, using horizontal netting instead of vertical mesh. However, this is an inferior solution for several reasons. First, it is not clear that this option would be equally effective, as it would not place a vertical obstacle between potential suicide victims and the 400-foot drop. Further, it would substantially increase the physical risk to rescue personnel at the scene, who would have the task of climbing onto the netting to save would-be jumpers. The netting itself might also attract thrill-seekers, which would place an additional burden on emergency services. Finally, the costs of maintaining a horizontal barrier are likely to be substantial, as the removal of litter and debris from the netting will be a frequent, labor-intensive, and dangerous job.

Set against these considerable disadvantages, the benefit of preserving the view for motorists cannot be justified. In the matter of seconds it takes to cross the bridge by car, views of the canyon would be only partially obscured by the vertical grid-mesh, which minimizes the visual impact of the barrier relative to other options outlined in the Environmental Impact Report. The scenic vistas of Highway 154 can be enjoyed continuously and from numerous points along its 32-mile length, including overlooks where drivers can stop and appreciate their surroundings in safety.

In short, the construction of a physical barrier to prevent suicides at Cold Spring Bridge is both essential and tragically overdue, and the vertical grid-mesh option represents the best solution. I wholeheartedly support Caltrans in its efforts to see this project through to completion.

Yours sincerely,



Jennifer Aydelott Lake Caltrans_Letter_of_Support.pdf support-the-cold-spring-bridge-safety-barrier_011011.pdf

January 12, 2011

Caltrans
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50 Higuera St.
San Luis Obispo, CA 93401

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Yours sincerely,



Jennifer Aydelott Lake

California Residents, Friends and Families of Suicide Victims

	Name	From	Comments
1.	Sigrid De Ruyck	Gent, Belgium	
2.	Jenny Lake	Weston, MA	My cousin Matt died at Cold Spring Bridge on September 8. Please help make his death the last. Support the barrier.
3.	Filomena Pereira	Melksham-wiltshire, United Kingdom	
4.	Mieke Bernaards	Gent, Belgium	
5.	Dan cappello	lawrence, PA	
6.	susan marden	North Vassalboro, ME	SAVE LIFE
7.	John Tucker	New York, NY	
8.	Staci Galvin	Falling Waters, WV	
9.	John Smith	Ellenburg Dep, NY	
10.	Freddie Geisler	Saint Petersburg, FL	
11.	Lynn Hamilton	San Luis Obispo, CA	My close friend Matt died at the bridge on Sept. 8. Keep others alive by supporting the barrier.
12.	Alfred Aydelott	Weston, MA	
13.	Adrienne Giske Aydelott	Weston, MA	
14.	betty greenwell	Coxs Creek, KY	by completing a barrier to help save lives would be a godsend for that area. what are the tax dollars being used for if not to help the people that live there & pay taxes ?
15.	Randy Illian	Portland, ME	
16.	Elizabeth Walsh	Naperville, IL	
17.	Emmett McCann	London, United Kingdom	<p>Matt died earlier this month after jumping from the Cold Springs Bridge. He was a leader in the San Luis Obispo community, a lover of music, politics and literature, a loving son, brother and cousin, and one of my good friends. Every night since I learned of his passing I have asked myself what I could have done to help Matt ease the pain that ravaged him in silence. I ask myself what calls I could have made, what letters I could have written. But the reality is, there was nothing -- I did not know the extent of his angst.</p> <p>Like so many who make this final choice, he kept all of this inside of him, while projecting to the outside world that same aura of radiance, love and understanding that made him who he was. I do know though, that if there had been a simple barrier, something that would have made him</p> <p><i>(continues on next page)</i></p>

	Name	From	Comments
17.	Emmett McCann	London, United Kingdom	<p><i>(continued from previous page)</i></p> <p>pause for just a minute, he may have picked up the phone and made that call for help -- and now maybe today I'd be emailing with him about the latest Pernice Brothers album instead of writing on this blog.</p> <p>I find it sheer insanity that anyone could object to something as simple as a barrier on a crowded intrastate highway, when it could make the difference between another human being carrying out the ultimate act of sorrow and pausing to call for help. For those who make arguments about suicide victims "finding another way", I can only say that you must have no idea of the pain and sadness that so many people feel right now about Matt's passing, and those who passed before him.</p> <p>Please, show some compassion. Please, construct this barrier.</p>
18.	James Sieck	Chicago, IL	<p>If this saves one person it is worth it. I can't imagine that this barrier wouldn't at least be able to do that. Even if it just slows things down for that person long enough for help to arrive. We put barriers on lots of things for people's safety. The bridge is not a scenic overlook. Anyone who thinks that has never walked across it. I have. There should be a safety barrier there regardless of the bridges proclivity for suicide attempts. Just try walking across that bridge. It is very unsafe and not at all a place for a scenic overlook. Please, realize we have barriers all over the world and put one here, for the many good reasons it should be....not the least of which are suicide attempts. Don't let this turn into some stupid psychological discussion of suicide. Just put up the barrier and make the bridge safer for anyone who ever happens to be walking across it. Thank you.</p>
19.	Jane Meister	Chicago, IL	
20.	Deborah Del Signore	Chicago, IL	<p>The facts in support of the creation of the barrier made me realize for the first time, that the tragedy that just hit our family may have been avoided and in fact was not an inevitable event. This makes me so much more interested in supporting the creation of the barrier at Cold Spring Bridge so that fewer families in the future have to endure the horror that ours did. Please allow the barrier to be built.</p>
21.	Franziska Eber	Oldenburg, Germany	
22.	Nathan Aydelott	Chicago, IL	<p>Any bridge that is used so regularly as a means to commit suicide should have a safety barrier put up. In particular, the Cold Spring Bridge should have such a barrier installed - it is not a scenic overlook as it is on a highway with virtually no area to walk. Simply walking on the bridge is extremely dangerous.</p> <p>The overwhelming majority (about 90%) of individuals who</p> <p><i>(continues on next page)</i></p>

	Name	From	Comments
22.	Nathan Aydelott	Chicago, IL	<i>(continued from previous page)</i> survive suicide attempts or are prevented from completing suicide do not commit suicide subsequently. Individuals who are prevented from committing suicide by jumping have a lower rate of subsequent completed suicide than individuals who attempt suicide by other means. My brother died at the Cold Spring Bridge, and I believe that he would be alive today if there had been a barrier there. Please put a stop to the political wrangling that has prevented the construction of a safety barrier at the Cold Spring Bridge. Continuing to allow preventable deaths to occur at this location is unethical and violates the public safety provision of the state constitution.
23.	Mike Kellogg	Chicago, IL	I was a close friend of Matt Aydelott who sadly committed suicide on the Cold Springs Arch Bridge in September. Knowing Matt, I can't help but think that his decision at dawn that morning was an impulsive one that could have been prevented. Numerous studies overwhelmingly support that a barrier would have prevented his death. While we can't bring Matt back, we can take steps to make sure that another person loved by many does not suffer the same fate. Sincerely, Mike
24.	JOSSIE ROSS	Quebec City, Canada	
25.	Lisa Firestone	Santa Barbara, CA	
26.	Jennifer Kellogg	Chicago, IL	
27.	Marion White	Auckland, New Zealand	
28.	Glenn Byrnes	Sylmar, CA	
29.	Matthew Stern	Minneapolis, MN	I lost a dear friend. I appreciate the value of a beautiful view but there are plenty of those and those of us who have lost someone there will not get them back. I think doing every thing we can to prevent another tragedy at the bridge (especially because its so easily preventable) is the right thing to do.
30.	Liam Monahan	St. Paul, MN	
31.	Anne Meister	Minneapolis, MN	
32.	Paul Burke Journey Home	Virginia Beach, VA	We are supposed to care about each other - when a simple barrier can be built to solve a problem like this we need to do it !
33.	Peter Vantine	Minneapolis, MN	This seems like a reasonable and feasible measure to prevent loss of life. I have become aware of this "local" issue from afar, having lost a friend who took his life by jumping from Cold Spring Bridge.

	Name	From	Comments
34.	Emily Dziedzic	Portland, OR	Constructing a suicide barrier at the Cold Spring Arch Bridge will ensure that no one ever dies at this bridge again and will enable this bridge to succeed as a civil engineering monument rather than allow it to continue being the monument to tragedy that its current design flaws permit.
35.	Karen Aydelott Robbins	Pasadena, CA	I am Matthew's mother. For his family and friends this is a heartbreaking and unbearable loss of a wonderful man who gave so much of himself to all of us and to his colleagues and every young person with whom he worked at Cuesta College. So many people are better for having known Matthew. The value of his life as lived is immeasurable; the value of all that he would have done for others were he still with us exceeds any cost of a suicide barrier on a bridge such as the Cold Spring Arch Bridge. Please support the effort of Caltrans to insure a safer environment for all of us and to prevent any more families having to endure such a loss as ours and the many before who have come before.
36.	Joni Kelly	Santa Barbara, CA	Placing suicide prevention barriers on Cold Spring Canyon Bridge is long overdue. How many more people must needlessly take their own lives before the community realizes that it is something that *should* be done. We need to send a message as a community to the desolate people who go up there that we CARE about them and that the world is a much better place with them in it.
37.	Laura Telep	San Francisco, CA	I personally know a family that lost a loved one on this bridge. Please install the safety barrier!
38.	Jillyanne Michelle Cape	Sweet Springs, MO	
39.	Erika Stone	Lake Ridge, VA	
40.	Michael Dziedzic	Portland, OR	
41.	Chris Sherman	Pasadena, CA	Understand the facts about what barriers can do. Listen to the voices of the healthcare professionals. Understand the potential loss and impact. Listen to your heart. Take action today.
42.	Veronica Juarez	Los Angeles, CA	
43.	Frederick Findley	South Pasadena, CA	Please construct the safety barrier on the Cold Spring Arch Bridge to save lives.
44.	MARY BETH NOBS	PASADENA, CA	
45.	Tish Osborn	Edina,, MN	Though I have not driven over this bridge, I can see from the photograph that there is nothing to stop someone from jumping, nor possibly an automobile going at a high speed, from going over the edge of this structure. Death in this manner is a tragic loss and with safety barriers, I am quite <i>(continues on next page)</i>

	Name	From	Comments
45.	Tish Osborn	Edina,, MN	<i>(continued from previous page)</i> certain these numbers could be reduced substantially. The statistics presented here on the number of persons taking their life from this bridge is absolutely unexceptable and must be addressed. I am burdened with the loss of both a father and a brother by suicide, and the loss of Matthew Aydelott, a close and dear friend, is no less painful. I encourage and support immediate action.
46.	cAROL SHERMAN	pasadena, CA	
47.	Mickie Robbins, LMFT	Manhattan Beach, CA	You have, in your hands, the opportunity to save people's lives and to prevent the untold grief of survivors of suicide. Please vote to support the building of a safety barrier on Cold Spring Arch Bridge.
48.	Ellyn Cole	Huntington Beach, CA	I didn't know Matt Aydelott personally, but I do know his family, and I did attend his memorial service. I know how depression can make one feel helpless and want to make the pain stop. The history of this bridge with regard to suicides is much like one we have in Vermont called The Quechee Gorge. These places need to be made less accessible for people who are desperate so they might just take that extra moment or two that will not only save their lives but save their families from the pain and sorrow once they are gone.
49.	JANE Reitz	Manhattan Beach, CA	Please consider this. If it saves one life how wonderful.
50.	Karin Robbins	West Hollywood, CA	My stepbrother and friend, Matthew Aydelott, jumped to his death from this bridge on September 8, 2008. Matt was a very special human being, who was in a lot of pain when he died. Perhaps, if he had come upon a barrier when he went to the bridge there would have been time for his family and friends to help him find another way out of his pain. Please do what you can to prevent another family from having to go through this.
51.	Genesis Vegan Anti-Consumerist	United States, United States Minor Outlying Islands	
52.	Mary Slavet	Weston, MA	A barrier on the Cold Spring Arch Bridge is the only way to reduce its attraction to potential suicide victims. Any delay between arriving at the bridge and jumping off allows that much more time for police to respond to calls from passers by. Doing nothing to stop this tragedy is, in fact, allowing, even encouraging, more suicide attempts at the site. Doing nothing is simply not acceptable under the circumstances. It's a no-brainer.
53.	Steve Dale	Frankston,victoria, Australia	
54.	Pam George	Sydney, Australia	

	Name	From	Comments
55.	Julie Sandness	Chicago, IL	
56.	gloria judson	aptos, CA	
57.	Elena Luoto	Edina, MN	
58.	Brent Meister	Minneapolis, MN	
59.	laird d b	Chorley, United Kingdom	
60.	Bill Craig	Kempton, Germany	
61.	Margaret Flowers	Weston, MA	
62.	Ashley Guadalupe	Salem, MA	
63.	kim fredericks	e. rutherford, NJ	
64.	jim cox	deer park, IL	"Suicidal individuals do not deserve our protection, as they have made a choice to end their lives". This is the most ridiculous thing I've ever heard. I guess abused women don't deserve protection because they made the choice to be with that person.
65.	jim cox	deer park, IL	"Suicidal individuals do not deserve our protection, as they have made a choice to end their lives". This is the most ridiculous thing I've ever heard. I guess abused women don't deserve protection because they made the choice to be with that person.
66.	Melinda Huestis	Chicago, IL	
67.	Tom Veitch	Aurora, IL	Please approve the request for a safety barrier to protect all that use the bridge. It is unfortunate the bridge has been involved in so many deaths so construct the barrier to deter this and the ill effects it has on all involved from family and friends to public workers.
68.	karen blaha	naperville, IL	
69.	Betsy van Orman	Ogunquit, ME	Matt Aydelott was a man who if saved would have been helped with therapy and could have been a real contributor to society. What a a shame for all of usn to have lost him.
70.	Ken Hampian	San Luis Obispo, CA	I have sent a more detailed letter to the SBCAG expressing my feelings. However, from my perspective as a city manager (San Luis Obispo), nothing is more important than the safety of the public....fine people like Matt who are in pain; and the rescue personnel who have been so often called to the bridge.
71.	David Cox	San Luis Obispo, CA	A friend and workmate ended his life on the Cold Springs Bridge in 2002. It was an impulsive and irrational act, that had someone or something stopped him, it is likely the tragedy would probably have been averted. I urge you to move forward on the construction of the safety barrier.
72.	Anna Mraz	Minneapolis, MN	

	Name	From	Comments
73.	Sophie Taylor	Portsmouth, United Kingdom	People in this frame of mind don't think. They just do. With disastrous consequences. These could easily be avoided. Please also see the 'Free Aung San Suu Kyi' petition.
74.	Cindy Hansen	Chicago, IL	
75.	par ridder	minneapolis, MN	
76.	Mike Wing	Chicago, IL	
77.	Howard Greynolds	Chicago, IL	One life is enough for this barrier to be put up. Matt was a great guy and to know this tragedy could have been avoided long enough for him to reach out and talk to someone and maybe be alive is truly tragic.
78.	Aimee Fuller	La Mesa, CA	A safety barrier installed may save the lives of many people in the future!
79.	Laura Cadieu	Irvine, CA	I believe it is a true tragedy for someone to take their own life! Can you imagine the immense pain someone has to be in to take that step? My hope is that ANYONE who is dealing with depression, solemn sadness or fear will feel comfortable enough to discuss their feelings with a loved one before taking a step like this...without the stigma that comes from a society that doesn't allow weakness! My prayers are with anyone and everyone who either knew Matt or felt that they could relate in some way or another!
80.	Anton Anderson	Altadena, CA	This safety barrier should be built. Let this tragedy be the last.
81.	Nina Firestone	Santa Barbara, CA	
82.	Krystal Robbins	El Segundo, CA	
83.	Simos Tarabatzis	Kavala, Greece	
84.	Harry Redinger	Altadena, CA	It just makes sense to do something to stop this location as being the final destination in the lives of those seeking an ill-logical solution to a temporary life situation. Harry V. Redinger
85.	kathy peters	Marquette, MI	Treatment works...
86.	james lung	venice, CA	Though our first reaction may be that individuals will just take their lives by some other method, research shows that there would be a net reduction in suicide by erecting a physical barrier.
87.	Sarah Jelencic	Fox Point, WI	I hope that you never have to experience the loss of a loved one through suicide. It is devastating. As a result of what is often a split second decision made at a time of overwhelming (but temporary) personal pain, a valuable life is lost and the lives of all of those who have been touched by that life are irrevocably and negatively impacted. The cost to society is much, much more than the loss of one person. We know that Cold Spring Bridge has, <i>(continues on next page)</i>

	Name	From	Comments
87.	Sarah Jelencic	Fox Point, WI	<i>(continued from previous page)</i> and will increasingly, become a magnet for people in this type of temporary, but overwhelming pain. With that knowledge, how can we as a society refuse to build a barrier that would not only put an end to the fatal attraction of this particular bridge, but also make the bridge safer for everyone? Everyday that we delay building this barrier, we risk the loss or devastaton of more lives. Please, please support this barrier. I have lost two people I loved dearly to bridge suicides. You do not want to, and you should not want others to, experience this type of loss especially if you have the means to prevent it.
88.	Shayla Thiel-Stern	Minneapolis, MN	Please consider adding a barrier to this bridge. Matt Aydelott, the most recent person to use this bridge to take his life, was a good friend and an amazing guy in general, and I personally believe there is a chance he'd still be with us now (or at least for a little bit longer) if this bridge didn't look so inviting to him at the end of one particularly dark night. He made a difference in the lives of so many in Santa Barbara County through his work at Cuesta College that I think the Board of Supervisors owes him -- and all of them -- a second look at moving ahead with adding a taller railing or barrier to this bridge.
89.	Ania Ritter	Minneapolis, MN	Every death is tragic. Suicides from this bridge can and should be prevented. Please install a safety barrier.
90.	Oriana Sanders	Santa Barbara, CA	I fully support the barrier project. As a citizen I would rather sacrifice tax payer dollars and potential alteration of "scenic view" in order to save even one life. I can't understand how the arguments against the project can even be considered to have merit when we are evaluating the worthiness of protecting human life. Please, if you are within reasonable distance of the public hearing on October 16th, come and show your support for the project!
91.	Maura McDermott	Delmar, IA	Please prevent more lives from being taken! By not taking action against this, it is enabling individuals to injure themselves and take their own lives. Please install the necessary barriers!
92.	YVonn Teh	Los Angeles, CA	
93.	Josephine Ferrigno-Stack	Westfield, NJ	Please construct the safety barrier - a small change will prevent great tragedy.
94.	Christina Kitchen	Camanche, IA	If your objection to the proposal is based on cost, if it was your life, or the life of your daughter, son, best friend...would the value of a dollar matter?
95.	Kristin Robbins	Torrance, CA	Please do something about the danger of this bridge!! 46 lives lost is way too many!!! My step brother was one of them and the pain we all have is not something one can put into words. Help make a change!

	Name	From	Comments
96.	Jacob Avidon	Oakland, CA	Please install a barrier on the Cold Spring Arch Bridge to prevent the tragic deaths of loved ones. Sincerely, Jacob Avidon
97.	Jenna Stanley	Chicaco, IL	I hope that the safety barrier is a no brainer...It's not only for suicide prevention...but how about preventing others from dropping children or pets or whatever. In dire times..people aren't logical...and if already this many lives have been lost...we need to do something. God Bless all the family and friends that have been touched with this tragedy. May Matt's death not have been in vain and other lives saved.
98.	Nicole Wolter	Melrose Park, IL	Friends and families of suicide victims have an enormous burden to bear. My prayers go out to all those who loved Matt. I applaud the efforts of those who are trying to make a difference in others lives after the loss of a loved one.
99.	Lisa Wenger	Rancho Palos Verdes, CA	
100.	Lisa Szymanski	Venice, CA	This is important on many fronts regarding safety: http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html
101.	diane bobich	San Carlos, CA	The cost is too great NOT to put up a barrier.
102.	Pam Boland	Grovetown, GA	
103.	Nate Rogers	Santa Barbara, CA	
104.	Panagiotis Rigopoulos	Patras, Greece	
105.	Lindsay McCann	London, United Kingdom	
107.	Ralph Xx	Freeland, Germany	As a psychiatrist, I cannot agree more with the need to erect a physical sfaety barrier
108.	Lisa Rubinstein	Winchester, MA	
109.	Amy Idzik	Chicago, IL	A very good friend of mine lost her son to this bridge. PLEASE do something so no one else has to go through the grief she has had to.
110.	Michelle Nuehring	marion, IA	
111.	Marlo Cornejo	Pasadena, CA	
112.	Camille Rillorta	Pasasdena, CA	
113.	Phyllis Pollack	Worcester, MA	In honor of the work of Jennifer Lake has done on behalf of this effort.
114.	Dave` Metz	Somerville, MA	As a Professor of Medicine I feel that this is a cheap way to prevent further deaths.
115.	Charles van der Horst	Chapel Hill, NC	
116.	Heather Murphy	Seattle, WA	
117.	Agnieszka Gal	Elmwood Park, IL	

	Name	From	Comments
118.	Christopher Castaneda	Chicago, IL	
119.	Lance Dodes	Newton, MA	
120.	Debra LOOMIS	Middletown, CA	Please do this!
121.	Maggie Cox	SLO, CA	WE LOST A LONGTIME FRIEND AND VALUED EMPLOYEE IN 2002 ON THE COLD SPRINGS BRIDGE. I HOPE THE INSTALLATION OF A BARRIER WILL PREVENT THE LOSS OF ANYONE ELSE-- AND THE STAGGERING PAIN ENDURED BY THOSE LEFT BEHIND.
122.	JoAnn Kubick	east windsor, CT	95% of people who have attempted suicide do not go on to die by suicide. This is something that YOU can do to help to stop this tragic loss.
123.	Marie DeSousa	BROAD BROOK, CT	SUICIDE PREVENTION PROGRAMS ARE OFTEN NOT FUNDED AND LOCATIONS LIKE THIS BRIDGE ONLY HELPS TO FUEL SOMEONE'S THOUGHT PROCESS WHEN THEY ARE CONTEMPLATING SUICIDE.
124.	Mary B. Strauss	Oakland, CA	This beautiful bridge will still be beautiful with a lifesaving suicide barrier installed. Each saved life is important to all of us.
125.	David K. Anderson	Arroyo Grande, CA	I have a professional relationship with a mother of a son who recently died. What a loss, had the barrier been installed the young man would still be able to talk to Mom. This should be a no brainer.....Please install the barrier NOW...
126.	Karolina Krysinska	Spring Hill, Aruba	
127.	Donna Murphy	Riverside, CA	
128.	Can Atik	Ankara, Turkey	
129.	Laura McGrath	Sudbury, MA	Dear Board of Supervisors, I have traveled across the Bourne Bridge on Cape Cod in Massachusetts for many years, both before and after a suicide prevention barrier was installed. It remains the best place to get a sweeping view of the Cape Cod Canal. The barrier doesn't alter the view in such a way that the viewing experience is disappointing.
130.	joel rubinstein	Winchester, MA	I am a psychiatrist and believe strongly in this cause. Suicide is often something someone comes to in the moment. Anything that gives a person pause will be very effective. Many beautiful bridges have such barriers including the golden gate and the bourne here in massachusetts. It is crucial. Yes people may find other ways, but on the other hand, they may resolve the depression or the problem and live productive wonderful lives.

	Name	From	Comments
131.	Katie Osborn	Brooklyn, NY	
132.	Lisa Sieck	Chicago, IL	My friend died on this bridge, someone's brother, someone's friend, someone's son is gone. My friend Matt's death is a true tragedy, it very well could have been prevented if he didn't know about a place where too many other people ended their lives. Please, please don't let this happen to other people, don't let this happen to someone you love.
133.	milda Bublys	ny, NY	
134.	Mary Villarreal	Laguna Vista, TX	
135.	Wieslaw Gal	elmwood park, IL	
136.	kaye slavet	san mateo, CA	please add a safety barrier to this bridge. if it saves even one life, it will be worth it.
137.	arnold slavet	Weston, MA	Matt's death has been a devastating loss to my family, his friends and to the many hundreds of people he helped in his work. More than anything else I am haunted by the period of time between reports of Matt on the bridge and the arrival of rescue personnel. There is little doubt that a barrier would have bought him (and all of us) more time - to change his mind and to be rescued. To leave the bridge without a barrier for even one more day is unthinkable. The barrier must be built as soon as possible.
138.	JOHN ROBBINS	PASADENA, CA	The Cold Spring Arch Bridge lacks the basic safety provisions for any person that through emergency or other reason might have to cross the bridge on foot. It also has proven to be a high incident suicide attraction without restraint. Please install a physical safety barrier to neutralize the unsafe conditions and save future lives. The safety of our families and law enforcement professionals hangs in the balance. Upstanding citizens could not allow such hazardous conditions to continue.
139.	Melissa Nemeth	Lake Forest, CA	My father committed suicide by jumping off the Cold Springs Bridge in 2001. Its time to end this needless deaths of people who have families that loved them. Just because a person is suicidal does not mean they will commit suicide and a physical deterrent is the preferred method by therapists who know that once the person has time to think it through, the decision may change. Please lets not make any other family go through the tragic death of a loved one due to this bridge.
140.	Ann Lippincott	Santa Barbara, CA	If one person died at Cold Spring Arch bridge, it would be one too many. The human cost is too high. I am 100% in support of such a barrier.
141.	edward emerson	santa barbara, CA	
142.	Majka Burhardt	Buolder, CO	
143.	Christina Powell	Bozeman, MT	

	Name	From	Comments
144.	gerald slavet	wellesley, MA	
145.	Frederic Dick	La Jolla, CA	
146.	Natasha Kirkham	London, United Kingdom	
147.	Laurie Gilbert	Venice, CA	It's shocking that this bridge would not have these safety measures in the original plan for building. It's especially concerning - especially with the proof of so many suicides at this location. It's so obvious that something should be done now. Please make this important change to this bridge - or it will continue to be an easy location for suicides. It could even allow accidents that should never happen... Laurie
148.	Linda Jorn	Minneapolis, MN	
149.	Sarah Somers	Chapel Hill, NC	Suicide is often the result of a passing impulse. Many who unsuccessfully attempt suicide can get help and live long and happy lives. Please make it harder for people to end their lives based on a moment of despair.
150.	Jennifer Cosenza	Brookline, MA	We can make a difference by constructing the safety barrier.
151.	Bill Jewell	Sterling, VA	
152.	Susan Cartledge	new york, NY	
153.	Margaret Clayton	Santa Barbara, CA	
154.	Everett Clayton	Santa Barbara, CA	
155.	Jill Eichner	Alexandria, VA	As a friend of the relatives of Matt Aydelott, I support the safety barrier so that future suicides can be prevented.
156.	H Thompson	Seattle, WA	I lost a friend to suicide many years ago, and it still hurts. If a safety barrier prevents people from taking their own lives and permanently affecting the lives of the people who love them, how can it not be a good thing?
157.	Andrea Price	Washington, DC	I am writing to support changes to prevent suicides from this bridge. It's not only the lives of the 46 who have died already, but the number that will be added to that each year. It has been shown in Washington DC that the addition of a barrier on one of the bridges over Rock Creek Park that was attracting suicides did NOT increase the number of suicides on a similar bridge 1/4 of a mile away. This disproves the notion that people will easily change their plans if thwarted - no, it may not happen anyway. But it WILL happen if nothing is done to prevent it. Thank you for your time.
158.	Dana Bowman	Palo Alto, CA	Please construct a barrier since this bridge is too tempting of a landmark for people who are depressed. They would not take their life if it was not so easy. Being near USCB, <i>(continues on next page)</i>

	Name	From	Comments
171.	John Brooks	Tiburon, CA	We lost our 17 year old daughter and only child to the Golden Gate Bridge this past January, and finally after 7 decades the bridge board has voted for a safety barrier. The objections to the Cold Spring Bridge barrier - including Garrett Glasgow's "study" - are utter nonsense. The cost to your barrier is a fraction of the cost for the GGB barrier. This should be a no brainer. By doing nothing the naysayers are signalling that they are quite comfortable with the deaths continuing. To the supporters of a barrier stay strong. We are aligned with you and stand ready to help in any way. To underscore the seriousness and urgency of this tragedy, it was reported that a UC Dave student recently jumped to her death from the GGB. How many more deaths does it take for people to wake up ... unless the death strikes close to home...
172.	Matthew Tolander	West Hollywood, CA	
173.	bertie anderson	Greenville, PA	
174.	LISA LAPLACA	SANTA BARBARA, CA	
175.	Anna Zamir	Santa Barbara, CA	
176.	Robert Feinberg	Santa Barbara, CA	
177.	Andrew Cranmer	Goleta, CA	Let's make it harder for those despairing souls who are seeking a permanent solution to a temporary problem. If suicide from the bridge becomes more difficult to do, lives will be saved.
178.	nicki frank	Los Angeles,, CA	
179.	Irma Catlett	Santa Barbara, CA	I drive across the bridge at least twice a day, sometimes four. As I have become more aware of the problems of suicide and the preventions, I CAN NOT understand why the barrier is taking so long to build. Too much time has been spent in discussion. When I weigh the aesthetics of a bridge with lives being saved there is no question in my mind. Build the barrier!!!!!!
180.	Arlene Moody	Santa Barbara, CA	We need the barrier to save lives. Please!!!!!!!
181.	Sarah Adams	Santa Barbara, CA	No amount of money or aesthic value can compare to the value of a human life. Please install the barriers and keep our community safe. Sarah Adams
182.	Julie Lubin	Santa Barbara, CA	
183.	Jessica firestone	Santa Barbara, CA	This seems like the highest priority if it can save even 1 life.
184.	Geoffrey Parr	Santa Barbara, CA	I feel that this is a very important step in our efforts to stop suicides
185.	BRUCE ARDEN	santa barbara, CA	Thank you for your support of this important project
186.	Kristina Jansen	Santa Barbara, CA	

	Name	From	Comments
187.	Heather Quisenberry	Pittsburg, CA	If constructing a barrier saves one life, it was worth the cost. Why is it that people balk and fuss over the cost associated with doing the right thing until it is one of our loved ones who perishes. Once suicidal, always suicidal in a MYTH per Seiden Phd. UC Berkeley Study, "Where Are They Now" & common professional opinion in the mental health community. To use this excuse is ignorant; it is a fallacy that cannot hold up under research. I hope we get to the day that SAFETY BARRIERS ARE PART OF THE ORIGINAL CONSTRUCTION. Adding one later is the moral imperative to correct a design safety flaw. We are human beings. We need to try to protect and respect life without first asking "What's in it for me?"
188.	kelly Kirscht	palo alto, CA	
189.	Gloria Cevallos	San Francisco, CA	It is well past time for those who are selfish or unwilling to open their hearts and their minds to the truths about suicide. It is well past time to place a barrier on the bridge.
190.	Tamara Short	Santa Barbara, CA	
191.	Richard Tubis	Santa Barbara, CA	It well past the time when we construct these barriers to save lives and stop impulsive suicide victims.
192.	Paul Trent	Santa Barbara, CA	
193.	Hugh O'Halloran	Bayside, WI	
194.	Joyce Catlett	Santa barbara, CA	
195.	Ali Ironside	Santa Barbara, CA	I have terrible images in my mind every time I drive across this bridge, physical barriers are such an easy way to help stop the tragic events that have happened here. It seems criminal not to do it.
196.	Trish Lane	Santa Barbara, CA	That moment when one feels such pain, is alone, has access to the means to take one's life (the bridge in this case) ... that culminates in attempts on ones life are fairly limited times. Just because the unfortunate cliché in our culture ... "suicide - a permanent solution to a temporary problem" has become cliché -- doesn't make it less true. Most of the combinations that result in suicide are truly temporary. So many people are saved by having one of those moments blocked, intervened upon, interrupted, etc. It is our responsibility to make sure that - to the extent we can (and here we can) give people a chance to obtain treatment and help. As a suicide survivor, I'm glad to be alive. If there were better laws in this country ... I wouldn't have had the means to have needed to survive - and without the stigma on getting help ... so many more will take the route of help instead of death. 25 years later ... a lot of therapy, study in suicide ... a lot of work with people ... VERY, VERY few truly want to die. Let's make sure that they get to make healthy choices, are offered affordable <i>(continues on next page)</i>

	Name	From	Comments
196.	Trish Lane	Santa Barbara, CA	<i>(continued from previous page)</i> treatment and are supported through whatever is causing thier pain and desperation.
197.	Nancy Del Signore	Kattskill Bay, NY	
198.	Sara Bartlett	Santa Barbara, CA	I feel very strongly that a safety barrier should be constructed at Cold Springs Bridge to reduce or eliminate suicidal jumps from this location. . . I think this humane solution is in the interest of our community. Continuing to allow preventable deaths to occur at this location is highly unethical. In addition to the needless loss of human life and resulting stress to many individuals involved in each attempt, suicides and suicide attempts divert the energy of law enforcement and recue personnel from other potential rescue efforts.
199.	Benjamin Levin	Whitefish Bay, WI	Please act to construct the barrier as soon as possible. Each life saved or lost at the Cold Springs Bridge will affect family and friends throughout the country.
200.	LeAnn Wooldridge	Hayward, CA	My heart goes out to Matt's family and friends. I lost my boyfriend to the Golden Gate Bridge in July of 2007. In a moment of despair, he chose a permanent solution to a temporary situation. A barrier may have given him more time to think about what he was about to do, just as a barrier on the Cold Springs Bridge would give to someone in a similar frame of mind. One wouldn't put a loaded gun in front of someone who is depressed or mentally ill... but that is exactly what these bridges without barriers are. Remove the loaded gun... put a barrier up now!
201.	David Baldwin	Santa Barbara, CA	
202.	nancy miller	Manhattan Beach, CA	Anything we can do to create physical or mental means of preventing suicide is a huge step forward in acknowledging the fact that suicides affect everyone in society. Family, friends, associates, work-related and acquaintances are all affected by a suicide. Any barricade or temporary distraction from the act of suicide could save a live.
203.	Elizabeth Cardone	Jamestown, NY	
204.	Karen Campbell	Citrus Heights, CA	
205.	Larry Bonney	Milwaukee, WI	
206.	Brian Belanger	Londonderry, NH	Create a barrier on the Cold Springs Bridge. Do the right thing. This is not a political issue. It is a human issue. How many people have to die to make the answer clear? I would think that number was exceeded long ago.
207.	Marianne Wolf	Pismo Beach, CA	Create a barrier on the Cold Spring Bridge. It will save lives. Life is precious. I drive the road often between Shell Beach & Ventura. The barrier will enhance safety for all.

	Name	From	Comments
208.	steve firestone	santa barbara, CA	
209.	Sally Bemus	Palo Alto, CA	
210.	Richard Seiden	Los Angeles, CA	
211.	Tamsen Firestone	Santa Barbara, CA	I am familiar with the work of the Glendon Association so I am very aware of the suicides that have occurred on the Cold Springs Bridge. I have heard from survivors of suicide, from police officers who have been at the scene of suicides, and from suicide survivors themselves. All of them have spoken passionately about how a barrier would stop people from committing suicide and interrupt the self-destructive process these people are in.
212.	Janice Tagart	San Francisco, CA	The Golden Gate Bridge in San Francisco was approved last Friday to build a suicide barrier--after 71 years and over 1,300 deaths. Don't let more people die at the Cold Spring Arch Bridge. Take action now.
213.	Jeffrey Kobulnick	Los Angeles, CA	
214.	Richard Smith	santa Barbara, CA	
215.	Rev. Birte Beuck	Oakland, CA	Some suffering we cannot avoid. This suffering we CAN avoid. Bridge barriers save lives.
216.	Sonia Salinas	Los Angeles, CA	Constructing this barrier is a small project that will prevent immense tragedy.
217.	Christine Kaplan	Lafayette, CA	I absolutely support a barrier on this bridge... it has been proven around the world on other magnet structures that have easy access, that building a barrier does save lives.
218.	Eve Meyer	San Francisco, CA	
219.	Leah Walker	Liberty, MO	
220.	Erika Brooks	Tiburon, CA	There is not 1 legitimate reason that a physical barrier would not save lives. Not one. People who are in a suicidal state of mind, or are overcome by an urge , are not thinking clearly or rationally. Their thinking and emotions are impaired. To those individuals, who oppose a physical safety barrier on the Cold Spring Arch Bridge, I have just one question: Would you wish for yourself or one of your loved ones to be allowed to drive home after a party, if they are clearly impaired? Or would you prefer that the host at the party, establishment recognize that they are impaired and make sure they got home safely in some other way? Not putting up a physical barrier on the Cold Spring Arch bridge, when it is proven that people are dying there while they are in a disabled state of mind, makes the people who are against a barrier,indirectly complicit in the deaths of the people who end their lives there because there is no barrier.

	Name	From	Comments
221.	Julie Steckel	Santa Barbara, CA	I am a psychotherapist who deals with depression and suicidal ideation. It is a good idea to have a barrier on the bridge because suicidal wishes are often transitory and frustrating the attempt will save the lives of people with mixed feelings. Those who are serious about it will find another way..
222.	Jonathan Bollag	SANTA BARBARA, CA	
223.	Cassandra Gaeto	San Luis Obispo, CA	
224.	Dulcey Simpkins	Westminster, CO	Please consider the scientific evidence about barriers and the irreplaceable value of these lives in your community and nationwide. Thank you!
225.	Isabel Oliveira M.A., MFTI	Santa Barbara, CA	The look of the bridge should not be more important than people's lives. The argument that suicidal persons would kill themselves anyway after being deterred by the barrier is not true, just look at the studies!
226.	john kevin hines	san francisco, CA	My name is Kevin Hines, I attempted to die by suicide in 2000 by jumping from the Golden Gate Bridge, It is a miracle that I survived and I am one of less than 2% that have. Recently the Board of the Golden Gate bridge have voted to put up a suicide prevention net much like a safety barrier. Think of the lives lost to both the GGB and the Cold Springs Canyon bridge. These are people who have loved ones, these are families destroyed forever. We need to save those who simply cannot save themselves. Please sign this petition, what if it were a family member or friend of yours.
227.	Tami Smason	Los Angeles, CA	
228.	Judith R. Brown	Santa Barbara, CA	I have not lost anyone through suicide from the bridge but I have so much sympathy for those who have - or may in the future - that I want to support their efforts in having a barrier constructed.
229.	Steve Feinberg	Santa Barbara, CA	I drive over the bridge and it is very painful to see the emergency crews there with traffic stop. Seems to be happening more all the time. I don't want this bridge to become famous for jumping like the Golden Gate Bridge in San Francisco.
230.	Mary Malik	San Luis Obispo, CA	Bridge safety barriers are an effective way of saving lives. I am very much in support of a physical safety barrier on the Cold Spring Arch Bridge.
231.	Dan Bech	Oakland, CA	We need to put prevention measures in place when it comes to suicide. The tragedy and detestation that suicide causes is unnecessary and preventable.
232.	Meg Kinney	New York, NY	
233.	sally stewart	Santa Barbara, CA	

	Name	From	Comments
234.	Christy Frank	San Gabriel, CA	I support the construction of a physical safety barrier on the Cold Sprign Arch Brdige. I believe the safety barrier will prevent future deaths, including suicides.
235.	Douglas Spellman	Newbury Park, CA	I believe it is absolutely imperative that we, in any and every means at our disposal, successfully curtail the possibility of people committing suicide off this historic and beautiful bridge. I believe anyone voting against a life saving physical barrier is voting against the future of humanity itself.
236.	Alicia Mitchell	Santa Barbara, CA	
237.	jon slavet	san francisco, CA	My step-mother's nephew tragically took his own life by jumping off this bridge. We hope and pray you listed to those who sign this petition. Thank You. Jon
238.	Lynn Newman	Palo Alto, CA	It's time we created a barrier that would prevent those who might change their minds if deterred for a while, from jumping off the bridge.
239.	Christina Hale+	PASADENA, CA	Everyone should have a second chance to contemplate, evaluate, breathe deeply upon, and to think about those that love him, one's purpose in life, or the pursuit thereof. A physical barrier oblidges a moment of reflection on a life well-lived; and a life-instilling, breath-taking view of the bounty and vast beauty of nature and personal connections is invaluable. A physical barrier on the Cold Spirng Bridge would give a moment of pause...equally to those on the edge and to those confronting life's challenges without support.
240.	Jenai Wu	boston, MA	as a clinical psychologist I strongly support a construction of a barrier as a remedy to prevent suicides.
241.	corey linberg	Goleta, CA	
242.	heidi ortiz	santa maria, CA	I travel over the bridge everyday on my way to work. There definately needs to be a safety barrier in place to prevent loss of life.
243.	Colleen Carpenter	Fort Wayne, IN	Please help save lives. Contrary to popular belief, the people who will come to your bridge to jump but find a barrier won't necessarily find another way to die. Research shows that most will not find another way to die.
244.	Christopher Lake	London, United Kingdom	Have the courage to do the right thing. Build the barrier.
245.	Marilyn Doyle	Weston, MA	
246.	Nathan Shadle	Santa Barbara, CA	
247.	Martin Aydelott	Minneapolis, MN	Some tragedies are preventable, and sometimes government actually is the answer.
248.	Jenny Taylor	Cypress, CA	

	Name	From	Comments
249.	Brian Bellucci	Scituate, MA	PLEASE build this safety barrier. It is a shame that 46 people have died and nothing has been done to prevent this. I knew Matt Aydelott and I'm deeply saddened that his death could have been prevented.
250.	Beth Monge	Littleton, CO	
251.	MIKE MONGE	LITTLETON, CO	
252.	Andrew Stockwell	Somerville, MA	
253.	Patricia Duma	Anaheim, CA	
254.	Gil Varon	Santa Barbara, CA	
255.	Janice DeLoof	Fullerton, CA	Everything must be done to prevent a suicide. It is often an impulsive action.
256.	Louise Mooney	Berea, OH	One death is one too many. 46 is a tragedy and failure to build a barrier is governmental mismanagement. Please listen to the voices of the citizenry.
257.	Marsha Peterson	Santa Barbara, CA	I strongly believe that limiting access to the means to commit suicide by building a barrier at Cold Spring Bridge will be an effective way to decrease a suicidal person's chances of following through on a temporary, irrational urge and allow time for them to get appropriate help. Marsha Peterson, RN, MS, MFTI
258.	Marni Goldenberg	San Luis Obispo, CA	
259.	Allison Andrade	San Francisco, CA	I am a big supporter of the barrier idea. !!! Allison Andrade
260.	Jill Davies	San Luis Obispo, CA	
261.	Nissa Belanger	Londonderry, NH	
262.	Zachary Thomas	Wilbraham, MA	
263.	Julia Inobe	Lafayette, CA	
264.	Amber Del Signore	Queensbury, NY	
265.	Father Stacy	Sovang, CA	The bridge will still look beautiful with a suicide barrier. Drivers should be looking at the road and not the view. I have an MS in rehabilitation counseling and my professional experience tells me that this is the right thing to do. For 31 years I was the priest at St. Mark's Episcopal Church in Los Olivos. I buried people, friends and neighbors who jumped from the bridge. A barrier is a positive thing.
266.	Jordan Oathout	Queensbury, NY	
267.	Mark Del Signore	Queensbury, NY	Simply reading the entries on this Petition one can feel the pain and sorrow of those impacted by Matt's recent passing, as well as those who passed before him. After three years of "discussion" and three deaths at the bridge since February of this year, it would seem that there is more than red tape on the hands of those delaying the <i>(continues on next page)</i>

	Name	From	Comments
267.	Mark Del Signore	Queensbury, NY	<i>(continued from previous page)</i> construction of a barrier to prevent future tragedies. I implore those decision makers reading this petition to make the right decision before another person's untimely death at the Cold Spring Arch Bridge.
268.	Michael Calawa	Westwood, MA	
269.	Patricia Lobenberg	Santa Barbara, CA	For more impact, add a personal comment here
270.	David Chen	Chillicothe, OH	
271.	marc hyman	santa barbara, CA	Please build the safety barrier. If only one life is saved, it is worth it.
272.	Meghan Talt	Santa Barbara, CA	Please support this safety barrier!! It can save lives. Thank you.
273.	Mimi Muraoka	Santa Barbara, CA	I strongly support the Cold Spring Bridge barrier as proposed. There is no other effective, practical way of preventing people from easily taking their own life. This will make it more difficult. Other easier to obtain methods (pills) are less lethal than jumping from that bridge. Mimi Muraoka
274.	Rebecca Sweeney	Santa Barbara, CA	Santa Barbara's beautiful landscape will continue to shine through the barrier. We have a moral obligation to stop the Cold Spring Bridge from being used for suicide. It has become an easy option for those under mental duress. Future lives saved by the barrier will enhance Santa Barbara's beauty exponentially.
275.	Lara Mondae	Chicago, IL	
276.	Stuart and Janie Goldenberg	San Luis Obispo, CA	We hope this lovely bridge can be made safer with a physical safety barrier.
277.	Kathleen Petty	Lompoc, CA	
278.	Michael Olstad	San Marcos, TX	Recently my sister lost her oldest son when he made the decision to jump off the bridge. Often these decision are made spontaneously and without a lot of fore thought. A barrier may very well have made a difference, giving Matt the opportunity to seek out and use the support that was in place and available for him. Is not the cost of a barrier worth one life . . . let alone 46?! The decision you make WILL, or not, save lives! Thank you for your consideration, Michael Olstad
279.	Matthew Monge	Cypress, CA	Please help prevent more suicides from this bridge.
280.	Anna Campbell	Santa Barbara, CA	re no turnouts on the bridge where you can stop and enjoy the view. However, there is a magnificent panoramic view when you go over the pass right before the bridge, and there is a turnout right after the bridge where you can stop and enjoy the view as long as you like. It is s24 miles from <i>(continues on next page)</i>

	Name	From	Comments
280.	Anna Campbell	Santa Barbara, CA	<i>(continued from previous page)</i> Cachuma Lake to Santa Barbara, and longer to Solvang or other destinations, with beautiful views all along the way. Is two seconds of a diminished view worth even one life? I don't believe so. The "beauty" argument against the barrier is much ado about nothing. However, the argument for the barrier is strong. More than forty lives have been lost. Each dear one who dies has family and friends who will feel the loss every single day. I know, because my Skye died, not on that bridge, but whenever I cross that bridge I feel the pain of those families. Build the barrier now, before there is another needless death. If there were an intersection where 42 people had died, it would have been fixed years ago. It's high time to fix the bridge, and when the barriers are up, we will drive on that bridge and be able to fully enjoy the view.
281.	Peggy Schramm	Waukegan, IL	
282.	Dan Braun	Goleta, CA	
283.	Gary Lazarski	Chicago, IL	
284.	Ingrid G.	Chicago, IL	
285.	Jennifer Bellucci	Sditate, MA	
286.	Doris Noyes	Dingston, NH	
287.	Jeremy McKittrick	Montpelier, VT	
288.	Susan Lynch-Aumaier	Fountain Valley, CA	Why in the world weren't barriers put up when the bridge was built? That's just common sense.
289.	Neile King	Brooklyn, NY	No view is as beautiful as a human life.
290.	James Costello	Hingham, MA	
291.	Nanette Orman	Half Moon Bay, CA	As a member of the Psychiatric Foundation of Northern California, with an interest in suicide prevention at the Golden Gate Bridge, I want to add my signature to this petition as well. It is inhumane not to protect those who are impulsive or severely mentally ill from ending their lives when they are not rational. Some day they will be healthier mentally, and they will then be grateful that they were protected.
292.	Lisa Rivera	Nipmo, CA	Please move forward with the barrier. If it were more difficult for people to access it may give someone precious moments to reconsider their options. Years ago my father worked on this bridge. There had been a suicide while the men were working on the bridge, and this traumatically effected my father and the men he worked with. Now with the loss of Matt Aydelott, I want to do my utmost to be involved in helping this project move forward. The community has lost a treasure, and maybe if he had a few more moments to reconsider is plans, he would still be with <i>(continues on next page)</i>

	Name	From	Comments
292.	Lisa Rivera	Nipmo, CA	<i>(continued from previous page)</i> us. The opponenets of the bridge want to keep it original design, but if it evokes horrible thoughts to residents who have to drive over it, what good is that. Please move forward with the barrier. Thank you.
293.	anne greaney	santa barbara, CA	46 deaths are enough! I am thrilled that SBCAG voted unanimously yesterday to have CalTrans proceed with building the bridge. A barrier will be the most aesthetically pleasing sight in Santa Ynez. A tribute to those who have needlessly lost their lives!
294.	Mary Teetor Zablotny	San Francisco, CA	My husband and I lost our delightful 18-year-old son nearly four years ago when he jumped from the Golden Gate Bridge. The devastating impact on our lives continues to this day. Suicide is not a rational, freely chosen act. It is always a response to unbearable psychological or physical pain. Suicide is how mental illness kills people, in the cruelest way, by forcing them to turn their own hands against themselves. In these cases -- the major cause of suicide -- the deaths should be considered to be deaths as the end result of a disease process. Just like any other deaths due to illness! It follows, then, that society does have a moral responsibility to do everything it can to prevent suicides. I have been working, along with many other people, to get a suicide barrier built on the Golden Gate Bridge. We have finally succeeded in persuading the Golden Gate Bridge Board to vote for erecting a net under the Bridge; the next battle for us is finding the money. So I have heard all the arguments against suicide barriers. They go something like this: Suicide is a choice, and so we should just let them do it; they're all just mental cases anyway; they'll just go somewhere else to kill themselves; the bridge is so beautiful, and a suicide barrier would ruin its beauty. I am so disgusted by these arguments, and the others that I have not included. In the case of the Golden Gate Bridge, those of us who advocated for a suicide barrier did our homework and presented the Bridge Board and the public with rafts of facts, studies, and numbers. Those who were against a suicide barrier kept trotting the same tired dreck over and over, with no facts to back up their allegations. They stubbornly clung to their willfully ignorant misconceptions. I most fervently support the construction of a suicide barrier on the Cold Spring Bridge. If there is anything else I can do in support of this effort, please feel free to contact me.
295.	Jenni Olson	San Francisco, CA	I have been involved in the Golden Gate Bridge suicide barrier campaign for many years. This is a public safety issue. Please erect a barrier.

	Name	From	Comments
296.	Seiko Niimi	Pomona, CA	
297.	DC Robbins	Los Angeles, CA	Having recently lost a family member at this bridge, I see no downside...at worst cars driving by at 50 MPH miss a little visibility. Seems like a small price to pay for potentially saving a life...
298.	Noah Kaplan	Oakland, CA	
299.	lisa wollack	mpls, MN	One person is so precious. Please don't waste more time.
300.	Dave Hull	Oakland, CA	The body of research is clear that barriers are the best prevention of suicides on bridges, that most bridge suicides can be prevented, and that most attempters once prevented do not subsequently kill themselves. Dave Hull, president, Bridge Rail Foundation (Kathy Hull, October 26, 2003)
301.	Lourdes Hernandez	Arcadia, CA	I didn't know Matt Aydelott, but I know his mother and the anguish she's going through as a result of Matt's death. It is imperative that safety barriers be placed on bridges. If the barriers were there, it would prevent such tragedies.
302.	Loretta Boyle	Penngrove, CA	
303.	Jan Poppe	Midland, MI	As with cancer, there is probably none amongst us who hasn't had suicide touch their lives in some way. Like cancer, suicide isn't anyone's "fault" and if we, the people, can help in even the simplest ways (like signing a petition), then it is our responsibility to do so.
304.	kayla unger	lafayette, CA	
305.	Peppe Rose	Paso Robles, CA	On September 8th, 2008 I lost a friend and colleague when Matt Aydelott listened to the voice of pain. To quote Matt's cousin, Jenny, "Pain has a voice and it lies". The choice Matt made to end his life by leaping off the Cold Spring Bridge took a valuable and contributing member of the San Luis Obispo County's society. Matt touched many lives as he championed the youth of our county. He used his unique talents and empathy for the underserved and often unseen populations to guide them towards self-sufficiency and life-long learning. Statistically, we can divide the number of suicides by the number of years the bridge has been here and come up with arguments for and against the barrier. We can also multiply the number of suicides by the number of people affected by that suicide. In Matt's case besides loving family and friends, the programs Matt supervised for Cuesta College served between 400-600 youth a year. He will be greatly missed as we try to continue to working toward his goals and visions for these programs. I am in support of a barrier for the Cold Spring Bridge. As the bridge stands today it is unsafe. We have an opportunity to add safety for the cars and families traveling <i>(continues on next page)</i>

	Name	From	Comments
305.	Peppe Rose	Paso Robles, CA	<p><i>(continued from previous page)</i></p> <p>the bridge and for the police, sheriff and other community service workers that monitor the bridge. And we can take away the lure of the suicide magnet this bridge has become.</p> <p>The Psychiatric Foundation of Northern California supports a suicide net for the Golden Gate Bridge. They contend the effectiveness of barriers have been dramatic at landmarks like the Empire State Building and the Eiffel Tower. In fact, in a study of 515 people who were stopped from jumping off the Golden Gate, 94 percent of them were still alive or had died naturally long after their thwarted attempts. (Tribune, October 11, 2008) We can't know the impact to society by removing the ease of using the Cold Spring Bridge as an answer to despair and hopelessness. But I will take those odds if this lure is taken away and a different voice other than the voice of pain could be heard by a person reaching despair and the choice to continue to be with us is made.</p>
306.	Brooke Winkler	San Francisco, CA	
307.	Doris Lew	Napa, CA	<p>Please build a barrier to stop the needless deaths. My husband and I lost our son to a suicide on the Golden Gate Bridge in May 2007 and do not wish the kind of hell we are still living through on anybody else.</p>
308.	pamela reeves, m.d.	goleta, CA	<p>I am a practicing psychiatrist and have been very vocal about this issue. It is nothing short of extreme neglect if this barrier is not erected. Please take a look at the information available about the Cape Cod bridge, which has an excellent barrier, has prevented numerous suicides, and was built by the Army Corps of Engineers. These tragic deaths are preventable in many cases with barrier protection. It is also extremely traumatic for the rescue teams/EMTs that risk their own lives as retrieve the body. It poses not only a physical danger to them, but to their emotional well-being as well. I have treated patients who have considered this their only 'fool-proof' method of completing their suicide (was able to successfully intervene, thankfully), their families, and some of the rescue workers who have done this community a tremendous service and have suffered the impact of these dangerous retrievals. Please. we must get this done.</p>
309.	Dan Woods	New Hope, MN	
310.	Monique Owens	University Center, MI	<p>Suicide is such a terrible tragedy on any front. This bridge is begging to be more safe to prevent future loss.</p>
311.	Katie McCain	Avila Beach, CA	
312.	Ryan McCain	San Luis Obispo, CA	<p>This is a no brainer people. Wake up.</p>

	Name	From	Comments
313.	sandy simpkins	newcastle, WA	In Seattle we have a bridge that people over the years have jumped off of and they have installed some safety devices and the numbers have dropped. I know constructing safety devices does make a difference. I hope the barrier is put into place soon.
314.	Kim Elberg	Cardiff, CA	
315.	Kelsey Kehoe	Los Osos, CA	I have known someone personally who has committed suicide off of this very bridge. It would save many more lives and prevent further attempts if a physical safety barrier was constructed ASAP.
316.	Mark Lytle	Glendale, CA	The cost of a life is immeasurable. Don't buy into the "if they're going to do it, they're going to do it." Every moment of a life saved counts. Is the Anti-tax movement more important than life itself? I met Matt two or three times, know his mother and this is one of the greatest tragedies I've ever heard of. Build the Barrier.
317.	Braden Rhett	RHinebeck, NY	ANY effort to spare someone's life and spare the suffering of their loved ones is worth it - please take the important step of installing a safety barrier seriously and act now. Thank you.
318.	Gilles and Lissa Lalonde	Redondo Beach, CA	We are so saddened by the loss of such a fine man. We both send our love and support to the Aydelott family. We can only hope that the safety barrier will be constructed soon to help other families avoid such a terrible loss.
319.	Robert McCain	San Luis Obispo, CA	
320.	Summer Frappier	arroyo grande, CA	
321.	Jane Oish	San Luis Obispo, CA	My daughters boss recently took his life on this bridge. He may have thought longer about his decision if access had been denied to him. Time he may have used to seek help.
322.	Betsy Crowfoot	Carpinteria, CA	I drove across the bridge today right after the most recent suicide victim. The beauty of this bridge and setting is marred by the thought that so many people have lost their lives here - please find a way to halt and deter these suicides. If it weren't so easy, perhaps these people would think again ...
323.	Dorie Zabriskie	Ventura, CA	
324.	Jillian Marut	Buellton, CA	We need to save those who simply cannot save themselves! Please sign this petition, what if it were a family member or friend of yours? These are people who have loved ones and their families are destroyed forever. Please do what you can to prevent another family from having to go through this pain. I lost a family member to suicide many years ago, and it still hurts.

	Name	From	Comments
325.	Jason Tarman	Santa Barbara, CA	A review of suicide prevention literature clearly indicates that placing barriers between a person with suicidal thoughts and their plan does make a difference. Most people who are suicidal have very specific plans and thwarting them may give them the opportunity to make a different choice or get some help.
326.	Wistosis Parker	Santa Barbara, CA	
327.	Jeff Shaffer	Santa Barbara, CA	Yes, please create the safety barrier.
328.	Chaplain Fr. Jon-Stephen Hedges	Isla Vista, CA	In bitter-sweet memory of those we've lost, as neighbors to all who suffer and for the very soul of our community... we are duty bound to provide this chance to pause and choose life.
329.	Ruth Wilson	Santa Barbara, CA	I am a MFT and know the seriousness of suicide. The barrier is a step in the right direction.
330.	Susan Short	Santa Barbara, CA	
331.	Jamila Gonzalez	Carpinteria, CA	
332.	Christie Tarman	Santa Barbara, CA	I feel very strongly about the positive impact this barrier can have.
333.	Abbie Dawn	Emo Track, Australia	
334.	L Pitts	santa barbara, CA	
335.	E Pitts	santa barbara, CA	
336.	Sara Monteabaro	Santa Barbara, CA	My dear friend, Andrew Popp died on July of 2005 by jumping off the Cold Springs Bridge. His death could have been prevented if there were barriers to the bridge. Please allow this safety barrier to be built, not only in prevention of those who may use the bridge in the future, but in remembrance of those who have already passed.
337.	Shawn Morey	Santa Barbara, CA	Please help build this, I have been affected by these tragedies personally.
338.	Paige McIlmoil	San Diego, CA	
339.	richard rivera	san francisco, CA	
340.	Marc Ferguson	santa barbara, CA	
341.	Emil Kraus	Santa Barbara, CA	
342.	Samantha Everson	Santa Barbara, CA	Please help prevent suicide
343.	Jodi Barrett	Santa Barbara, CA	
344.	Edward Peszewski	Isla Vista, CA	I haven't lived in wonderful Santa Barbara more than 6 years but in a close knit town like Santa Barbara where everyone eventually seems to meet everyone it didn't take me long to be indirectly affected by someone tragically taking their own life on Cold Spring Arch Bridge. Any effort and divergence of funding (however small) to discourage future tragedies is worth it. Thanks.

	Name	From	Comments
345.	Steve Ponce	Santa Barbara, CA	
346.	john simmon	goleta, CA	Save lives please
347.	Tarielle Thomasco	santa barabara, CA	
348.	Christopher Deem	Goleta, CA	
349.	Steve Sharer	San Luis Obispo, CA	One more is one too many. Many of us have lost someone very close and special to us on this bridge. We shouldnt let it be an easily accessible suicide location anymore. Build these barriers and eliminate another person from using it to kill themselves. I no longer live in Santa Barbara but I grew up there and lost my teammate and friend Andrew Popp on the Cold Spring bridge.
350.	Timothy Petter	Santa Barbara, CA	
351.	Jeff Menzel	santa barbara, CA	my best friend jumped off the bridge when i was sixteen years old... i dont want anyone to have the pain i and my friends and their families to go through.... its the only way we can keep this tragedy from happening more and more
352.	RENE CASTANEDA	SANTA BARBARA, CA	
353.	Deborah Samson	Los Angeles, CA	
354.	tegan rasmussen	santa barbara, United States Minor Outlying Islands	
355.	Brenda Cockrell	Tampa, FL	This MUST PASS!!!! One More Tragedy is one too many!
356.	Johnny Escalera	Santa babara, CA	
357.	Dimitri Anthes	Santa Barbara, CA	I knew one of the 46.
358.	Cody Brown	santa barbara, CA	one of my best friends jumped off this bridge in the summer of 2005. to everyone who opposes putting up a fence or ANYTHING to prevent people from using this bridge as a way to commit suicide, every time you cross that bridge think of the victims and the families/friends of those people that have been hurt so bad because you don't want to ruin the "beautiful" view. this needs to stop.
359.	Thomas Burdette	Owings Mills, MD	
360.	Lindsay Gould	Santa Barbara, CA	There needs to be a barrier!! We've lost too many people in Santa Barbara!
361.	samantha curnow	santa barbara, CA	Please take time and have your friends or anyone you know do this. Too many people have lost their lives. RIP Popp, love you.
362.	reid lathan	New Haven, CT	
363.	Mindy Petter	Santa Barbara, CA	
364.	Chase Fisher	san diego, CA	
365.	Timothy Petter	Santa Barbara, CA	

	Name	From	Comments
366.	Ali Head	Santa Barbara, CA	RIP ANDREW POPP, FOREVER YOU WILL BE LOVED
367.	cory anglin	santa barbara, CA	
368.	Elena Daly	Santa Barbara, CA	help save a life.
369.	Alycia Paulsen	Arroyo Grande, CA	
370.	Brenna Zimmerman	Santa Barbara, CA	
371.	Evan Janke	santa barbara, CA	
372.	Sara Nickel	Santa Barbara, CA	I personally knew a teenage boy who took his life by jumping off this bridge- tragedies like his can and need to be prevented
373.	angie szily	gladstone, Australia	i lost my son to suicide on the 26 th dec 2004.but he hung himself..here in aus they are using the jumper nets,,its a good idea at least people will have the reality to change there minds when they land in them..im all for them..if it saves life it is a must &&&
374.	Jamie Rowswell	Cut Knife, Canada	
375.	Esperanza Daly	Santa Barbara, CA	
376.	Michelle Barnett	Goleta, CA	
377.	Verity Harvey	Monkton, Nr Honiton, United Kingdom	
378.	Krysta Falloon	Solvang, CA	
379.	Cody McGray	Summerland, CA	
380.	Megan Gentile	santa barbara, CA	
381.	Ben Tilbury	perth, western australia, Australia	
382.	Russell Brown	Santa Barbara, CA	
383.	Jenny Luth	Harvard, IL	
384.	Michael Spiker	Los Angeles, CA	
385.	Jessica Ray	Santa Barbara, CA	
386.	Sarah McGinnis	San Diego, CA	
387.	Jo Ellen Brainin-Rodriguez	San Francisco, CA	As a psychiatrist in San Francisco who has known at least a dozen patients over the last 20 years that have either jumped or attempted to jump from the GGB, I urge you not to wait the 70 plus years we have had to wait for a barrier. 95% of people who attempt to jump for our bridge do not go on to suicide by other means. Please do the right thing!
388.	Sarah Barkley	Santa Barbara, CA	
389.	Venessa Hurka	Santa Barbara, CA	
390.	Amanda Curnow	Santa Barbara, CA	
391.	Jenna Palmer	santa ynez, CA	An old man tried to jump off the bridge on my way to work today. I was the first to stop and call 911... It saved his life.

	Name	From	Comments
392.	Lauren Rinderknecht	Santa Barbara, CA	
393.	PATRICIA TORRES	GOLETA, CA	I THINK IT IS TIME THAT WE TAKE OUR BUDGET'S MONEY AND USE IT FOR SOMETHING THAT WILL ACTUALLY MAKE A DIFFERENCE IN THE COMMUNITY. IT IS FUNNY HOW WE DON'T HAVE MONEY OR THE RESOURCES TO BUILD THIS BARRIER BUT WE DO HOWEVER HAVE IT TO SPEND LIKE CRAZY FOR "BEAUTIFICATION" PROJECTS AROUND SANTA BARBARA, PLEASE USE THE MONEY TO SAVE A LIVE WE DON'T REALLY NEED ANOTHER ROW OF 'CORN STACKS' AT MILPAS, WHO OR WHAT HAVE THEY HELPED IN OUR COMMUNITY.
394.	Caitlin Olson	Santa Barbara, CA	Please build the Cold Spring Safety barrier for everyone's benefit.
395.	Miriam Lindbeck	Santa Barbara, CA	This is one more important step to not only prevent deaths like this, it is a raising of the people's awareness of suicide and to wake up to what it takes to hopefully prevent it. Those who are troubled enough to want to take their lives may possibly hear that more and more people are concerned with suicide overall, and that means them personally. If they think no one cares or understands, this is a big sign people do care with all their heart and hopes.
396.	William Etling	Santa Ynez, CA	This vital project is long overdue.
397.	Crystal Menicola	East Rutherford, NJ	
398.	clarissa nieves	galloway, OH	
399.	Amber Mondae Bozman	Milwaukie, OR	
400.	Jacob Mercer	Santa Barbara, CA	
401.	Kevin Purcell	Orland Park, IL	Hi, First, I wish to thank all those who made this possible. According to The American Association of Suicidology (suicidology.org) there are at least six people that are affected by each suicide. However, this might be considered a conservative figure. When someone ends his/her life/emotional pain, they are paining many others. I hope that the stated goal is not only achieved, but exceeded by leaps and bounds.
402.	Jennifer Gardner	Melbourne, FL	
403.	Cynthia Young	Satna Barbara, CA	As they are falling to their death, how many people must think, "Wait, Stop, I really don't want to do this." Please put up the saftey barrier!
404.	Heidi Straub	Santa Barbara, CA	I have a relative that jumped from the bridge in November 2008 and just two months after his death, another person has jumped. <i>(continues on next page)</i>

	Name	From	Comments
404.	Heidi Straub	Santa Barbara, CA	<p><i>(continued from previous page)</i></p> <p>There are opinions on both sides -- some say it is useless to stop someone who wants to commit suicide and others that say if a person can just be re-directed only briefly the problem may be averted for the rest of their life. I read an excellent article and I urge you to review it: http://www.edhat.com/site/tidbit.cfm?id=2780</p> <p>One particularly memorable quote from this article: "Arthur Rubinstein, 20, attempted suicide in 1908 in Berlin, tying his robe belt to a clothes hook in the closet and jumping off a chair. He landed on the floor, as the hook pulled out of the ceiling. The world famous conductor died of natural causes in 1982 at age 95, after a brilliant career."</p> <p>I've seen many bridges with a barrier and to me, it really doesn't seem like such a huge imposition on the view. I can say from personal experience now that the family and friends of the victim would love to have one chance, no matter how small, to be able to save their loved one.</p>
405.	Jan Adelson	Santa Barbara, CA	I personally know 2 people who jumped to their death off of this bridge. If it had been harder to accomplish, perhaps these folks could have gotten the help they needed to reconsider ending their lives.
406.	tim adelson	santa barabara, CA	
407.	Paul Morrison	Aurora, CO	Seems obvious. Just do it.
408.	tammy kronen	santa barbara, CA	I recently lost a friend to suicide on the bridge. Build the barrier to stop the suicides.
409.	Tricia Wardlaw	Santa Barbara, CA	
410.	Norma Mitchell	Santa Barbara, CA	My dear friend just jumped over the weekend and did not live. I say blow the flipping bridge up!
411.	Kathy Essen	Arroyo Grande, CA	There should be a barrier for so many reasons. My friend's cousin recently committed suicide on this bridge; the possibility exists that if there had been a barrier she might have been able to get past that moment. A barrier would also benefit our rescue personnel - they put their lives at risk every time someone attempts a jump. My prayers are with all those who have lost family members and friends on this bridge.
412.	Joy Brown	North Hollywood, CA	<p>LYNNE CARPENTER MARTYNIUK, my best friend of 36 years and a lifelong member of the Santa Barbara community, jumped to her death from this bridge in the early morning hours of Saturday, April 4, 2009. She did not plan this. It was an impulsive act based on personal issues and more importantly, prescribed drugs that messed her up and made her delusional. She left behind 3 beautiful kids, a loving husband, a sister, a brother, her mom and dad, aunts, uncles, friends and a community that will never</p> <p><i>(continues on next page)</i></p>

	Name	From	Comments
412.	Joy Brown	North Hollywood, CA	<p><i>(continued from previous page)</i></p> <p>forget her. She was beautiful, athletic, compassionate, strong, loyal, funny, and wise. As a lifelong resident of the area, she knew of the lure to this bridge that so many had found as a way out. But she would drive out of her way to go under the bridge, not over it and she didn't care who laughed at her for doing that. This has left a hole in my heart that will never go away and I will never be ok with this. I am haunted by her death and how she did it. It takes a lot of unfortunate courage to do what she did and I keep wondering did she suffer, did she scream, did she have time to regret it on the way down? So many questions this form of suicide takes and no one wants to talk about it. My family and friends tell me not to think about the jump but it's the only part I can think about, the only part I can't get over. The people that think this damn bridge is so beautiful never lost someone they love over the edge so they don't know the full impact and how dangerous it really is. It's too dangerous just to walk across, let alone drive. I don't care if 1 million people a year are inconvenienced by driving 50 miles out of their way if it saves 1 more life. Because that could be the life of someone YOU love. I've read all of the studies on how this might only deter suicides instead of preventing them. If you can at least deter someone from doing this, you have a chance to save them before they try it again. Lynne didn't have a chance. That railing is only 32 inches tall, it was windy, it was dark, she was alone and in so much pain. A motorist found her near the road below. Would you want to be that person? The longer it takes to build this barrier, the more possibility there will be of someone else going over the edge. So please, in memory of LYNNE CARPENTER MARTYNIUK and the 47 other souls taken here, put up the damn barrier for the safety of all of us. SO PUT YOUR MONEY WHERE YOUR MOUTH IS IF HUMAN LIFE IS MORE IMPORTANT THAN SOCIAL CONVENIENCE AND PUT UP THAT DAMN BARRIER ALREADY. Of course, it's going to cost a lot of money to build this but wouldn't you rather have people thanking you for saving their life instead of loved ones blaming you for taking it? The only beauty possible here is for no more deaths to occur and that no else has to endure the impossible pain of losing someone they love from this place. To the people who didn't know her, LYNNE CARPENTER MARTYNIUK is now just a number, a statistic, a casualty. To everyone who did know her, like I did, she is so much more than that and I want her life to serve as an example and something good can come out of this so that 1 more person can live.</p>
413.	Terri Jackson	Greensboro, NC	In memory of Lynne and in celebration of Joy.
414.	Cindy Everman	santa barbara, CA	

	Name	From	Comments
415.	Ronald Brown	North Hollywood, CA	
416.	Lana Campbell	Ventura, CA	
417.	Theresa Vogel	Santa Barbara, CA	
418.	Sue H	Santa Barbara, CA	Amen Joy Brown! You said and have been thinking everything that has haunted me about what Lynne might have been experiencing that horrendous night! My dear friend of 28 years, Lynne Carpenter Martyniuk might very well still be here if that bridge barrier had been put up long ago. And thank you Joy, for telling everyone that this was NOT something that Lynne in a nonstressed out mode, and NOT full of garbage prescriptions and OTC's would have EVER DONE IN A BAZILLION YEARS!!!! She'd be the FIRST one to go off about someone leaving their kids and family behind. She'd have NONE of it!!!! That' why this is so utterly heinous, surreal and the stuff of nightmares!!! Many factors play into this but the final act could have been avoided. That bridge doesn't even have sides high enough to prevent a car with a blown out tire or sudden loss of control or brakes from going over the side!!!!
419.	Nancy DeCaro	Santa Barbara, CA	Maybe, just maybe Lynne would not have done this if she found the barrier on the bridge, and would have had a couple extra minutes to think about how many people loved and supported her, and she would still be with us right now. Please, please think of human lives first, and not just archetecture.
420.	Erin O'Connell	SANTA BARBARA, CA	I have known two people in the past 30 years that have jumped over the Cold Spring Bridge. This community is small enough that if nothing is done to prevent people from jumping, over time, EVERYONE here will be effected by knowing someone that will jump. Please install the safety barriers!!!
421.	Danelle Mangone	Santa Barbara, CA	
422.	Lisa Housel	Port Hueneme, CA	I worked with Lynne at Cottage Hospital and I am still trying to deal with her loss. I would have never thought that she would commit suicide. People must be in so much pain to die in a brutal way. The barrier needs to be placed. How many more people need to die before we get the attention of the politicians. Lynne was such a wonderful nurse and would give 100% everyday even after 17 years! Good bless her family during this time. We love you Lynne and you will be missed. I wish you were still here so I could tell you in person...
423.	Lisa Martin	santa barbara, CA	
424.	William Henderson	Santa Barbara, CA	For more impact, add a personal comment here

	Name	From	Comments
425.	Abby Burnett	Franklin, ME	I knew Lynne Carpenter Martynuik because her husband Paul is my ex-husband's stepbrother. Lynne's death has been haunting me since I heard about it last week. I am so very sorry for her family and friends. This bridge is terrifying in every way. The hell with the view; if you want a view climb a mountain. Put a barrier up under this ugly monster, fast.
426.	Mark Meinzer	Santa Barbara, CA	I knew two unrelated people from my high school who jumped off this bridge. Something needs to be done. Any barrier to make it more difficult for the person in crisis could be their saving grace.
427.	Julie Miller	Santa Barbara, CA	Attach signs to the barrier with phone numbers such as suicide hotlines, counseling, etc.
428.	John Adams	San Diego, CA	I support a safety barrier - studies show barriers are effective. The longer it takes, the more lives will be lost. Rest in peace Mike and Lynne.
429.	Mary Johnston	Santa Barbara, CA	In honor of Lynne
430.	Shirley Obermeyer	Santa Barbara, CA	In loving memory of Lynne Carpenter Martynuik
431.	Mary O'	Santa Barbara, CA	The pain of losing a loved one to suicide is unparalleled. My heart goes out to all of Lynne's friends and family. I did not know her but I had the good fortune of having her as one of my nurses at Cottage. What a tragic and senseless loss of a bright spirit. The barrier needs to go up. Now.
432.	gregg peterson	Livermore, CA	
433.	James Eddy	santa barbara, CA	I worked with Lynn at Cottage Hospital back in '04 and she was the kind of nurse you wanted on your shift. Her expertise in the field of nursing and her sense of humor on the unit kept me entertained all night. I'm in disbelief. My thoughts and prayers go out to her family. I support a barrier for the bridge
434.	Linarosa Vendrasco	Sanata Barbara, CA	
435.	Shelley Hession	Carpinteria, CA	
436.	jennifer valadez	santa barbara, CA	
437.	Korinne Rowles-Farmer	Goleta, CA	
438.	Delois Thomas	North Hollywood, CA	If anyone ever feels that they have nothing worth living for, there is always help for you to know that you are someone special to somebody
439.	Alisa Togami	Fresno, CA	In memory of Lynne Carpenter Martynuik
440.	Vanessa Marquez	Goleta, CA	For all of those that have lost their lives because of this bridge.....
441.	Kaitlyn Riddle	Goleta, CA	

	Name	From	Comments
442.	Anna Lincoln	Santa Barbara, CA	
443.	Jennifer Howard Kicinski	Seattle, WA	
444.	John Dillard	Santa Barbara, CA	
445.	Luz Rodriguez	Santa Barbara, CA	I will never be able to cross this bridge without thinking to myself "what if?"...you will be greatly missed Lynne
446.	Karen Grandberry	Lompoc, CA	
447.	Rita Barton	Santa Barbara, CA	
448.	Michelle Hollander	Buffalo, NY	
449.	Cathy Fortunato	Santa Barbara, CA	Sad that such a beautiful bridge would be such a magnet for people with a spontaneous thought to "end it all". Facts speak for themselves: it's time to build the barrier. Please let's do it. Now.
450.	Rhonda Reid	Pomona, CA	
451.	Erica Garcia	Lompoc, CA	In memory of Lynne Martyniuk
452.	Scott W. Robinson	Santa Barbara, CA	Don't let one more family go through the pain and agony that Lynne's family is going through now. People say that if you put up this barrier people will only find another way to end their life. Maybe so, maybe they will resort to something else, maybe they will find a way over or around the barrier...but just may it will give that person the extra 10 or 20 seconds needed to say to themselves "what the hell am I doing!?". Maybe if Lynne had that sliver of time to ask herself that question, there would be one less name on this list of those who decided that their only way out of their pain was over the side of the Cold Spring Bridge. Let's not let another life be lost. People always ask the families of those who've died "what can I do?" Well here it is! Ask everyone you know to sign this petition and let's end this senseless waste of life. I add my name to this list in memory and in honor of my friend and classmate Lynne Carpenter Martyniuk. Scott W. Robinson SBHS Class of '81
453.	Judy Soliman	Buellton, CA	
454.	Susan O'Connor	Concord, NH	The arguments against the bridge barrier ("people will just try another way", etc.) have been shown to be false again and again. The barrier must be put up in memory of Lynne and so many others - and for the futures of those who will be saved by stopping this suicide magnet from being such an easy way to make a tragic choice that can never be undone.
455.	Stephanie Clark	Fairfax, CA	

	Name	From	Comments
456.	kari hebert	Santa Barbara, CA	If people knew that there was this barrier that could exist, they wouldn't have the chance or thoughts to jump off this bridge, like so many loved ones.
457.	Carole Rollins	Santa Barbara, CA	I am whole-heartedly in favor of a suicide prevention barrier on Cold Springs Bridge. I believe the studies that show barriers work. Compassion predominates over dollars.
458.	RASHAN Fogle	Charlotte, NC	
459.	Jennifer Smith	Santa Barbara, CA	Please put the damn barrier up. Lynne was one of my best friends from the first day i moved here in 1977. I was so shocked to hear about her death. My hearts and prayers go to her family. She was there when my brother passed away. Lynne i will miss you dearly. I HAVE ALWAYS WANTED A BARRIER UP ON THAT BRIDGE. or even a shock electronic fence. anything to prevent any more pain to families from that bridge. i agree with Joy with everything she said. IN MEMORY OF EVERY FALLEN SOULS that went off the bridge, suicide or accidently.
460.	Daphne Forrest	Hesperia, CA	
461.	AJ Eddy	Reading, MA	
462.	Ramona mason	santa barbara, CA	
463.	David Keller	Quincy, CA	
464.	Jennifer Ahn	Goleta, CA	
465.	Don McDermott	Santa Barbara, CA	This bridge, if built today, probably could not be built without the barrier. This one location has a solution. The cost could be considered high but the cost is justifiable. Please support this CalTrans project. Condolences to all family members and friends who mourn their loved ones.
466.	Mary Janicek	Santa Barbara, CA	
467.	Suzi Serbin	Santa Barbara, CA	So sad that this is how locals know the bridge - as the place to jump. Isn't it time we changed that? It's disappointing to me that we haven't already. In honor of Lynne and Mike...
468.	robert crane	santa barbara, CA	we must do something to make it more didfficult to jump off this bridge
469.	Ernesto Quiroz	Goleta, CA	A a resident in Santa Barbara County, I wholeheartedly support the construction of a safety barrier on the Cold Spring Arch Bridge. Suicide is an option sought by a troubled individual and we have had 48 of them. As community members we need to do what we can to channel these people towards more viable solutions and take away the temptation of doing something in desperation.BUILD THE BARRIER.

	Name	From	Comments
470.	Karen Brazelton	Costa Mesa, CA	Just heard yesterday about a friend of mine's family member who committed suicide last weekend...too much of this these days...anything that can be done to help save lives, is worth it!
471.	kathleen Benner	Santa Barbara, CA	
472.	Derrick+Diane Venzor	Arroyo Grande, CA	As if a friend or family member losing a loved one is not pain enough, suicide plays cruel mind games on those left behind. Not one single moment can define the life of one individual. Lynne our heart aches for your fellowship.
473.	peggy and alan nakashima	santa barbara, CA	
474.	Claudia Chotzen	Santa Barbara, CA	I am so sorry to hear that another beloved friend and family member and a wonderful member of our community has ended their life this way. I have thought there should be a barrier on that bridge for years.
475.	Mark Childs	Santa Barbara, CA	
476.	LuAnne Abercrombie	Santa Barbara, CA	This bridge is a structure built by human hands. We as a community need to preserve the integrity of this structure we built by adding any and all safety provisions to it, so that it is used solely for the purpose we intended, namely transportation into and out of Santa Ynez valley. This bridge was meant to be an asset to the community, clearly it has not acheived this goal in full measure therefore, we need to continue to build a barrier on it to make it safe.
477.	Eric Sappenfield	Chatham, NJ	
478.	Amy Locke	Santa Barbara, CA	
479.	Christine Sobenes	Oak View, CA	Two people I know have jumped off this bridge. I want a barrier!
480.	Lee Scheuermann	Santa Barbara, CA	For the families of Lynn and Mike and many others
481.	Michiel McHugh	Carpinteria, CA	The barrier would remove an easy choice for what is often an impulsive act. That single delay may prevent the act entirely, allowing a soul in trouble just enough time to work it out and go "bad idea, really bad idea." Build it now.
482.	Russell Abbey	Woodland Hills,, CA	The beauty of the view from the bridge is forever marred by friends (Lynne and Mike) who have stepped off of it. It is something to get across with a heavy heart. Please put up the barrier.
483.	Irene and John O'Brien	San Jose, CA	
484.	Jamie Gumins	Santa Barbara, CA	
485.	Leslie Holtzman	Santa Barbara, CA	WE NEED THE BARRIER NOW!
486.	cheri mckinney	santa barbara, CA	
487.	Stacy L. Christopher	Santa Barbara, CA	In honor of the Martyniuk family.

	Name	From	Comments
488.	Nancy Fields	Santa Barbara, CA	
489.	Johnny Malec	Northglenn, CO	
490.	Laura Wilson	belton, MO	
491.	Karen Adams	Chicago, IL	
492.	Frederic Kass	Santa Barbara, CA	
493.	Teresa Mcmillian	Kansas City, MO	
494.	Sarah Kass	Santa Barbara, CA	Our family has suffered the loss of 3 personal friends, represneting both genders, 3 generations, and various socio-economic groups. we have a responsibility as a community to diminish the attractive suicide potential of the Cold Springs Bridge. Please put up a barrier.
495.	Lynda Thompson	Thousand Oaks, CA	
496.	beth janss	santa barbara, CA	
497.	Jacqueline Vanderlaan	Santa Barbara, CA	Human life is precious and so much more valuable especially to family and friends and even to those who never even new the person. If that barriar can save even one life, it's worth it. If those who oppose this can get involved with one of those families and hear their story of the guilt, sorrow, pain they indure. Maybe they would change their minds. Time for action and save lives.
498.	aaron frederick	lompoc, CA	Anything to protect the those who are lost and need some protection to keep them in our world.
499.	Michele Janee	Santa Barbara, CA	I know barrier would have saved my dear friend Lynne. It was a spontaneous moment of craziness and the bridge was too easy.
500.	Shauna A	carlsbad, CA	
501.	Dina Alvarez	Santa Barbara, CA	
502.	Janet Billiet	Cypress, CA	
503.	Mary Vrinotis	Jamaica Plain, MA	
504.	Catherine Barber	Somerville, MA	
505.	Brenda Zuber	Roanoke, TX	
506.	Allison Taylor	Santa Barbara, CA	We NEED a barrier. I sign this in honnor of all the family and friends whom have endured such a loss. Please pass this on to all your family and friends. It can save a life.
507.	Jody Taylor	Santa Maria, CA	For more impact, add a personal comment here
508.	Cordelia Woodruff	Santa Barbara, CA	For more impact, add a personal comment here
509.	Gina Watkins	Denver, CO	For more impact, add a personal comment here
510.	Leslie Deems	Alameda, CA	
511.	Sabrina Glazbrook	San Francisco, CA	
512.	Shannon murphy	Santa Barbara, CA	

	Name	From	Comments
513.	Lethia Nall	New York, NY	Anything that could help give a person a reason to hesitate before attempting their own demise, is worth it a thousand times over.
514.	Harlina Thornton	novato, CA	
515.	Robyn Cass	Carpinteria, CA	Asthetics is no reason to stop construction of a barrier. The architecture of the bridge will remain beautiful, and maybe a life or two will be saved.
516.	Lisa Ouellette	Lakewood, CA	Give someone a second chance at life.
517.	Brian Kenney	Maple Grove, MN	Matt was a grade school friend of mine, and I was deeply saddened by this news. If only there had been a better barrier in place, this tragedy could have been prevented.
518.	Tamara Blessing	Santa Barbara, CA	Last week I lost a life long friend. His way to heaven was off this bridge. He was only 28 years old. With a barrier or something maybe he would still be here. Maybe not. But that maybe is enough.
519.	Kev Gar	santa barbara, CA	Build it..and they wont come!!
520.	Nicole Swartz	Murrieta, CA	
521.	LeAnn Ashton	Santa Barbara, CA	To many lives have been lost due to this barrier not in place. IT NEEDS TO STOP...
522.	April Baird	Santa Barbara, CA	Just last week my best friend lost a part of her heart. Her life long friend since she was 4 years old killed himself by jumping off the cold springs bridge. I have never felt so helpless during a time of need for one of my closest loved ones. If i could make this petition act, in any small measure to find a way to help prevent another tragic act that torments the hearts of the one's left behind and takes the hearts of the one's who decide to leave us, i would sign it over and over again until it's built overnight.
523.	Elizabeth Venegas	santa barbara, CA	I lot my brother-in-law last Wednesday jumping off this bridge. Many people that jump off are because of a mentel illness so they are not in the right state of mind. Please put something so that it wont happen anymore.
524.	Sergio Rodriguez	Oxnard, CA	to whom it may concern, suicide is something that is done without thinking, putting up a safety barrier at this bridge will make suicide harder and will have to be thought of more. Just making the person think about suicide will help prevent a lot more deaths due to suicide.
525.	Anne Farrell	Del Mar, CA	
526.	Joe Silva	Santa Barbara, CA	Please put up a safety barrier.
527.	Jessica Fox	Lompoc, CA	
528.	Carrie Patterson	santa barbara, CA	

	Name	From	Comments
529.	rene block	Santa Barbara, CA	We cant have a temptation like that for clinically depressed people or people who are having a negative effect from their medication. PLEASE put up the barrier now before another parent or child or spouse loses their life. It should have been built when the bridge was built. Its now become pure neglect of an obvious catastrophe!
530.	Terri McDonald	American Canyon, CA	Too many tragedies have happened in such a short time. How many more need to die before this issue is resolved?
531.	Lindsey LeBlanc	Santa Barbara, CA	I'm signing this in honor of those who have ended their lives off the bridge, and the paramedics and other rescue workers who face physical risk and emotional trauma after each suicide jump. Most people who are prevented from committing suicide do NOT go on to kill themselves. Please put up the barrier now. The longer this debate rages on, the more lives will be lost.
532.	Amy Donatello	Arroyo Grande, CA	
533.	Vanessa Elias	Santa Barbara, CA	I drive thru the pass almost once a week and i hate crossing over the bridge for fear of seeing someone jump. something needs to be done!!
534.	Monica Elias	Santa Barbara, CA	
535.	Brooke Sandell-Villa	Las Vegas, NV	
536.	Ginger Sandoval	Santa Barbara, CA	
537.	Jennifer Elkins	Goleta, CA	RIP Andrew, you will always be in our hearts.
538.	Allie Breke	Santa Barbara, CA	There needs to be more safety here, too many amazing people have died
539.	Sandra C	Solvang, CA	Stop the Death Toll!!
540.	Kelly Cole-Smith	Santa Barbara, CA	Can't understand why people don't want to put a safety barrier here. Too many have tragically ended their lives.
541.	Danell Knox	Santa Barbara, CA	Be safe Santa Barbara.
542.	Jen Willis	Summerland, CA	Please protect people from harming themselves and install a proper safety barrier on Cold Spring Arch Bridge. Suicide is a matter to be taken seriously. There is no coming back. Thank you for your kind time and attention to this important matter.
543.	serena baird	santa barbara, CA	
544.	Lauren Franz	Santa Barbara, CA	
545.	Joan Fogel	Santa Barbara, CA	For more impact, add a personal comment here "Permanent solution to a temporary problem"
546.	Casandra Terjani	Oxnard, CA	
547.	Geneva de la Rouge	Santa Barbara, CA	Doing constructive work progresses our plant.
548.	Sheana Green	Santa Barbara, CA	

	Name	From	Comments
549.	Kristie Rohleder	Highland, CA	
550.	James Smith	Santa Barbara, CA	
551.	Erin Morey	Santa Barbara, CA	
552.	Danielle Duran	Burbank, CA	YOU ARE IN MY PRAYERS & HEART ALWAYS! I LOVE YOU D! XOXO D
553.	rachel kory	Santa Barbara, CA	
554.	Adriana Jorge-Zamora	Orange, CA	
555.	Estela Hayward	Westminster, CA	One small fence can give someone who was going to use this bridge to kill themselves, a chance to think things over, to get some help.
556.	Angie Garcia	Costa Mesa, CA	
557.	Raymundo Landavazo	Dulzura, CA	
558.	Cindy Vargas	Corona, CA	
559.	Jesus Lara	Santa Ana, CA	Hope to get this barrier up soon!
560.	Denise Aguirre	Stanton, CA	
561.	Marlena Saldana	FULLERTON, CA	
562.	Juanita Jorge	anaheim, CA	My Cousin commit suicide from these Bridge, on October 13, 2009, He was 28 yrs. old, my whole family was very impact, is time to do something about these Bridge.
563.	nicole d	Marina del Rey, CA	This earth is not the same without you. I miss you terribly. You will always be in my heart and prayers.
564.	Julia Alpi-Kindred	Huntington Beach, CA	I support building this barrier-I support saving lives. From a quick search of the CalTrans presentation: "How effective is it? Studies show that when people are prevented from jumping, they do not go on to commit suicide by other means. Example: Of 515 people who were prevented from jumping off the Golden Gate Bridge, only 6 percent went on to commit suicide." Cost: "May 2006-Preliminary Estimate: \$200,000 to \$300,000"
565.	Rosa Jorge	Corona, CA	RIP Jerardo Ramirez
566.	Anita Duplessis	Santa Ana, CA	
567.	shawn Kayvon	Irvine, CA	I hope by signing this, the suicidal rate decreases and hopefully stops. Shawn Kayvon
568.	Abel Rubalcava	Panorama City, CA	

	Name	From	Comments
569.	julie haupt	santa barbara, CA	We need to do more to protect the mentally ill people in our community. My spouse is mentally ill, and I often wonder if i'm going to be mourning him on this bridge someday. Hopefully by getting this petition passed it will help prevent more deaths and show respect for the people who have passed on here.
570.	Krista Adams	Yorba Linda, CA	I have two people in my life who have been affected by a death of a loved one that jumped from this bridge. I live in a different county and yet the tragedy has reached to myself and loved ones down here. It is senseless and needs to have a stop put to it. My friend's father jumped from the bridge ten years ago and if a barrier had been installed in the intervening time then the death of another friend's young cousin could have been prevented. Please, think of all the people this is affecting! This is not a matter of statistics, but of human lives.
571.	Susan Adams	Yorba Linda, CA	All we can do to protect our mentally ill loved ones is mandatory! Discussion since 2005 is unacceptable. I am signing to show my support for this petition as the building of the safety barrier will save lives.
572.	Rocio Anguiano	Anaheim, CA	
573.	Ray Dolen	Yorba Linda, CA	Lets help to put an end to an easy means of ending ones life.
574.	Nazee nazari	mission viejo, CA	please end the tragedy by building the barrier.
575.	JOSE MENDOZA	YUMA, AZ	I BELIEVE THAT ALL BRIDGES SHOULD HAVE BARRIERS, MAY GOD BLESS ALL THE FAMILIES THAT HAVE HAD A LOVED ONE DIE BECAUSE BARRIERS WHERE NOT IN PLACE.
576.	ANGELICA RUVALCABA	SUNLAND, CA	
577.	Teresa Alcalá	Corona, CA	Please help prevent other suicides, their families need them.
578.	Melinda Huerta	Cerritos, CA	
579.	amanda Armstrong	pacifica, CA	
580.	Martin Camp	Goleta, CA	Please do not allow the personal-obstructionist crusade of one, or possibly only a few selfish individuals to deter you from proceeding with barrier construction. it is absolutely the right thing to do. Please urge those in authority in this matter to bravely use their own intellect, and make a decision to install the barriers, and not defer to even more "studies" that most often only add technical confusion, instead of moral clarity. For instance, how seemingly frivolous is the concept of our law enforcement having to continuously monitor TV cameras on the bridge, then sending police officers to <i>(continues on next page)</i>

	Name	From	Comments
580.	Martin Camp	Goleta, CA	<p><i>(continued from previous page)</i></p> <p>speed up to San Marcos Pass to try to talk someone out of killing themselves, while the barrier prevents this situation all on its own?</p> <p>Please consider that EVERYONE that has jumped there has died, and that every person that will jump in the future will die.</p> <p>Please consider the heartbreak of their friends and families.</p> <p>Please consider the mental trauma to the attending law enforcement and rescue-and-recovery personnel.</p> <p>The psychological toll on the personnel whom are tasked with recovering the broken bodies, and even later attending to the grim remains is seldom if ever talked about, but has a horrible price that can never be assigned monetary value.</p> <p>Thank you.</p>
581.	Dale Weber	Santa Barbara, CA	In memory of Andrew.
582.	Sara Salvado	Queluz, Portugal	
583.	Arleen Osborn	chino hills, CA	
584.	Amber Saucedo	Dubai, United Arab Emirates	
585.	Lenette Viegas	na, CA	Living near the Golden Gate Bridge, we have first hand knowledge of how devastating it can be to lose a loved one by suicide. Statistics have proven 96% of those who attempted suicide by jumping off the bridge did not attempt again. Lives are far more valuable than views.
586.	Rachel Viegas	San Francisco, CA	
587.	Borg Drone	Edinburgh, United Kingdom	
588.	Christinia Green	Chesterton, IN	
589.	Sally Hughes, LMFT	Santa Barbara, CA	<p>I am the mother of a brilliant, clean and incredibly accomplished 23-yo son who killed himself after spotting a perfect suicide spot off Highway 49 in Cupertino. It was Cupertino's first suicide there, and the last. Cupertino immediately redesigned the highway and effectively corrected the problem.</p> <p>I can't believe my own beautiful city, my son's birthplace, has not followed suit. I am ashamed of the city's apparent priorities to value a pretty, 20 second view over human life and longevity, proven over 55 times now (and at least 8, or 14.5%, in 2009 alone) to be an increasingly visible, easily accessed and instantly effective magnet for suicide. Traumatized or depressed people scan for such "easy, quick and certain" ways out of psychic pain, to use whenever it feels bad enough. Inside the exactly half-true</p> <p><i>(continues on next page)</i></p>

	Name	From	Comments
589.	Sally Hughes, LMFT	Santa Barbara, CA	<p><i>(continued from previous page)</i></p> <p>"cup half empty", already hurting people are stuck, cut off and bereft. Lacking perspective, emotionally isolated and psychically constricted in a kind of nihilistic tunnel vision, most suicides are impulsive, desperate acts to escape a "worst case" moment that time, space, human connections and maturation 99% guarantees will ease, often instantly, in ways impossible for them to imagine in the moment. But if they just hang on, survive and get help, most will never feel so bad again. Simple suicide statistics prove this: for every completed suicide, there are between 100-200 attempts. That means that 1 of 100-200 attempts are actually fatal. Furthermore, there are many stories of people who have prevented up to 100s of suicides simply by asking the suicidal person in to share a cup of tea and talk (Australia, 2010). As a therapist, I have personally interceded and halted three suicide attempts by simply being aware and knowing what to do. Suicide is a problem solved through awareness and prevention, not to blithely "blame the victim", forget about and survive. Suicide is the 3rd most likely killer among our youth, but suicide ranks stably in 11th place for the U.S. population as cause of death.</p> <p>One of the many students who jumped to their deaths from Cold Springs Bridge had just gotten his first "B". Young people with developing but immature minds are serious risks for suicide.</p> <p>We have a duty to protect our children in particular and public safety in general. Please err on the side of life, the true and immeasurable gift of God, and not on the side of what man made. Remember that esthetics is not about a pretty view, it's about what it symbolizes. There's a great spot near the bridge where cars can pull off safely and admire the view. The bridge is for DRIVING PEOPLE SAFELY ACROSS. The view can be a distraction we can't afford. Already Highway 154 is incredibly dangerous without Cold Springs Bridge being a final destination with that horrific, an ad hoc graveyard of callously treated and forgotten souls below.</p>
590.	Thomas Biesanz	Santa Barbara, CA	Help people think twice about saving their lives.
591.	MARY HUNTER	SANTA BARBARA, CA	MARY M HUNTER
592.	Steven Boehm	Santa Barbara, CA	Don't believe that the beauty of the vista is more beautiful than the beauty of the people who will be saved by installing a barrier. We should tolerated the 5 seconds of interrupted vista - the clear vista will be there on the other side.
593.	Dan Schaeffer	Solvang, CA	

	Name	From	Comments
594.	Jina Carvalho	Santa barbara, CA	The leading mental health organizations dedicated to suicide prevention support the construction of physical barriers on bridges to stop suicide
595.	Kevin Carpenter	Goleta, CA	
596.	Katelyn Slosson	Simi Valley, CA	Knowing someone has died there makes it a sad place to go past. I recently had someone close to us commit suicide. I can no longer look at the place the same way and I get sad.
597.	Lindsey Hyslop	Santa Barbara, CA	
598.	Karen Hyslop	Santa Barbara, CA	
599.	Kimberly Sanchez	santa Barbara, CA	
600.	Steve Hyslop	Santa Barbara, CA	
601.	Jasmin Parvin	Santa Barbara, CA	
602.	Aubrey Ellsworth	Santa Barbara, CA	
603.	Negin Parvin	Santa Barbara, CA	
604.	Barbara Herring	Santa Barbara, CA	
605.	E Onja brown-lawson	santa barbara, CA	It is imperative that the suicide barrier is put in place, even if for no other reason but to show that the community cares. Erecting it is a form of concern and caring, and would not only prevent the suicide, but would also allow one to reflect on it's reason for being there, and their reason for life.
606.	Paul Tonkin	Santa Barbara, CA	
607.	kelsey brown	santa barbara, CA	
608.	Barbara Watson	Santa Barbara, CA	Hopefully this important project will be completed.
609.	Sam & Lee Alfano	Santa Barbara, CA	We need to save lives in distress
610.	Shannon Miller	Santa Barbara, CA	
611.	Brian Melton	Pocatello, ID	
612.	JR Rodrigues	Brighton, United Kingdom	I hope the work will be safely completed.
613.	Marcia Sherman	Santa Barbara, CA	
614.	Laura Lynch	Santa Barbara, CA	
615.	Diane Lacey	Santa Barbara, CA	Please don't allow the selfish so-called "friends" (of a Bridge?) have a say in what could save even ONE more life! They don't care. Sad, but true, a suicidal person will find a way somehow, but why keep this option Open & inviting? Thanks.
616.	Edward Ruiz	Santa Barbara, CA	Witnessed someone jumping ,glad I witnessed and not the wife or kids,no one should have to see this,and for those that love the view while driving over the bridge,do us all a favor and watch the road forget about the view,it is only <i>(continues on next page)</i>

	Name	From	Comments
616.	Edward Ruiz	Santa Barbara, CA	<i>(continued from previous page)</i> two lanes for crying out loud watch the road.Now as for the bridge it'self well it is one of our own personal engineering marvels right in our own backyard,This is best viewed from under it not from driving over it....At the rate those are jumping off right now if I were to take my Godson to view this marvelous engineering from the road down below there's a good chance he might see a bit more....We need this...Witnessing what I have can definetly impact a child not in a good way..
617.	LUPE REYES	SANTA BARBARA, CA	PLEASE BUILD BARRIER NXT PERSON CAN BE YOU LOVEONE
618.	Gunther Gottschalk	Goleta, CA	
619.	stephen shugart	goleta, CA	absolutely put barrier up ASAP; barrier will help prevent suicides for sure. it will look o.k.too
620.	Patricia Mickelson	Carpinteria, CA	
621.	John Cloonan	Ventura, CA	
622.	Peter Stone	Santa Barbara, CA	
623.	Pedro Nava	santa barb, CA	Barriers save lives. No more families should suffer because of another death from the bridge.
624.	Ingrid Brown	Goleta, CA	
625.	Lynne Lowe	Santa Barbara, CA	A friend of mine jumped off that bridge many years ago. Please put up the barrier to prevent others from ending their lives there too.
626.	Nicole Sattler	El Cerrito, CA	
627.	Jean Miller	Irvine, CA	How many more have to die?
628.	Alicia Ponsar	Mesa, AZ	
629.	juan gonzalez	aliso viejo, CA	I would like thank you for taking the time to see my comment so please help us we don't want anymore deaths
630.	Susan Adams	Yorba Linda, CA	
631.	Donna Hamblin	RSM, CA	
632.	Kim Le	Santa Ana, CA	
633.	maria gonzalez	aliso viejo, CA	
634.	Sheri Collins	buena park, CA	
635.	David N Moore	Bridgeport, CT	
636.	Don Adams	Yorba Linda, CA	



Lisa LaPlaca
<lisa@lisalaplacainteriordesign.com>

01/05/2011 09:10 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Bridge barriers on Highway 154

I feel so strongly about putting up these barriers as I am aware of the number of suicides that happen at that bridge every year. Any organization that would place the MINOR aesthetic compromise over a human life is unfathomable to me. Also, the way that the gentleman (and I use that term loosely) with the kayak spends his time hanging around the bridge is such a moral abuse and not to mention an abuse of our county services-I have personally called 911 with concern that he was someone contemplating suicide. The people at 911 know him well-that is reprehensible that an adult would play around with peoples serious concerns and also to have no regard for directing the energies of our 911 service people to respond to a hoax.

I hope these comments have helped.

Regards,

Lisa LaPlaca

[Lisa LaPlaca Interior Design](#)

3888 State Street, Suite 103

Santa Barbara CA 93105

805.687.0700 office

805.455.4725 cell

www.lisalaplacainteriordesign.com

lisa@lisalaplacainteriordesign.com



Jan Lewis
<jlewis@acorntech.com>

01/05/2011 02:47 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Cold Springs Bridge

Dear Mr. Fowler,

I support the barriers for the Cold Springs Bridge. I am a frequent traveler on Highway 154 and I just can't imagine someone jumping off that bridge. Please put up the barriers.

Thank you.

Jan Lwis

Jan Lewis
Acorn Technologies, Inc.
330 Wilshire Boulevard, 2nd Floor
Santa Monica, California 90401
Telephone 310-394-7800
Fax 310-564-1851

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Joan Lisetor
<jlisetor@prodigy.net>
01/23/2011 12:30 PM

To <matt_c_fowler@dot.ca.gov>
cc Dayna Whitmer <dayner@comcast.net>
Subject Suicide barriers

I urge you to support the suicide barriers on the Cold Springs Bridge in Santa Barbara. It will save lives. Thank you.

Joan Lisetor, Writer Producer
3020 Bridgeway, Suite 180, Sausalito, CA 94965
Lisetor.com LifeStoriesDVD.com 415/332-7340



Gwendolyn McClure - Brooks
Institute
<GMcClure@brooks.edu>
01/04/2011 03:36 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc "maureens444@gmail.com" <maureens444@gmail.com>
Subject support barriers

Dear Matt,
I support the barriers to be erected on the cold springs bridge along the hwy 154 in Santa Barbara.
Thank you,

Dr. Gwendolyn McClure
Instructor
Brooks Institute-"Passion, Vision, Excellence"
27 E. Cota Street
Santa Barbara, CA 93101
(888) 304-3456 (toll free)
(805) 966-3888 (office)
(805) 690-7664 (fax)
<http://www.brooks.edu>



Cynthia McNulty
<mcnultyx@yahoo.com>

01/03/2011 09:31 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Please build the barrier!

Hi Mr. Fowler,

I'd like to go on record as in support of building a bridge barrier to help prevent suicide.

Sincerely,
Cynthia McNulty, LCSW

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Thank you, Cynthia: <http://www.santabarbaratherapy.org/therapists/therapist.html?tid=392>

 Please consider the environment before printing this e-mail



<ajmoody1@yahoo.com>

01/05/2011 01:41 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>

cc

Subject Please

We must have barriers on Cold Springs Bridge. Everything I know and have learned tells me it saves lives. Please.

Arlene Moody
ajmoody1@yahoo.com



Rebecca Norton at SBHS
<norton@sbsdk12.org>

01/05/2011 12:16 PM

To: Matt Fowler <matt_c_fowler@dot.ca.gov>

cc

Subject: In Support of Building the Cold Springs Canyon Bridge Barrier

Dear Mr. Matt C. Fowler (Caltrans Senior Environmental Planner),

Unfortunately, I am unable to attend the public hearing this evening. As a concerned citizen and school psychologist, I am writing to urge the County and Caltrans to build a barrier to prevent suicides on the Cold Springs Canyon Bridge.

This sends a strong message to individuals considering suicide as well as the community in general that we care about them and that we both literally and figuratively have "safety nets" in place to prevent self-harm. Suicide is a critical and devastating national public health issue that warrants a multi-faceted prevention approach.

Very Truly Yours,
Rebecca L. Norton

--

Rebecca ("Becki") Norton, M.Ed.
School Psychologist, S.B. Schools
Santa Barbara High School (805) 966-9101 ext. 232 Mon., Tues., & alt. Fridays
McKinley Elementary School (805) 966-9926 ext. 131 Weds., Thurs., & alt. Fridays



David Ortiz
<dortiz@lacolinajhs.org>
01/05/2011 01:09 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Suicide Barriers on the Cold Springs Bridge

Matt,
I unequivocally support this project. Anything we can do to deter people from harming or killing themselves is an absolute must. I have reviewed the proposed design and see little to no distraction from the aesthetics of the environment. Please let me know what I can do to help. Personally, we were devastated by the suicide of our dear friend, Andrew Popp, five years ago. It is time to take a stand.
Respectfully,
David L. Ortiz



"Ortiz, Heidi [MNTUS]"
<Hortiz1@ITS.JNJ.com>
01/04/2011 04:43 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Spring Bridge barrier

Hello Mr. Fowler,

I am a concerned citizen that drives over the Cold Spring Bridge each day to work from the Santa Ynez Valley into Santa Barbara. I feel very strongly that there should be suicide barriers built on this bridge since the railing is so low to the ground. Those who jump from this bridge have no chance to survive. Putting time between the person's suicidal impulse and the action to take their lives is crucial and why restricting access to means for suicide is so important. I fully support Cal Trans in the effort to build this barrier and hope that your organization succeeds.

It's so sad that so many people have died on this bridge and I feel barriers can help with this dangerous landmark.

Kind regards,
Heidi Ortiz
Marketing Services Manager/Marketing Specialist
Mentor Worldwide LLC
201 Mentor Drive
Santa Barbara, CA 93111
Direct: 805-879-6247
Fax: 805-967-3013
Please note new email: hortiz1@its.jnj.com



Geoff Parr
<gparr@glendon.org>
01/04/2011 03:26 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge Barrier

Hello, I wanted to register my strongest support for the proposed Cold Springs bridge barrier. I think the barrier will save lives and should be constructed as soon as possible. Restriction of means has been shown to reduce suicide. I believe the proposed barrier will not degrade from the appearance of the bridge, but even if it did I believe that saving lives is clearly more important than aesthetic considerations of the bridge architecture.
Yours Sincerely

Geoff Parr
Director of Film Production
The Glendon Association
115 West Canon Perdido St
Santa Barbara, Ca 93101
805-681-0415 extn 229
Visit us at <http://www.glendon.org>



Go Green, Keep it on screen - think before you print



Debbie Peterson
<debbiepeterson 9@gmail.com>
01/12/2011 07:12 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Bridge

Dir Sir,

I have been on that bridge a few times. My special needs sister lives in Goleta and my Mom lives in Santa Maria. We have camped at Lake Cachuma and crossed Bridge with my Jeep loaded with kids and camping gear, and in my minds eye. the visions are scary, what if the wind blows to hard, what if there is a drunk driver, what if someone is texting, and on and on. I NEVER take my eyes of the road for a second when on this bridge.

PLEASE put barriers.

Thank you

Sincerely,
Debbie Peterson



Stephen Phillips
<drscp@sbcglobal.net>
12/30/2010 11:18 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Canyon Bridge

Please erect barriers at Cold Springs Canyon Bridge. It will save lives, a much more important consideration than aesthetics.

Best,
Dr. Stephen Phillips

Stephen C. Phillips, J.D., Psy.D.
License No. PSY17868
9171 Wilshire Boulevard, Suite 350A
Beverly Hills, California 90210
(310) 275-4194 | Fax: (310) 788-0988

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Jeff Pienack
<JPienack@t-mha.org>
01/03/2011 05:17 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge SB 154

To whom this concerns:

It is a very good idea to take better security measures at this location by actually building barriers on this bridge. It will surely save lives and \$\$ in the many ways described in your studies. Please act on this now.
Thank you.

Jeff Pienack
Job Developer
Supported Employment Program
Transitions Mental Health Association
805-540-6550 c 805-305-3172
jpienack@t-mha.org
www.t-mha.org



Teresa Pietsch
<Teresa@fumcsb.org>
12/17/2010 02:28 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc
Subject suicide barriers

Suicide barriers can save lives. It is important to complete the suicide barriers on the Cold Springs Canyon Bridge.

Teresa Pietsch
Coordinator of Christian Education
First United Methodist Church
305 E. Anapamu St.
Santa Barbara, CA 93101
805-963-3579



Jennifer Purdy
<Jpurdy7@aol.com>
12/30/2010 08:05 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge Barrier

Dear Sir:

Please use this email as my support for the suicide barrier on the Cold Springs Bridge being built. I can not imagine that the beauty of the bridge could be worth the grief and sadness that families have suffered and continue to suffer due to this bridge being so available to those whose sadness have overwhelmed them.

Thank you,

Jennifer Purdy
1308 San Rafael Ave
Santa Barbara, CA 93109



Brian Robbins
<brian.robbins9873@gmail.com>

01/24/2011 09:07 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Cold Springs Canyon Bridge Barrier Project

Dear Mr. Fowler,

I am writing in support of your efforts to proceed with the building of a barrier, that will amongst other things help deter suicides, on the Cold Springs Canyon Bridge.

My step brother, Matthew Aydelott, jumped to his death on September 8, 2008, from the bridge. If there were a barrier in place, to inhibit access to the edge of the bridge, the events of that fateful evening could have turned out much differently. Research shows that efforts to deter bridge jumps do not lead to increases in suicides by other means. Additionally, suicide is linked to depression, and it is only a matter of seconds to dissuade disaster and enable sufferers to pursue medical assistance and continue with their lives.

Thank you for considering my thoughts, regarding the Cold Springs Canyon Bridge Barrier. Please keep this project alive.

Thoughtfully yours,

Brian Robbins
Son, Brother, Step-Son, Step-Brother, Husband and Father



John Robbins
<johnrobbins2001@msn.com>

01/24/2011 12:13 PM

To <Matt_C_Fowler@dot.ca.gov>

cc Karen Aydelott <kaydelott@aol.com>

Subject DRAFT SUPPLEMENTAL ENVIRONMENTAL IMPACT REPORT

Dear Mr. FOWLER,

Attached is a letter supporting Caltrans construction of the Suicide Barrier on the Cold Spring Canyon Bridge.

If you have any questions or additional information would be of assistance please contact me at this address.

Thank you,



JOHN ROBBINS JR LTR BRIDGE 110124.doc

JOHN ROBBINS
524 Bluerock Drive
San Luis Obispo, California 93401
805-547-1557

January 24, 2011

To: Matt Fowler, Senior Environmental Planner
Caltrans
50 Higuera Street
San Luis Obispo, CA 93110

Subject: Cold Spring Canyon Bridge Suicide Barrier

Dear Mr. Fowler,

As a former resident of Los Angeles County and an avid endurance cyclist, I am very familiar with the Cold Spring Canyon Bridge having traveled Highway 154 dozens of times each year for over 20 years. The purpose of most of those trips was to participate in bike rides in Santa Barbara and San Luis Obispo Counties. I witnessed the substantial improvements on 154 over the last decade and always wondered what Caltrans would do with that bridge, which to me was too dangerous to ride a bike across or even walk across, if, for some reason my car failed and I had to get to the other side.

Matthew Aydelott was my stepson, a gifted young teacher in the prime of his life. The current bridge appears to have no pedestrian safety features, no walkway and no satisfactory pedestrian railing, I know that the bridge is not intended to be a pedestrian walkway, but in an emergency it becomes one; just as in a traffic emergency on Highway 101, Highway 154 (as I've witnessed) becomes a truck route jammed with 18 wheelers and other vehicles that 154 was not intended to accommodate, but can. I believe that this lack of pedestrian safety features makes the bridge an easy and unrestricted solution for a person bent on self-destruction even for a short period of time. The proposed barrier would alleviate that condition.

I was disappointed when work was halted after money had been spent on the bridge last year and I understand that the aesthetics of the barrier were the reason. I drove the route this weekend in both directions and find it troubling that anyone while driving across the bridge (about 13 seconds @ 55 mph) is first concerned about aesthetics, when the lanes are narrow and the road has a concrete car barrier...the first priority should be looking at the road until the concrete car barrier ends after the bridge.

Nothing will bring Mathew back, but there had been others since and without the barrier, surely there will be more: not just sad, but irresponsible. I appreciate the excellent work that Caltrans has done developing the barrier alternative and fully support it's construction.

Sincerely,

John Robbins

e-mail – johnrobbins2001@msn.com



Krystal
<Krystal@epitaph.com>
01/23/2011 10:01 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Spring Bridge

Hi Matt,

I am writing to urge you to proceed with the work of building a barrier, to deter suicides, on the Cold Spring Canyon Bridge.

As a family member of Matt Aydelott, who did jump from the Cold Spring Bridge on Sept. 8, 2008. I feel strongly that a barrier could have saved his life. I think that research shows that barriers on bridges do deter suicides without an increase in suicides by other means. I know by building this barrier it won't bring Matt back but my hope is that it might potentially prevent this heartache from happening to other families!

I believe that the beauty of the area wouldn't suffer at all with a barrier on the bridge, but for those that feel this isn't the case... I say isn't humanity most important?

Thank you for taking the time to consider my thoughts in support of a suicide barrier on the Cold Spring Canyon Bridge.

Krystal Robbins



Mickie Robbins
<mickie.c.robbins@gmail.com>
m>
01/21/2011 03:54 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Spring Bridge

Dear Mr. Fowler:

I am writing to urge you to proceed with the good work of building a barrier, to deter suicides, on the Cold Spring Canyon Bridge.

I believe that Caltrans has prepared a thoughtful Supplemental Environmental Impact Report that addresses concerns related to the beauty of the bridge and the surrounding environment.

As a friend of Matt Aydelott, who did jump from the Cold Spring Bridge on Sept. 8, 2008. I believe that a barrier would have saved his life. I further believe that research shows that barriers on bridges do deter suicides without an increase in suicides by other means.

As a Mental Health professional I know that the decision to suicide is often made in a split second and if it is prevented at that moment people can get help and continue on with their life.

Thank you so much for considering my thoughts which are presented to you in support of a suicide barrier on the Cold Spring Canyon Bridge.

Sincerely,
Mickie Robbins, LMFT.



Camerin Ross
<camerinross@yahoo.com>
12/30/2010 04:20 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Re: Support Suicide Barriers on the Cold Springs Canyon Bridge

12/30/10

Dear Mr Fowler,

Being a near native (44 years) of the SF Bay Area, I am familiar with the pros and cons of suicide barriers on the Golden gate bridge. I am also familiar with the emotions of those who appose the aesthetics and cost of suicide barriers and people whose lives have been forever changed by a friend or family member who in a moment of desperation chose to end their lives by suicide.

Before I understood the real consequences and potential for the prevent of suicide, I believed those persons who wanted to end their lives would just choose another way. So my thinking was why should we (the tax payers) pay huge sums of money for some "ugly" contraption? Since then, my life has been touched by suicide and several have involved the beautiful, seductive, and deadly Golden Gate bridge. When this controversy comes up in conversation now, I ask one question, "If it were YOUR daughter or son, friend or family member, peering over the railing of the bridge, how much would a barrier be worth to you...?"

I realize this emotional appeal is rather dramatic, and research backs up the rationale that if you give people in extreme emotional pain less access to deadly means of carrying out their plan, more lives can be saved from impulsive suicides.

I urge you to do everything you can to make the creation of a barrier on the Cold Springs Bridge a priority.

Sincerely,
Camerin Ross
Tiburon, CA

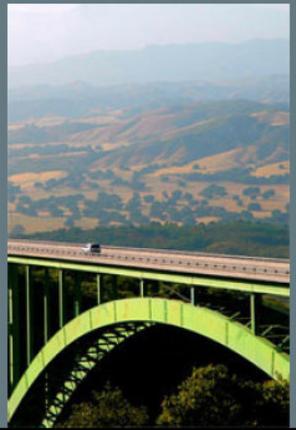
Doctoral Candidate, Clinical Psychology
CA School of Professional Psychology, PhD Program
Alliant International University, San Francisco Campus

Registered Psychological Assistant PBS 35218
to Deborah Brenner-Liss, PhD PSY 9452
APTED
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A Mindful Approach to Lasting Happiness with Food, Weight and Body Image!
info@CamerinRoss.com
<http://CamerinRoss.com>

From: Jina - The Glendon Association <Jina_The_Glendon_Association@mail.vresp.com>
To: camerinross@yahoo.com
Sent: Thu, December 30, 2010 3:31:17 PM
Subject: Support Suicide Barriers on the Cold Springs Canyon Bridge

[Click to view this email in a browser](#)

	December 30, 2010
	Suicide Barriers on the Cold Springs Canyon Bridge – need public comment
	<p>Building barriers on the Cold Springs Bridge is a safety project that will save lives at one of the most lethal spots in five counties. Those who jump from this bridge have no chance to survive. Putting time between the person's suicidal impulse and the action to take their lives is crucial and why restricting access to means for suicide is so important and is 5th Goal of the US National Strategy for Suicide Prevention.</p> <p>We need your comments of support in order to complete the suicide barriers on the Cold Springs Canyon Bridge.</p> <p>Please make your comments known by January 24, 2011 Email matt c fowler@dot.ca.gov</p> <p>You can submit written comments to: Caltrans 50 Higuera Street San Luis Obispo, CA 93401 Attn. Matt C. Fowler, Senior Environmental Planner</p> <p>For California residents, please attend</p>

	<p>the public hearing:</p> <p>Date: Wednesday, January 5, 2011 Time: 5:30 p.m. to 7:30 p.m. Place: San Marcos High School: 4750 Hollister Ave, Santa Barbara, CA 93110</p> <p>You can view and learn more about the barriers here.</p> <p>Barriers save lives, read what the research says here.</p>
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The Glendon Association
115 W Canon Perdido
Santa Barbara, California 93101

[Read](#) the VerticalResponse marketing policy.





Anne Josie
<keepitrealsb@gmail.com>

01/01/2011 02:20 PM

To <matt_c_fowler@dot.ca.gov>
cc ME <keepitrealsb@gmail.com>, Beth Ross
<Bross@co.santa-barbara.ca.us>
Subject I Support Suicide Barriers on the Cold Springs Canyon
Bridge

Dear Matt C. Fowler,

I have reviewed research showing that suicide barriers at places like Cold Springs save lives, and unequivocally support the construction of the barrier as soon as possible to prevent further loss of precious human life.

While I am a great appreciator of the natural beauty and sweeping views in the area, I do not believe the structure as proposed would have what could be called a significant effect on these assets, especially weighed against the life-saving effect the barrier would have on suicidal people drawn to the bridge and first responders.

Thank you for your time and consideration.

Best regards,

Elizabeth A. Ross
1426 Garden Street Apt 12
Santa Barbara CA 93101
(805) 618-8229



Tamara Short
<tamarashort@gmail.com>

01/24/2011 01:48 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge Barrier

Good afternoon,

I'm writing to let you know that I fully support the barriers being put up on the Cold Springs Bridge. I have lived in Santa Barbara for the last 20 years and drive over the bridge at least a few times per week. I drove over the bridge just moments after someone recently jumped and would love to know that I never have to worry about experiencing that again.

Thank you for your time,
Tamara Short



Robert Slayton
<bobsleyton1949@yahoo.com>

01/05/2011 09:18 PM

To <matt_c_fowler@dot.ca.gov>
cc

Subject barriers on hwy 154

Anything that you can do to help get the barriers up would be appreciated. Taking any opportunity to make it more difficult for the person contemplating suicide is a worthwhile effort.

Robert Slayton
805-895-2684



<bobsleyton1949@yahoo.com>

01/05/2011 04:11 PM

Please respond to
<bobsleyton1949@yahoo.com>

To <matt_c_fowler@dot.ca.gov>
cc

Subject Bridge barrier

It is hard to impossible for me to understand placing material or even ecstatic value over the preciousness of a human life. I hope those of you that are fighting to make this barrier a reality know their are those of out here that get it, and are grateful to all that are fighting for the hope of a second chance for anyone in need of it. And to those opposing I hope if you are one of the lucky ones that wake to the reality of how truly priceless a person can be and the true value of a second chance. I pray for you that this awakening does not come from a tragic loss of someone you would do or give anything to change the coarse of history and give them a second chance.

Thank you again Robert Slayton
Sent via BlackBerry by AT&T



Richard Smith
<resmith1502@gmail.com>

01/05/2011 07:38 AM

To <matt_c_fowler@dot.ca.gov>
cc

Subject 154 Bridge Suicide Barriers

Dear Matt,

This e-mial is to express my support for installing suicide prevention barriers on the the 154 Bridge. It has been proven that this type of barrier saves lifes.

RIchard Smith



Steve Smith
<ssmithts@gmail.com>
01/05/2011 01:40 PM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge Barrier

I support implementing a barrier on the Cold Springs Bridge as a measure to help prevent suicides.

Sincerely
Steve Smith

From: Joan Speirs <joanspeirs@yahoo.com>

To: Jensen David & Karen <djdvm@yahoo.com>

Sent: Wed, January 5, 2011 11:41:36 AM

Subject: sorry for the inconvenience

David,

I don't seem to have the correct e mail address to send my comments for the barrier at cold Springs Bridge?

Can you forward on if you know the correct address or provide me with the correct e mail?

Thanks,

joan

"Caltrans,

After loosing a friend to suicide on the Cold Spring Bridge, I began attending meetings on suicide prevention. I am convinced a barrier is a deterrent to suicide and feel we would be remiss and impractical if didn't proceed with plans to put a barrier in place .

Hopefully this project can move forward in a timely, cost effective manner! I believe the barrier saves life's.

Sincerely,

Joan Speirs"

matt_c_fowler@dot.ca.gov

Joan Speirs

El Ranchito de las Llamas y Alpacas

605 Rancho Alisal Drive

Solvang CA 93463

home 805-688-2821

cell: 805-455-6539

fax 805-688-1226

www.alpacasfun.com

joanspeirs@yahoo.com



sally stewart
<stewartsally@msn.com>
12/17/2010 02:47 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Spring Arch Bridge

I am a fully confirmed supporter of the bridge barrier for our "suicide" bridge.

I was a Glendon Association volunteer for three years.

I'm a suicide survivor - my daughter jumped to her death eight years ago.
She jumped from her 7th floor apartment balcony.

I'm a member of the newly-formed Santa Barbara Resource Network, a group of people who are being trained in depth to assist First Responders when people have witnessed or been affected by traumatic incidents, i.e., homicide, suicide, gang fights, etc.

We do NOT need more trauma in our lives. Please build the bridge.

Sally Stewart
705-8765
stewartsally@msn.com



sally stewart
<stewartsally@msn.com>
12/30/2010 05:28 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject The Cold Spring Arch Bridge

I am 100% in favor of adding a barrier to this bridge. I know from personal experience that people who have attempted suicide and have been thwarted by another person or a barrier have not gone on to kill themselves at another place or in another way.

Please allow this barrier to be built to save the lives of depressed individuals as well as to protect those who are first responders or involved in retrieving the bodies.

Thank you.
Sally Stewart
Suicide Survivor, Two Relatives



Maureen Sullivan
<maureens444@gmail.com>
01/04/2011 03:29 PM

To <matt_c_fowler@dot.ca.gov>
cc

Subject I support barriers!

Hello,

I would like to say that I am **STRONGLY** in favor of putting barriers on the cold springs bridge. It is not a safe bridge for anyone, let alone someone who is teetering between their will to live and their desire to end emotional and mental pain. Suicide is a permanent solution to a temporary problem. With the right support, suicidal people can go on to enjoy healthy lives. All studies show that barriers lower the suicide rates in the entire geographic region. Their presence communicates that the lives of the community **MATTER**. It is **INSANE** not to put these barriers up, knowing how much pain and suffering they will prevent for our friends, family members and neighbors. I pray that sanity and reason will prevail and we will get these barriers on our bridge!

Sincerely,

Maureen Sullivan
Santa Barbara, CA
805-895-6705

--

Thanks,
Maureen



Paul Trent
<paultrentjr@gmail.com>
01/03/2011 09:33 PM

To <matt_c_fowler@dot.ca.gov>
cc

Subject Cold Spring Bridge barrier

I strongly support the barrier. All the experts say it will save lives. I'm certain they are right.
Paul Trent



<burbonas@cox.net>

01/05/2011 07:07 AM

To <matt_c_fowler@dot.ca.gov>

cc

Subject bridge

I personally know 3 people who have killed themselves from the Cold Springs bridge. I believe that it a barrier will greatly help reduce this. If people don't have a quick, easy way to do it, they may reach out and get help. Please reinstate this barrier project.

Thanks,

Barbara Urbonas



Gil Varon - Encore

<gvaron@encorerenewable.com>

01/05/2011 03:14 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Comments on adding Suicide Barrier to Cold Springs Bridge

Dear Mr. Fowler,

I am a long term resident of Santa Ynez, that works in Santa Barbara and commutes over the bridge nearly every day of the week.

While I love the beauty of the bridge as you cross it, I value human life far more and urgently request that you approve the Barrier Fence.

I have seen one Suicide scene moments after it happened and the anguish I saw is etched on my mind. I also have a friend that tried to save a Suicide victim and nearly was killed themselves in the effort. In addition, I have read of several Police Officers that almost were killed trying to save Suicide victims.

Suicide can be prevented and people can have a second chance in life. I'm afraid if the Barrier does not go up, these people will never get a second chance.

I have looked at the information provided, including views of the Barrier and do not feel the Barrier will change the beauty of the bridge.

Thank you for your consideration.

Gil Varon

Vice President of Sales

Encore Renewable Energy

805-967-8060 x262

gvaron@encorerenewable.com

<http://www.encorerenewable.com/>



Andra White
<andrawhite@mac.com>
01/05/2011 11:43 AM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Spring Bridge

Hi Matt,

Please know that I completely support that barriers absolutely need to go up on Cold Spring Bridge a.s.a.p..

I know it will save lives!

Please do what needs to be done to discourage people from killing themselves.

Thank you,

Andra White



<dayner@comcast.net>

01/05/2011 04:11 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Cornell report on suicide barriers

To: Matt C. Fowler, Senior Environmental Planner

Re: Safety project of placing barriers on the Cold Springs Canyon Bridge

Mr. Fowler,

I understand I am able to submit comments regarding the visual aesthetics of the barriers on the bridge.

I would like to state that first and foremost, any life is beyond comparison to any view.

While people are driving their focus should be on the road and not on the view. I understand that is why vista points are created and I believe there is one located near this bridge.

There shouldn't be any discussion beyond that.

However, knowing that a select group of individuals sued to have this safety endeavor delayed and halted completely, I feel compelled to include a recent report regarding bridge barriers. This was a consultation by specialists in the field of suicide and prevention requested by Cornell University following the cluster of suicides that occurred there recently.

I am including some specific paragraphs but cite some pages that are most pertinent to your issue.

Of note is the section where the arguments against any barriers are addressed, beginning on page 22.

My husband and I presented 22 scientific abstracts from around the world to the Board of Directors of the Golden Gate Bridge, Highway and Transportation District, most of which are referenced in this report.

They requested their staff to obtain the full reports to review themselves.

When dealing with actual facts, they came to the reasonable conclusion that aesthetics can be maintained while death is prevented.

I certainly hope this information can be used to help others understand the actual issue at hand is the safety of those who are unable to reason clearly and not a scenic view.

Thank you for your time and consideration,

Sincerely,

Dayna Whitmer
268 Dartford St.
Hercules, CA 94547
Mother of Matthew Whitmer, missing since 11/15/2007
Suspected Golden Gate Bridge suicide victim

<http://www.cornell.edu/caring/docs/062010-cu-consultation-report-extended.pdf>

Consultation to Cornell University
Beautrais, Gould, & Caine
July 2010

Here are a few of the paragraphs from this report:

Jumping is a violent, highly lethal method of suicide. Case fatality (the fraction who die of all those who attempt suicide using this method) is estimated at over 30% for jumping from all structures and buildings, and *is far higher (over 90%) for higher bridges*. Death is usually inevitable from jumps from five stories.

“Suicide hotspot” is a term that is loosely defined but typically used to describe a specific site,

usually in a public location, which is used frequently as a location for suicide, has easy access, and which gains a reputation and media attention as a place for suicide. All the world’s leading suicide hotspots appear to be jumping sites. The Golden Gate Bridge in San Francisco is a readily apparent example of an iconic suicide hotspot.

The available scientific data regarding suicide deaths and attempts related to jumping from bridges strongly suggests that *most individuals who jump from iconic sites are ambivalent, act impulsively, choose a specific site, and if thwarted from an attempt at that site at a particular time, will survive*. The decision to attempt suicide may be a transient response to a particular set of emotional circumstances that resolve with time. If access to a lethal means of suicide is denied during this time, the individual may make a suicide attempt with a less lethal method or make no attempt at all. These observations are consistent with evidence that many of those who make

suicide attempts are impulsive and suggest that measures to prevent suicides by jumping may be worthwhile by delaying or averting some fraction of impulsive suicide attempts. They are also consistent with a large body of evidence that suggests that restricting access to a range of methods of suicide may prevent suicides, and not immediately lead to method substitution.

SUMMARY, RECOMMENDATIONS, AND CONCLUSION

Given the data available in the literature – about jumping, contagion and clusters, and youth suicide – together with the rapid unfolding of a major media event *locally and nationally*, the urgent decision and implementation of a program of protection initiatives on the Cornell campus and for the Ithaca community was an essential and prudent effort to staunch the likelihood of any further suicides from the local bridges. Immediately placing barriers on the bridges was one component and certainly the most visible. *It was an essential demonstration of the University's commitment to safety above all else*, and it was entirely in keeping with what has been shown to work in other settings.

Our summary of the scientific literature underscores that the available scientific data regarding suicide deaths and attempts related to jumping from bridges strongly suggests that *most individuals who jump from iconic sites are ambivalent, act impulsively, choose a specific site, and if thwarted from an attempt at that site at a particular time, will survive.*

We see no alternative but to promote safety and caring for vulnerable persons as the central driving elements of this discussion. *It is our recommendation that temporary barriers that meet*

standards of effectiveness remain in place, until permanent safety measures can be built

PREVENTING SUICIDE BY JUMPING FROM BRIDGES OWNED
BY THE CITY OF ITHACA AND BY CORNELL UNIVERSITY

CONSULTATION TO CORNELL UNIVERSITY

"EXTENDED REPORT"

JULY 2010

ANNETTE L. BEAUTRAIS, PH.D.
SENIOR RESEARCH SCIENTIST
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YALE UNIVERSITY SCHOOL OF MEDICINE

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ERIC D. CAINE, M.D.
JOHN ROMANO PROFESSOR AND CHAIR
DEPARTMENT OF PSYCHIATRY
UNIVERSITY OF ROCHESTER MEDICAL CENTER

CONSULTATION TO CORNELL UNIVERSITY – EXTENDED REPORT

INTRODUCTION

The City of Ithaca and Cornell University are faced with a challenge unlike any encountered elsewhere in the world. The settings that are essential identifying features of the community – and symbols that have been embraced universally as attractions – also are the sites of suicide, one of the least understood and most meaning-laden of all human actions. Rather than having one site for suicide – a so-called “hotspot” that has become iconic – Ithaca’s bridges and gorges collectively stand as the points of concern. Moreover, while there has been intensive attention to scientifically designing and testing public health and individually oriented approaches to preventing suicide, this is a young science where results are preliminary and definitive evidence is lacking.

It is within this context that we were asked to consult on both immediate and intermediate-term response to the recent deaths of students who jumped from bridges on or adjacent to the Cornell campus. The focus of this report necessarily emphasizes the matters of the moment, maximizing safety to save lives, considering the continuance of temporary barriers that are offending in appearance to all eyes, and suggesting steps that can facilitate a safe transition to a more settled set of outcomes – built on collaborative discussion among the diverse groups that ultimately must have a “say.” Such collaborative and collective responses are essential for any efforts, if they are going to have a chance of proving effective.

BACKGROUND

Ithaca is a city of 30,000 people situated at the south end of Cayuga Lake, the longest of the Finger Lakes of Central New York. It is famed for its natural beauty, with steep and spectacular wooded gorges and dramatic waterfalls. Ezra Cornell, in founding the University on the heights above downtown Ithaca and Cayuga Lake, decided 150 years ago to tie the identity of the school to its gorges, purposefully building the new campus between these magnificent landmarks. Cornell University registered 20,633 students this past academic year – 13,931 undergraduate and 6,702 graduate students.

To this day, students and faculty choose Cornell because of its scenery and surroundings, and its offer of a vibrant intellectual culture outside of a dense urban environment. The campus area includes seven bridges that cross the main gorges, and members of the Cornell community, including students, faculty, and staff, traverses these bridges daily, often on foot. The University owns four of the bridges, while the City of Ithaca owns the other three. Four of the seven have served as significant sites for suicides, considering deaths over the course of decades, with the most from the two Stewart Avenue bridges, which are owned by the City.

The rate of suicide over time at Cornell University has been consistent with national suicide data in higher education, despite Cornell’s reputation as having had an elevated rate. However, six Cornell students died by suicide during this immediately past (2009-10) academic year, five of these on or near the campus, including three who jumped from bridges or an adjacent gorge edge in close temporal proximity during February and March, the last two within two days. These six deaths constituted a statistically significant ($p < .001$) as well as a clinically meaningful

suicide cluster^{1 2} The cluster generated substantial and persisting local, national, and international media attention. This news coverage, in turn, served to raise the risk level for further suicides among Cornell students – and among vulnerable people living in Ithaca and Upstate, or for those who might come to Ithaca from distant places to die.

In response, the University, with the City's consent, installed temporary chain link fences across all seven bridges. The erection of the temporary barriers over the spring break prompted extensive public discussion, both supportive and acrimonious, including protests that the barriers were a blot on the landscape, that they would not deter anyone with a strong intent to kill him/herself, or that their presence might be regarded by vulnerable individuals as so depressing that they might become more distressed.

The initial agreement to place the barriers included a time-certain deadline in early June for removal on the City-owned bridges, which recently has been extended another 10 weeks. It was within this context that we visited Ithaca on 3-4 May 2010 to view each bridge, and talk with students, faculty, administrators, and Ithaca leaders. Our group included three suicide researchers: Dr. Eric Caine, Chair of Psychiatry, University of Rochester Medical Center, and an alumnus of Cornell; Dr. Madelyn Gould, Professor, Departments of Psychiatry and of Public Health (Epidemiology), Columbia University/New York State Psychiatric Institute, and Dr. Annette Beautrais, Senior Research Scientist, Department of Emergency Medicine, Yale University School of Medicine. In addition to our data gathering, we were asked to provide education about suicide and suicide prevention for Cornell faculty, staff, and students, and for Ithaca city leaders and community, and to add expert input to the discussions of policy makers.

This extended report includes the key issues conveyed at the consultation meetings, and the major findings and recommendations of the consultants. It includes detailed summaries of the literature and in-depth commentary note included in the Executive Summary and the "Basic Report," though the recommendations and much of the pertinent text are identical.

KEY ISSUES AND RELEVANT DATA

As part of its urgent response to the deaths in February and March 2010, in particular, the University initiated a series of coordinated steps to augment its already considerable efforts devoted to mental health promotion and suicide prevention. Central to these, temporary barriers were placed on six of the seven bridges over local gorges, and the seventh was closed. *This action was an essential demonstration of the University's commitment to safety above all else*, and it was entirely in keeping with what has been shown to work in other settings. It is important to underscore that this was not the only aspect of the University's response.

Three critical issues served to drive the urgency of the needed decisions and to shape future discussions – 1) the nature of youth suicide, suicide contagion, and clusters; 2) documented jumping from iconic sites, most especially bridges; and overshadowing these, 3) the extent of media coverage of the recent suicides and its lasting impact. The latter issue has been deeply intertwined with all that we consider.

Nature of youth suicide

Youth suicide represents a preventable cause of premature death, claiming approximately 250,000 lives worldwide annually between the ages of 10-24.³ After motor vehicle crashes and homicide, suicide remains the third-leading cause of death in the U.S. between the ages of 10 and 24 years.⁴ Each year, suicide accounts for more than 4000 deaths among 15-24 year olds in the US - one youth suicide every 2 hours.

During the last three decades an extensive body of research has accumulated about the individual, genetic, psychiatric, social, cultural and contextual factors associated with youth suicidal behavior.⁵⁻⁷ Risk factors and characteristics of youth suicidal behavior show remarkable congruency across countries and cultures. Risk factors for suicidal behavior range from micro-level genetic factors, to meso-level family influences to macro-level social influences (e.g., unemployment rates) and global issues (e.g. Internet-supported social networking), all of which can lead directly or indirectly to suicidal behavior.

Individual vulnerability is strongly influenced by genetic susceptibility to mental health problems, especially to mood disorders, substance abuse, anxiety disorders and antisocial and offending behaviors.^{6, 8} Contextual factors (means of suicide; media climate) and life stresses are additional influences. Only a subgroup of those at risk of suicide, because of psychiatric illness and local and broader social adversity ever attempt or die by suicide because there is variability in the diathesis or predisposition to suicidal behavior.

Demographic risk factors. The suicide rate in the US rises gradually during the late-teens.⁹ Rates are four times higher for males yet attempt rates are far higher in females, suggesting both a different pattern of risk factors for suicide and nonfatal suicidal behavior, and that preventive approaches may need to be tailored to the two sexes. The “developmental nature” of risk factors differs across the lifespan, again pointing to the need for distinctive preventive interventions that are tailored to address the specific challenges of different age groups.

Family history of suicidal behavior. A family history of suicidal behavior is a strong risk factor for suicide and suicide attempt¹⁷; up to 45% of the variance in suicidal behavior is genetic in origin.^{17 10} Suicidal behavior is also associated with a family history of aggression and anger.¹¹

Personality/cognitive factors. Certain personality factors and cognitive styles (including self-esteem, hopelessness, neuroticism, impulsivity, aggression, perfectionism, self-consciousness, social disengagement, cognitive rigidity) may predispose youth to suicidal behavior by framing perceptions and reactions to stressful situations in negative ways.^{12,13}

Sexual orientation. Risks of suicide attempt and suicidal ideation, and of developing mood, substance use and anxiety disorders, are increased among gay, lesbian, bisexual and transgendered youth.¹⁴⁻¹⁷

Physical illness. Neurodegenerative and chronic illnesses (including diabetes, Huntington’s disease) increase risk of suicide attempts and suicide; even the *perception* of physical illness in young males confers risk. SSRI medications have been associated with increased suicidal behaviors – not suicides – during clinical trials, and may point to increased real-world idiosyncratic risks, and more recently similar iatrogenic adverse effects have been suggested for

antismoking, obesity, acne and anti-epilepsy drugs as well as anabolic steroids abused by young athletes.¹⁸

Psychiatric illness. Mood or substance use disorders, schizophrenia, anxiety, conduct and antisocial behaviors and/or personality disorders (borderline or antisocial personality disorders) are present in most youth who attempt suicide or die by suicide.^{6, 8, 19-30} Mood disorders are the single disorder most commonly linked to suicidal behavior. Alcohol and substance use disorders increase risk of suicidal behavior, especially in older male youth; binge drinking increases suicide risk, especially in those with depression and stressful events.³¹ Anxiety disorders, including panic disorder and PTSD, increase risk of suicidal behavior; risk is greatest for GAD (generalized anxiety disorder) in association with depression.

Psychosocial factors and exposure to adversity, trauma and stress. Some young people with suicidal behavior have been exposed to childhood and family adversity (sexual, physical or emotional abuse or neglect, poor parental care; family violence; parental separation or divorce; parental psychopathology, impaired parent-child relationships, and institutional or welfare care).^{32,33} Stressful life events (relationship and interpersonal problems and losses, disciplinary or legal crises; financial problems; academic or work-related problems, and bullying) may precipitate suicidal behavior in young people who are already vulnerable to suicide.^{21, 28, 30}

Media reporting. The ways in which traditional print and tele-media report suicide can influence vulnerable young people who are susceptible to contagious effects of knowing family or associates who have died by suicide.^{5, 34} Cautious, muted reporting can help reduce risk of imitative single and cluster suicides, especially when specific sites or methods are repeatedly involved.³⁵

Access to means of suicide. Access to lethal means of suicide such as firearms and pesticides increases risk of suicide and may convert ambivalent and impulsive suicide attempts into deaths, particularly in rural areas.³⁶ Restricting access to means and sites of suicide is an effective, but often under-valued, approach to suicide prevention.³⁷

Protective factors. Strong religious, family and/or cultural ties tend to protect against suicidal behavior with protective effects likely exerted by proscription against suicide and promotion of social linkages, with these linkages and prohibitions also decreasing the risk of psychiatric disorders (including depression, substance abuse, offending and antisocial behaviors), with which suicide is associated. Colleges and universities have lower suicide rates among their student populations than rates among non-matriculated peers. Whether this reflects admission-related 'selection factors' versus campus-based protective effects (e.g., cohesive community, available mental health services), or a combination of effects, has not been studied empirically in a fashion that would allow disentangling these potential protective influences.

New communication technologies. New communications technologies (including cell phones and the internet) now exert a dominant and global influence on young people's educational and social lives. The Internet is a leading source of information for young people about suicide and readily accessed sites encompass suicide prevention, both antisuicide and prosuicide promotional materials, and factual information.³⁸ Internet sites are implicated in inciting and facilitating suicidal behavior; however, the internet holds promise for providing support,³⁹ individual email counselling⁴⁰ and screening and therapeutic programs to address depression.⁴¹

In sum, the evidence about risk and protective factors provides an empirical background for considering interventional and preventive approaches, and clearly indicates that suicide is multifactorial and complex. However, not all risk factors are equally supported by evidence, nor are all equally important. Studies that have generated Population Attributable Risk (PAR) estimates suggest that mental health factors (mood, substance abuse, anxiety and antisocial disorders, and previous suicidal behavior) make the strongest and most consistent contribution to risk of suicidal behavior. These results imply that the major, but not sole, focus of suicide prevention efforts should be directed at minimizing rates of psychiatric disorders and addressing the risk factors and life pathways that lead to these disorders. Compounding the lack of data, few of the programs that purport to prevent youth suicide have been subjected to systematic evaluation to establish efficacy, effectiveness or cost-effectiveness.

These general considerations serve to ‘frame’ the issues now confronting Cornell and Ithaca, which specifically relate to the *clustering of suicides, contagion, and jumping from iconic sites*. The latter has been a prior focus for discussions, as we learned during our visit, but the powerful forces of recent events have again spotlighted a phenomenon that has long existed in Ithaca and the surrounding region.

Clusters and Contagion

While individual risk factors, such as depression, anxiety, and substance abuse, have long been shown to exert a significant role in the etiology of suicide, mounting evidence also supports the role of imitation and modeling in suicide. The importance of modeling on suicide behavior has been suggested primarily by two areas of research: 1) clusters or “outbreaks” of suicide defined by temporal-spatial proximity; and 2) media influence on subsequent suicide related behavior. A brief review of these two sources of evidence as they relate to the current apparent suicide cluster at Cornell University is presented.

Terminology. A succinct review of nomenclature is presented to facilitate an understanding of information presented since the terms “clusters” and “contagion” are often used indiscriminately in the literature.⁴² A suicide “cluster” refers to an excessive number of suicides occurring in close temporal and/or geographical proximity. Suicide “contagion” is the process by which one suicide facilitates the occurrence of a subsequent suicide. Contagion assumes either direct awareness through contact or friendship with the suicide victim, word of mouth knowledge, or indirect transmission through the media.

“Cluster Suicides.” Early research provides descriptive accounts of suicide “epidemics” that rely heavily on anecdotal accounts of suicide behavior, usually case history methodology (see Gould and Davidson, 1988⁴³). Suicides that appear to be clustered or related (cluster suicides) have been noted in a variety of populations, including community samples, such as college students, and selected samples, such as incarcerated individuals and psychiatric inpatients.⁴⁴ Collectively, these studies reinforce the concept that exposure to another person’s suicide can precipitate *imitative* suicidal behavior, related to *temporal, geographic, and/or interpersonal proximity* as well as individual vulnerabilities.

Nevertheless, the interpretability of case history studies⁴⁵⁻⁴⁷ has been seriously hampered due to the presence of selection bias and frequent lack of a comparison group. During the past two decades, research in suicide clusters has shifted methodologically and qualitatively from

descriptive to inferential studies, reflective of the development and application of statistical techniques, such as the Scan statistic, the Knox procedure, and Poisson distribution modeling^{42, 48} to detect statistically significant clustering effects. These techniques most typically examine *discrete time intervals* to define a *unit of frequency of suicide* within a *finite assessment period*, within specific *geographic boundaries* to delimit spatial variables, and *comparing observed and expected frequencies*. Of the inferential statistical studies (see⁴⁴⁻⁴⁹), several clearly provide evidence of time-space clustering⁵⁰⁻⁵⁸, while others have found no such effects,⁵⁹⁻⁶³ or offer mixed results.^{59, 64-66}

Cluster suicides appear to be predominantly a phenomenon of adolescents and young adults.^{52, 53, 67-69} An inferential study employing stratified samples to investigate age-related effects among large-scale national populations has found that the cluster suicides are observed primarily among teenagers and young adults (15-19 and 20-24 year olds).^{52, 53, 67} In these studies, the relative risk of suicide following exposure to another individual's suicide was 2 to 4 times higher among 15-19 year olds than among other age groups, and was also significantly increased among college-aged individuals. This might explain why the majority of studies involving adolescents found significant clustering of suicide, whereas clusters have not been as prevalent or clearly evident among adult populations.

An ongoing national psychological autopsy study of youth cluster suicides (Gould et al., in progress) has yielded important findings that inform the current discussion regarding the pattern of youth suicide clusters, including the average size and duration of clusters, demographic characteristics of cluster suicide decedents and their communities, the nature of the suicide method, as well as the relationships among decedents. This research has involved a case-control study of 208 decedents, 13-20 years of age, who died as part of 53 suicide clusters across the U.S. identified between 1988 and 1996; they were compared with a matched sample of 105 "singleton" suicides. 1) Clusters ranged in size from three to 11 cases (mean = 3.9, sd = 1.6). Two-thirds of the clusters consisted of three cases. 2) The duration of clusters varied from one to 357 days (mean = 80 days; sd = 58.1 days); the interval between the first and second cases in the cluster varied from two to 103 days, with one cluster deemed to be an extension of another one that occurred two years earlier in the same community. 3) The relationships among individuals who died in a suicide cluster were relatively distant – victims were not likely to be close friends. 4) The deaths of the first cases in the suicide clusters, in comparison to the singleton controls, were more likely to have occurred in public locations. 5) There was significantly more publicity surrounding the deaths of the first cases in the cluster compared to that of the singleton controls. 6) The first cases in the cluster took fewer precautions to minimize interference during the suicide acts than the singleton controls. 7) The first cluster cases were more likely to be impulsive (i.e., planning for less than one day) than the singleton suicides.

Media Influences. The association between exposure to media coverage of real-life suicides and subsequent self-injurious behavior has been investigated for more than three decades. While research on cluster suicides indicates the plausibility of direct modes of transmission (e.g., person-to-person) of suicide clustering, studies investigating media influences on subsequent suicides point to alternative pathways of transfer.

Reviews of nonfictional suicidal stories^{49, 70-72} provide substantial evidence for a suicidal imitative effect. Consistently, the magnitude of the increase in completed suicides following a

suicide story has been shown to be proportional to the amount, duration, and prominence of media coverage. Moreover, a publicized method of suicide has been shown specifically to increase the subsequent use of that particular method.^{73, 74} There is also some evidence that the impact of suicide stories is greatest for teenagers,^{75, 76} though recent experiences in Hong Kong and Southeast Asia now suggest that this can be potent across middle adulthood as well, and spread by both print media and the Internet.⁷⁷

In the past decade, investigators have begun to acknowledge the potential impact of the Internet. While research on the Internet and adolescent suicides is in its inchoate stage, it demonstrates the disturbing power of the Internet. Case reports underscore that youths as well as adults have turned to the Internet for detailed instructions on suicide methods and have received encouragement to commit suicide or made suicide pacts;⁷⁸⁻⁸⁰ indeed, the Internet has given rise to the phenomenon of “cybersuicide” pacts, the formation of suicide pacts that involves strangers meeting over the Internet and acting together, including meeting one-another to kill themselves together.^{81, 82}

In conclusion, teenagers and college-aged students are particularly vulnerable to suicide contagion. It appears that cluster suicides may be more impulsive than other suicides, at least at their onset, and the factors that may precipitate a suicide cluster include a public location of the death followed by a large amount of publicity. Moreover, publicity of a particular suicide method appears to lead to subsequent increases in the use of that method. Of increasing concern, the Internet has the potential to rapidly amplify such exposure. Case reports underscore that youths as well as adults have turned to the Internet for detailed instructions on suicide methods and have received encouragement to kill themselves or have made suicide pacts.

Considering the research evidence as a whole, the public nature of deaths from bridges in Ithaca increased the likelihood that the 2009-10 suicide cluster would continue unabated without protective actions. More jumping deaths in Ithaca would have added further to community trauma and international notoriety, which together could have had an even greater impact on the perceived serenity and beauty of the local gorges and parks.

Jumping to Death from Iconic Sites

Jumping is a violent, highly lethal method of suicide. Case fatality percent (the fraction who die of all those who attempt suicide using this method) is estimated at over 30% for jumping from all structures and buildings,⁸³ and *is far higher (over 90%) for higher bridges*^{84 85-87} Death is usually inevitable from jumps from five stories. The incidence of suicide by jumping varies markedly around the world, and tends to be much higher in places which provide opportunities for jumping, such as cities with extensive high rise housing. Paradoxically, however, it is the far less common suicides by jumping from iconic sites which attract a disproportionate media attention and coverage.

Characteristics of individuals who die by jumping. A series of studies has attempted to characterise those who jump from bridges. However, results from these studies are not consistent. Some studies suggest that younger individuals, predominantly male, and those with

more severe mental illnesses (including schizophrenia and psychotic disorders) are over-represented amongst those who jump, while other studies have not found the same features.

This inconsistency in findings might be explained by such factors as the small numbers of suicides in some of these descriptive studies, the proximity of some jumping sites to psychiatric hospitals, the notoriety of the site in question, and the frequency of suicide by jumping in the countries of study. It may be that sites near psychiatric hospitals attract individuals who are, or have recently been, inpatients at the hospital, while those sites that have widely known reputations as suicide sites (such as the Bristol Suspension Bridge, or the Golden Gate Bridge) attract individuals from a wider geographic area and a range of different (including non-psychiatric) backgrounds with these people choosing the site largely because of its reputation rather than because of proximity and accessibility.

Features of sites where suicides by jumping occur. “Suicide hotspot” is a term that is loosely defined but typically used to describe a specific site, usually in a public location, which is used frequently as a location for suicide, has easy access, and which gains a reputation and media attention as a place for suicide.^{88, 89} All the world’s leading suicide hotspots appear to be jumping sites. The Golden Gate Bridge in San Francisco is a readily apparent example of an iconic suicide hotspot.

Sites may acquire reputations for suicide in spite of relatively small numbers of suicides from these sites. For example, Grafton Bridge in Auckland had a local reputation as a site for suicide despite having only one suicide per year.⁹⁰ Similarly small numbers were associated with other iconic sites – the Bern Muenster Terrace in Switzerland with 2.5 deaths per year,⁸⁹ and the Bristol Suspension Bridge in the UK with an average of eight suicides each year.⁸⁴

The process by which a site attains iconic status as a place from which to jump is not clear. It may, in part, be a consequence of media reporting. Despite recommendations to the contrary, journalists persist in asserting that suicides from public sites are newsworthy. This newsworthiness may be argued in light of the relatively unusual method of death: Jumping is an uncommon method of suicide in many countries, and jumping from bridges is especially rare in comparison to other more accessible methods – *in most settings*. There are many potential attractions to jumping for some individuals: The public aspect of the suicide and the site, the beauty or aesthetic appeal of the structure (e.g., the Golden Gate Bridge), the cultural significance or social meaning of the setting (e.g. Mt. Muhara in Japan), or the hazard that the suicide may pose for the public (which exists, for example, if a bridge extends over an expressway with the risk that other lives may be endangered when someone jumps).

There is some evidence, albeit conjecture for those who have died, that people tend to make their choice of method of suicide based upon their perceptions of what they understand to be certain to achieve death, to be quick, to be readily available, and to avoid risk of disfigurement (as conveyed by survivors of settings such as the Golden Gate Bridge).⁹¹ Jumping fulfills these conditions. However, the symbolism and romanticism associated with an iconic or symbolic suicide site appear to play a decisive additional role for those who choose to jump from such sites.⁹²

Thus, while there is no clear account of the mechanisms by which particular sites acquire iconic status as places for suicide, it seems likely that this process involves a combination of a public

place, an attractive location, an aesthetically pleasing structure, the nature and persistence of media reporting of suicides from the site, and the development of local history, tradition and myth. All these features likely combine to render Ithaca as “an iconic site” for suicide. Unlike other settings in the United States or internationally, this attribution appears to relate to the region and its gorges generally, rather than to one specific bridge, promontory, or park.

COMPREHENSIVE APPROACHES TO SUICIDE PREVENTION

The field of suicide prevention research field is confronted by challenges not faced in other areas of public health or clinical care. Reiterating our earlier point, this is a young science where results are preliminary and definitive evidence is lacking. To achieve success, prevention efforts must grapple with:

- 1) An inability to discriminate the relatively few ‘true cases’ from the large numbers of ‘false positive’ cases of psychiatrically ill or emotionally distressed individuals who describe many of the same thoughts and plans as those who seriously injure or kill themselves. No doubt, a failure to discriminate is compounded by the low base of suicide in the general population in the face of common complaints, symptoms, and signs of psychopathology. To date, available data reveal virtually no clinical characteristics that can be used at the individual level to distinguish those who will go on to die by suicide from those who will not.
- 2) The large number of ‘false negative’ individuals who escape preventive detection by family, and physicians or other professionals, and proceed to kill themselves.
- 3) The difficulty of clinical and social services to *reach* potentially lethal individuals in settings not *designed* for preventive or treatment interventions (e.g., the courts, schools), even when it is known that they bear many indicators of elevated risk.
- 4) The *lack of a coordinated strategy* of suicide prevention that can deal effectively with myriad local, regional, state, and national agencies and organizations that could, in theory, play a role in preventing suicide.
- 5) Our paucity of understanding about how best to define and mobilize protective factors that may diminish the impact of risk factors.

In its response to challenge #1, the University inevitably will have to ‘overestimate’ its approach by making many services generally available, as *it is not possible to determine with individual precision which ones of many distressed people ultimately will die by suicide*. While this can be viewed as a dilemma, it also should be seen as the opportunity to greatly benefit the health and mental health of the community of students, faculty, and staff.

For the second, increased training and vigilance are useful, in addition to vigorously combating the social and personal stigma of seeking care for emotional problems. These already have been the targets for many of the University’s activities that were begun during the past decade. But there will be times when suicidal people continue to be unrecognized or actively seek to avoid detection. *It is specifically for those individuals, and for the times when people at risk cannot be reached (#3), for whom “means control” is especially important!* Regarding challenge #4, Cornell together with the City of Ithaca can take leadership in defining a coordinated strategy; they are relatively

defined communities where clear, visionary leadership will be essential. And finally, the steps that Cornell already has undertaken to build a “caring community” are central to the health and mental health promotion that is the essence of the final challenge. Framed this way, it is easier to begin to discuss specific measures.

Overview: Settings and strategies for current and potential youth suicide prevention activities

SETTINGS	STRATEGIES
Individuals	<ul style="list-style-type: none"> ▪ Pharmacotherapy, pharmacogenomic therapy ▪ Psychological/behavioral treatments ▪ Psychosocial interventions ▪ Combinations of pharmacotherapeutic / psychological / psychosocial therapies
Families	<ul style="list-style-type: none"> ▪ Early intervention programs ▪ Parenting support ▪ Support / mentorship programs for at-risk youth ▪ Family-based therapy (e.g. MST)
Schools & universities	<ul style="list-style-type: none"> ▪ Screening and risk monitoring ▪ Treatment programs ▪ Curriculum based education ▪ Skills building ▪ Peer education and support ▪ Faculty and gatekeeper education ▪ Case finding ▪ At-risk group support / mentoring ▪ Cyber-based screening, therapy, skills building and wellbeing promotion ▪ Institutional support and protocols ▪ Means restriction ▪ Promotion of positive mentally healthy, caring community
Health care systems	<ul style="list-style-type: none"> ▪ Emergency Departments screening and ED-initiated treatment programs ▪ GP education, screening, treatment and management ▪ Hospital based inpatient /outpatient programs
Communities	<ul style="list-style-type: none"> ▪ Community gatekeeper programs ▪ Telephone crisis lines ▪ Faith-based programs ▪ Safe storage programs ▪ Cyber-based screening, education, treatment ▪ Media suicide reporting resources ▪ Promotion of positive mentally healthy, caring communities ▪ Means restriction, including barriers for iconic

	bridges or subways
National/state	<ul style="list-style-type: none"> ▪ Means restriction ▪ Mental health literacy and public education/destigmatization ▪ Mental wellbeing promotion ▪ Alcohol legislation ▪ Social welfare policies

When considering the published literature for approaches to prevention, it is essential to consider the ‘young’ nature of the field. Very little work, with the potential exception of studies involving the US Air Force ⁹⁵ and initiatives at a broad society level (e.g., changes in drug packaging or the composition of cooking gas), have shown *sustained effects as measured by lower rates of suicides*.

Individual-level Strategies

Pharmacotherapy. Strong linkages between depression and suicidal behaviors have led to substantial investments to prevent suicide by treating those deemed at-risk with antidepressant medications. Published data at the population level have reported decreases in suicide rates related to their administration, but the aggregate nature of the observational data precludes firm conclusions. ^{94, 95, 96, 97} Clinicians, patients, and families often attest to the powerful impact of effective treatments. However, the role of antidepressants in reducing youth suicide became controversial after the FDA ‘black box’ warning following concerns about higher rates of suicide-related adverse event reports in pediatric clinical trials of SSRIs, even as there were no deaths. Taken together, the findings of these reviews suggest that fluoxetine has a favorable risk/benefit ratio; other antidepressants may confer a short-term, modest risk of increased non-fatal suicidal behavior for those ages 24 years and younger. Overall, based on observations of the relationship between SSRIs prescription rates and suicide, they will likely benefit most young people to whom they are prescribed under expert guidance. ⁹⁸ Anecdotally it is clear that many of the cases reported in FDA hearings related to a general lack of supervision by PCPs after prescribing what they thought were relatively benign compounds (being unaware of reported adverse events). Suicide while taking antidepressants is *extremely* uncommon in young people. Indeed, the clear majority of suicides (including youth) occur in those who are not on medication, as reported in post-mortem toxicology studies, and the results of psychological autopsy studies have repeatedly emphasized a lack of appropriate psychiatric treatment, not an over-prescription of medications.

Psychotherapy. A range of psychological and psychological therapies has been shown to reduce suicidal ideation, and suicide attempts. ⁹⁹ Therapies include individual psychological therapies (cognitive behavioral therapy, CBT; interpersonal behavioral therapy, IPT; problem-solving therapy, PST; individual dialectical behavioral therapy, DBT, multisystemic therapy, MST, and group-delivered DBT). These results have been demonstrated under carefully controlled experimental or research-supported conditions, and their real-world implementation has yet to be studied in any depth.

Institution-based Strategies.

Schools and universities. Schools, colleges, and universities often are viewed as institutional settings that provide good organizational contexts for screening and intervening for suicide risk. A wide range of school-based programs has been developed including: screening programs to identify and refer those at risk; didactic suicide and depression awareness programs; gatekeeper programs for adults who have contact with young people; combined peer support and gatekeeper programs, and skills-based and competency-promoting programs. Some programs combine more than one of these approaches. However, few programs have been systematically evaluated for short or long-term efficacy, effectiveness, safety or fidelity, or suicide outcomes, and the widespread implementation of some programs, such as suicide awareness programs, has been controversial. The awareness programs have found no gains, a lack of behavioral changes despite positive changes in attitudes, or have reported undesirable effects including more maladaptive behavior, reluctance to refer friends for help, potentially harmful changes in attitudes, and iatrogenic effects resulting from bonding amongst deviant peers grouped together for program delivery.^{100, 101} Positive or promising effects have been reported for some screening programs, some skills building programs, and programs that combine gatekeeper and peer education with screening and referral.^{102, 103} Properly developed programs can be promulgated without fears of contagion.¹⁰⁴ Case-finding approaches which screen for depression, substance abuse and/or suicide risk and refer at-risk young people for treatment have been shown to effectively enhance the likelihood that students at risk for suicide will get into treatment¹⁰⁵ and offer safe¹⁰⁶ alternatives to the risks associated with didactic suicide awareness programs. Nevertheless, screening programs need further study to improve the specificity of screening tools, and to explore the extent to which those identified as being at risk have a short or longer term subsequently reduced risk of suicidal ideation or attempt.

In college-aged students online web-based screening can be delivered via personal computers. School and college group-delivered social problem solving and interpersonal therapy have shown reductions in suicidal behaviors, albeit in small samples, and online real-time CBT shows effectiveness for all ages.

Primary Care. Programs that support primary care practitioners to recognize, treat and manage psychiatric disorders related to suicidal behavior, particularly major depression, are amongst the most promising approaches to suicide prevention but few have focussed on youth.^{107, 108} Brief screening tools for use in primary care settings show promise in identifying at-risk youth and in managing adolescents with depression,¹⁰⁹ but their impact on suicidal behavior has not been assessed.^{110 111}

Emergency Departments (EDs). EDs are sites where young males who might not visit other health care facilities seek treatment for trauma, alcohol and violence-related injuries. Increasing numbers of young females are also being seen at EDs for binge drinking and alcohol intoxication. Young males and females who make suicide attempts are typically seen at EDs and discharged home without admission. Reductions in suicide attempts are reported for low cost psychosocial interventions of sending friendly letters from mental health services attached to EDs to patients in the months following their ED visit for a suicide attempt to provide contact and remind people that assistance is available,^{112 113} patients given a token to allow re-admission whenever they choose,¹¹⁴ skills-based and supportive therapy¹¹⁵ specialized ED care and family therapy (which included training workshops for emergency room staff, a videotape

aimed at modifying families' treatment expectations, and an on-call family therapist);^{116,117} brief problem solving to enhance outpatient treatment adherence,¹¹⁸ and rapid response treatment.¹¹⁹

Community-level Strategies

Media guidelines. Efforts to educate and encourage the media to report suicide accurately, responsibly and in a muted manner have reduced mortality.¹²⁰ However, media guidelines in their current form are inappropriate for youth-relevant Internet sites. Suitable guidelines are needed, and currently being prepared.

Telephone crisis services (“hotlines”). Hotlines have been shown to reduce callers' suicidal ideation,¹²¹⁻¹²³ but their impact on community suicide rates has yet to be demonstrated unequivocally. Among youth, the low utilization of hotlines and the negative attitudes toward them¹²⁴ is particularly distressing in light of recent evidence of the short-term efficacy of hotlines for youth who use this resource.¹²⁵ Efforts are continuing to optimize hotlines' effectiveness and outreach to suicidal individuals by expanding them to include on-line and texting capabilities. Such services may provide intervention if counselors are trained and empathic, and may act as a conduit to specialist assistance by providing information and referral.

Restricting access to means of suicide. Restricting access to means of suicide reduces suicidal behavior.¹²⁶ These efforts include reducing the availability of toxic pesticides, minimizing the toxicity of vehicle exhaust gas and domestic gas, erecting safety barriers at jump sites, reducing the pack size and points of sale of analgesics which are toxic in overdose, legislative restrictions on access to, and safe storage of, firearms, and, in institutional settings, modification or removal of potential ligatures and ligature points. For this consultation, the specific questions pertain to using barriers to restrict access and prevent jumping from multiple bridges into gorges, where collectively the community and the settings are “iconic.”

Measures to Prevent Suicide by Jumping

In this context communities can consider a variety of approaches to enhance suicide prevention through interceding in jumping. First we will summarize these, followed by greater elaboration.

Barrier approaches to deter individuals from jumping include:

- Install additional permanent safety barriers.
- Retain temporary barriers until permanent safety barriers are installed.
- Proposed barrier options need to be designed to take into account the following issues.
 - Barriers must deter and impede an individual from jumping from a bridge.
 - Barriers must have a minimal visual and aesthetic impact on the bridges.
 - Barriers must have a minimal visual and aesthetic impact on the surrounding geography and natural environment.
 - Barriers should not significantly impede current pedestrian access to and over bridges.
 - Barriers must be structurally and aerodynamically stable.

- Barriers must be easy, and not costly, to maintain and clean.
- Barriers should be cost-effective to construct and install.
- Barriers should not risk presenting a physical challenge to be overcome in daring (not suicidal) activities.

Augmenting non-barrier approaches that buttress primary barrier-based efforts to deter individuals from making suicide attempts by jumping from specific sites include:

- Signage and telephone access to crisis lines, with telephone “help” boxes placed at bridge accesses or on bridges.
- Surveillance measures.
- Security patrols on bridges.
- Closed circuit television cameras (CCTV) on bridges.
- Restricting pedestrian access to jumping sites.
- Improved rescue and response efforts.
- Prudent building codes for bridges, applied to new constructions and repairs.
- Muted media reporting.
- Training gatekeepers to pre-emptively identify individuals at-risk of self-harm.

Complementary community approaches to deter individuals from making suicide attempts by jumping include:

- Strengthen and promote mentally healthy and caring university and Ithaca communities.
- Improve student access to mental health services.
- Promote student help-seeking in times of crisis or stress.
- Promote faculty, staff, and student recognition of at-risk students, and student peer support.
- Improve after-hours access to emergency mental health services.
- Educate students, faculty, staff, and the local community about suicide risk, and best practices in suicide prevention, in general, and in preventing suicide by jumping from bridges, in particular.
- Address misperceptions and misinformation about suicide in campus and city communities.

Table 1 lists the range of approaches that have been suggested in efforts to reduce suicides by jumping from specific sites or structures.^{88, 127} Each is described in further detail.

Barriers. Physical safety barriers may exert their effect by averting impulsive attempts, by preventing access to sites which have symbolic significance for suicide (for which other less attractive sites are not substituted), by forcing attempters to substitute less lethal methods or by providing suicidal people with evidence that people care enough to try to prevent suicide.

While barriers may take various forms (railings, glass screens, mesh screens), to be effective they need to be at least 250 cm or higher and built in such a way that they do not offer a foothold for potential jumpers.^{128, 129}

Restricting access to jump sites. Some bridges and other sites have instituted measures to restrict pedestrian access. While this action may be thwarted by people who drive cars onto the bridge or take taxis which they then leave, it may prevent some suicides and may make the task of surveillance easier. Pedestrian access is restricted to the Bosphorus Bridge in Istanbul, for example. However, reports suggest that a significant number of suicidal individuals take taxis onto the bridge and then leave the taxi and jump ¹³⁰.

Signs and telephones offering help. Signs providing contact details of telephone help lines have been installed at some sites favoured for jumping. In some cases telephones are provided to allow suicidal individuals to make direct calls to crisis help lines. There are few evaluations of this intervention. However, Glatt reported that hotlines on the mid-Hudson Bridge in the US which were linked to an emergency psychiatric service were used by 30 of 39 potential jumpers. While one of the 30 callers went on to die by suicide, five of the nine non-callers subsequently jumped and died ¹³¹. Reductions in suicides by vehicle exhaust gas have been reported when similar signage and hotline measures were employed in isolated car parks in the New Forest in the United Kingdom ¹³². Concerns exist, however, that such signs and phones may risk promoting suicide to individuals who might not otherwise think of it ¹³¹.

Training gatekeepers. A further approach is to train local gatekeepers (such as bridge staff, police, traffic officers, security guards, ambulance personnel) who might, in the course of their day to day work, encounter suicidal individuals about to jump. This is a “sentinel approach,” but the likelihood of meeting an individual about to jump is low and the cost-effectiveness of such training, accordingly, is low as well. There appear to be no evaluations of this approach.

Surveillance measures. A range of surveillance measures including closed circuit surveillance cameras at both ends of a bridge, police patrols, dedicated suicide patrol officers or self-appointed or unpaid volunteers, lighting systems providing the equivalent of full daylight, and loudspeakers allowing two way communication in real-time with anyone on the bridge, have been introduced at some popular sites. For example, the Golden Gate Bridge has security cameras and has been patrolled by a team of dedicated suicide prevention officers since 1996 ¹³³. While there are claims that 30 suicides are prevented each year and that suicides have reduced following the introduction of these patrols, 20 people (one each fortnight) each year still elude these patrols and jump to their death. The failure of these methods undoubtedly contributed to the recent decision to build barriers on the Golden Gate. More generally, there have been few formal evaluations of surveillance measures and in many places in which they have been implemented, suicides have not been eliminated. These measures may be ineffective because patrols cannot monitor all parts of a structure (for example, a long bridge) at one time, even with the assistance of security cameras and other measures.

Improved rescue and response efforts. Enhanced response and rescue times for both water rescue and emergency medical response may improve survival rates for those who do jump. Some of those who jump survive the impact but drown before they are rescued. However, there do not appear to be any reports describing the implementation and evaluation of such measures.

Muted media reporting. All media guidelines for reporting about suicide recommend that reporting be muted in general and that method and site information in particular not be

reported.¹³⁴ It is also recommended that media not report on preventive measures implemented at specific sites since such reports may serve to advertise both suicide and the site¹³⁵. In Britain these risks have been explicitly acknowledged by editors, and guidelines against reporting excessive details of method of suicide have been incorporated into the Editors’ Code of Practice¹³⁶.

Cautious, muted reporting has been shown to be effective in reducing suicides. For example, the introduction of muted media reporting of subway suicides in Vienna was followed by significant reductions in such suicides¹²⁰. Two recent studies imply that the introduction of barriers (at the Clifton Suspension Bridge in the UK) and a safety net (at the Bern Muenster Terrace in Switzerland) which reduced suicides from these sites, have had a flow-on effect of reducing suicides by jumping in the surrounding area, presumably because of reduced media reports of suicide from these sites^{89, 137}.

Prudent building codes for buildings and structures. Most of the measures discussed above have been developed and framed as efforts to prevent the relatively small number of high-profile suicides from iconic sites. However, most of these measures (including muted media reporting, barriers, signage, hotlines, surveillance, gatekeeper training) could be applied in various degrees in efforts to prevent suicides from a range of structures and high-rise buildings. For some sites, there is also the option of enhancing building codes to encourage the incorporation of safety features (such as barriers, safety glass in rooftops, enclosed stairwells, restricted access to rooftops and balconies, restricted window apertures) into designs of new buildings, particularly residential housing, but also institutions such as hospitals, prisons and juvenile detention centres, and other structures which might be expected to become attractive sites for jumping (for example, those near high schools, youth centres, universities, psychiatric hospital units). There are a series of informal accounts of the effectiveness of such measures applied to high rise residential units in some Asian cities, but no published accounts of the clear adoption, implementation and evaluation of some of these measures when applied for suicide prevention reasons.

Legal and related issues. There are an increasing number of studies which show that barriers, safety nets and muted media reporting are effective in reducing and preventing suicides by jumping from specific sites, and further, that there is no evidence of transfer to other sites and some evidence of a decrease in suicides by jumping in the surrounding area. These findings suggest that these approaches are now moving towards becoming best practice in suicide prevention. In turn, the development of best practice guidelines for preventing suicide by jumping raises important issues about the accountability and liability of authorities with responsibility for bridges, structures and sites from which people jump. Recent years have seen an increased awareness of patient rights and increasing litigation over failure to protect the public from risk. Growing awareness of the fact that suicide mortality and morbidity may be reduced by appropriate barriers could well, in the future, become further grounds for consumer-led litigation.

Table 1. Measures to Prevent Suicide from Jumping Sites.

PREVENTIVE MEASURE	ARGUMENTS IN FAVOR	ARGUMENTS AGAINST
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Physical safety barriers	<ul style="list-style-type: none"> ▪ Good evidence of effectiveness, well evaluated ▪ May delay or avert suicide attempt ▪ Recommended by people who survived suicides by jumping ▪ Reduces access for impulsive attempters ▪ Shows that someone cares ▪ Appears to reduce suicides by jumping in surrounding area ▪ Reduces media reports of suicide by jumping 	<ul style="list-style-type: none"> ▪ Costly ▪ Aesthetically unappealing ▪ Engineering challenges to add barriers to existing structures ▪ Have to address public opposition
Restricting pedestrian access to sites	<ul style="list-style-type: none"> ▪ Restricts access yet avoids all arguments associated with barriers ▪ Improves capacity for surveillance (no pedestrians anywhere on bridge) ▪ Low cost 	<ul style="list-style-type: none"> ▪ No evidence of effectiveness ▪ Thwarted by taking cars or taxis onto bridge
Signage and telephone access to crisis lines	<ul style="list-style-type: none"> ▪ Some good evidence of effectiveness ▪ Low cost 	<ul style="list-style-type: none"> ▪ May alert others to idea of suicide ▪ Rely on suicidal individual to make the call ▪ Rely on crisis line to respond appropriately
Surveillance measures	<ul style="list-style-type: none"> ▪ Human contact may be important in persuading not to jump, showing care 	<ul style="list-style-type: none"> ▪ Weak evidence of effectiveness ▪ Paid patrols expensive ▪ Rely on patrol intervening efficiently and effectively
Training gatekeepers	<ul style="list-style-type: none"> ▪ Low cost ▪ Increased chance of appropriately alerting emergency services 	<ul style="list-style-type: none"> ▪ Likely low cost- effectiveness ▪ Likelihood of encountering suicidal individual is low ▪ No evidence of effectiveness
Muted media reporting	<ul style="list-style-type: none"> ▪ Good evidence of effectiveness ▪ Low or no cost 	<ul style="list-style-type: none"> ▪ Poor compliance by journalists
Improved rescue and response efforts	<ul style="list-style-type: none"> ▪ Might save some who survive fall but drown 	<ul style="list-style-type: none"> ▪ No evidence of effectiveness
Prudent building codes	<ul style="list-style-type: none"> ▪ Good evidence of effectiveness ▪ Easier to incorporate safety measures when planning buildings, structures 	<ul style="list-style-type: none"> ▪ Gaining regulatory authority, acceptance as industry standards problematic

After Aitken et al. 2005⁸⁸

Summary. Table 1 provides a summary of the preceding section, including the arguments in favor and against each of the range of proposed measures. The single most effective measure involves the installation of physical safety barriers. Evidence of effectiveness for other measures is either lacking or weak.

Evaluation of the Impact of Barriers for Preventing Suicide. A small number of studies have formally evaluated the impact of installing barriers at suicide sites. All studies show barriers are effective in reducing suicides from that site, without displacement to neighboring sites, and sometimes accompanied by a reduction in suicides by jumping in the surrounding region. These studies are described in more detail below:

Clifton Suspension Bridge, Bristol, England. Bennewith and colleagues⁸⁴ examined the effect of installation of barriers on the Clifton suspension bridge, Bristol, England in 1998 on local suicides by jumping. Bridge deaths halved from 8.2 per year (1994 - 1998) to 4.0 per year (1999 - 2003; $P < 0.008$). (Note: Only the main arches were fenced; suicides migrated to the unfenced edges of the bridge). Although 90% of the suicides from the bridge were by males, there was no evidence of an increase in male suicide by jumping from other sites in the Bristol area after erection of barriers. The authors claim this study provides evidence for the effectiveness of barriers on preventing site-specific suicides and suicides by jumping overall in the surrounding area.

Bern Munster Terrace, Bern, Switzerland. Reisch and Michel (2005)⁸⁹ reported that the city of Bern has a high percentage of suicides by jumping (28.6%). The highest number of deaths (mean 2.5 per year) occurred at the Muenster Terrace. In 1998, after a series of suicides, a safety net was built to prevent people leaping from the terrace and to avoid traumatization of people living in the street below. After the installation of the net no suicides occurred from the terrace. The number of people jumping from all high places in Bern was significantly lower compared to the years before, indicating that no immediate shift to other nearby jumping sites took place. Furthermore, they found a moderate correlation between the number of media reports and the number of persons resident outside Bern committing suicide by jumping from high places in the city.

Ellington Bridge, Washington, D.C. O'Carroll et al. (1994)¹³⁸ reported the effect of the construction of barriers on the Ellington Bridge in Washington D.C. – Prior to installation of barriers, an average of four people a year died by jumping from the bridge. In the five years following installation of barriers, there was only one suicide from the Ellington Bridge. The number of suicides from nearby Taft Bridge, only one block away, where no barriers had been installed, remained the same.

Grafton Bridge, Auckland, New Zealand. The experience in New Zealand served as something of a “natural experiment,” with a so-called “A-B-A” design – a determined effort to remove barriers against best advice followed by the clear demonstration of the potent effects of barriers and a powerful demonstration of the impact of their absence. Beautrais examined suicide patterns before and after removal of protective barriers from Grafton Bridge in Auckland, New Zealand.⁹⁰ There were three suicides during the four years immediately before the barriers were removed (1992 - 1995); there were 15 suicides in the ensuing four years following their

removal (1996 – 2002). Beautrais and colleagues ¹³⁹ published a further paper in 2009, after barriers had been *reinstalled* on Grafton Bridge, noting that with the new barriers in place there had been no suicides from the bridge.

Memorial Bridge, Augusta, Maine. Pelletier ¹⁴⁰ reported that during the 22 years after barriers were installed at the Memorial Bridge in Augusta, Maine, in 1983, there were no suicides. Prior to the barrier installation there had been a total of 14 suicides. The conclusion from this CDC study was that the safety fence installed in 1983 was effective in preventing further suicides from the Memorial Bridge. The number of suicides related to jumping from other structures in Augusta remained unchanged following installation of the fence, suggesting that suicidal individuals did not seek alternative sites.

Table 2. Summary of formal studies evaluating impact of bridge barriers

SITE	INTERVENTION and OUTCOME	REFERENCE
Ellington Street Bridge, Washington, DC	Barriers reduced number of suicides from 25 in the previous 7 years to 1 in the 5 years after the installation of barriers.	O'Carroll and Silverman, 1994 ^{138,140}
Clifton Suspension Bridge, Bristol, UK	Barriers halved the number of suicides from 8 to 4 per year.	Bennewith et al, 2007 ⁸⁴
Bern Muenster Terrace, Switzerland	Safety net reduced suicides from 2.5 per year to 0.	Reisch & Michel, 2005 ⁸⁹
Memorial bridge, Augusta, Maine	14 suicides prior to installation of barriers; after barriers in place no suicides in 22 years.	Pelletier, 2007 ¹⁴⁰
Grafton Bridge New Zealand	3 suicides in the 4 years prior to the barriers being removed. After removal, 15 suicides in 4 years, since reinstallation of the barriers, there have been no suicides.	Beautrais et al, 2009 ¹³⁹

Informal Reports regarding the Impact of Bridge Barriers on Preventing Suicide. Apparently many suicide researchers and communities have come to view as a foregone conclusion that installing barriers on bridge or promontories will reduce suicides. Therefore, they either do not conduct formal evaluations of the impact of such installation or do not seek to publish the results. For example, it appears that the recent installation of barriers at the Jacques Cartier Bridge in Montreal has not resulted in a publication about the subsequent reduction in suicides from the bridge, although this finding is informally well-known (personal communication). **Table 3** summarizes some of the many anecdotal, informal reports of the impact of installing bridge barriers at sites of suicide by jumping.

Taken together the formal evaluations and the informal reports suggest that barriers reduce suicides by jumping at the site at which they were installed and, perhaps, in the surrounding area. However, the low base rate of suicide, and particularly of suicide by jumping, makes small changes in *total* suicide rates (by all methods) difficult to detect. For this reason it is usually not

possible to determine if the installation of barriers (or other safety measures) at a particular site reduces the *overall* rate of suicide.

Physical safety barriers may exert their effect by averting impulsive attempts, by ‘buying’ time for reconsideration or rescue, by preventing access to sites which have symbolic significance for suicide (for which other less attractive sites are not substituted), by forcing attempters to substitute less lethal methods or by providing suicidal people with evidence that people care enough to try to prevent suicide.

While barriers may take various forms (railings, glass screens, mesh screens), to be effective they need to include the typical features of effective public safety barriers installed in a number of bridges around the world:

- Height in excess of 2.5 meters;
- Gaps between members of less than 150 mm, but ideally less than this;
- No foot or hand holds which might assist in climbing;
- Curved at the top of the barrier towards the pedestrian side;
- Predominantly smooth vertical members;
- Provide the impression of a daunting visible deterrent.

Table 3. Reductions in suicides following installation of barriers

Site	Outcome	Reference
Sydney Harbor Bridge	Barriers reduced the incidence of suicides to 1% of the original level	Harvey and Solomons (1983) ⁸⁷
Empire State Building	Fenced the 86 th floor observation platform after 16 suicides between 1931 and 1947; number of suicides reduced since. The nearby Chrysler Building and Rockefeller Centre had no increases in suicides as possible alternative sites.	Seiden and Spence (1982) ⁹²
Adelaide multistory car park - prominent jumping site	Safety grilles reduced incidence of jumping suicides to 0; no other car parks became alternative sites.	Pounder, 1985 ¹⁴¹ Goldney, 1986 ¹⁴²
Gateway Bridge, Brisbane	Barriers reduced number of suicides. No increases in jumping suicides from nearby Storey Bridge (possible alternative site).	Cantor and Hill, 1990 ¹⁴³
Mt Muhara, Japan	Barriers reduced number of suicides.	Ellis and Allen, 1961 ¹⁴⁴
Eiffel Tower, Paris	Barriers reduced the number of suicides.	Derobert et al, 1965 ¹⁴⁵
Arroya Seco Bridge, Pasadena, California	Barriers reduced the number of suicides.	McWilliams, 1936 ¹⁴⁶

Arguments against installing barriers to prevent suicide. Despite the preponderance of formal and informal evidence that bridge barriers reduce suicide by jumping, people invariably raise a series of public objections. *These objections show a remarkable similarity across sites and societies.* Common themes include:

- Suicide is inevitable in suicidal individuals and barriers at one site will not prevent their deaths.
- If barriers are installed at one site, suicidal individuals will substitute another site or another method.
- Barriers will decrease the aesthetic appeal of a site.
- The cost of barriers is poor value in order to save a small number of lives.
- The engineering challenges posed by adding barriers to existing structures are substantial and expensive to overcome.
- Money would be better spent on some other aspect of mental health care or suicide prevention than on barriers to prevent suicides in a few individuals in whom suicide was inevitable.
- The preservation of the historic and aesthetic values of sites is more important than attempting to save the lives of unhappy individuals who are going to kill themselves anyway.

None of these objections withstands critical scrutiny. *First*, as shown above, there is now strong evidence that installing safety barriers does reduce risks of suicide at known jumping sites.

While the *second argument*, that suicidal individuals substitute another method or site if barriers are installed at the favoured site, is commonly expressed, it flies in the face of a large body of objective evidence that informs understanding about the relationships between the accessibility of a specific method or site of suicide and suicidal behavior:

- As a general rule, restricting access to a specific method or site will result in reduced rates of mortality and morbidity by that method.
- However, if the method or site that is restricted is substituted by another method (“means substitution”) or site, reductions in method-specific or site-specific rates of suicide may not translate to reductions in overall rates of morbidity and mortality. In populations, such means substitution occurs over a period of time – years – rather than days or months. This underscores that individuals often tend not to find a second method at the immediate time when they are obstructed from using the first. Thousands of lives may be saved in the interim between the initiation of means restriction and the full emergence of means substitution.¹⁴⁷
- Method restriction *at a particular site* may still be justifiable even as substitution may evolve over the course of time. When assessing the safety standards of any structure, it is important to ascertain the level of risk to public safety that a structure poses and to impose appropriate safeguards if the risk is substantial. If it becomes apparent that some specific feature of the social or physical environment facilitates or encourages suicidal behavior, we would argue the ethical imperative of removing access to that feature even when it is not possible to guarantee that substitution will not occur.
- Because of the complex relationships between access to methods (and sites) and suicidal behaviors, it is important that policies aimed at means restriction are thoroughly monitored

and evaluated, *especially if restricting the method imposes unwanted burdens on the majority of the population that is not “at-risk” for suicide.* ^{148,149}

Third, strenuous efforts have been made to construct esthetically pleasing designs in places in which suicide safety barriers have been installed; these can preserve much of the majestic view or natural surrounding beauty, and preserve or enhance the original design of the structure. At Grafton Bridge in Auckland, for example, clear, curved glass barriers on the pedestrian thoroughfare across the bridge preserve views, shelter pedestrians from the weather and prevent suicides. This design later was adapted for a bridge in Norway.

The vast majority of people who survive suicide attempts by jumping, or who are removed from bridges before they could jump, do not subsequently die by suicide although they tend to have higher rates of mental ill-health and associated suicide than the general population. ¹⁵⁰ Seiden studied, with a median follow-up period of 26 years, a series of 515 individuals who had been restrained from jumping from the Golden Gate Bridge in San Francisco. ¹⁵¹ Among those who died subsequently, violent death (by suicide, homicide, or accident) composed a greater proportion (5%) – compared to the *general population*. However, the vast majority (95%) did not die by suicide or any other violent means.

In a further study of people who were prevented from jumping, Rosen ¹⁵² interviewed people who had survived jumps from the Golden Gate and Bay Bridges in San Francisco. Four of the six interviewed said they would not have used any other method if the Golden Gate Bridge had not been available. All six favored the construction of a suicide barrier. They also suggested that newspapers cease coverage of suicides.

Summary. The population of people who jump from bridges are individuals who typically have severe mental illnesses (including psychotic disorders), which contribute to their vulnerability to suicide in a persisting fashion, or severe life stresses that recently or suddenly have greatly added to their vulnerable status. They may live near sites that have acquired reputations for suicide. Alternatively, they may be attracted to these sites and travel considerable distances to implement well-developed plans for death. The specific “iconic status” of sites and their attractions for jumping arise for various reasons, including past media reporting, a desire for publicity, the reputation of the site as a place for suicide, and the very beauty or grandeur that makes these settings attractive to all who are there. Often, these suicides appear sudden – “impulsive” – and it is certain that many have not been “well planned.” In the context of severe life stresses, they frequently are made by ambivalent people with uncertain feelings about their future; once stopped, their plans to die by suicide subside and they do not seek to kill themselves ever again.

The imposition of barriers and other measures to impede access to jump-sites may reduce a fraction of suicides. The clear majority of those who are restrained from jumping do not go on to make further attempts using other methods or sites. However, the low base rate of suicide, and especially of suicide by jumping, makes small changes difficult to detect. For this reason it can be difficult to detect a decrease in overall suicide rates following the installation of barriers or other safety measures. Nevertheless, there is clear evidence that barriers decrease site-specific suicide rates, and no clear evidence that substitution of other sites in the surrounding

region occurs. In fact, the available evidence suggests that suicide rates by jumping tend to decrease in the surrounding area.

Prevention efforts have now been strengthened by five recent studies which provide clear evidence for the effectiveness of safety barriers and a safety net at jumping sites.^{84, 89, 138} Beautrais, in program, 139, 140 Taken together with anecdotal accounts of reductions in suicides after the installation of barriers, this evidence now provides the basis for best practice to prevent suicides by jumping at popular sites. Best practice suggests that barriers should be added to sites which become popular for suicide by jumping, and should be a consideration in designing new structures.

Suggestions that barriers should be installed to prevent suicides have often met substantial public opposition on the grounds of cost, aesthetics, substitution of method and the inevitability of suicide. The increasing volume of evidence suggesting barriers prevent suicide implies that these challenges will be more difficult to mount and defend in the future.

SUICIDE IS PREVENTABLE

During the middle years of this past decade Gannett Health Services adapted to the Cornell Campus many of the features of the US Air Force program to prevent suicide. The work of the US Air Force since 1996 has shown that an organization can pull its resources together to prevent suicide, with a sustained demonstration of reduced rates.^{93, 153} Faced with a daunting increase in rates during the early-1990s, the Vice Chief of Staff of the US Air Force ordered his Surgeon General and all other component members of the service's leadership to work together to develop a sweeping and comprehensive program. Rather than view it as medically based, they developed a *community-oriented approach*, one that ultimately created an initiative involving 11 core elements. Included were attention to individual and family needs; workplace performance; education for command and non-commissioned officers, for all personnel and for members of the broader community; attention to mental health and inordinate alcohol use; reformulation of confidentiality policies; continuing surveillance; and perhaps most central to any programmatic effort, defined accountability.

In its version, Cornell developed a broadly based community health effort, a mental health advisory committee, and a combination of anonymous, student run, and campus-supported clinical services to greatly enhance access to care or support for those in need. It designed and implemented an array of educational programs for faculty, staff, and RAs, and consistently sought to destigmatize mental health concerns or service use, while also continuing to implement programs to reduce binge drinking and alcohol use across the entire University community. Of note, suicide on the Cornell Campus fell to zero during the three consecutive years following full implementation of the augmented programs, giving hope that they had in fact addressed many of the core issues leading to lethal suicidal behaviors.

Evaluation of the USAF Suicide Prevention Program made it evident that the whole was greater than the sum of its parts.^{93, 153} At the heart of the program was an unequivocal change in culture that espoused and implemented programs that offered help while seeking to remove the stigma of accepting help ("strong men can ask for help"). It was clear that the cohesive nature of the service, long-standing values affirming "the Air Force family," and a sustained commitment

that transcended the rotation of top leadership, all contributed to the capability of effectively implementing such a radical undertaking. The program led to a sustained decline in suicides, and just as important, in violent deaths and violence behaviors. Of note, the rates ‘spiked’ upward in 2004 at a time when implementation was lagging; leadership reapplied the program and enhanced monitoring, and since then rates have again fallen to prior levels.¹⁵³ It was evident that, what had been deployed as a suicide prevention program was, in fact, a program that broadly promoted social health and violence prevention.

In comparing the Cornell program with that of the Air Force, it is clear that there are important differences. The latter is a tightly organized, hierarchical community with a potent top-down command structure. One cannot say the same for universities. The USAF has many complementary measures of job performance and personal functioning, and seeks to maintain readiness on a war footing. Thus it can command data and access information sources not available to a university. Given the nature of a military organization, it did not seek to specifically control its most prevalent means of suicide – firearms – but imposed other safety measures in its ability to restrict personnel at risk.

Past community discussions in Ithaca over the course of decades have rejected any suggestion of bridge barriers. Thus, Cornell has until recently experienced what in hindsight can be seen as “a hole” in what could be viewed as a suicide prevention safety net. In light of the unique qualities and history of Ithaca and the University, no one would have chosen barriers preemptively were it not for the events of this year. Moreover, the three-year success of new initiatives gave rise reasonably to a sense that the University has been on the right track. However, mental health promotion and its linked suicide prevention efforts reflect a multilayered approach, where no one initiative or effort will ‘catch’ all potential deaths. While Cornell has developed and implemented what many would describe as “best practices” for university campuses, these ultimately were not enough in this particular community, one dotted with iconic sites for jumping. This must be placed for future consideration within the large body of evidence that suggests that restricting access to a range of methods of suicide may prevent suicides and save lives, and not immediately lead to method substitution.^{154 37 155}

UNIQUE CIRCUMSTANCES: WHAT MAKES THIS SITUATION DIFFERENT?

Ithaca is the ‘iconic’ site: A unique physical environment. Ithaca’s gorges and parks are famous, and in particular, it is the bridges across the gorges that make Ithaca an iconic suicide site. Most suicide hotspots are single sites. No single bridge in Ithaca has emerged as *the* favored site for suicide. Over the course of years, suicides have occurred from multiple bridges that cross the two main gorges that bound much of the Campus, as well as settings such as Taughannock Falls. (While mindful that it is located in Ulysses, most people associate it with Ithaca). No doubt, Cornell has become identified as an iconic suicide campus by implication – most of the suicides are students because they constitute a large proportion of the population and live near the bridges – but we also noted that *people have come from out of town in order to die in Ithaca*. The latter will not be ‘susceptible’ to the community and mental health promotion efforts that Cornell initiated during the middle of the last decade.

Thus, restricting access to community-recognized, accessible jumping sites has a substantial probability of reducing deaths by this means.¹⁵⁶ This does not guarantee that no one will die

using these sites, assuming very high levels of determination, nor does it inherently protect against other methods. Yet the literature is replete with studies that show both a short-term lack of substitution and clearly evidenced reductions in rates when means controls are widely applied across communities, or a nation.

We are extremely sensitive to the fact that many students and faculty chose Cornell because of its surroundings and in light of Ithaca's beauty. Viewing natural scenes, being in natural settings, and walking amongst trees together constitute a restorative environment, which has psychological, cognitive, and physical benefits. All decisions about making bridges safer must respect these important abiding concerns and values. If barriers are installed, they need to be minimally intrusive on the bridges and in keeping with the landscape. One cannot understate the importance of this issue, for Ithaca, for Cornell, and for generations yet to come.

Campus-community relationships and ownership. As noted, the ownership of the bridges is mixed; Cornell owns more, Ithaca owns those most frequently chosen. In essence, Cornell and Ithaca are handcuffed one-to-the-other, if the intent is to create a safe environment, where barriers serve to enhance protection for those who are missed by other approaches to suicide prevention.

Applicability of research to this set of problems. This is a *unique situation*, in geographic terms, without clear-cut precedent. Thus, one must glean from available research: Data on means restriction, knowledge regarding iconic sites, recent (some unpublished) findings on clusters, and an understanding of contagion as a social phenomenon. In such situations, decisions must make sense to the majority of involved parties, build on collective wisdom, and seek to create collective action. In light of the challenges noted earlier, tied to the great difficulty to recognize or intervene with the specific person who is about to die, suicide prevention depends on layered approaches that each 'capture' a small number of people, while enhancing the health of many. Barriers in Ithaca truly are the 'backstops' or community safety nets. If it is not possible to create a collective response, it is highly likely that people will continue to fall to their deaths.

Temporary barriers – putting them up and removing them. Immediately placing barriers on bridges owned by the City of Ithaca and by Cornell University was one component, and certainly the most visible, of a collective effort to urgently respond to suicides occurring this year in the proximity of the Cornell campus. *It was an essential demonstration of the University's commitment to safety above all else*, and it engendered a wave of negative comments and controversy on campus, in the City, among alumni, and in multiple media outlets. Moreover, these temporary barriers are an "eyesore." As we will discuss further, it is our opinion that removing them will, in effect, invite further suicides and expose both the University and the City of Ithaca to speculation about motives and relative values, and questions that they will be hard pressed to answer. Should Ithaca become the site of the next "A-B-A" experiment for the suicidology literature to document?

Does the summer break (allowing the barrier removal to occur when most students are gone) change the risk of barrier removal? Removing barriers during the summer break has been suggested as one solution. However, the bridges and barriers will remain a newsworthy topic, and the risk of suicides will persist. Community and student interest may reignite when students come back in the fall, negating any gain hoped for by removing barriers in the

summer. “Gorging out,” however much we may dislike that term, is not far from the lips of many on campus and in town, and among alumni, faculty, and staff as well as students. Even if undergraduate students are largely absent during the summer, graduate students and the entire Ithaca population, and those from afar, are still at risk. As noted in the report, clusters can continue or reignite later; summer breaks do not staunch memories.

Would lower temporary barriers deter deaths or carry the same risk as no barriers? This question confronts one with the problem of ‘proving the negative.’ While we cannot say with precision about the specific risk-reducing properties of lower barriers, which are somewhat harder to scale but not near the protective standards of permanent emplacements, we see a significant pitfall. Substituting temporary lower barriers risks criticism for not putting up higher ones (per recommendations); that is, for encouraging suicides by putting up lower, less formidable barriers that might prove a challenge to be overcome as a dare. Choosing barriers lower than a recommended height in order to preserve the view at the expense of a student life would be difficult to defend.

SUMMARY, RECOMMENDATIONS, AND CONCLUSION

Given the data available in the literature – about jumping, contagion and clusters, and youth suicide – together with the rapid unfolding of a major media event *locally and nationally*, the urgent decision and implementation of a program of protection initiatives on the Cornell campus and for the Ithaca community was an essential and prudent effort to staunch the likelihood of any further suicides from the local bridges. Immediately placing barriers on the bridges was one component and certainly the most visible. *It was an essential demonstration of the University’s commitment to safety above all else*, and it was entirely in keeping with what has been shown to work in other settings. It is important to underscore that this was not the only aspect of the University’s response, which has included bringing together diverse elements of the Campus and Ithaca communities, engaging in frank discussion, offering crisis support as well as augmented educational and counseling services, and working collaboratively with outside consultants to rapidly and deliberatively define future potential courses of action.

The current temporary barriers are an “eyesore.” *And, as in all human communities, there will be suicides in Ithaca and on the Cornell campus in the future. The issue for all to consider is this: How much do you want these to be associated with your bridges and your gorges?*

Our summary of the scientific literature underscores that the available scientific data regarding suicide deaths and attempts related to jumping from bridges strongly suggests that *most individuals who jump from iconic sites are ambivalent, act impulsively, choose a specific site, and if thwarted from an attempt at that site at a particular time, will survive.* The decision to attempt suicide may be a transient response to a particular set of emotional circumstances that resolve with time. If access to a lethal means of suicide is denied during this time, the individual may make a suicide attempt with a less lethal method or make no attempt at all. These observations are consistent with evidence that many of those who make suicide attempts are impulsive and suggest that measures to prevent suicides by jumping may be worthwhile by delaying or averting some fraction of impulsive suicide attempts. They are also consistent with a large body of evidence that suggests that restricting access to a range of methods of suicide may prevent suicides, and not immediately lead to method substitution.

Inevitably, given both the history of suicide in Ithaca and the recent publicity (notoriety) that surely increased Ithaca's reputation as a suicide site, there will be more suicides and some will come from farther away to end their lives. When the next suicide occurs, it will be deemed even more newsworthy than in the past. If the barriers are removed, it will generate especially adverse news coverage. There will be speculation regarding why barriers were taken down, when 'experts' now point to 'best evidence' suggesting they should be installed permanently. There will be media-led speculation (and assignment of responsibility) about who estimated the risk and assessed the value of the life (lives) lost versus the cost or esthetics of barriers. In turn, this type of news coverage may render the sites even more risky.

As we discussed when visiting the campus and speaking with many individuals, we recognize the deep need to preserve the beauty that is so much a part of living in Ithaca and attending Cornell University. No one would ever choose to obstruct the views of the gorges or waterfalls, or impede access to the natural surroundings that truly are special for residents, students, and visitors. We also recognized that this problem of dying by throwing oneself into the gorges is long-standing, and it is apparently contagious. It certainly has become a lightning rod in the community and in the print and Internet media.

We see no alternative but to promote safety and caring for vulnerable persons as the central driving elements of this discussion. It is our recommendation that temporary barriers that meet standards of effectiveness remain in place, until permanent safety measures can be built. There are many approaches to such measures, and the expertise regarding what will work best for the different bridges – in a fashion that is respectful of the glorious beauty of the settings – is beyond our skills.

It is one thing for us to make a recommendation; it is another for Ithaca and Cornell to create an effective community discussion that can forge a common approach to saving lives. Truly, everyone is 'in this together.' The longer term success of any comprehensive prevention agenda – of which barriers are but one part – depends on building coalitions for collective actions.

Acknowledging the glare from widespread coverage in the national media and the unfortunate notoriety of this year's deaths, permanent barriers must be in place to address heightened suicide risk and perceptions that will not be undone, or at a minimum, will not change for many years. The contagion risk that arose this year will not soon abate!

A central lesson from the US Air Force related to the power of leaders to set dramatic change into motion, and to use their institutional authority to develop both a culture and infrastructure needed to save lives and to create a healthier community, and ultimately, to sustain the needed self-scrutiny to foster continuous improvement. It has been difficult nationally to transport this lesson to other settings, and for communities to come together to build the array of interwoven efforts needed to prevent suicide. Cornell and Ithaca together, by necessity, are now confronted with such an opportunity. It is our recommendation that the involved leaders use their positions to create the collective movement needed. We recognize the potential costs – in dollars, social capital, and political futures. At the same time we see this as a potentially galvanizing cause, one that builds towards a national model of health and community-academic collaboration.

Consultation to Cornell University – Extended Report
Beautrais, Gould, & Caine
June 2010

APPENDIX

Suicides, Probable Suicides, and Non-fatal Suicidal Jumps
from Bridges and Gorge Edges* on East Hill in Ithaca, NY

1990 – 2010

Affiliation	21 - Year Total	
College Students		15
Cornell University	14	
Ithaca College	1	
Community		14
Ithaca residents (1 IHS student)	10	
Non-Ithaca residents	4	
TOTAL		29

* Only one jump was from a gorge edge distant from a bridge

Compiled by Gannett Health Services at Cornell University, this record incorporates information available as of June 29, 2010 from the following sources:

Suicide Prevention and Crisis Service; Ithaca Fire Department

Ithaca Journal, Cornell Daily Sun

Cornell University Police, Counsel's Office, Public Affairs, and Office of Community Relations

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01/23/2011 12:17 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Cold Springs Canyon Bridge Suicide Barriers

Mr. Fowler,

I would appreciate you including this article about the future of suicides. Specifically, page 26 and 27 about means restrictions!

http://www.sprc.org/library/ChartingTheFuture_Fullbook.pdf

It also cites the Harvard School of Public Health and the program on Means and Restrictions of suicide

<http://www.hsph.harvard.edu/means-matter/>

Thank you again for the department's effort to stop unnecessary loss of life!

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**Charting the Future of Suicide Prevention:
A 2010 Progress Review of the National Strategy
and Recommendations for the Decade Ahead**

Prepared by

the Suicide Prevention Resource Center (SPRC)

and

Suicide Prevention Action Network USA (SPAN USA),

a division of American Foundation for Suicide Prevention

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Department of Homeland Security (DHS)
Department of Justice (DOJ)
Department of Education (ED)
Department of Veterans Affairs (VA)
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The response to a non-random, web-based survey conducted by SPAN USA was robust and these responses provided a wealth of insight from the perspective of survivors of suicide loss and suicide attempts; national, tribal, state, and community leaders; mental health providers, teachers, trainers, and researchers. Over 300 individuals took the time to respond to this independent survey.

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I. INTRODUCTION

During the second half of the twentieth century, suicide prevention leaders launched pioneering efforts in disparate areas of research, training, and survivor support. These early days of suicide prevention were marked by a lack of formal organization and little funding. The focus was on creating a compelling vision that would bring interested people on board and attract support. By the late 1990s these varied strands began to come together to form the beginnings of a *suicide prevention movement*. The movement was bolstered by new developments in research and interventions, changes in public attitudes about both suicide and the larger field of mental health, and the advocacy of survivors of suicide loss.

In 2001, the Surgeon General of the United States issued the National Strategy for Suicide Prevention (NSSP).¹ It laid out a series of goals and objectives designed to reduce the incidence of suicide and suicidal behaviors in the United States. Inherent in the NSSP was an acknowledgement that suicide is a serious public health problem that is preventable. The NSSP established a common reference point that allowed the disparate elements of the movement to see their own priorities, while influencing each of them to work within the overall framework of the NSSP. It has also served as a guide identifying shared goals that provided a credible rationale for gaining public and financial support. The process of creating the National Strategy provided leaders of elements of the nascent movement with an opportunity to work together, to learn that there was strength in collaboration, and to reach a consensus on shared goals.

Since publication of the NSSP, activity in the field of suicide prevention has increased exponentially. Government agencies at all levels, schools, not-for-profit organizations, and businesses have initiated programs and public awareness campaigns to address suicide risks. But despite these increased suicide prevention efforts, figures show that in the U.S., the overall rate of suicide in the past decade remains essentially unchanged. In some demographic groups the rates are decreasing significantly; however, in others, the trend is upward. One can only conclude that, to date, the intensity and approach of the prevention campaign has not been a match for the complexities and depth of the suicide problem.

Each year since 2001, over 30,000 people have died by suicide in the United States – twice the number who die as a result of complications related to HIV/AIDS. Suicides outnumber homicides by three to two, and every year some 650,000 persons receive treatment in emergency rooms following suicide attempts.²

This document reviews developments in the field of suicide prevention in the nine years since the NSSP was published. As a snapshot taken at the end of this decade, it identifies the areas of most important progress as well as the critical areas that have gone relatively unaddressed. It also explores new issues or initiatives that have emerged to claim attention or offer solutions since the development of the NSSP in 2001. Its purpose is to inform discussions about future initiatives to achieve the ultimate public policy goal behind the NSSP: *to reduce the morbidity and mortality of suicidal behaviors*.

“The National Strategy has become a rallying cry for the movement; it gave us a foundation to build on and credibility advocating in the Statehouse.”

–SPAN USA Survey Respondent

A. BACKGROUND

Until quite recently, with the exception of some support for suicide-related mental health research, suicide received meager attention in national policy. In 1996, survivors of suicide loss who saw the need to mobilize attention and the political will to prevent suicide formed the Suicide Prevention Advocacy Network USA (SPAN USA)³ and launched a campaign to encourage the development of a national suicide prevention strategy for the United States. SPAN USA's efforts resulted in two Congressional Resolutions⁴ recognizing suicide as a national problem and suicide prevention as a national priority, providing further impetus to develop a national suicide prevention strategy.

Based on recommendations from the United Nations and World Health Organization, the advocates also sought the creation of a public/private partnership to promote suicide prevention. They succeeded in gaining the support of Dr. David Satcher, then Surgeon General and Assistant Secretary for Health, U.S. Department of Health and Human Services (DHHS), to jointly sponsor a national conference that convened in Reno, Nevada, in October 1998. The "Reno Conference" was chaired by Surgeon General Satcher and attended by a broad array of participants including many federal agencies; representatives from every state and the District of Columbia; researchers; health, mental health and substance abuse clinicians; policymakers; suicide survivors; consumers of mental health services; and community activists and leaders. Based on the conference recommendations, Dr. Satcher issued a "Call to Action to Prevent Suicide" in July 1999,⁵ declaring suicide a serious public health problem requiring attention and action. This initial effort to create an agenda for the nation framed suicide prevention through three constructs: Awareness, Intervention, and Methodology (AIM), and enumerated fifteen broad recommendations consistent with a public health approach to suicide prevention. The Reno Conference can be seen as the founding event for the modern suicide prevention movement and the "Call to Action" became its original charter.

At about the same time, two major DHHS reports brought new focus and attention to mental health and suicide. Dr. Satcher's office issued "Mental Health: A Report of the Surgeon General," the first publication of its kind, late in 1999. And in 2000, DHHS's sweeping agenda for the nation's public health, "Healthy People 2010,"⁶ established targets for reducing the number of deaths by suicide within ten years. Advocates for suicide prevention seized the opportunity presented by these Federal efforts to shine a spotlight on suicide and related mental health concerns and to continue the push for a national suicide prevention strategy.

In early 2000, the Secretary of Health and Human Services established a Federal Steering Group (FSG),⁷ to "...ensure resources identified...for the purpose of completing the National Strategy [for Suicide Prevention] are coordinated to speed its progress."⁸ The FSG reviewed the recommendations of both the Reno Conference and the "Call to Action" and set an agenda to develop a comprehensive plan that would foster the development of suicide prevention activities at the local and State levels. This plan was the National Strategy for Suicide Prevention (NSSP).

The NSSP proposed a sweeping agenda of eleven goals and 68 objectives designed to offer a comprehensive and integrated approach to reducing the loss and suffering from suicide and suicidal behaviors.⁹ The NSSP was to serve as a wide-ranging "catalyst for social change, with the power to transform attitudes, policies, and services."¹⁰ It proposed an ambitious framework for tackling suicide, which included promoting public awareness campaigns and research on suicide alongside measures designed to address suicidal behavior on multiple levels including those associated with

the individual, peers, family, community and society. For the broader suicide prevention community, the NSSP immediately provided a common point of reference and a resource for advocacy at the state and local levels, while directing more attention on the needs of those bereaved by suicide.

In 2002, shortly following the release of the NSSP, the Institute of Medicine published "Reducing Suicide: a National Imperative,"¹¹ which delved into the state of the science of suicide prevention, knowledge gaps, prevention strategies, and research designs. The following year, the President's New Freedom Commission on Mental Health issued its report, "Achieving the Promise: Transforming Mental Health Care in America,"¹² highlighting in its first recommendation the need for a "national level public-private partnership to advance the goals and objectives of the NSSP. . ."

One of the central objectives of the NSSP was to establish a public/private partnership to oversee its implementation. The partnership was to be funded with government and private monies, with a mission to ensure suicide prevention is a national priority, and to serve as the catalyst for implementation of key goals and objectives of the National Strategy for Suicide Prevention (NSSP).¹³ In the fall of 2008, SAMHSA initiated a project to lay the groundwork for such an initiative by funding this report.

"Having a National Strategy is important. However, it appears that people have run with the easy parts, but have not implemented the harder parts of the strategy."

- Key Informant

B. METHODS

This report provides an overarching look at the field of suicide prevention in the United States to identify in broad terms what has occurred since the release of the NSSP, as well as to highlight the most critical areas in which more needs to be done.

No formal evaluation of the impact of the NSSP was planned or conducted when the NSSP was released. Since the NSSP has served as a common reference point for those involved in the field, its organization was used in this report to identify and organize issues, actions, and problems. The review was designed to draw upon a wide variety of information culled from public sources and knowledgeable informants to paint a picture of developments and areas of need to be addressed in the field of suicide prevention.

By offering the advocates, clinicians, researchers, public health workers, decision makers and survivors engaged in suicide prevention a picture of what has been accomplished and what remains to be done, it is hoped this review will serve as a guide for measuring future progress, as well as a point of departure for further deliberations about how best to pursue the national goals embodied in the NSSP.¹⁴

To develop a methodology for the review, Project Team members:

- Convened a Planning Group comprised of individuals having important content knowledge of suicide prevention and the status of suicide prevention activities across the country;
- Asked Planning Group members to identify priority areas for information collection and provide input on topics to include in the review;

- Reviewed previous efforts that examined the progress of the NSSP;
- Identified the activities proposed by the NSSP that had transpired since 2001; and
- Identified activities that had occurred in the field that were not identified or proposed in the NSSP.

Based on initial discussions with the Planning Group, the Project Team decided to concentrate its efforts primarily in the following domains that were deemed central to understanding the developments and accomplishments since the NSSP was issued:

- Public education and awareness
- Public policy
- State planning and implementation
- Training
- Clinical care
- Research
- Surveillance
- Translation of science to practice

Additionally, the Planning Group identified cultural competence, coordination of care, and the central role of grassroots initiatives at State, Territorial, and local levels as cross-cutting themes to consider.¹⁵

Information gathering methods included:

- **Compilation of Existing Information.** The search for evidence of the implementation of the NSSP encompassed a wide variety of websites and various published and unpublished reports describing organizations, programs and projects. Additionally, SPRC staff conducted library research on several topics, including survey data on public perceptions, public information campaigns, accreditation standards for mental health clinical training programs, and the contents of State suicide prevention plans. Additionally, SPAN USA and SPRC staff prepared summary descriptions of activities that met specific NSSP objectives.
- **Interviews with Key Informants.** Approximately 30 leaders with a broad range of expertise and involvement in suicide prevention at local, state and national levels were identified and participated in interviews.
- **Web-based Survey.** The project also benefitted from a web-based survey independently conducted by AFSP/SPAN USA in the summer of 2009 to gather the impressions of people across the broad suicide prevention community regarding progress made and important priorities for the future. Over 300 people participated, including survivors of suicide loss and suicide attempts; national, State, Territorial, and community leaders; Federal government officials, public and mental health providers; suicide prevention coalition members and leaders; teachers, trainers, and researchers.

This report is a synthesis of the information gathered through all three approaches.

The recommendations in this report were generated to add definition and clarity that is not reflected in the NSSP. They are founded on current perspectives and understanding that was not possible nearly a decade ago. This report does not repeat recommendations included in the NSSP.

II. REVIEW FINDINGS

A. OVERVIEW

There is much good news to celebrate in this report. In some instances, the initiatives called for in the NSSP have come to complete fruition, and often with considerable effect. It seems obvious that the NSSP has provided the roadmap for myriad activities, and consequently, a number of very important objectives have been achieved. Additionally, there have been significant achievements in the field that had not been anticipated at the time of the NSSP's launch. Unfortunately, in some areas evidence of attention or success is lacking. Overall, the amount of activity that contributes to suicide prevention nationwide is exponentially greater than in 2001 and continuing to grow. Unfortunately, the field lacks a means to assess the results or to guide future action. And on the single most important indicator, the rate of death by suicide, the results are mixed.

A number of factors that go beyond the boundaries of suicide prevention have contributed to the developments reviewed here. Chief among them are the public's engagement in a larger debate encompassing healthcare and mental health issues generally, and the willingness of opinion leaders or celebrities to speak out about their own battles with mental illness and suicidal thoughts and behaviors.

Some of the most noteworthy achievements that will be discussed include:

- Federal and State legislation that has advanced suicide prevention planning and programming, detection of suicide risk, and access to care.
- Establishment of the Suicide Prevention Resource Center, offering training, technical assistance, tools, and informational resources to the field.
- Establishment of a Best Practices Registry.
- Establishment of the Suicide Prevention Lifeline, a national network of suicide prevention crisis centers.
- Development of the initial elements of the National Violent Death Reporting System.
- Training for members of the public in detection of suicide risk and appropriate response.

Areas with movement, but requiring more still more investments:

- Research into the causes of suicide and effective interventions.
- Development, evaluation, replication, and implementation of more evidence-based programs, practices, and treatments.
- Development of effective public awareness and social marketing campaigns, including targeted messages for specific segments of the population that can change attitudes, norms and behaviors.
- Training of clinicians and other service providers to detect and respond to suicide risk.
- Access to behavioral healthcare and improved coordination across the continuum of care.
- Training for behavioral health providers in evidence-based or evidence-informed practices.
- Collaboration among agencies within States and Territories, and among Federal agencies, and improved communication between the states, tribes, territorial and federal entities.
- Public education campaigns to reduce access to lethal means.
- Culturally competent interventions for tribes and other minority and culturally diverse populations.
- Suicide prevention in rural areas.
- Suicide prevention among middle-aged and older adults.
- Integration of suicide prevention practice into substance abuse programming.
- Timely dissemination of national injury surveillance data.
- Development and enhancement of monitoring systems for nonfatal suicidal behavior at the state, territorial, and local level.
- Coordinated national leadership to guide the direction and progress of the field.

The findings in this report are organized according to the AIM outline used in the Surgeon General's Call to Action and the NSSP itself: *A-Awareness, I-Intervention, M-Methodology*. An additional section highlighting the unique needs of special populations constitutes a fourth section. Recommendations that expand on or go beyond the objectives of the NSSP are inserted throughout the Findings section and are listed in Appendix A.

B. AWARENESS

The NSSP's Awareness goals and objectives focused on increasing awareness of suicide as a serious, but preventable public health problem, developing broad public support for action, reducing the stigma associated with mental disorders and substance abuse, and seeking services for these problems and for suicide risk. Recommended action included public information campaigns, convening of forums, use of the internet to disseminate information, recruiting new groups and institutions to suicide prevention, and establishing a "national coordinating body" to implement the NSSP.

"We need to get people other than the choir to sing. We need journalist associations, employee assistance programs, chambers of commerce, defense lawyers, pediatricians, family practitioners, and others to get involved. They see people every day that may be at risk and we need them to partake in the conversation."

- Key Informant

Perhaps the most notable observed change in the intervening years since the NSSP launch in 2001 is the public's heightened awareness about suicide in general. According to polling conducted by a

national health research firm, 78% of Americans believe that many suicides are preventable with appropriate research, interventions, and services; moreover, 86% of the population believes that it is important to invest in suicide prevention.¹⁶ Unfortunately this is the only survey that polled these questions; follow-up surveys are needed.

Growth in the number of state, territorial, and community coalitions, many of which have broadly disseminated suicide prevention training for the general population, has contributed to this increasing awareness. New Federal legislation, particularly aimed at preventing suicide by young adults and veterans, both reflects this increasing public awareness and public support and further advances it. Federal investments have also led to the development and widespread marketing of a national suicide prevention hotline. Not all of these achievements were anticipated or called for in the NSSP; however, they occurred within the context of awareness building activities encouraged by the NSSP.

Two unexpected societal developments also helped push suicide and suicide prevention into the limelight: record high numbers of suicides among members of the Armed Forces and anecdotal evidence that the recent economic crisis that forced millions of workers out of jobs, homes, and health care, contributed to an increase in suicidal behaviors. These tragic social problems have had the ancillary effect of drawing public awareness – and government response – to the issue of suicide prevention.

The following section of the review highlights several public awareness and public information campaigns and legislative initiatives that have helped raise awareness about suicide prevention since 2001.

1. Public Awareness and Public Information Campaigns

The suicide prevention movement has invested extensively and with great enthusiasm to increase awareness and understanding of the problem of suicide. Although the relationship to these activities cannot be determined, this review found a general agreement that public attitudes towards mental illness in general and suicide in particular have become less stereotyped and more compassionate. People seem to be more willing to discuss mental health problems and suicide openly, and to see the need for treatment (for themselves and others). This review also found a growing body of information about how to craft and disseminate effective communications campaigns, as well as brief documents providing recommendations for safe and effective messaging for suicide prevention.¹⁷

Public awareness campaigns have been undertaken by a host of public and private sector actors. Recently, three Departments of the Federal government have invested significantly in campaigns: Defense, Health and Human Services, and Veterans Affairs. These involve broadcast media PSAs, web presence, advertising on public transportation, and various leaflets and handouts. Additionally, many state and territorial suicide prevention efforts, including some funded with Federal grants, have launched state-wide public information campaigns, although no conclusions can be drawn about the specific impact these have had and there has been little or no research to evaluate their effectiveness.

Statewide public information campaigns were specifically recommended in the NSSP. A resource scan by the Suicide Prevention Resource Center (SPRC) in the spring of 2009 found that at least seventeen state agencies had developed public information campaigns with some level of statewide distribution:

Alabama	Louisiana	Pennsylvania
Alaska	Mississippi	Rhode Island
Arizona	Montana	Tennessee
Colorado	New Hampshire	Texas
Hawaii	New York	Vermont
Kentucky	Oregon	

Additional states described diverse media efforts that were not statewide or where planning had been delayed. In general, information was unavailable on the proportion of the population reached by these campaigns, and even the campaigns with some statewide implementation varied widely in their scope, intensity, and focus. Thus, it is unclear how many campaigns meet the NSSP's goal to increase the number of states in which public information campaigns exist.

The scan also revealed that examining statewide campaigns does not capture the full scope of public education work in suicide prevention since 2001. One private organization that has taken a lead in public awareness is Suicide Awareness Voices of Education (SAVE). SAVE has developed numerous campaigns, some of which were designed specifically to increase public knowledge of suicide prevention ("Prevent Suicide--Treat Depression," "Stop Depression from Taking another Life," "Treat Depression As If Your Life Depends on It," "You Can Too!" among others). SAVE reports that its campaigns alone have appeared in nearly every state in the country and have rendered over one billion exposures, however the effects of these mass media efforts have not been evaluated.

Another example is the success the field has had in raising broader awareness and public support through the establishment of walks for suicide prevention. The Out of the Darkness Walks organized by the American Foundation for Suicide Prevention (AFSP) were launched with a 26-mile walk in 2002 in Washington, DC and have since expanded to nearly 200 locations in 47 states. According to AFSP, more than 100,000 people have walked for suicide prevention to date, generating over 120 million media impressions via print, radio, and television stories that focus on suicide causes, warning signs, and the need for prevention. The walks have also served to promote local suicide prevention resources and the National Suicide Prevention Lifeline. Again, evaluation is lacking, so that the larger effects on attitudes and beliefs of Americans about suicide and suicide prevention are not known.

In addition to these examples, scores of groups have produced suicide prevention posters, brochures, advertisements, or other communications materials in recent years. Few of these products, however, have been evaluated

"A much better job needs to be done in the public health and suicide prevention community in terms of tailoring messages for specific audience segments. The suicide prevention community needs to look at how they develop products, services, and outreach with an eye to the way corporations have developed their own plans, products, etc., using the best communications science."

- Key Informant

in any systematic way. It is also not clear how many, if any, of the public information efforts were developed using principles for developing effective communications content: following a systematic planning process, conducting audience research, pretesting materials, using the communications efforts to support other related prevention approaches, and evaluating effectiveness.

A sampling of public information materials found many messages that are generally consistent with suicide prevention goals, for example: messages that promote help-seeking behavior, promote available resources, and emphasize that mental illness is treatable. Other aspects of the messages, however, raise potential concerns. For example, many general awareness materials provide statistics or statements about the extent of suicide that may leave the impression that suicide is relatively common—it is not. This practice may serve to normalize suicide, which runs counter to the safe messaging recommended by most suicide prevention experts. Presumably this information is designed to demonstrate the gravity of the problem and the need for action. Although messages of this tenor are well suited for policymakers who can direct resources toward suicide prevention, they may be harmful if given to the general population. Furthermore, many messages calling for “action” fail to specify what actions should be taken and how to take them.

Another concern is that many materials do not appear to be tailored to a defined target audience. No single slogan or message works for everyone, yet many campaigns seem to have a very general audience in mind. It is plausible that developing campaigns with the goal of reaching a large proportion of the population, such as a typical “statewide” campaign, results in messages that are less tailored, which according to the communications literature, are typically less likely to result in change. Experts in health communications suggested that the NSSP’s objective to reach large portions of the population with public information campaigns has had the unintended consequence of prompting states and suicide prevention organizations to produce a few, very generalized campaigns rather than many campaigns tailored for distinct segments of the population.

Lastly, it appears that many of the current public awareness efforts “stand alone” without the benefit of being an integral part of a more comprehensive plan. These efforts are more effective when integrated with a comprehensive, multi-level suicide prevention program.

As a final note on the subject, the general consensus of informants for this report is that perceptions of stigma related to suicidal thoughts and behaviors still hinder our national dialog on the topic, as well as the overall progress at the community level.

Recommendation 1: Develop and implement plans to increase the proportion of public awareness and education campaigns that reflect both the fundamental principles of health communication and the safe messaging recommendations specific to suicide.

Recommendation 2: Promote the importance of using public awareness and education campaigns as an adjunct to other interventions rather than as stand-alone initiatives. Whenever possible, health communications campaigns should have much more specific goals than simply “raising awareness.”

Recommendation 3: Promote the development of public awareness and information campaigns that are tailored for and targeted toward specific audiences and that describe the actions those audiences can and should take to prevent suicidal behaviors.

2. Federal Policy and Program Initiatives

This review found widespread recognition of the notable legislative and policy successes of the past decade to promote suicide prevention awareness and advance practice in the field. Two significant Federal legislative accomplishments since 2001 were passage of the Garrett Lee Smith Memorial Act and the Joshua Omvig Veterans Suicide Prevention Act.

a. Garrett Lee Smith Memorial Act

The Garrett Lee Smith Memorial Act (GLSMA) is generally regarded as the single most significant legislative accomplishment in the field of suicide prevention during the past decade. The Act was named for the son of Senator Gordon Smith (R-OR), a college student who died by suicide in late 2003. It combined two legislative initiatives that were moving through Congress simultaneously: one for early identification and intervention of youth at risk for suicide and another focused on college suicide prevention programs. With strong support and advocacy by the suicide prevention and mental health communities, the bill passed unanimously in the Senate and with a strong majority in the House, and was signed into law by President George W. Bush in October 2004.

The GLSMA created the first significant Federal grant program directed specifically at suicide prevention. Since its enactment, GLSMA has provided grants to States, Tribes, Territories, and institutions of higher education to develop and implement youth and college suicide prevention programs. As of 2009, these grants have been made to 44 States, one Territory, 20 Tribes or Tribal consortiums, and 87 colleges and universities. In addition, the GLSMA authorized ongoing funding for a Suicide Prevention Resource Center (SPRC). As of March 2009, \$142.3 million had been appropriated for GLSMA grant programs, of which \$20.73 million have been allocated to SPRC.¹⁸ (The SPRC is described in more detail below.)

“Nationally, there has been a great increase in the amount of information available via the web through various suicide prevention organizations. This has aided groups on the State and local level to better assess their needs and access resources. Groups are better linked with each other and the subject is being discussed more openly on a local level.”

- SPAN USA Survey Respondent

b. Military Suicide Prevention

Reports of high rates of suicide within the ranks of military members and veterans have elicited concern on the part of the U.S. Congress and strong responses from policymakers within the Departments of Defense and Veterans Affairs. Over the past decade each military service has launched suicide prevention policies and programs numbering in the hundreds in an effort to promote overall mental wellness and to combat suicide. More programs, unfortunately, do not necessarily result in desired outcomes, and to date there has been relatively little effort to scientifically measure the effectiveness of these programs, though this situation is changing rapidly, as will be discussed later in the report.

c. Joshua Omgig Veterans Suicide Prevention Act

The Joshua Omgig Veterans Suicide Prevention Act (named for a veteran of Operation Iraqi Freedom who died by suicide in 2005) was introduced in both the House and Senate in the 110th Congress. In the fall of 2007, the Senate and House passed the Omgig bill and it was signed into law by President Bush shortly afterwards. The Joshua Omgig Veterans Suicide Prevention Act (P.L. 110-110) directs the Secretary, Department of Veterans Affairs (VA), to develop and implement a comprehensive program designed to reduce the incidence of suicide among veterans. The law requires that the program include staff education, mental health assessments as part of overall health assessments, a suicide prevention coordinator at each Department medical facility, research on best practices for suicide prevention among veterans, research on mental healthcare for veterans who have experienced sexual trauma, 24-hour availability of mental healthcare to veterans, a toll-free hotline for veterans staffed 24/7 by appropriately trained mental health personnel, and outreach to and education for veterans and the families of veterans. In the past few years the Department of Veterans Affairs has become one of the most vibrant forces in the U.S. suicide prevention movement, implementing multiple levels of innovative and state of the art interventions, backed up by a robust evaluation and research capacity. More information on the VA's efforts appears later in this report.

d. Government Performance and Results Act (GPRA)

The Government Performance and Results Act of 1993 seeks to shift the focus of government decision making and accountability away from a preoccupation with the activities that are undertaken—such as grants dispensed or inspections made—to a focus on the results. In 1998, the Health Resources and Services Administration became an early adopter by establishing youth suicide as a performance measure for grants under its Maternal and Child Health Bureau. The Department of Justice has also invested significantly in improved surveillance of suicides among incarcerated populations and among clients of the juvenile justice system.

Indian Health Service (IHS) has developed a suicide surveillance reporting tool to document incidents of suicide in a standardized and systematic fashion. The suicide surveillance tool captures data related to a specific incident of suicide, such as date and location of act, method, contributing factors, and other useful epidemiological information. Local and national reports can be sorted by a number of different variables including the number of suicide events by sex, age, community, tribe, and others. The FY 2010 GPRA target is to collect data on 1,700 events.

Although these practices within the Federal government appear to be raising visibility of suicidal behaviors as an important performance measure, there would seem to be many more opportunities to reflect suicide outcomes as salient performance measures for government grant programs in behavioral healthcare and medical care delivery, and social service programs for children, families, and aging, in particular.

Recommendation 4: Implement suicide related GPRA performance measures in government grant programs serving populations at increased risk for suicide, such as aging services; mental health, substance abuse, and healthcare; labor; education; and Tribal programs.

3. Communication and Collaboration

The NSSP emphasized the need for greater communication and collaboration in the field of suicide prevention and significant advancements have been made. Although the number of conferences and meetings devoted to suicide prevention has increased over the past decade, significant demand exists for more opportunities to share ideas in face-to-face settings. Suggestions include conferences and symposia at both the national and regional levels that draw on knowledge and expertise within the suicide prevention community and provide a venue to share best practices and research findings.

a. Institutional Players

Two groups, the Federal Working Group (FWG) on Suicide Prevention and the National Council for Suicide Prevention (NCSP), were cited by stakeholders as examples of collaborative efforts among institutional players and national advocacy organizations. The Federal Working Group on Suicide Prevention meets to discuss Federal agency initiatives, promote collaboration among Federal agencies working on preventing suicide, and hear reports on current and potential programs. The FWG also requests and compiles input from its members on the efforts undertaken at the various Federal agencies that support any of the NSSP goals and objectives. A “Compendium of Federal Activities” is updated twice yearly.¹⁹

The National Council for Suicide Prevention (NCSP), a coalition comprised of the nine national not-for-profit organizations each solely dedicated to suicide prevention, meets quarterly by telephone and once a year in person to discuss and work to advance the major initiatives in the field of suicide prevention. Discussions include roles and activities of Federal and state programs, research, advocacy, prevention, and intervention and postvention programs. The NCSP meets with the Federal Working Group twice per year, once in person and once by telephone, annually with the Director of NIMH, collaborates with other national organizations on various projects (such as the NCSP Suicide/Inhalant Abuse project with the Alliance for Consumer Education), develops position statements and acts as a collective voice for suicide survivors.

“In the past, there were regional conferences where states were allowed to bring ten members of their state team. A lot of progress was made because of that. It’s time to revisit a national conference tied to suicide prevention.”

• Key Informant

“There is more communication, but we have failed to take advantage of the networks that have been created. Communication has been established, but not nearly enough. We need to figure out how to use the potential that lies there.”

• Key Informant

b. Federal Government, States, Tribes and Territories

The review found a growing recognition that communication across the field has been greatly enhanced by the Garrett Lee Smith Memorial Act grant program and through the targeted efforts of SPRC and many others, and that these initiatives have given rise to the growth of Statewide and local coalitions, peer-to-peer learning, and web-enhanced communications and collaboration among communities of practice.

“The breadth of the NSSP was its weakness. We need to prioritize.”

- Key Informant

It is also apparent, however, that many individuals working in the field remain unaware of the Federal efforts or feel that insufficient Federal effort trickles down to the community level. Furthermore, they think that there are too many duplicative efforts, e.g., state after state and community after community creating the same or similar public awareness and training materials. Also, states do not appear to be communicating their own successes or best practices with the Federal government or with other states. There is certainly a perceived need for yet more communication and coordination between the Federal government and the state entities responsible for suicide prevention. There also appears to be an unsatisfied desire for more communication among Tribes, where suicide rates are particularly high and more sharing of experiences could be helpful.

4. Public-Private Partnership

Before the ink was dry on the NSSP, Federal partners began the work of designing the organization that would fulfill a crucial piece of the strategy—a public-private partnership to catalyze and coordinate the implementation of some elements of the strategy. Planning efforts continued through the early years of the new century and included an elaborate effort to harness ideas generated by suicide prevention leaders across the country in an effort to prioritize the nation’s agenda. The latest planning activity along these lines is this report and the work of the National Action Alliance Planning Group that guided it. In the first quarter of FY 2010, the seed funding to establish a National Action Alliance for Suicide Prevention (Action Alliance) was identified both from SAMHSA and from within the Suicide Prevention Resource Center grant. After extensive consultation with leaders in both the public and private sectors of the suicide prevention community, an Executive Secretary was hired and the foundation for the Action Alliance was laid. First-year plans call for naming two co-chairs (one from each of the public and private sectors), recruiting 30-40 Executive Committee members, establishing an action agenda for the first years’ activities, and developing a broad funding base to sustain the work of the body. Not only will this entity provide coordination and set priorities for many of the nation’s suicide prevention efforts, it will help advance a desperately needed recruiting effort to build collaborative relationships with other professional and service groups that can play key preventive roles for suicidal individuals.

C. INTERVENTION

The NSSP Intervention goals and objectives focused on statewide comprehensive suicide prevention plans, promotion of evidence-based clinical practices and prevention programs, training and technical assistance for community-based prevention programs, and training for recognition and

management of suicide risk. Professionals in the field of suicide prevention agree that early and effective intervention to prevent suicides requires that a number of factors be in place: better training and resources for gatekeepers—individuals in the community who identify people at risk for suicide and intervene, first responders, clinicians and public health officials; effective approaches to recognizing depression and other mental illnesses that contribute to suicide; adequate access to and coordination of care; reducing and restricting access to the lethal means by which individuals die by suicide; and high-functioning State and local suicide prevention coalitions that foster communication across the various components of public and private health and social service organizations. This section of the review examines the most prominent initiatives that have occurred in these areas.

1. State and Community Suicide Prevention

The NSSP called for comprehensive State suicide prevention plans that a) coordinate across government agencies, b) involve the private sector and c) support plan development, implementation and evaluation in communities. The NSSP noted that in 2001, while a number of States had suicide prevention plans, few were comprehensive and the plans did not uniformly link public health, mental health, and substance abuse programs. Moreover, not all addressed the entire life course and few involved all key stakeholders, such as education, justice, social services, and the private sector.²⁰

Today there are 48 States with plans – clearly a sign of progress. As would be expected, the plans still vary in their depth and comprehensiveness, but the state of the field is far advanced from 2001. As mentioned above, it is evident from highlights of interviews with the leadership of several States that these plans and programs would benefit from increased coordination or more uniform practices.

The primary foci of State efforts appear to have been in increasing awareness, training of gatekeepers and building volunteer networks. As noted elsewhere, much of the work has been targeted towards youth suicide prevention. However, many States have developed or revised plans to address suicide across the life course and have intentionally designed multi-level approaches for the range of youth and elder serving organizations as well as the health and mental healthcare delivery systems. Interest in military and veteran suicide has further expanded State leaders' scope of addressing suicide prevention.

State suicide prevention leaders have varying levels of suicide prevention experience, with some involved in the field for over 15 years and others for as little as one. Many are engaged in the larger fields of violence prevention, mental health, or public health. A few have staff dedicated to suicide prevention; for others, suicide prevention is one of many areas of responsibility. In many cases, their efforts appear to have centered on developing Statewide plans and emphasize the importance of local participation. However, only a few States have intentionally addressed the third element of the NSSP's call to States: supporting the work of planning, implementation, and evaluation at the community level. Where community-level activities are evident, local groups have generally functioned on shoestring budgets and the recent economic downturn has further challenged their financial stability.

This review found that Michigan, Ohio, and Tennessee had each innovatively focused support on the community-level coalitions. In most cases, the State provided technical assistance (TA) to communities, including series of training and TA conferences, in order to help them identify existing or build new public health-oriented collaboratives to advance suicide prevention. The community collaboratives included a cross-section of social and health service providers and they recognized faith-based organizations as being especially important in reducing stigma and providing venues for community training. Each of these States was able to track, by county, the progress in planning and implementation and used this information to plan future activities.

Another community-level pioneering effort of note is the Connect Suicide Prevention Program developed by NAMI NH (The National Alliance on Mental Illness New Hampshire). Utilizing a public health approach that includes planning, implementation and evaluation, Connect addresses key objectives of the NSSP by working across systems and bringing together individuals, peers, families, service providers and State agencies to address community risk and protective factors and work together on suicide prevention efforts. Through the use of “best practice” protocols and training, Connect reduces gaps between service providers, improves access to care and promotes an integrated community response to suicide prevention, intervention and postvention.

“As a mental health practitioner and suicide survivor, I have become more aware of the growing number of resources. I’m not sure, however, that most people are aware of the resources that exist.”

- SPAN USA Survey Respondent

Recommendation 5: Promote more active and systematic state support of suicide prevention planning, implementation, and evaluation at the community level; systematically share successes across States.

2. Suicide Prevention Resource Center

One of the most significant advances since the launch of the NSSP was the establishment of the Suicide Prevention Resource Center (SPRC). Created by funding through SAMHSA in October 2002, SPRC now receives funding through the Garrett Lee Smith Memorial Act and represents an important step towards fulfilling the NSSP goal to develop “one or more training and technical resource centers to build capacity for States and communities to implement and evaluate suicide prevention programs.” Its work has become a central focus for efforts to address many of the NSSP’s Intervention goals and objectives.

SPRC provides a wide variety of services, including: technical assistance, training programs, field support, library and clearinghouse, and support for GLSMA grantees -- all of which are intended to

improve the development, implementation, and rigorous evaluation of effective suicide prevention programs and practices.

Information, products, and services are disseminated through SPRC's website, www.sprc.org; its online library, <http://library.sprc.org/>; its online and face-to-face training programs; webinars in various formats; and consultation and support directly from its expert staff. Consultation can occur by telephone, email, or face-to-face at meetings and conferences. The SPRC website is accessed by over 20,000 unique visitors monthly. SPRC also produces a weekly electronic newsletter for online subscribers, "The Weekly Spark." The Weekly Spark contains announcements and information about suicide, suicide prevention and mental health issues, offering brief summaries of national, state and international news; analyses of relevant research findings; descriptions of funding opportunities, and links to additional resources. SPRC has produced well over 50 reports or other printed resources²¹ to support suicide prevention activities across the medical and public health sectors.

3. Access to and Coordination of Effective Care

a. Evidence-based Therapies

Since the release of the NSSP, research has produced the first evidence that certain psychotherapies, such as cognitive behavioral therapy (CBT) and dialectical behavioral (DBT) are effective in preventing repeat suicide attempts. One study provided ten weeks of CBT for attempt survivors, who were identified after presenting themselves in an inner city emergency room. After eighteen months, the results showed a fifty percent reduction in repeat attempts.²² Both of these therapies are included in the National Registry of Evidence-based Programs and Practices and the SPRC/AFSP Best Practices Registry. Unfortunately, there is a shortage of clinicians trained to provide these evidence-based psychotherapies. Some experts believe that until clinical training programs for the major mental health disciplines include training in these evidence-based therapies, the gap between research and clinical practice will remain.²³

b. Coordination and Continuity of Care

It is generally agreed that the healthcare system fails to adequately address continuity and coordination of care. This is a particular problem for suicidal individuals. One of the most common, and perhaps detrimental, examples is suicidal patients who are treated in emergency departments. In this setting, patients generally don't receive adequate treatment to address underlying mental illnesses or substance use problems; nor do they leave connected with the kind of follow-up outpatient care that could expedite their recovery.

The importance of providing follow up services promptly after emergency department discharge is highlighted by findings from the South Carolina National Violent Death Reporting System that 10% of all the suicides in South Carolina had been seen in an emergency department within the previous 60 days.²⁴ The importance of prompt follow-

"We are putting screening in the hands of primary care people, but treatment rates are down. The treatment for depression has actually decreased in the last five years."

- Key Informant

up after inpatient discharge was also highlighted by a major study by the Veterans Administration which showed the period after inpatient discharge to be the time of greatest risk for suicide for depressed veterans.²⁵ The potential benefits of intervening at these times is also highlighted by the fact that the only two randomized controlled trials that have shown reductions in death by suicide both involved follow-up after an acute suicidal crisis²⁶. Additional research focusing on the potential for suicide prevention during and after emergency department care is now also being supported by both NIMH (ED SAFE), and the Veterans Administration (SAFE Vets).

According to a June 2009 draft report produced by the AAS on behalf of SPRC, "suicide is a public health problem for which continuity of care is one essential means for effective prevention." The report suggests that continuity of care forms "a solid, patient-centered framework around which to organize health care systems."²⁷

Recommendation 6: Expand efforts to provide effective follow up care after emergency department discharge of suicidal persons.

Recommendation 7: Expand efforts to provide effective follow up care after inpatient discharge of suicidal persons.

There is also consensus among stakeholders that primary care settings must become more engaged in suicide prevention. Too often, primary care clinicians do not have the training to identify and respond to suicidal patients. Additionally, they may not have the ability to follow up with a referral to a mental healthcare professional, simply because there are not enough mental healthcare professionals to adequately treat the population at large. In 2009, SPRC, with its partner the Western Interstate Commission for Higher Education, released a toolkit for use in rural primary care practices to help incorporate evidence-based and evidence-informed suicide prevention approaches into day-to-day practice.²⁸ In the same year, AAS released a 1.5 hour training module tailored specifically to primary care clinicians. These resources support what may become the preferred approach to care: co-management of patients by primary care and mental health professionals using a chronic disease management model in the context of the patient-centered medical home. This emerging model will need to undergo rigorous evaluation and continuous quality improvements.

Recommendation 8: Promote evidence-based and evidence-informed practices for reducing suicide risk among primary care patients.

Although examples of comprehensive, community-wide efforts to provide coordinated care may be limited, noteworthy progress is being made in the Departments of Veterans Affairs (VA) and Defense (DoD). Within the VA, as of October 2009, there are over three hundred suicide prevention coordinators servicing all VA Medical Centers. These Suicide Prevention Coordinators interact regularly with the VA Suicide Prevention Hotline and help provide monitoring, follow-up, and enhanced services to veterans identified as at high risk. The Blue Ribbon Work Group that examined the VA's suicide prevention programs found that the Veterans Health Administration (VHA) had developed a comprehensive strategy to address suicides and suicidal behavior that included a number of initiatives and innovations that hold great promise for preventing suicide attempts and completions. Evaluation of the impact of these efforts will be of critical importance not only to

promote continuous improvement in VHA's suicide prevention efforts, but also to inform suicide prevention efforts across the Nation and reach veterans who do not utilize VHA services.

In the Department of Defense, intensive efforts are being made to utilize a community-wide approach to suicide prevention and to coordinate suicide prevention efforts during transitions between installations and between deployment and garrison, as well as when leaving the services and transitioning to VA care. Efforts are being made to coordinate treatment between VA and the DoD through data sharing. This data sharing will assist service members and veterans, so that the records of diagnoses and treatment can follow service personnel to the VA after a tour of duty or following discharge from military service. Given the complexities of these government systems, it bears watching to see what kind of progress is made and whether it can be replicated in the private sector health system.

"There's no doubt at all that as we look at health reform, mental health coverage is a critical part of making Americans well and healthy, and early identification, ongoing treatment, access to psychotropic drugs ...are critical components."

- The Honorable Kathleen Sebelius, Secretary, Health and Human Services, April 2009

Recommendation 9: Evaluate and assess practices being implemented in the VA for dissemination to the broader healthcare delivery system.

Recommendation 10: Evaluate and assess practices being implemented in the Department of Defense for potential dissemination for community-based suicide prevention efforts.

Recommendation 11: Promote collaboration between public and private partners to engage military families and veterans families in suicide prevention efforts.

c. Mental Health Parity and Addiction Equity Act (2008)

Culminating a decade long campaign by the mental health community, the Paul Wellstone and Pete Domenici Mental Health Parity and Addiction Equity Act of 2008 was enacted into law on October 3, 2008. This Federal legislation aims to equalize benefits for mental health and substance use disorders with medical and surgical benefits in group health insurance plans covering more than fifty employees. The law became effective on January 1, 2010. Under this new law, 113 million people across the country have the right to non-discriminatory mental health coverage, including 82 million individuals enrolled in self-funded plans (regulated under the Employee Retirement Income Security Act [ERISA]), who are not covered by state parity laws.²⁹

While the NSSP specifically called for parity laws at the State (rather than the Federal) level, the enactment of this Federal legislation marks a major advancement in the field of suicide prevention. Experts agree that roughly ninety percent of adolescents and adults who die by suicide suffered from depression or another diagnosable mental or substance use disorder, or both, at the time of their death.³⁰

In July 2008, Congress also applied mental health parity to Medicare, to be phased in beginning in 2010. The law provides Medicare mental health equity by reducing the currently required fifty percent mental health co-insurance to twenty percent, making it on par with coverage for all other outpatient services.³¹

Once the Federal parity law was effective in January 2010, many patients who were effectively locked out of the system because they could not afford treatment may have found coverage more affordable. But that solution can only be applied to consumers with appropriate healthcare insurance; far too many persons at risk are left out of the system altogether and a familiar pattern develops: emergency treatment in a hospital setting without follow-up outpatient care, until, all too often, the next suicide attempt occurs. A point raised by many informants to this review was that progress in suicide prevention will only be achieved through fundamentally better access to and coordination of an unbroken chain of care among healthcare providers.

d. State Level Insurance Parity

As mentioned earlier, the NSSP specifically called for an increase in the number of States that require health insurance plans to cover mental health and substance abuse services on par with physical health services. In 2001, 34 States had a mental health parity law.³² Since 2001, an additional nine States have established mental health parity laws,³³ and seven States have increased the scope of their laws.³⁴

4. Substance Abuse and Suicide

The NSSP focused attention on the connection between substance abuse and suicide prevention in several areas, including reduction of stigma, increasing access to and community linkages with services, and improvement of reporting and portrayals in the entertainment and news media.³⁵ Compared to the emphasis placed on mental health problems, the suicide prevention movement has focused relatively little attention on the intersection of substance abuse and suicide since 2001.

The Substance Abuse and Mental Health Services Administration (SAMHSA) has taken two specific steps to achieve the NSSP goals that include substance abuse. In 2009, SAMHSA released, *“Addressing Suicidal Thoughts and Behaviors in Substance Abuse Treatment.”*³⁶ This Treatment Improvement Protocol (TIP) is designed to serve as a set of best-practices guidelines based on empirical evidence and expert consensus for addressing suicidal thoughts and behaviors in the treatment of substance use disorders. At the end of 2008, SAMHSA released a white paper on the topic of suicide prevention and substance abuse.³⁷ The paper, *“Substance Abuse and Suicide Prevention: Evidence and Implications: A White Paper”* summarizes what is known about the interrelationship and provides an overview of advances that have been made in the last decade.

Outside of SAMHSA, a major advancement in terms of access to substance use services was achieved as part of the previously discussed Federal mental health parity law and State laws on mental health parity. The Federal law (as well as several State laws) mandates that substance use services along with mental health services must be provided in parity with physical health services.

Recommendation 12: Increase efforts to integrate suicide prevention practices into substance abuse prevention and treatment services.

5. Training Initiatives

The NSSP recognized that effective and evidence-based training that cuts across all strata of professions and services is fundamental to suicide prevention. This requires that standards be adopted so that a variety of individuals in and outside of the healthcare professions receive adequate training in recognizing the signs of suicide risk. Gatekeepers, defined as persons who interact with people in environments of work, play, or natural community settings, are likely to be the first persons to recognize signs of at-risk behavior or warning signs. Healthcare professionals (apart from trained mental health specialists) also regularly come in contact with persons at risk during the normal course of their practice and need adequate training. And mental health professionals need better and more focused training on recognizing, assessing and managing suicide risk in their patients, including the use of evidence-based therapies.

a. Clinical Training

QPR Institute (QPR stands for Question, Persuade, Refer) led the field in developing the first suicide triage training program for practicing clinicians, called the QPRT Suicide Risk Assessment and Risk Management Training Program. The program was first introduced in 1998 and is designed to reduce morbidity and mortality among healthcare consumers by standardizing the detection, assessment, documentation and management of patients at elevated risk for suicidal behaviors in all settings and across the age span. The QPRT Suicide Risk Assessment and Risk Management Inventory (adult, pediatric and hospital versions) are guided clinical interviews developed through expert consensus and are anchored in the existing scientific literature on suicide risk assessment. The curriculum consists of eight modules. Eight hours of classroom time, or ten hours in the online version, are required to complete the program. An expanded three-credit college classroom version is available through the School of Social Work at Eastern Washington University, Cheney, Washington. Since 2001, about 7,000 individuals have been trained in this program, and over 100 faculty certified to teach the curriculum.

One of SPRC's earliest training initiatives engaged the American Association of Suicidology and its members for the purpose of articulating core competencies for mental health professionals in detecting, assessing and managing suicide risk. Their work resulted in the first comprehensive, competency-based curriculum for practicing mental health professionals that could be widely disseminated. By 2005, SPRC had established a distinct unit known as the SPRC Training Institute to disseminate the resulting one-day course, "Assessing and Managing Suicide Risk" (AMSR). According to its developers, the workshop is designed for psychiatrists, psychologists, licensed counselors, social workers, with tailored versions for college and university counseling center staff and employee assistance professionals. A faculty of 77 mental health professionals,

"There is little research on the effectiveness of training. Without this kind of research, you can train people, but you can't guarantee that it will really [contribute] to suicide prevention."

• Key Informant

located throughout the United States, has been certified to deliver the AMSR course. Over 250 workshops have been delivered to more than 11,000 mental health professionals in 40 states and five countries outside the U.S. An independent multi-site evaluation showed that training participants achieved significant gains in knowledge and self-assessments of both confidence and competence, all sustained through the six-month follow-up assessment. The program is a collaboration of AAS and SPRC.

Subsequently, AAS developed the “Recognizing and Responding to Suicide Risk: Essential Skills for Clinicians” (RRSR) program, designed to train mental health professionals to better recognize patients at risk for suicide and develop assessment-based treatment plans to manage that risk.

According to AAS, the two-day curriculum, delivered by a faculty of master trainers, is based in the knowledge content developed by the AAS for the AMSR program (see above) and includes additional emphasis on skill development, particularly on the formulation of risk and its associated treatment planning. RRSR training has been provided to over 2,000 clinical mental health professionals in twenty-four States and one Canadian province. Eighty percent of those professionals trained to date reported having made specific changes in their clinical practice as a consequence of the RRSR training.

“I think there are still deficiencies in the standards for risk assessment and management. I find many of my professional colleagues think they have enough information until something happens.”

- SPAN USA Survey Respondent

Recommendation 13: Evaluate the capacity of continuing education clinician training programs to produce behavioral outcomes that improve clinical practice and outcomes. On the basis of evaluation, make curriculum improvements if needed; promote mass dissemination of continuing education to practicing behavioral health providers.

b. Gatekeeper Training

Thousands of first responders, correctional workers, crisis line volunteers, law enforcement professionals, clergy, teachers, school counselors, nurses, and other community and professional helpers in a position to have first contact with persons at risk for suicide are the “gatekeepers,” those individuals on the frontlines of suicide prevention. Curricula for gatekeepers range from one hour education to raise awareness to two-day trainings to teach and build intervention skills. While more evidence is needed to determine the effectiveness of these curricula, there is an assumption in the suicide prevention community that training gatekeepers needs to be an integral part of comprehensive suicide prevention programs. Several of the gatekeeper training programs listed in the Best Practices Registry (the SPRC/AFSP listing of reviewed suicide prevention programs) are described below. The gatekeeper programs developed by QPR Institute and Living Works Education were the first developed and are by far the most widely utilized.

The QPR Institute offers two levels of gatekeeper training. The first, QPR Suicide Triage (8 hours), is a derivative program of QPRT for any and all persons who, during the course of their work, may encounter individuals in crisis. The QPR Suicide Triage Training program teaches participants how to probe for and initially assess immediate risk for suicidal behaviors, and how to immediately enhance protective factors. QPR Suicide Triage training includes the same two foundation lectures presented to mental health professionals in the clinical training curriculum discussed above, as well as

exercises and skill building role plays. The second, the QPR Gatekeeper Training for Suicide Prevention program (1-2 hours), is designed to teach people in all walks of life, lay and professional, how to recognize, assist and refer potentially at-risk individuals to further assessment and care. QPR Gatekeeper Training programs have been delivered to over 750,000 individuals since 2001.

LivingWorks Education (LWE) developed Applied Suicide Intervention Skills Training (ASIST) in 1983 to facilitate early prevention and intervention for at-risk individuals with thoughts of suicide. The two-day ASIST program offers both a standardized and a customized gatekeeper workshop designed for members of care-giving groups. Since 2001, more than 180,000 individuals have participated in ASIST workshops. To complement ASIST, a shorter (3 hour) curriculum, safeTALK, was designed in 2004-2005 by LWE to teach participants to recognize and engage persons who might be contemplating suicide and connect them with the appropriate community resources. Over 60,000 have participated in this training since 2006.

The Yellow Ribbon International Suicide Prevention Program, founded in 1994, has developed two gatekeeper training programs. "Be a Link!" is a two-hour adult gatekeeper training program. The program can be implemented in a variety of settings including schools, workplaces, and community organizations. The training provides participants with knowledge to help them identify youth at risk for suicide and refer them to appropriate resources for help. "Ask 4 Help!" is a one-hour curriculum for senior high, middle school and college students that provides students with knowledge intended to increase help-seeking for themselves or on behalf of others.

The evidence base for gatekeeper training is still weak, despite widespread dissemination. The few randomized control trials that exist suggest that "train everyone" models using brief curricula do little more than raise awareness and that targeting more in-depth training to individuals fulfilling roles that put them in meaningful relationships with the target population would be more cost-effective. A summary of gatekeeper training research concluded that the practice "holds great promise as part of a multi-faceted strategy".³⁸

Recommendation 14: Continue to evaluate and refine gatekeeper training in various contexts; modify curricula in a continuous quality improvement mode. Implement gatekeeper training in the context of comprehensive suicide prevention programs.

c. Community Competency Training

In 2002, SPRC contracted with AAS to develop a suicide prevention curriculum that addresses the core knowledge and skills necessary for States and communities to develop comprehensive and effective suicide prevention plans and programs.

From this curriculum, the SPRC Training Institute developed "Strategic Planning for Suicide Prevention (SPSP): Core Competencies in Community Suicide Prevention" -- flexible offerings for suicide prevention coalitions and planning groups. SPRC has teamed with SPAN USA to train 29 coalitions and groups, with a total enrollment of 825 individuals since September 2005. This training is designed to enhance participants' leadership and collaboration skills, and, in line with the NSSP, prepare them to collect and present suicide-related data and information, select and implement

suicide prevention programs in their communities, and determine the effectiveness of the programs they implement. The training also helps build participants' comfort and ability to communicate effectively about suicide and suicide prevention.

A number of trainings beyond those identified above are in use in communities across the nation, but are disseminated much less broadly. For all, rigorous evaluations looking for behavioral changes as outcomes are needed.

Recommendation 15: Develop and widely disseminate training on core public- health competencies, including strategic planning, to coalition members via the World Wide Web.

d. Standards for Clinical Training Programs

The NSSP calls for training of healthcare professionals in recognizing at-risk behaviors and delivering effective treatment. Recognizing that many health professionals are inadequately trained to assess, refer, treat and manage suicidal clients and patients (NSSP, p 79), this goal seeks to fill gaps in training programs and raise accreditation standards for programs training nurses, physicians, physician assistants, social workers, psychologists, counselors, and other relevant professions.

This review looked for evidence of changes in accreditation and/or provider testing standards since 2001 by searching published literature on the websites of relevant professional associations and accrediting bodies. It examined standards for eleven professional groups: physician specialties (psychiatry, family practice, pediatrics and emergency medicine), substance abuse counselors, employee assistance professionals, and behavioral health providers (psychology, social work, psychiatric nursing, counseling, and marriage and family therapy). Only the Council for the Accreditation of Counseling and Related Educational Programs (CACREP) had increased attention on suicide in its 2009 standards compared to the previous version, dated 2001. Two other accrediting organizations (National Association for Alcoholism and Drug Abuse Counselors [NAADAC] and Employee Assistance Certification Commission [EACC]) have limited mention of suicide in their certification examinations, but not accreditation standards. Suicide is briefly mentioned in the Accreditation Council for Graduate Medical Education (ACGME) Program Requirements for Pediatrics, but is absent in training standards for the medical specialties of psychiatry, family practice and emergency medicine. Finally, suicide prevention models appear in the core curriculum in psychiatric nursing (2009), but are not mentioned in accreditation standards for training programs (2010).

Although this review found no accreditation standards pertaining to training programs in either addiction counseling or employee assistance, it found limited mention of suicide in exams for National Certified Addiction Counselor Levels I and II and for Master Addiction Counselor, and the test content and exam blue print provided by the EACC.

Anecdotally, we know that many individual training programs have voluntarily added curricula addressing suicide risk and treatment since 2001 and many professional associations and organizations have sought to provide their membership with relevant information and education on the topic. For instance, in Massachusetts, Garrett Lee Smith grant funding helped incorporate this

training into social work programs. Furthermore, it would seem reasonable that the high quality curricula developed for continuing education programs (discussed above) could be incorporated into professional training programs, speeding the enhancements. Since suicide shares risk and protective factors with many other behavioral health problems, including other forms of violence, it would seem important to link suicide related curricula with training covering other related topics. Still, without including a requirement in the accreditation standards for these training programs and testing professionals on that content in certification and licensing exams, the quality of this education will continue to vary widely and may not be congruent with the evidence base.

Recommendation 16: Convene organizations that establish standards of accreditation for professional and clinical training programs to develop and implement plans to ensure all training programs within specific professions include curricula on recognizing, assessing, and managing suicide risk and certification exams include questions on this content.

Recommendation 17: Incorporate extant curricula, or newly develop curricula content, to teach state of the art, evidence-based practices in professional training programs and continuing education offerings.

e. State Training Requirements for School Personnel

In recent years, laws have been passed by several State legislative bodies recommending, or in some cases requiring, in-service training in suicide prevention for school personnel. The States are: California, Colorado, Louisiana, Massachusetts, Michigan, Mississippi, New Jersey, New York, Tennessee, Vermont, Virginia and Wisconsin.

The growth in the efforts to secure passage of legislation for suicide prevention training has often been triggered by grassroots efforts. One example is the campaign by the Jason Foundation in Tennessee for legislation to require that annual in-service training for teachers and principals include at least two hours of suicide prevention education. Other States have since adopted the language used in the Tennessee legislation.

Recommendation 18: Evaluate the cost and effectiveness of state-wide teacher training initiatives; use evaluation results to inform policy in States and Territories.

6. Survivor Support

The NSSP recognized that the aftermath of suicide deserves expanded and enhanced survivor support initiatives and calls for research to determine how best to assist survivors.³⁹ In the years since the NSSP's release, the movement to support survivors has intensified significantly. Local Outreach to Suicide Survivors (LOSS), developed by the Baton Rouge Crisis Intervention Center, is a pioneer in the movement, dating back to the 1970s. It is designed to offer immediate support to survivors as close to the time of death as possible. LOSS acts as a first response team when a suicide occurs, offering resources and hope to the newly bereaved, frequently at the scene of the death.⁴⁰

Since the NSSP was issued, other organizations responded with numerous survivor support initiatives such as support groups and support group facilitator training, and active postvention approaches (such as the LOSS program), reaching out to the newly bereaved. For 20 consecutive years SAVE has sponsored its Annual Suicide Awareness Memorial where hundreds of people from many states gather to hear survivor stories and a national speaker, look at the Faces of Suicide pictures, and listen to a Memorial reading of the names of those who died by suicide. SAVE also started the first Named Memorial program for survivors to remember their loved ones that includes a Named Memorial Wall and on-line memorial recognition program.

In 2008, AFSP launched its Survivor Outreach Program designed to facilitate volunteer “veteran” survivors who listen, show support, and provide information about local resources. Both the AFSP website and the AAS website offer lists of support groups and these sites are updated periodically. The Link Counseling Center of Atlanta, Georgia, is an example of an organization that delivers periodic training for leaders of support groups for survivors of suicide. AFSP also offers regionally disseminated face-to-face trainings for support group leaders annually and has a self-study package for those unable to attend in-person trainings. Online survivor support services and networking are also available, further reducing geographical barriers to peer support that is so important to many survivors.

National Survivors of Suicide Day, an initiative of AFSP, occurs annually on the Saturday before Thanksgiving. On that day, healing conferences for recently bereaved survivors take place throughout the U.S. Each conference site is organized locally by AFSP chapters or independent local groups and organizations; each is connected via a ninety-minute AFSP webcast. The program began in 1999, prior to the NSSP, and has grown from twenty conferences in 2001 to more than 200, including a rising number of international conference sites.

Finally, the Healing After Suicide conference occurs annually in conjunction with AAS’s annual conference. The purpose of the Healing Conference is to: provide survivors with education and resources to help deal with their personal grief; assist mental health providers and other caregivers in understanding the unique grief and needs of survivors; and provide assistance to leaders of existing support groups and to participants who want to establish new support. In the recent past, the conference has been co-sponsored by AAS and SPAN USA; it features speakers, Lifekeeper Memory Quilts, and a healing ceremony.

Efforts to conduct research to better understand survivor issues are few. NIMH hosted a conference that proposed questions for survivor-focused research in 2003. Subsequently, SPAN USA issued a call for research papers, as well as a survey of support group practices, and AFSP is initiating a survivor registry of persons willing to participate in research.

“Important resources have been developed with an eye towards broad-based prevention efforts, as well as survivor support. But, nationwide, the level of funding for suicide prevention research remains woefully low in light of the over 30,000 lives it takes each year.”

- SPAN USA Survey Respondent

Recommendation 19: Conduct research to better determine the effects of suicide on the bereaved and to identify effective approaches to mitigate those effects.

7. Means Restriction

Individuals who attempt suicide are often ambivalent and/or acting impulsively during a short period of crisis.⁴¹ Easy access to highly lethal means of suicide, such as firearms, also puts individuals contemplating suicide at increased risk.⁴² In recognition of this, the NSSP placed an emphasis on promoting efforts to reduce access to lethal means and methods of self-harm.

During the course of this review and in conversations with key informants, several means restriction programs in the past decade were highlighted as exemplary. For instance:

- Harvard University operates “Means Matters,” a public awareness program designed to educate the public that the means by which people attempt suicide—the how—is as critical to reducing the number of suicides as the why, when, and where.
- A Washington State based consortium (including the Harborview Injury Prevention Center) disseminates the LOK-IT-UP Campaign to raise awareness about the importance of safe firearm storage, inform the public about safe storage options, and promote the availability of safe storage devices. LOK-IT-UP is supported by: firearm retailers, firearm owners, parents, schools, law enforcement, public health, elected officials, community organizations, and healthcare providers.
- In New Hampshire, Dartmouth’s Injury Prevention Center disseminates Counseling on Access to Lethal Means (CALM), a curriculum to train health and mental healthcare providers in conducting firearm safety counseling with clients and family members to reduce access by at-risk individuals. NAMI NH’s Connect program includes protocols and training in reducing access to lethal means.
- The Maine Youth Suicide Prevention Program focuses on means reduction for guns, pills, and rope, and has also created a video titled “Kids and Guns: Making the Right Choice.”
- Montana has two means restriction programs focused on law enforcement to provide gun locks at community events and train communities on safe storage practices through public service announcements.
- Oregon addresses lethal means by providing information for healthcare professionals on screening for access to lethal means among their potentially suicidal patients.

Recent statistics point to a slight decrease in deaths (from 5.27 per 100,000 in 1999 down to 3.86 per 100,000 in 2006⁴³) by poison gases (including car exhaust emissions). And while there is no way to demonstrate a direct correlation between the downturn in death by carbon monoxide poisoning, some experts believe that stricter auto emissions standards may be responsible.

According to recent research both in the U.S. and worldwide, suicide prevention barriers on bridges have been shown to be effective at reducing suicide in some circumstances.^{44 45} These studies have

been used to promote U.S. policies aimed at adding barriers to bridges that have become magnets for suicide.

Most notable among these is the Golden Gate Bridge in San Francisco, which has seen over 1,300 suicide deaths since it opened in the late 1930s. The decision by the Golden Gate Bridge District in October 2009 to recommend installing netting to prevent further suicides from the San Francisco landmark illustrates how evidence can be used to bring about change. (It must be pointed out, however that the Golden Gate Bridge barrier has not yet been funded.) Despite the evidence that bridge barriers save lives, the high cost of installation remains a barrier to wider use, as does public objections to their aesthetics. The debate continues.

8. National Suicide Prevention Lifeline

In 2001, the evidence base regarding crisis lines was insufficient to warrant including them in the NSSP. Still, suicide crisis hotlines had become fixtures in many communities. The Samaritans, for example, operate a national and international network of locally-based hotlines that have been serving communities for over forty years. In the late 1990s the first effort to create a national suicide hotline received funding with the help of the late Senator Paul Wellstone (D-MN). Funding from SAMHSA provided through a cooperative agreement with the American Association of Suicidology (AAS) in September 2001 provided the first Federal support for a national suicide prevention hotline network utilizing the number 1-800-SUICIDE. In January 2005, SAMHSA established the National Suicide Prevention Lifeline through a grant with the Mental Health Association of New York City and its subsidiary Link2Health Solutions. Now, the Lifeline responds to callers to the hot line number 1-800-273-TALK (8255) and to 1-800-SUICIDE with assistance 24 hours a day, seven days a week. The Lifeline serves as a central switchboard connecting callers to a crisis center geographically nearest the caller from among a national network of more than 140 crisis centers in 49 states. Services are provided in English or Spanish language. A feature added in 2007 allows a caller to the National Suicide Prevention Lifeline to press “1” and be connected to a VA call center. After one year of operation, 62,000 veterans, family members, and friends of veterans had called the Lifeline/VA option. Among those callers, there had been 1,400 rescues initiated to prevent possible tragedies. In the summer of 2009, the VA added a one-to-one “chat service” for veterans who prefer reaching out for assistance using the internet.⁴⁶ Lifeline also operates a website, www.suicidepreventionlifeline.org, designed to deliver clear messaging and easy navigability to consumers in crisis, and participates on social networking sites.

Call volume continues to increase. In the month of January 2008, Lifeline answered just over 39,000 calls. By May 2009, the monthly count swelled to over 54,000. (It is worth noting that approximately 25% of those 54,000 calls were in some way related to the economic downturn).⁴⁷

With the advent of SAMHSA’s national coordinating role and through evaluations funded by SAMHSA, evidence for the

“I have seen a focus by the Federal government to formalize the role of suicide prevention stakeholders. This has led to research of crisis hotlines and studies on best practices in prevention. My hope is that over the ensuing years there will be greater support and recognition for hotlines and the role they play in the community.”

- SPAN USA Survey Respondent

effectiveness of crisis lines has grown rapidly.⁴⁸ New evidence conclusively showed that hotlines serve callers who are at serious risk for suicide, that callers exhibit significant decreases in intent to die, hopelessness and psychological pain from the beginning of the call to the end of the call, and that these effects continue for weeks after the call.⁴⁹ Other research also showed that adolescents who called a teen suicide prevention hotline demonstrated significant reductions in suicidal ideation and urgency, and a significantly improved mental state.⁵⁰

While there is increased evidence of the effectiveness of suicide prevention hotlines, the quality of services provided has been shown to vary.⁵¹ The recent development and dissemination by the National Suicide Prevention Lifeline of evidence-based standards with which to assess suicide risk in persons who call suicide prevention hotlines should increase the consistency and quality of hotlines.⁵²

9. Media

This review identified important advancements in the interface between suicide prevention and the media—news, entertainment, and social media. Shortly after the release of the NSSP, a collaboration of the AFSP, AAS, the Annenberg Public Policy Center, and representatives of several Federal agencies produced and promoted a set of media recommendations entitled, “Reporting on Suicide: Recommendations for the Media” (2001). These Recommendations have been actively disseminated through national, State, and tribal organizations to members of the press and their influence is seen in various degrees over the years in stories written and broadcast on the topic of suicide.

In the entertainment media, the Entertainment Industries Council leveraged SAMHSA funding to facilitate collaborative work between the Hollywood creative community and suicide prevention experts to produce “Picture This: Depression and Suicide Prevention,” a guide for creators in the entertainment industry which addresses issues within the realm of depression and suicide prevention. Published in 2008, the guide has fostered in-depth discussions between screenwriters, producers, and suicide prevention experts that, in at least some cases, have changed for the better the artistic approach to dealing with suicide content.

In May 2009, SAMHSA funded a new media summit. The summit brought together over fifty experts on suicide prevention, online safety and social media and, along with new media entrepreneurs, collaborated on ways to integrate suicide prevention practices into the virtual landscape. Other Federal agencies were represented, including the Department of Veterans’ Affairs and the Indian Health Service. The event offered a unique cross-fertilization of ideas among people from varied and diverse professional backgrounds.⁵³

A follow-up workshop aimed at updating and revising the 2001 media recommendations in light of new media took place in August 2009, sponsored by SAVE, AFSP, and the Annenberg Public Policy Center and supported in part by SAMHSA. Participants included staff from several Federal agencies, Lifeline, AAS, SPRC, and other suicide prevention national organizations, researchers, and experts in journalism and new media. New research, new technologies, input from content experts and others have been brought to the workgroup revising the recommendations.

Going forward, a number of key informants pointed to the potential roles for new media in the field of suicide prevention.

D. METHODOLOGY

The goals and objectives in the NSSP under Methodology focused on promoting research and evaluation in suicide and suicide prevention, and improving the data on which research, practice, and service planning are premised. A description of the leading initiatives of the last several years follows:

1. Best Practices Registry

Funded by SAMHSA, the Best Practices Registry for Suicide Prevention (BPR) received high marks from the key informants to this review. The foundational work for the BPR began in 2002 as collaboration between AFSP and SPRC to identify evidence-based practices in suicide prevention. In 2005, SAMHSA's National Registry of Evidence-based Programs and Practices (NREPP) took over this role and the AFSP/SPRC effort transitioned toward gathering expert and consensus guidelines and programs and practices that adhered to accepted standards in the field specific to suicide and suicide prevention. Now, programs listed on the Best Practices Registry address dozens of goals and objectives of the NSSP. These programs provide training and resources for people of all ages and roles.

The BPR is available online at SPRC's website (www.sprc.org). It is divided into three sections: Section I: Evidence-Based Programs (derived from SAMHSA's NREPP); Section II: Expert and Consensus Statements; and Section III: Programs that Adhere to Standards. Section III is further divided into four sub-categories: awareness materials, education and training programs, screening programs, and protocols and guidelines. Section II and Section III items are objectively reviewed for inclusion by AFSP and SPRC staff or independent expert consultants. As of July 2010, there were 65 unique items listed on the BPR.

2. Surveillance

Collecting accurate data on the number of suicide attempts is difficult because data are not pooled from the many potential sources such as emergency departments, in-patient hospital records, and urgent care centers. However, some important progress has been made recently. In 2009, SAMHSA's "National Survey on Drug Use and Health" reported on suicidal thoughts and behaviors among adults for the first time.⁵⁴ The report found that "In 2008, an estimated 8.3 million adults aged 18 or older (3.7 percent) had serious thoughts of suicide in the past year, 2.3 million (1.0 percent) made a suicide plan, and 1.1 million (0.5 percent) attempted suicide."⁵⁵ Estimates from other studies indicate there were averages of 507,000 visits to U.S. hospital emergency departments in each of 2005 and 2006 for self-directed violence, the majority of which are suicide attempts.⁵⁶

The 2007 Youth Risk Behavior Survey found that among US high school students during the 12 months before the survey, 14.5% had seriously considered attempting suicide, 11.3% had made a plan about how they would attempt suicide, 6.9% had attempted suicide, and 2.0% had made a

suicide attempt that resulted in an injury, poisoning, or an overdose that had to be treated by a doctor or nurse.⁵⁷

Although the Centers for Disease Control and Prevention (CDC) has tracked deaths by suicide in all states since 1933, very little is known about the risk factors and circumstances that preceded each death. To fill in some of these surveillance gaps, the NSSP specifically called for a “national violent death reporting system” to gather information from several data sources that are not otherwise linked. Key informants cited the National Violent Death Reporting System (NVDRS) as a significant tool in better understanding the public health challenges of suicide prevention. In fiscal year 2002, Congress appropriated funds for the development and implementation of NVDRS, to be housed within the Centers for Disease Control and Prevention (CDC). The NVDRS collects data on violent deaths from four primary sources: death certificates, police reports, medical examiner and coroner reports, and crime laboratories. Individually, these sources explain violence only in a narrow context; together, they provide a more complete answer to the questions that surround violent death: who, what, when, where, how, and, in many cases, why. The NVDRS is critical in that it provides markers for potential points for intervention and ways to evaluate and improve violence prevention efforts.

It will be sometime in 2012 before we know definitively whether or not suicide rates climbed in the U.S. in response to the financial stress and unemployment brought on by the economic crisis.

Initially, six States were funded for inclusion in the NVDRS, and by Fiscal Year 2009, Congress had appropriated \$3.5 million to continue funding the implementation of NVDRS in a total of 18 States.

The CDC now makes available the NVDRS data for public use through the Web-based Injury Statistics Query and Reporting System (WISQARS) at www.cdc.gov/ncipc/wisqars. This interactive database, produced and managed by CDC, provides customized reports of fatal and nonfatal injury-related incidents and has been widely used by researchers, practitioners, Federal agencies, not-for-profit organizations, the media, and the public engaged in suicide prevention.

In December 2008, the CDC released “Deaths from Violence: A Look at 17 States Data from the National Violent Death Reporting System 2004-2005”.⁵⁸ Several journal articles containing preliminary NVDRS data have been published and can be found at www.cdc.gov/injury. These documents provide further detail which States are using to produce annual reports on suicide that integrate data from multiple State data management systems.

Only with the expansion of the NVDRS to include all fifty States, the territories, and the District of Columbia will a comprehensive tracking system enable researchers and planners to fully identify regional differences in the patterns of violence across the country and tailor policies accordingly.

NVDRS alone, though, is not the solution to all our surveillance needs. Timeliness of reporting for national death certificates is a core issue. Due to the complex analytical methodologies the CDC uses to provide standardized, age-adjusted suicide data by county and State, there is more than a three year gap between the close of a calendar year and when data for that year become available. In 2010, 2007 data will be the most recent available. It will be sometime in 2012 before we know definitively whether or not suicide rates climbed in the U.S. in response to the financial stress and unemployment brought on by the economic crisis. Another glaring need is reliable suicide data for

deaths that occur in healthcare settings, as there is currently no national system that can provide these crucial data.

In addition to the problems regarding mortality data, the quality and availability of the data on nonfatal suicidal behavior are equally problematic. The concerns about discrepancies in nomenclature and accurate reporting apply here even more than with suicide deaths. Except for rare exceptions, there is neither systematic nor mandatory reporting of nonfatal suicidal behavior in the United States at the State, territorial, or local level, nor is there routine systematic collection of non-suicidal intentional self harm data.⁵⁹ The NSSP calls for State-produced annual reports on suicide attempts and completions, integrating data from multiple State data management systems. To our knowledge, this is still occurring on a very limited basis.

Recommendation 20: Develop methodologies that are capable of providing preliminary estimates of suicide rates and rapidly detecting meaningful changes in rates for specific demographic groups at the national level.

Recommendation 21: Develop a system to collect reliable data on suicide deaths that occur in healthcare settings.

3. Research Funding

A review of funding since the NSSP's release shows an increase in overall Federal funding for the National Institute of Mental Health and a fifty percent increase--from \$23 million in 2001 to \$36 million in 2007--in research dollars for suicide prevention. That allocation declined, however, in 2008 to \$31 million. Outside of NIMH funding, the National Institutes of Health have awarded over 1,100 grants for research on suicide and suicide prevention between 2001 and 2007, with over 300 grants in 2007 alone.⁶⁰ The other agencies within NIH that are involved in suicide research include:

- National Institute on Alcohol Abuse and Alcoholism (NIAAA)
- National Institute of Child Health and Human Development (NICHD)
- National Institute on Drug Abuse (NIDA)
- National Center for Research Resources (NCRR)

In 2004, the National Institute of Mental Health, with additional funding from the National Institute on Drug Abuse and the National Institute on Alcohol Abuse and Alcoholism, awarded grants to three Developing Centers for Interventions for the Prevention of Suicide (DCIPS), as called for in the 2002 IOM Report. The purpose of these five-year grants was to establish core support for building research infrastructure for the study of preventive and treatment interventions for suicidality (severe ideation, attempts, deaths) related to mental health, substance use disorders and alcohol use disorders by qualified institutions with active research programs but without the existing capacity to mount the extensive and highly integrated research effort expected of an advanced center (e.g., Advanced Centers for Interventions and Services Research). The three Developing Centers were established at Columbia University College of Physicians and Surgeons, the University of Pennsylvania Medical College and the University of Rochester Medical College. After completion of the initial five year grants, NIH discontinued the program.

Research is also funded by the private sector. For instance, AFSP annually awards grants for research on suicide and its prevention, averaging \$1.5 million per year since 2001. In 2006, AFSP launched a research initiative focused on developing and testing procedures that could ultimately lead to a national suicide attempt registry. AFSP collaborated with the three DCIPS universities on a two-year pilot project based in psychiatric emergency rooms. The results of the pilot project are not yet published.

Most recently, in response to the alarming spike in suicides among soldiers, the National Institute of Mental Health (NIMH) signed a memorandum of agreement with the U. S. Army in October 2008 authorizing the NIMH to undertake the largest study of suicide and mental health among military personnel ever conducted, with \$50 million in funding from the Army and \$10 million from NIMH.⁶¹ In July 2009, NIMH announced that an interdisciplinary team of four research institutions⁶² will carry out the study. Researchers will be identifying risk and protective factors for suicide among soldiers and developing a stronger evidence base for effective and practical interventions to reduce suicide rates and address associated mental health problems.

Additionally, NIMH awarded the first of five annual \$3 million grants for Emergency Department Safety Assessment and Follow-up Evaluation (ED-SAFE), a multi-site trial based at Massachusetts General Hospital. The goal of the research project is to improve outcomes for suicidal individuals seeking care in emergency departments through better identification, care and follow-up.

The suicide prevention work of the VA is buttressed by the newly formed VISN 2 Center of Excellence at Canandaigua, NY. This center, associated with the University of Rochester, will lead suicide prevention research and evaluation for much of the VA's suicide prevention work.

In spite of these expenditures, this review found that suicide prevention stakeholders consider the relative paucity of both public and private sector funding for research to be one of the most serious challenges facing the suicide prevention community. Federal funding for research into suicide and suicide prevention is still disproportionately low when compared to government funded research into other leading causes of death, and recently, there has been a roughly nine percent decrease in funding in public health-oriented suicide prevention through the CDC. However, additional Federal government-funded research may be forthcoming as the Department of Defense and the Department of Veterans Affairs grapple with the rising numbers of suicides among the ranks of serving military and the possible spillover among veterans. Still, these funding increases do not begin to raise the level of funding for suicide prevention research to that of illnesses such as HIV/AIDS or breast cancer, conditions that kill numbers of Americans in roughly the same order of magnitude.

“Much of the research being conducted continues to be clinical or epidemiological in nature. This is needed, but I would also like to see more research focusing on prevention activities.”

- SPAN USA Survey Respondent

Recommendation 22: Support the development of a robust suicide research infrastructure that is commensurate with the magnitude of the public health burden.

Recommendation 23: Fast-track research to develop and evaluate effective therapies, as well as non-clinical suicide risk management techniques that take into account the widespread non-acceptance of mental health treatment modalities.

E. SPECIAL POPULATIONS AND EMERGING ISSUES

The need for cultural competency in planning and providing suicide prevention services and programs was a cross-cutting theme throughout this review. When dealing with mental health, substance abuse, and suicide prevention and intervention, specialists in suicide prevention deemed it crucial to take into consideration the way in which different cultures handle these issues. They also acknowledged that "culture" signifies more than ethnicity and noted that there are work and professional cultures, geographically-based cultures, and distinct cultural patterns within age groups.

Key informants emphasized that specific, targeted messaging and "community-based" approaches, as well as culturally-appropriate policies and services, are needed in order to reach distinct groups of people. The information below identifies notable prevention efforts that target specific audiences.

1. Youth

Attention to the youth suicide rate was called for in the NSSP and a number of initiatives designed to address youth suicide have been launched since 2001, most notably as a result of the Garrett Lee Smith Memorial Act, discussed elsewhere in this report.

The youth suicide rate, which had been declining from the late 1990s to 2004, reversed course and began to rise again about the same time evidence emerged that prescription medications for depression and other mood disorders were associated with suicidal ideation in youths and young adults. The U.S. Food and Drug Administration eventually imposed a "black box warning" label (a stark notice on the package insert that warns of potential serious adverse effects) on many antidepressant medications. Although causality has not been established, key informants to the review suggested that the black box warning had an unintended chilling effect on both prescribing rates and willingness by families with adolescent children to use the medications. After the warnings appeared, prescribing rates fell precipitously and the youth suicide rate ticked up. Since then, the rate has decreased slightly each year from 2005 to 2007, when it fell below the 2003 benchmark.

Since the NSSP launch, two groups have focused exclusively on advancing the cause of suicide prevention and mental health on college campuses. Active Minds, founded in 2001 by a survivor of a loved one's suicide, is a bona fide grassroots movement that has grown to include over 270 chapters nationwide. Active Minds brings programming to college campuses through its chapters by organizing "National Day without Stigma," "National Stress-Out Day," and "Send Silence Packing" and helps students avail themselves of counseling and other campus/community resources.

The Jed Foundation, founded in 2000 by a family after losing their college-age son to suicide, has contributed to college campus suicide prevention programs nationwide through its program “Half of Us,” in collaboration with mtvU and the Transition Year Project. The Foundation has also sponsored the development of important tools and guides for campus policymakers, including “Framework for Developing Institutional Protocols for the Acutely Distressed or Suicidal College Student” and “Student Mental Health and the Law.”

In response to the particularly challenging problem of suicide in the juvenile justice system, the U.S. Department of Justice’s Office of Juvenile Justice and Delinquency Prevention (OJJDP) released a report entitled “Characteristics of Juvenile Suicide in Confinement.” The February 2009 report examines 110 juvenile suicides that occurred in confinement between 1995 and 1999. It describes the demographic characteristics and social history of victims and examines the characteristics of the facilities in which the suicides took place. Drawing on this data, the researchers offer recommendations to prevent suicides in juvenile facilities.⁶³

Recognizing the elevated rates of suicidal thoughts and attempts among Gay, Lesbian, Bisexual, and Transgender (GLBT) youths, AFSP, SPRC, and the Gay and Lesbian Medical Association cosponsored a November 2007 workshop of researchers and other stakeholders. The meeting led to a private foundation grant to enable national GLBT organizations to develop accurate messaging around suicide prevention, encouraging them to make suicide prevention a part of their agenda. Research is being conducted to determine the best approaches and messaging for this segment of the population. At about the same time, SPRC released “Suicide Risk and Prevention for Lesbian, Gay, Bisexual, and Transgender Youth,” a comprehensive summary of the evidence base with recommendations for practice.

Finally, in recent years, the use of social media by teens has led to unfortunate incidents of cyber-bullying, triggering the suicides of vulnerable young people. This is an area under-examined and difficult to legislate because of freedom of speech issues, but policymakers are beginning to acknowledge the problem.

2. Military and Veterans

Of all of the programs designed to prevent suicide within the military, the evidence-based U.S. Air Force Suicide Prevention Program is pointed to as a notable success. The Air Force Suicide Prevention Program, adopted in the mid-90s and supported by the top command, leveraged the leadership culture to tighten social connections and support, promote responsible help-seeking for personal problems, and to strengthen life skills among airmen. The program produced statistically significant reductions in suicides that were also accompanied by reductions in homicide, family violence, and unintentional injuries.⁶⁴ More recent experience in this program has illustrated the need for ongoing attention to sustain the effectiveness of prevention programs and it is currently undergoing refinement.

Since engaging in Operations Iraqi Freedom and Enduring Freedom, suicides have been occurring in alarmingly high numbers within the ranks of combat forces, including National Guard and Reservists. This is leading Defense and other Departments of the Federal government to address suicide prevention urgently and forcefully.

A variety of suicide prevention programs has been launched in each branch of the military in an effort to promote overall mental wellness and prevent suicide. There is general agreement, however, that more programs do not necessarily produce the desired outcomes and to date there has been only minimal effort to measure the effectiveness of these programs or the consistency of their implementation. Still, a *Washington Post* editorial in late July 2009 suggested that the Pentagon's efforts to gather and share data represent a benefit to society as a whole, the potential result of which "could result in strategies with applications in the struggle against suicide nationwide."⁶⁵ One of the biggest challenges is developing methods to provide support and services to National Guard and Reservists after they return to their civilian jobs post-deployment.

Addressing suicides among veterans, a Blue Ribbon panel assembled in 2008 found that the Veterans Health Administration "has developed a comprehensive strategy to address suicides and suicidal behavior that includes a number of initiatives and innovations that hold great promise for preventing suicide attempts and completions".⁶⁶ The VA has placed fulltime suicide prevention coordinators in every medical center, hired thousands of additional mental health professionals, provided training to clinical staff throughout its healthcare delivery system, launched a 24/7 crisis line/internet chat with the capacity to connect immediately to the VA's system of electronic health records and suicide prevention coordinators, and developed a Center of Excellence for suicide prevention to lead its research effort in suicide prevention. This is perhaps the most comprehensive suicide prevention initiative in history.

3. Adults in Mid-life

Much attention has focused on the young in this past decade, but suicide rates remain high and are climbing for other age groups. A five-year analysis of the nation's death rates released by the Centers for Disease Control and Prevention found that the suicide rate among 45- to 54-year-olds increased 20 percent from 1999 to 2004, a larger increase than any other age group during the same period.⁶⁷

Middle-aged men die by suicide at twice the baseline rate of other Americans and most of these men are employed.⁶⁸ Although white males account for eighty percent of all suicide deaths, there has been relatively little focus on this high-risk demographic group. Key informants noted that the workplace is a prime spot for early intervention due to the large amount of time people spend in the workplace "community" as well as the availability of an existing structure for face-to-face interaction: employee assistance programs (EAP). Nevertheless, efforts that seek to prevent suicides, even within EAPs, often do not mention suicide, but instead provide guidance (e.g., "tip sheets" for managers) on how to handle extreme stress.

Recommendation 24: Convene a task force to address suicide among adults in mid- life.

4. Racial/Ethnic Minorities

This review found many suicide prevention initiatives that have been designed to meet the specific needs of racial and ethnic populations in the nine years since the NSSP was launched. Although key

informants and stakeholders reported that new initiatives were being undertaken, they also reported that support for such efforts has been limited. This is true for each of the groups identified as "specific populations" by DHHS: African Americans; Hispanic/Latino Americans; Asian American/Pacific Islanders; and Native Americans, American Indians, and Alaska Natives.

Certain cultural practices may actually serve as protective factors, such as strong family ties, religious practices, and cultural ceremonies and rites. For instance, a sense of "belonging" and "ethnic identity" that comes from being part of a distinct ethnic group may serve as protective factors.⁶⁹ On the other hand, many barriers continue to slow the progress of suicide prevention among these groups. In some instances, cultural norms include stigma that prevents people from seeking help for mental health problems. While religion can be a protective factor, in certain segments of the culture religious beliefs about suicide hinder intervention or healing, particularly in the wake of a suicide attempt or death. In the course of this review, stakeholders repeatedly pointed to the shortage of service providers who understand a particular culture or speak the same language, limiting service availability. At times, the needs of smaller groups are not considered in broad-based suicide prevention efforts. For example, public information campaigns trying to reach the largest possible audience often overlook the unique needs of specific minority groups.

Fortunately, grassroots initiatives have emerged to advance the NSSP among minorities. One, the Asian American Suicide Prevention Initiative (AASPI), began in Chicago, IL, in 2005. Another older organization, the National Organization for People of Color Against Suicide (NOPCAS) provides a range of resources and organizes conferences to help communities of color address suicide. It offers a Counseling Certification Program in partnership with the QPR Institute, organizes support groups for survivors in three states, compiles listings of African American publications and dissertations, and maintains a Speakers Bureau. And Federal programs have been legislated, including the Garrett Lee Smith Memorial Act grants and various other SAMHSA initiatives, such as the Native Aspirations Program, which support suicide prevention efforts directed at Tribal youth. Promising interventions have also been developed in tribal communities such as the Athabaskan initiative,⁷⁰ Zuni Life Skills Program,⁷¹ and follow up with youth who attempt suicide in the White Mountain Apache tribe.⁷²

Other developments include adaptation of QPR for Native Americans and other populations; adaptation of AMSR to be culturally competent; the Lifeline Tribal initiative; and culturally competent materials developed by state and campus grantees.

New technologies are being used to reach specific target populations most notably via the internet. The Federal government's Indian Health Service (IHS) maintains a Community Suicide Prevention website, which provides culturally appropriate information on best and promising practices, training opportunities, and the tools for adapting mainstream programs for Tribal needs. Likewise, SAMHSA offers culturally appropriate information on its website. YouTube videos are appearing that aim to de-stigmatize and prevent suicide for specific groups such as Latino teens; however, most current

"Broadly, people do recognize the unique needs of groups and that they are different for each cultural group, which is encouraging. People understand that there are many factors in the causality of suicide. People are confident that what works for Latina girls is not necessarily what works for Native American girls."

- Key Informant

examples of these were produced by individuals who may not be paying attention to consensus recommendations regarding safe and effective messaging. Since its inception, SPRC has continued to develop resources and provide support for suicide prevention with racial and ethnic minorities, including: providing information on suicidal behavior in specific groups (LGBT white paper and resources, suicide fact sheets developed by SPAN USA); hiring Tribal Prevention Specialists who are expert in culturally competent work with Native communities; creating AI/AN pages on SPRC's website; joining IHS's national and international suicide prevention committees; and partnering with national organizations serving various minority populations.

5. Attempt Survivors

Addressing the needs of attempt survivors continues to challenge the suicide prevention movement. Recent attempt survivors struggle with many aspects of reintegration into their homes, schools, workplaces and communities. Feelings of shame, self-doubt, fear, and embarrassment are just some experiences attempt survivors describe. Spouses, parents and others need help adjusting and evidence-based information and programs need to be designed to help in this process.

Only in the last decade and a half have there been organized efforts to meet these needs. In 1996, Kenneth Tullis, M.D., of Memphis, Tennessee, along with fifteen other survivors of suicide attempts, founded Suicide Anonymous, the first-ever twelve-step program for people struggling with suicidal ideation and behaviors. A year later, the Organization for Attempters & Survivors of Suicide and Interfaith Services (OASSIS) was founded by James T. Clemons, Ph.D., the first national organization that included attempt survivors as a focus. Since then, activity in support of attempt survivors has steadily accelerated. In October 2005, the first National Conference for Survivors of Suicide Attempts, Healthcare Professionals, Clergy, and Laity was sponsored by OASSIS and SPAN USA and held in Memphis. The summary report of that conference is one of the first documents to articulate the perspectives of attempt survivors.⁷³ Two years later, in July 2007, the National Suicide Prevention Lifeline sponsored a project which provided even more specific and rich information to better serve suicide attempt survivors.⁷⁴

The suicide prevention community recognizes that a previous suicide attempt is one of the strongest known predictors of suicide. According to one study of individuals who had survived a serious suicide attempt, almost half went on to make another attempt or subsequently died by suicide within five years.⁷⁵ Obviously, more and better strategies to reach and support survivors of suicide attempts are needed. The *After An Attempt* brochures, originally developed collaboratively by SPRC and NAMI and now distributed by SAMHSA, are tools in English and Spanish that were developed for distribution in hospitals to provide basic information to families and attempters.⁷⁶ Other tools are needed, too. Support groups, informational DVDs, and trainings for healthcare providers are required to ensure that survivors of an attempt, along with their families and friends, receive the support, advice, and information they need to find the most direct path to recovery.

Recommendation 25: Take steps to ensure evidence- based therapies discussed in the Intervention section of this report are available to more suicide attempt survivors.

Recommendation 26: Develop, evaluate, and disseminate other evidence- based clinical and non- clinical interventions for survivors of suicide attempts.

6. Older Adults

Older adults have become a focus in suicide prevention due to the extremely high rates among one segment of that population, white older men. Consequently, since 2001, many national and regional conferences have featured the topic and many states have broadened or are in the process of broadening their suicide prevention strategies to include older adults. Some States (e.g., Oregon and Maine) have separate plans for this age group. Mental health parity for Medicare is now being phased in so that seniors in the U.S. will pay the same co-pay (20%) for mental healthcare as for physical healthcare. In 2008, SAMHSA launched an initiative to develop toolkits for use in specific venues; a toolkit for senior living communities, *Promoting Mental Health and Preventing Suicide: A Toolkit for Senior Living Communities* was the first to be produced and is available for download from SAMHSA's website (http://download.ncadi.samhsa.gov/ken/pdf/SMA10-4515/toolkitoverview_final.pdf). Fortunately, the suicide rate for older Americans has been trending downward for nearly a decade.

"We have to have much more aggressive approaches to this population group. There is much stigma around older adult suicide, which creates limitations to access to care for this population."

- Key Informant

7. Emerging Issues

Since the NSSP was released in 2001 the knowledge base has significantly developed, both in the understanding of suicidal behaviors and for suicide prevention. Some of the new evidence has already been discussed in this report. There are a few emerging issues in the field, about which we are only beginning to learn, that have not been addressed; these are mentioned below as issues that will require more attention by the field.

The first is suicide risk among the disabled. Although some discussion emerged on this topic prior to the release of the NSSP, this population was not addressed in any way in the NSSP, an apparent oversight. Obviously, the problem is still pertinent. Another is that of bullying. Concern over suicide risk related to bullying, including cyberbullying, has gradually increased over the past years culminating in new legislation and ordinances being passed in an effort to protect vulnerable youths. There have been a number of studies published on bullying, and although most have found an association between bullying (both as a perpetrator and victim) and suicidal behaviors, the best studies' multivariate analyses have not found a direct link.⁷⁷ Further investigation of the issue should shed more light on the problem and suggest appropriate responses. A third emerging issue—briefly mentioned above—involves the role of social media, both as a contributor to suicidal behaviors (e.g., when acting as a mediator in the formation of suicide pacts among adolescents) or as an instrument for suicide prevention (e.g., when a person divulges their intent for suicide through their social media page, opening the door for intervention or rescue.) It will be important for the field to learn how to harness this rapidly developing component of our culture for prevention. The final emerging issue we will address here fortunately occurs very rarely, but is devastating when it does: postpartum depression related infanticide and suicide. Improved clinical screening, detection and treatment of postpartum depression should help reduce this risk.

The emergence of these issues in the past decade could serve as a signal to the suicide prevention field that strategies and tactics will need to continually evolve along with the social scripts that appear to play such powerful roles in determining patterns of suicidal behaviors.⁷⁸

III. CONCLUSION

In 2010, nearly nine years after the release of the NSSP, the suicide prevention movement can celebrate myriad signal accomplishments, perhaps foremost among them, suicide prevention initiatives taking shape at the community level all across the country. It is in the context of communities where, according to Surgeon General Satcher's NSSP Preface, "human relationships breathe life into public policy." These community efforts are inspired and supported by the confluence of survivor passion, committed professionals, research and evaluation, and public and private funding. It is also evident that suicide is now a topic more easily discussed within families and in the general public and that this openness makes it easier for some people to seek help for mental health problems. Still, many challenges remain. In spite of a more open public discourse, discrimination and negative attitudes persist toward those with mental illness, those who seek mental healthcare, and most germane to this work, those contemplating or attempting suicide. Furthermore, some of that growth in public discourse may have created unintended consequences of lowering the threshold to suicide for those already at risk by not following accepted principles of health communication around suicide.

"New funding opportunities will be needed to ensure we can implement the programs and practices we now know to be effective."

• Key Informant

This review showed that the suicide prevention movement is still young and many core objectives of the NSSP have gone unaddressed. The recommendations generated from this review (summarized in Appendix A) should serve to refine the roadmap originally established by the NSSP. The challenge ahead is to organize and strengthen a viable, sustainable suicide prevention movement that can finally achieve the goal set by the NSSP: to reduce the rates of suicide and suicidal behaviors. A synthesis of the findings from this review suggests several opportunities underpinning the future evolution of the suicide prevention movement:

1. **Leadership.** The diversity and independent initiative of members of the suicide prevention movement have been its strength, generating momentum and enthusiasm. Since so little was being done prior to the NSSP's release, it made sense to encourage multiple independent endeavors. Now, in addition to leadership for specific concerns or programs, the movement as a whole needs leadership – from both current and new leaders – to sort out what is working from what simply sounds good, to drive the agenda toward priorities that will best move the entire field forward, to respond to changing constituencies, and to shift strategies as needs change. Forming a National Action Alliance for Suicide Prevention can provide critical components of that new leadership.

2. **Evidence base.** Notwithstanding the remarkable advancements since 2001, there is still much we do not know about what is effective in reducing the toll of suicide. In the next decade we need to fast-track the research, development, testing and dissemination of both public health and clinical interventions to reduce suicide risk.
3. **New partnerships.** Suicide shares risk and protective factors with most other significant public health problems, yet too often, the suicide prevention movement operates in its own silo. To be maximally effective the movement must develop strong partnerships with other public health movements and integrate its “best practices” into those used in other communities of practice.
4. **Activism.** The emergence of activism in suicide prevention in the late 1990s led to the creation of the NSSP. A challenge for the movement now will be to bring together the pioneering generation of the movement – advocates, researchers, clinicians, and others – with new adherents and the many others who simply agree with the goals and want to help. Grounded activism can lead to legislative actions that will effect policy changes, resulting in more funding for research, and mandates for better access and treatment. The movement’s capacity for activism will be central to its future success.
5. **Collaboration and dialogue.** Collaboration has worked in the past and much more of it will be needed in the future to share knowledge and experience, forge common priorities or agendas, address new populations, and reach out to engage broader interest in suicide prevention. Dialogue is the foundation of collaboration. Using social networking tools and creating other new forums for dialogue and collaboration is integral to building a stronger, more effective suicide prevention movement. Strong dialogue will help prevent the field from endlessly re-creating wheels and spreading the limited funds too broadly to make a sustainable difference.
6. **Human and financial capital.** As the movement and its work evolve, organizations and initiatives will require significant new investment. Areas of accomplishment such as those identified in this review will require resources for continuation and ongoing improvement. Implementation of programmatic ideas shown to be effective will not occur without adequate financing and strong leadership. As the research enterprise matures, it increasingly warrants added resources. For many of those at highest risk for suicide, lack of health insurance poses one of the biggest barriers to recovery. Until now, funding for understanding and preventing suicide has borne no comparison to the magnitude of the public health problem. The total annual investment (public and private) is difficult to estimate reliably, yet it most certainly pales in comparison to even the direct medical costs associated with hospitalized suicide attempts, estimated to be over \$1.5 billion annually. A primary objective for the movement must be to expand and enhance financial support well beyond this level.

This review describes many remarkable accomplishments across less than a decade, with little benefit of strong national coordination. All this is to be celebrated. Now, having taken stock of where we are in this journey of implementing the NSSP, we see where some of the next decade’s challenges lie and we can begin planning to take next steps. This planning should be the first task of the National Action Alliance for Suicide Prevention.

APPENDIX A

Report Recommendations Summary List

Recommendation 1: Develop and implement plans to increase the proportion of public awareness and education campaigns that reflect both the fundamental principles of health communication and the safe messaging recommendations specific to suicide.

Recommendation 2: Promote the importance of using public awareness and education campaigns as an adjunct to other interventions rather than as stand-alone initiatives. Whenever possible, health communications campaigns should have much more specific goals than simply "raising awareness."

Recommendation 3: Promote the development of public awareness and information campaigns that are tailored for and targeted toward specific audiences and that describe the actions those audiences can and should take to prevent suicidal behaviors.

Recommendation 4: Implement suicide related GPRA performance measures in government grant programs serving populations at increased risk for suicide, such as aging services; mental health, substance abuse, and healthcare; labor; education; and Tribal programs.

Recommendation 5: Promote more active and systematic state support of suicide prevention planning, implementation, and evaluation at the community level; systematically share successes across States.

Recommendation 6: Expand efforts to provide effective follow up care after emergency department discharge of suicidal persons.

Recommendation 7: Expand efforts to provide effective follow up care after inpatient discharge of suicidal persons.

Recommendation 8: Promote evidence-based and evidence-informed practices for reducing suicide risk among primary care patients.

Recommendation 9: Evaluate and assess practices being implemented in the VA for dissemination to the broader healthcare delivery system.

Recommendation 10: Evaluate and assess practices being implemented in the Department of Defense for potential dissemination for community-based suicide prevention efforts.

Recommendation 11: Promote collaboration between public and private partners to engage military families and veterans' families in suicide prevention efforts.

Recommendation 12: Increase efforts to integrate suicide prevention practices into substance abuse prevention and treatment services.

Recommendation 13: Evaluate the capacity of continuing education clinician training programs to produce behavioral outcomes that improve clinical practice and outcomes. On the basis of evaluation, make curriculum improvements if needed; promote mass dissemination of continuing education to practicing behavioral health providers.

Recommendation 14: Continue to evaluate and refine gatekeeper training in various contexts; modify curricula in a continuous quality improvement mode. Implement gatekeeper training in the context of comprehensive suicide prevention programs.

Recommendation 15: Develop and widely disseminate training on core public- health competencies, including strategic planning, to coalition members via the World Wide Web.

Recommendation 16: Convene organizations that establish standards of accreditation for professional and clinical training programs to develop and implement plans to ensure all training programs within specific professions include curricula on recognizing, assessing, and managing suicide risk and certification exams include questions on this content.

Recommendation 17: Incorporate extant curricula, or newly develop curricula content, to teach state of the art, evidence- based practices in professional training programs and continuing education offerings.

Recommendation 18: Evaluate the cost and effectiveness of statewide teacher training initiatives; use evaluation results to inform policy in States and Territories.

Recommendation 19: Conduct research to better determine the effects of suicide on the bereaved and to identify effective approaches to mitigate those effects.

Recommendation 20: Develop methodologies that are capable of providing preliminary estimates of suicide rates and rapidly detecting meaningful changes in rates for specific demographic groups at the national level.

Recommendation 21: Develop a system to collect reliable data on suicide deaths that occur in healthcare settings.

Recommendation 22: Support the development of a robust suicide research infrastructure that is commensurate with the magnitude of the public health burden.

Recommendation 23: Fast- track research to develop and evaluate effective therapies, as well as non- clinical suicide risk management techniques that take into account the widespread non- acceptance of mental health treatment modalities.

Recommendation 24: Convene a task force to address suicide among adults in mid- life.

Recommendation 25: Take steps to ensure evidence- based therapies discussed in the Intervention section of this report are available to more suicide attempt survivors.

Recommendation 26: Develop, evaluate, and disseminate other evidence- based clinical and non- clinical interventions for survivors of suicide attempts.

**APPENDIX B
NSSP GOALS AND OBJECTIVES
Summary List**

Section 1: Awareness

1. Promote awareness that suicide is a public health problem that is preventable
2. Develop broad-based support for suicide prevention
3. Develop and implement strategies to reduce the stigma associated with being a consumer of mental health, substance abuse and suicide prevention services

Section 2: Intervention

4. Develop and implement suicide prevention programs
5. Promote efforts to reduce access to lethal means and methods of self-harm
6. Implement training for recognition of at-risk behavior and delivery of effective treatment
7. Develop and promote effective clinical and professional practices
8. Increase access to and community linkages with mental health and substance abuse services
9. Improve reporting and portrayals of suicidal behavior, mental illness, and substance abuse in the entertainment and news media

Section 3: Methodology

10. Promote and support research on suicide and suicide prevention
11. Improve and expand surveillance systems

SECTION 1: AWARENESS	
1. Promote Awareness that Suicide is a Public Health Problem that is Preventable	
Objective 1.1:	By 2005, increase the number of States in which public information campaigns designed to increase public knowledge of suicide prevention reach at least 50 percent of the State's population.
Objective 1.2:	By 2005, establish regular national congresses on suicide prevention designed to foster collaboration with stakeholders on prevention strategies across disciplines and with the public.
Objective 1.3:	By 2005, convene national forums to focus on issues likely to strongly influence the effectiveness of suicide prevention messages.
Objective 1.4:	By 2005, increase the number of both public and private institutions active in suicide prevention that are involved in collaborative, complementary dissemination of information on the World Wide Web.
2. Develop Broad-Based Support for Suicide Prevention	
Objective 2.1:	By 2001, expand the Federal Steering Group to appropriate Federal agencies to improve

	Federal coordination on suicide prevention, to help implement the National Strategy for Suicide Prevention, and to coordinate future revisions of the National Strategy
Objective 2.2:	By 2002, establish a public/private partnership(s) (e.g., a national coordinating body) with the purpose of advancing and coordinating the implementation of the National Strategy.
Objective 2.3:	By 2005, increase the number of national professional, voluntary, and other groups that integrate suicide prevention activities into their ongoing programs and activities.
Objective 2.4:	By 2005, increase the number of nationally organized faith communities adopting institutional policies promoting suicide prevention.
3. Develop and Implement Strategies to Reduce the Stigma Associated with Being a Consumer of Mental Health, Substance Abuse and Suicide Prevention Services.	
Objective 3.1:	By 2005, increase the proportion of the public that views mental and physical health as equal and inseparable components of overall health.
Objective 3.2:	By 2005, increase the proportion of the public that views mental disorders as real illnesses that respond to specific treatments.
Objective 3.3:	By 2005, increase the proportion of the public that views consumers of mental health, substance abuse, and suicide prevention services as pursuing fundamental care and treatment for overall health.
Objective 3.4:	By 2005, increase the proportion of those suicidal persons with underlying mental disorders who receive appropriate mental health treatment.
SECTION 2: INTERVENTION	
4. Develop and Implement Community-Based Suicide Prevention Programs	
Objective 4.1:	By 2005, increase the proportion of States with comprehensive suicide prevention plans that a) coordinate across government agencies, b) involve the private sector, and c) support plan development, implementation, and evaluation in its communities.
Objective 4.2:	By 2005, increase the proportion of school districts and private school associations with evidence-based programs designed to address serious childhood and adolescent distress and prevent suicide.
Objective 4.3:	By 2005, increase the proportion of colleges and universities with evidence-based programs designed to address serious young adult distress and prevent suicide.
Objective 4.4:	By 2005, increase the proportion of employers that ensure the availability of evidence-based prevention strategies for suicide.
Objective 4.5:	By 2005, increase the proportion of correctional institutions, jails and detention centers housing either adult or juvenile offenders, with evidence-based suicide prevention programs.
Objective 4.6:	By 2005, increase the proportion of State Aging Networks that have evidence-based suicide prevention programs designed to identify and refer for treatment of elderly people at risk for suicidal behavior.
Objective 4.7:	By 2005, increase the proportion of family, youth and community service providers and organizations with evidence-based suicide prevention programs.
Objective 4.8:	By 2005, develop one or more training and technical resource centers to build capacity for States and communities to implement and evaluate suicide prevention programs.
5. Promote Efforts to Reduce Access to Lethal Means and Methods of Self-Harm	
Objective 5.1:	By 2005, increase the proportion of primary care clinicians, other health care providers, and health and safety officials who routinely assess the presence of lethal means (including firearms, drugs, and poisons) in the home and educate about actions to reduce associated risks.

Objective 5.2:	By 2005, expose a proportion of households to public information campaign(s) designed to reduce the accessibility of lethal means, including firearms, in the home.
Objective 5.3:	By 2005, develop and implement improved firearm safety design using technology where appropriate.
Objective 5.4:	By 2005, develop guidelines for safer dispensing of medications for individuals at heightened risk of suicide.
Objective 5.5:	By 2005, improve automobile design to impede carbon monoxide-mediated suicide.
Objective 5.5:	By 2005, improve automobile design to impede carbon monoxide-mediated suicide.
Objective 5.6:	By 2005, institute incentives for the discovery of new technologies to prevent suicide.
6. Implement Training for Recognition of At-Risk Behavior and Delivery of Effective Treatment	
Objective 6.1:	By 2005, define minimum course objectives for providers of nursing care in assessment and management of suicide risk, and identification and promotion of protective factors. Incorporate this material into curricula for nursing care providers at all professional levels.
Objective 6.2:	By 2005, increase the proportion of physician assistant educational programs and medical residency programs that include training in the assessment and management of suicide risk and identification and promotion of protective factors.
Objective 6.3:	By 2005, increase the proportion of clinical social work, counseling, and psychology graduate programs that include training in the assessment and management of suicide risk, and the identification and promotion of protective factors.
Objective 6.4:	By 2005, increase the proportion of clergy who have received training in identification of and response to suicide risk and behaviors and the differentiation of mental disorders and faith crises.
Objective 6.5:	By 2005, increase the proportion of educational faculty and staff who have received training on identifying and responding to children and adolescents at risk for suicide.
Objective 6.6:	By 2005, increase the proportion of correctional workers who have received training on identifying and responding to persons at risk for suicide.
Objective 6.7:	By 2005, increase the proportion of divorce and family law and criminal defense attorneys who have received training in identifying and responding to persons at risk for suicide.
Objective 6.8:	By 2005, increase the proportion of counties (or comparable jurisdictions such as cities or tribes) in which education programs are available to family members and others in close relationships with those at risk for suicide.
Objective 6.9:	By 2005, increase the number of recertification or licensing programs in relevant professions that require or promote competencies in depression assessment and management and suicide prevention.
7. Develop and Promote Effective Clinical and Professional Practices	
Objective 7.1:	By 2005, increase the proportion of patients treated for self-destructive behavior in hospital emergency departments that pursue the proposed mental health follow-up plan.
Objective 7.2:	By 2005, develop guidelines for assessment of suicidal risk among persons receiving care in primary health care settings, emergency departments, and specialty mental health and substance abuse treatment centers. Implement these guidelines in a proportion of these settings.
Objective 7.3:	By 2005, increase the proportion of specialty mental health and substance abuse treatment centers that have policies, procedures, and evaluation programs designed to assess suicide risk and intervene to reduce suicidal behaviors among their patients.
Objective 7.4:	By 2005, develop guidelines for aftercare treatment programs for individuals exhibiting suicidal behavior (including those discharged from inpatient facilities). Implement these guidelines in a

	proportion of these settings.
Objective 7.5:	By 2005, increase the proportion of those who provide key services to suicide survivors (e.g., emergency medical technicians, firefighters, law enforcement officers, funeral directors, clergy) who have received training that addresses their own exposure to suicide and the unique needs of suicide survivors.
Objective 7.6:	By 2005, increase the proportion of patients with mood disorders who complete a course of treatment or continue maintenance treatment as recommended.
Objective 7.7:	By 2005, increase the proportion of hospital emergency departments that routinely provide immediate post-trauma psychological support and mental health education for all victims of sexual assault and/or physical abuse.
Objective 7.8:	By 2005, develop guidelines for providing education to family members and significant others of persons receiving care for the treatment of mental health and substance abuse disorders with risk of suicide. Implement the guidelines in facilities (including general and mental hospitals, mental health clinics, and substance abuse treatment centers).
Objective 7.9:	By 2005, incorporate screening for depression, substance abuse and suicide risk as a minimum standard of care for assessment in primary care settings, hospice, and skilled nursing facilities for all Federally-supported healthcare programs (e.g., Medicaid, CHAMPUS/TRICARE, CHIP, Medicare).
Objective 7.10:	By 2005, include screening for depression, substance abuse and suicide risk as measurable performance items in the Health Plan Employer Data and Information Set (HEDIS).
8. Increase Access to and Community Linkages with Mental Health and Substance Abuse Services	
Objective 8.1:	By 2005, increase the number of States that require health insurance plans to cover mental health and substance abuse services on par with coverage for physical health.
Objective 8.2:	By 2005, increase the proportion of counties (or comparable jurisdictions) with health and/or social services outreach programs for at-risk populations that incorporate mental health services and suicide prevention.
Objective 8.3:	By 2005, define guidelines for mental health (including substance abuse) screening and referral of students in schools and colleges. Implement those guidelines in a proportion of school districts and colleges.
Objective 8.4:	By 2005, develop guidelines for schools on appropriate linkages with mental health and substance abuse treatment services and implement those guidelines in a proportion of school districts.
Objective 8.5:	By 2005, increase the proportion of school districts in which school-based clinics incorporate mental health and substance abuse assessment and management into their scope of activities.
Objective 8.6:	By 2005, for adult and juvenile incarcerated populations, define national guidelines for mental health screening, assessment and treatment of suicidal individuals. Implement the guidelines in correctional institutions, jails and detention centers.
Objective 8.7:	By 2005, define national guidelines for effective comprehensive support programs for suicide survivors. Increase the proportion of counties (or comparable jurisdictions) in which the guidelines are implemented.
Objective 8.8:	By 2005, develop quality care/utilization management guidelines for effective response to suicidal risk or behavior and implement these guidelines in managed care and health insurance plans.
9. Improve Reporting and Portrayals of Suicidal Behavior, Mental Illness, and Substance Abuse in the Entertainment and News Media	
Objective 9.1:	By 2005, establish an association of public and private organizations for the purpose of promoting the accurate and responsible representation of suicidal behaviors, mental illness

	and related issues on television and in movies.
Objective 9.2:	By 2005, increase the proportion of television programs and movies that observe promoting accurate and responsible depiction of suicidal behavior, mental illness and related issues.
Objective 9.3:	By 2005, increase the proportion of news reports on suicide that observe consensus reporting recommendations.
Objective 9.4:	By 2005, increase the number of journalism schools that include in their curricula guidance on the portrayal and reporting of mental illness, suicide and suicidal behaviors.
SECTION 3: METHODOLOGY	
10. Promote and Support Research on Suicide and Suicide Prevention	
Objective 10.1:	By 2002, develop a national suicide research agenda with input from survivors, practitioners, researchers, and advocates.
Objective 10.2:	By 2005, increase funding (public and private) for suicide prevention research, for research on translating scientific knowledge into practice, and for training of researchers in suicidology.
Objective 10.3:	By 2005, establish and maintain a registry of prevention activities with demonstrated effectiveness for suicide or suicidal behaviors.
Objective 10.4:	By 2005, perform scientific evaluation studies of new or existing suicide prevention interventions.
11. Improve and Expand Surveillance Systems	
Objective 11.1:	By 2005, develop and refine standardized protocols for death scene investigations and implement these protocols in counties (or comparable jurisdictions).
Objective 11.2:	By 2005, increase the proportion of jurisdictions that regularly collect and provide information for follow-back studies on suicides.
Objective 11.3:	By 2005, increase the proportion of hospitals (including emergency departments) that collect uniform and reliable data on suicidal behavior by coding external cause of injuries, utilizing the categories included in the International Classification of Diseases.
Objective 11.4:	By 2005, implement a national violent death reporting system that includes suicides and collects information not currently available from death certificates.
Objective 11.5:	By 2005, increase the number of States that produce annual reports on suicide and suicide attempts, integrating data from multiple State data management systems.
Objective 11.6:	By 2005, increase the number of nationally representative surveys that include questions on suicidal behavior.
Objective 11.7:	By 2005, implement pilot projects in several States that link and analyze information related to self-destructive behavior derived from separate data systems, including for example law enforcement, emergency medical services, and hospitals.

NOTES

- ¹ "National Strategy for Suicide Prevention: Goals and Objectives for Action." (2001). U.S. Department of Health and Human Services. Washington, D.C.
- ² "Reducing Suicide: A National Imperative." (2002). Institute of Medicine, National Academy of Sciences, p. 1.
- ³ SPAN USA changed its name to the Suicide Prevention Action Network USA on August 9, 2002. SPAN USA merged as a division of the American Foundation for Suicide Prevention (AFSP) on May 1, 2009.
- ⁴ Senate Resolution 84 and House Resolution 212 of the 105th Congress.
- ⁵ "The Surgeon General's Call to Action to Prevent Suicide." (1999). U.S. Public Health Service. Washington, D.C.
- ⁶ "Healthy People, 2010." (2001). U.S. Department of Health and Human Services. Washington, D.C.
- ⁷ Federal Steering Group included representatives from the Office of the Surgeon General, Centers for Disease Control and Prevention, Health Resources and Services Administration, Indian Health Service, and National Institute of Mental Health. The group also liaised with personnel from the Departments of Agriculture, Defense, Interior, Justice, Labor, Transportation, Veterans Affairs, as well as the National Science Foundation and the Office of National Drug Control Policy.
- ⁸ "National Strategy for Suicide Prevention: Goals and Objectives for Action." (2001). U.S. Department of Health and Human Services. p. 20.
- ⁹ "National Strategy for Suicide Prevention: Goals and Objectives for Suicide Prevention - Summary." (2001). U.S. Department of Health and Human Services.
- ¹⁰ *Ibid.* p. 27.
- ¹¹ "Reducing Suicide: A National Imperative." (2002). Institute of Medicine, National Academy of Sciences. <http://www.iom.edu/CMS/3775/3838/3843.aspx>.
- ¹² "Achieving the Promise: Transforming Mental Health Care in America." (2003). President's New Freedom Commission on Mental Health, Goal 1.1.
- ¹³ "National Strategy for Suicide Prevention: Goals and Objectives for Suicide Prevention." (2001). U.S. Department of Health and Human Services. p.52.
- ¹⁴ At the time this project was initiated, the assumption was that it would be the prelude to establishing a National Action Alliance for Suicide Prevention, a public/private partnership to oversee implementation of the NSSP. Significant changes since then, particularly in the economy, suggest the prudence of re-examining the feasibility of that approach.
- ¹⁵ The Project Team relied to a significant degree on previous work supported by SAMHSA to utilize stakeholder input in prioritizing the goals and objectives of the NSSP. The "Moving Forward" project report was completed in 2006 but has not been published.
- ¹⁶ Charlton Research Company. "Investment in research saves lives and money." (2006). Parade/Research!America Health Poll. Retrieved from <http://www.researchamerica.org/uploads/poll2006mentalhealth.pdf>.
- ¹⁷ Suicide Prevention Resource Center. "Safe and Effective Messaging for Suicide Prevention." (2006). <http://www.sprc.org/library/SafeMessagingfinal.pdf>.
- ¹⁸ Source: SPAN USA.
- ¹⁹ For the latest report, go to the IHS website: http://www.ihs.gov/NonMedicalPrograms/nspn/File/FINAL_NSSPCompendium_032709_v2.pdf.
- ²⁰ NSSP, p. 64.
- ²¹ A comprehensive list of SPRC publications can be viewed at <http://library.sprc.org/browse.php?catid=28>.
- ²² Brown GK, Ten Have T, Henriques GR, Xie SX, Hollander JE, Beck AT. (2005). "Cognitive Therapy for the Prevention of Suicide Attempts: A Randomized Controlled Trial," *JAMA*, 294(5): 563-570.
- ²³ Weissman MM, Verdelli H, Gameroff MJ, Bledsoe SE, Betts K, Mufson L, et al. (2006). "National Survey of Psychotherapy Training in Psychiatry, Psychology, and Social Work," *Archives of General Psychiatry*, 63: 925-934.
- ²⁴ Charles Bradberry. Personal communication. December 19, 2007.

- ²⁵ Valenstein M, Kim HM, Ganoczy D, McCarthy JF, Zivin K, Austin KL, et al. (2009). "Higher-risk Periods for Suicide among VA Patients Receiving Depression Treatment: Prioritizing Suicide Prevention Efforts." *Journal of Affective Disorders*, 112: 50-58.
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- ²⁸ Western Interstate Commission for Higher Education (WICHE) Mental Health Program and Suicide Prevention Resource Center. (2009). "Suicide Prevention Toolkit for Rural Primary Care." <http://www.sprc.org/library/primer.pdf>.
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Suicide Prevention Resource Center
Education Development Center, Inc.
55 Chapel Street
Newton, MA 02458
877-438-7772
www.sprc.org



Harvard School of Public Health

Means Matter

Suicide, Guns, and Public Health

Most efforts to prevent suicide focus on *why* people take their lives. But as we understand more about *who* attempts suicide and *when* and *where* and *why*, it becomes increasingly clear that *how* a person attempts--the means they use--plays a key role in whether they live or die.

"Means reduction" (reducing the odds that an attempter will use highly lethal means) is an important part of a comprehensive approach to suicide prevention. It is based on the following understandings (click on each to learn more):

- [Many suicide attempts occur impulsively during a crisis.](#)
- [Intent isn't all that determines whether an attempter lives or dies: means also matter.](#)
- [90% of attempters who survive do NOT go on to die by suicide later.](#)
- [Access to firearms is a risk factor for suicide.](#)
- [Firearms used in youth suicide usually belong to a parent.](#)
- [Reducing access to lethal means saves lives.](#)

Firearm access can be a politically-charged topic. This website, however, is designed to introduce a non-controversial, "lethal means counseling" approach to reducing a suicidal person's access to firearms and other lethal means.

Who We Are

The [Harvard Injury Control Research Center](#) is dedicated to reducing injury through training, research, intervention, evaluation, and dissemination. The Center has published hundreds of studies on injury topics ranging from motor vehicle crashes to alcohol use to youth violence and suicide. The Center is part of the [Harvard School of Public Health](#).

The Means Matter Campaign is funded by [The Joyce Foundation](#) and the [David Bohnett Foundation](#).

Our Mission

The mission of the Means Matter Campaign is to increase the proportion of suicide prevention groups who promote activities that reduce a suicidal person's access to lethal means of suicide.

What's Happening In Your State?



Click on the map for state data on suicide, gun ownership, means restriction programs, and more

New Research

- (December 2010) A comparison of unplanned versus planned suicide attempters [More info](#)
- (November 2010) A change in Israeli military policy on firearm access off base is linked to a 40% reduction in military suicide rates. [More info](#)
- (June 2010) Being a resident or visitor to a high suicide region predicted suicide. The level of firearm use predicted suicide levels in both regions. [More info](#)
- (April 2010) A comparison of suicide rates before and after Australia passed a gun ban shows a decline coinciding with a reduction in availability of firearms. [More info](#)

Suicide Hotline



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Dean Williams
<yu78tr65@yahoo.com>

12/30/2010 07:25 PM

Please respond to
<dean@williams.name>

To <matt_c_fowler@dot.ca.gov>

cc

Subject yes on barriers

Mr. Fowler,

I am for suicide barriers on Cold Springs Bridge. I know someone who went there and was thinking about jumping. More than a year later, that person is still alive.

- Dean Williams



Michael Wolfe
<mike.wolfe@globalpwr.com>

01/04/2011 03:47 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Hello Matt, I live in Santa Barbara and support the barrier Cold Springs Bridge

Hello Matt, this is very important to me, I live just past Cold Springs Bridge and have deep roots in the community. We need to prevent suicides not just for the victims but for all the survivors in our community and their families.

Please convey my support

Mike Wolfe
VP Business Development
O: 805-683-3828 x306
M: 805-705-1868
Fax: 805-683-3823
5383 Hollister Avenue, Suite 100
Santa Barbara, CA 93111
mike.wolfe@globalpwr.com
<http://generators.globalpwr.com>



yolanda yturralde
<yolanda.mft@gmail.com>
12/30/2010 09:37 PM

To <matt_c_fowler@dot.ca.gov>
cc

Subject I support the railings

I do support railings on the Cold Springs Bridge.

Yolanda Yturralde
2131 Modoc Rd.
SB, cA 93101

please do not add me to any mailing lists



Jackie Zaldua
<jackiezalduas@yahoo.com>
01/24/2011 07:48 AM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge Barrier

Hello,

I am writing to say that I would like to see suicide barriers on the Cold Springs Bridge. I would like to see that something proactive is done to save a life. Please support this effort. Thank you.

Jackie Zaldua, M.A., RDI® Consultant
Supervisor, Verdugo Hills Autism Project

To learn more about RDI services visit: www.rdiguide.org



Ron Zamir
<ron.zamir@globalpwr.com>
01/05/2011 02:31 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Barriers on Cold Springs Bridge

Matt,

We need to move forward with the barriers on the Cold Springs Bridge without further delay.
How many lives do we need to spend?

Please do anything possible to move the project forward and to start saving lives.
Sincerely,

Ron Zamir
CEO
Global Power Supply
Phone: 805-683-3828 x309
Cell: 805-570-1239
ron.zamir@globalpwr.com
<http://generators.globalpwr.com>



aaron zweig
<2stableground@gmail.com>
01/04/2011 04:16 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject bridge fencing

matt,

i am in support for putting up a safety barrier on the cold springs bridge. it is such a simple
solution to impetuous and dangerous exposure. please count me in for fencing,

sincerely,

aaron zweig
338 s padre juan ave
ojai, ca 93023
805.798.1289

Response to comments from:

Rev. Alan R. Strout, First United Methodist Church of Santa Barbara, Sam S. Alfano, Retired Outdoor Management Officer, Los Padres National Forest, and 91 signature petition; Sally Hughes, LMFT), and 636 online petitioners, Michelle Arnerich, Alfred L. Aydelott, Karen Ayedelott, Jo Barrington, Bonnie Beedles, Ann Bennett, Alisa Benson, Jeff Bermant, Nancy Black, Rene' Block, Adriana Campos, Gail G. and Thomas K. Boehme, Larry Bogatz, Bruce James, Melissa Jones Cantekin, PhD, CA Psy 22622, Walton Clark, Jina Carvalho, Irma Catlett, Joyce Catlett, M.A, Child Mental Health Specialist, Ralph Chase, Becka Chester, Ed Csapo, Della Deats, Alvin Dias, John Douglas, Emily Dziedzic, Sarah Eichenbaum, Christine Farro, Steve Feinberg, Robert Feinberg, Sue Firestone, Tamsen Firestone, Carolyn Firestone, Nina Firestone, Steve Firestone, Michelle FitzGerald, Frank Oort, Barbara Franks, C. Fujimura, Janet Giler, Alannah Godwin, Barbara Grimmitt, Kenneth Hampian, Thomas Heck, Patrick Hines, Kevin Hines, Dave Hull, Ali Ironside, Sandra Jansen, Jean Johnson, Dana Kiesel, Jennifer Lake, including 636 online petitioners, Lisa LaPlaca, Jan Lewis, Joan Lisetor, Gwendolyn McClure, Cynthia McNulty, Arlene Moody, Rebecca Norton, David Ortiz, Heidi Ortiz, Geoff Parr, Debbie Peterson, Stephen Phillips, Jeff Pienack, Pietsch, Jennifer Purdy, Brian Robbins, John Robbins, Krystal Robbins, Mickie Robbins, Camerin Ross, Elizabeth Ross, Tamara Short, Robert Slayton, Richard Smith, Steve Smith, Joan Speirs, Sally Stewart, Maureen Sullivan, Paul Trent, Barbara Urbonas, Gil Varon, Andra White, Dayna Whitmer, Dean Williams, Mike Wolfe, Yolanda Yturralde, Jackie Zaldua, Ron Zamir, Aaron Zweig

Thank you for your comments. Your support for the project has been noted.



<deforek@aol.com>
01/23/2011 10:17 PM

To <Matt_C_Fowler@dot.ca.gov>
cc <ncginnes@es.ucsb.edu>
Subject Cold Spring Bridge:

Re: Cold Spring Bridge

January 23, 2011

Matt C. Fowler
Caltrans

Dear Mr. Fowler:

The State Office of Historic Preservation has recommended a horizontal net type barrier as a viable alternative. A net type suicide prevention solution has been approved for San Francisco's Golden Gate Bridge. A net solution was installed on a cliff in Bern, Switzerland in 1998. No deaths have been reported since.

1

Please seriously consider the net alternative especially along with raising the railing height. Much of the visual beauty and vistas that would be lost by a nine-foot seven-inch high vertical cage is retained.

2

The net only needs to support the weight of one person. The horizontal net barrier although visible from Stagecoach Road could be designed to blend into the bridge deck.

Thank you for your consideration.

Kellam de Forest
2651 Todos Santos Lane
Santa Barbara CA 93105

Response to comments from Kellam de Forest

Your opposition to the project has been noted.

Response to comment #1: Caltrans has reviewed information developed for the Golden Gate Bridge Barrier. The Golden Gate Bridge is a different type of bridge and is also in a completely different setting. When considering the different types of issues inherent for a horizontal net barrier design, as discussed above, versus the vertical grid/mesh design, the vertical grid/mesh is the preferred alternative.

A horizontal net barrier would have greater impacts on the National Register-eligible Cold Spring Canyon Bridge than the preferred alternative would. The bridge is eligible for its engineering qualities; the design and engineering of the substructure of the bridge are its most important character-defining features and the reason it has won engineering awards for its beauty. The design of the deck and railings are character-defining, but are less important than the substructure in conveying the engineering significance of the bridge.

The horizontal net barrier that has been chosen for the Golden Gate Bridge will still cause adverse impacts to that historic structure. Because of fundamental structural differences between the Golden Gate Bridge (a bulky, riveted suspension bridge hung from towers) and the Cold Spring Canyon Bridge (a slender, welded arch bridge supported by columns), a horizontal net barrier is feasible for the Golden Gate Bridge but not feasible for the Cold Spring Canyon Bridge without structural retrofit (physical modifications). In addition, the view from the Golden Gate Bridge is considered a character-defining feature – whereas it has been accepted by both the State Historic Preservation Officer and the Advisory Council on Historic Preservation that there are no “historic views” from the Cold Spring Canyon Bridge deck.

A horizontal net barrier will result in fewer adverse impacts to the Golden Gate Bridge than the alternatives that have been proposed for that structure, but it is incorrect to assume that a horizontal net would preserve the Cold Spring Canyon Bridge’s “historic and aesthetic qualities.” On the contrary, the installation of a horizontal net barrier would have adverse impacts to the bridge by obscuring portions of the substructure and by requiring a type of retrofit that would change the fundamental engineering of the structure – the engineering that makes the bridge eligible.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the bridge substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects the historic integrity of the National Register-eligible Cold Spring Canyon Bridge. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

Statements that the installation of a horizontal net barrier along with raising the height of the existing railing would protect the scenic value of the Cold Spring Canyon Bridge are speculative and unsubstantiated. Empirical evidence indicates that raising the height of the existing railing as suggested may also adversely affect views from the bridge. The current railing along the bridge is considered standard for the construction period of the structure.

Response to comment #2: The SEIR discusses in depth several types of horizontal net barriers and their effects on the Cold Spring Canyon Bridge. Five categories of horizontal net barriers [DSEIR, page 5] were considered as potential alternatives but eliminated from further discussion. Two refined designs were developed from these five categories of ideas: the Safety Net Alternative and the Cantilever Arc Barrier Alternative.

The Safety Net Alternative is developed in the Supplemental Report, which is included in the SEIR, Attachment 20. The Supplemental Report discusses in depth the impacts to the historic integrity of the structure as well as the evaluation of the adverse effects of the proposed Safety Net Alternative.

The Cantilever Arc Barrier Alternative is developed in the Feasibility Study included in the 2010 DSEIR, Attachment 39. This design, along with the constraints, was developed in conjunction with the State Historic Preservation Officer. The Feasibility Study discusses the impacts to the structure, including the impacts to the historic integrity of the structure.

In summary, horizontal net barriers have been rejected for the following reasons:

1. Unacceptable rescue response time

2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR. The suggestion of raising the existing bridge railings in combination with constructing a horizontal net barrier would likely cause an additional adverse effect by introducing a visual element that diminishes the property's historic integrity of design, feeling, and association. Increasing the height of the bridge rail would also not comply with the Secretary of Interior's Standards for Rehabilitation, Standards 2 and 9. Thus, inclusion of this aspect of the suggested alternative would further decrease the ability of the horizontal net barrier alternative to reduce impacts to historic resources.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to "Reduce the exposure to risks for emergency personnel..." In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff's office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

It should be noted that a horizontal net barrier would have to be designed for more than one person.



richard foster
<raepfoster@verizon.net>
12/11/2010 09:08 AM

To: <matt_fowler@dot.ca.gov>
cc:
Subject: Cold Springs Bridge Barrier

This is a complete waste of tax dollars. Determined suiciders will still use it. What's next? bars on all the windows of the Granada building? We need this money to go to road repairs or mental health programs. I came up on a vehicle right after a young man stopped and stepped over the barrier. He had a pick up truck with a camper shell. All he would need to do to get over your barrier would be step onto his truck hood, the roof, then the camper and presto he's past this barricade. Cal trans has done many fine landscaping projects in our area, we all appreciate the freeway widening and the recent resurfacing of 101. Keep up the good work but take a pass on this.
Richard Foster
7129 Del Norte Dr.
Goleta Ca.

1

2

Response to comments from Richard Foster

Your opposition to the project has been noted.

Response to comment #1: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #2: Regarding the possibility of persons using their vehicles to get over the proposed grid/mesh barrier, the purpose of the project is to help reduce the number of suicides at this location, not stop all suicides at this location.



Nancye Andriesse
<morningglory0204@cox.net>

To <Matt_C_Fowler@dot.ca.gov>

cc

01/03/2011 08:35 AM

Subject cold springs bridge.

**I have lived in Santa Barbara for 45 yrs. Going over the Cold Springs Bridge to Santa Inez valley has and still is, one of the most beautiful, natural drives in the world. It is almost like a huge back drop to a wonderful movie. The beauty of the bridge blends in with the scenery as if it came that way. My breath to this day is taken away with that vista. I believe that view must be preserved. There is no going back when that ugly high rise is added. To me it is like adding an elevated freeway in front of Yosemite's most treasured sights. I know there must be a way to put some sort of netting or deterrent under the bridge to try and prevent suicides. A horizontal barrier is what is needed. Personally, I think if people are intent on taking their lives they will do it someway or another. There are other places around to jump from, other ways to end their pain. Please reconsider attaching that monstrosity to our beautiful bridge and work from below. I am sure from our talented community we can come up with a better plan than Caltrans. Sincerely,
Nancye Andriesse 359 Greencastle cir Santa Barbara, Ca 93111**

1

2

Response to comments from Nancye Andriesse

Your opposition to the project has been noted.

Response to comment #1: A horizontal net barrier would have greater impacts on the National Register-eligible Cold Spring Canyon Bridge than the preferred alternative would. The bridge is eligible for its engineering qualities; the design and engineering of the substructure of the bridge are its most important character-defining features and the reason it has won engineering awards for its beauty. The design of the deck and railings are character-defining, but are less important than the substructure in conveying the engineering significance of the bridge.

It is incorrect to assume that a horizontal net would preserve the Cold Spring Canyon Bridge's "historic and aesthetic qualities." On the contrary, the installation of a horizontal net barrier would have adverse impacts to the bridge by obscuring portions of the substructure and by requiring a type of retrofit (physical modifications) that would change the fundamental engineering of the structure – the engineering that makes the bridge eligible.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the bridge substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects the historic integrity of the National Register-eligible Cold Spring Canyon Bridge. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

The proposed grid/mesh alternative is considered reversible (2009 FEIR page 37) since it can be removed at a later time, if an alternate solution arises in the future.

Response to comment #2: Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.



Jim Beltran
<jimbeltran@cox.net>
01/05/2011 09:28 AM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Suicide Barrier

Hello Mr. Fowler,

I grew up in Santa Barbara and am 8th generation from here. I know the history of the bridge and the suicides. I also know that when someone wants to commit suicide, they are going to do it.

I do not believe in the minority ruling the majority. I should not have to deal/put up with the needs of the very few. The percentage of people who have committed suicide compared to the people that live in the communities surrounding the bridge is miniscule. There were 60 suicides last year alone. More than the history of the bridge.

This is where Big Government gets in the way of the people. Quit trying to control us. Help is good, control, no good.

I feel for the families. I lost a niece to suicide (with a gun) a few years back. It hurt my sister terribly. Does that mean I support taking guns off the streets. No. She knew what she was doing and had planned it. If you put up barriers, these people will just find another bridge or some other way to commit suicide.

What happens if they pick a bridge that is not high enough? Then you now have a paraplegic relying on others (probably being supported by State taxes) for their care that didn't want to be here in the first place.

Don't control everyone to protect one from them self.

Jim Beltran

1

Response to comments from Jim Beltran

Your opposition to the project has been noted.

Response to comment #1: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion.

Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 Draft Environmental Impact Report (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties). Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

----- Forwarded by Matt C Fowler/D05/Caltrans/CAGov on 01/20/2011 04:17 PM -----



Rona Barrett
<rona@ronabarrett.com>
01/20/2011 09:42 AM

To <matt_c_fowler@dot.ca.gov>
cc

Subject Cold Springs Bridge

Dear Mr. Fowler:

I know this may sound very cold-hearted, but people who make up their minds to commit suicide, usually do so whether there is an obstacle in their path or not.

1

Having lost two friends over that bridge, I'm still not sure that a barrier needs to be put up. But the one you originally chose was really quite horrific! Your new idea seems like it could be more palpable. Are photos of other bridges using your new barrier idea available for the public to view? I have been travelling the Cold Springs Bridge for 25 years and the view still takes my breath away! If this new concept helped retain the beauty of the mountains and the views of this stretch of land, I could possibly vote for it.

2

Most sincerely,
Rona Barrett

Response to comments from Rona Barrett

Your opposition to the project has been noted.

Response to comment #1: As discussed in the 2009 FEIR on pages 18-24, studies by suicidologists indicate that barriers are effective in reducing suicides. Studies have shown that physical barriers on bridges help reduce suicides on bridges by jumping. Also, according to experts in the field of suicidology, there is evidence that people often do not go to another location to commit suicide.

Response to comment #2: Accurate photo-simulations of how the grid/mesh barrier will appear can be seen in the 2010 DSEIR and the Updated VIA [2010 DSEIR pages 14-19, Updated VIA Figs. 2-13].

A horizontal net barrier would have greater impacts on the National Register-eligible Cold Spring Canyon Bridge than the preferred alternative would. The bridge is eligible for its engineering qualities; the design and engineering of the substructure of the bridge are its most important character-defining features and the reason it has won engineering awards for its beauty. The design of the deck and railings are character-defining, but are less important than the substructure in conveying the engineering significance of the bridge.

It is incorrect to assume that a horizontal net would preserve the Cold Spring Canyon Bridge's "historic and aesthetic qualities." On the contrary, the installation of a horizontal net barrier would have adverse impacts to the bridge by obscuring portions of the substructure and by requiring a type of retrofit that would change the fundamental engineering of the structure – the engineering that makes the bridge eligible.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the bridge substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects the historic integrity of the National Register-eligible Cold Spring Canyon Bridge. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by

Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge.”



<lcantlay@aol.com>
01/22/2011 09:14 PM

To: <Matt_C_Fowler@dot.ca.gov>
cc

Subject: Horizontal barrier system.

Mr. Matt Fowler:

Friends of the Bridge is calling upon Caltrans to take the time needed to fully consider and select an appropriately designed horizontal barrier system for the Cold Spring Bridge, as proposed by the State Historic Preservation Officer and similar to the system that has been chosen for installation on the Golden Gate Bridge. A horizontal barrier system is a superior means of meeting all of the purposes of the Caltrans suicide barriers proposal for the Cold Spring Bridge without needlessly destroying the historic and aesthetic qualities for which it is widely renowned and treasured. We demand that Caltrans not cut corners in its haste to resume work on its deeply flawed and destructive tall vertical cage fencing barriers proposal.

Lynne Cantlay, PhD, MFT



Nancy Emerson
<fnemerson@verizon.net>
01/20/2011 02:48 PM

To <matt_c_fowler@dot.ca.gov>
cc

Subject Cold Springs Bridge

Hello,

I am writing about the Cold Spring Bridge and the suicide barrier. I am familiar with the site since I live in the Santa Ynez Valley. Suicide is a tragedy for family and friends, wherever it happens, but when it happens in a public place, it involves everyone in the tragedy, especially our government employees who must try to prevent suicides at public sites like this bridge or deal with the aftermath. So for these reasons, I agree with CALTRANS that it is important to prevent suicides from happening at this bridge. The horizontal alternative, while requiring additional engineering work, would enable the preservation of the bridge's vistas and historical aesthetic role and prevent suicides from happening here. I support the horizontal suicide barrier as the solution which will be acceptable to proponents and to most of the opponents.

I think it is important that a resolution satisfactory to the community be used since any barrier will be in place for a long time.

Nancy Emerson
2106 Creekside Dr
Solvang CA 93463

Response to comments from:

- Lynne Cantlay, PhD, MFT
- Nancy Emerson

Your opposition to the project has been noted.

The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge.

The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity. For the Cold Spring Canyon Bridge, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

Caltrans fully identified and assessed impacts to views from the Cold Spring Canyon Bridge in its analysis of project effects to the structure as a historical resource. The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to

its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the 2010 DSEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.



Sharyne Merritt
<pinot@sandpointvineyard.com>

01/05/2011 09:40 AM

To <matt_c_fowler@dot.ca.gov>
cc

Subject Cold Spring Bridge comment

Dear Mr. Fowler,

The the significant adverse impacts to visual/aesthetic resources are great enough to stop this project.

As I am certain you are aware, the Golden Gate Bridge suicide prevention project, with a history of far more suicides, has rejected the alternative of a suicide barrier because of visual/aesthetic impacts and opted for having a net. If you cannot reconsider the net, then I urge you to go with the no project alternative. I am not an expert on suicide, but I trust a considerable number of suicides will simply find another way to do it. The bridge does not cause suicides, it just provides a venue. Other venues are available and will be used.

1

2

Cal Trans money would save more lives making the rest of the 154 safe for drivers or by putting full time officers on the road to check for drunk drivers who are dangerous to others, not just themselves.

3

Thank you for considering my comments.

Sharyne Merritt, Ph.D.

Response to comments from Sharyne Merritt, PhD

Your opposition to the project has been noted.

Response to comment #1: A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. Caltrans fully identified and assessed impacts to views from the Cold Spring Canyon Bridge in its analysis of project effects to the structure as a historical resource. The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

Response to comment #2: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring

Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

However, as discussed in the 2009 FEIR on pages 18-24, studies by suicidologists indicate that barriers are effective in reducing suicides. Studies have shown that physical barriers on bridges help reduce suicides on bridges by jumping. Also, according to experts in the field of suicidology there is evidence that people often do not go to another location to commit suicide.

Response to comment #3: There are other projects on Route 154 designed to improve safety along the route. Caltrans is currently developing a safety project for the 154/246 intersection. The alternatives being considered for the 154/246 intersection include a roundabout and a signalized intersection. Centerline rumble strips have just been installed (in January 2011) along various segments of Route 154. The 154 Group II operational improvements project was also just completed and includes elements such as turn pockets for left turn movements from Route 154. There are officers who regularly patrol Route 154.



James Mills
 <lisleen@verizon.net>
 01/23/2011 02:46 PM

To <matt_c_fowler@dot.ca.gov>
 cc <mcginnes@es.ucsb.edu>, <lisleen@verizon.net>
 Subject Caltrans foray into mental health

Mr. Fowler:

I have driven Hwy 154 regularly since 1941. Over that period many lives have been lost there, often due to driver fault, but often due simply to the narrowness of cuts through which the roadway passes.

One can easily cringe, looking at the bicycle pathways of less than a foot space. Cyclists at risk are, for the most part, healthier younger people who should have years of useful, valued lives ahead of them. Sometime back an eight-year-old boy lost his life near Santa Clarita in a multiple car crash in a poorly lighted tunnel. If you have ever driven through the tunnel in Gaviota pass on the U.S. 101 you should feel some concern there, where better lighting could prevent a similar tragedy.

Seeing the many inept drivers on our highways one wonders what it would take to get Caltrans to make these highways safer. Charging Caltrans with making the highways safer is not working. A significant segment of suicidally inclined people are suffering with clinical depression. Their physicians and pharmacists, without fuss, stress to them avoidance of "noncompliance" in following their medication instructions. It is a public health problem.

1

Instead of following their charge, Caltrans has wasted years of time and an inexcusable amount of scarce funds in involving themselves where they are poorly qualified to act. If they had stuck to simply making the railings on the Coldspring Bridge safer for emergency personnel (CHP, fire, sheriff, et al.) instead of a ten foot monster, few would have objected.

2
3

It is more a matter of priorities, rather than aesthetics. Caltrans should get back on track where they have some experience, with the safety of people who anyone would presume would want to live. A few months back a man lost his life just a fraction of a mile east of the bridge when an eastbound "impaired" driver crossed over and hit his westbound car. If a small fraction of the money that has been blown by Caltrans had gone into a concrete center barrier along that route that man could still be alive.

4

Get real! Make the highways safer!!

Sincerely yours,

James G. Mills 1660 Fredensborg Way, Solvang, CA 93463 (805) 688-0712 .

Response to comments from James G. Mills

Your opposition to the project has been noted.

Response to comment #1: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #2: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #3: One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

Response to comment #4: As far as a median barrier along Route 154, a soft median barrier (centerline rumble strip) was completed in January 2011 at various locations along Route 154.



Jordan Mo
<jordanmo@verizon.net>
01/24/2011 11:28 AM

To <Matt_C_Fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge barrier

I wish to register my displeasure with the ongoing controversy over this ill-conceived project.

First of all, it is not needed. The Declaration of Independence cites the right to "life, liberty, etc." The clear extension of this idea is the right to die, or forego life. Many will be appalled at this interpretation, but it is my firmly held belief.

1

Second, any obstruction of the vista is a desecration of the bridge's original beauty.

2

Third, there are much better uses of taxpayer dollars. Mental health programs are negligently under-funded. (Don't tell me these funds are irrevocably assigned to highway use...they can be moved.)

3

Response to comments from Jordan Mo

Your opposition to the project has been noted.

Response to comment #1: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #2: It has been accepted by both the State Historic Preservation Officer and the Advisory Council on Historic Preservation that there are no “historic views” from the Cold Spring Canyon Bridge deck.

In designing a suicide barrier, Caltrans’ efforts have focused on minimizing impacts to the bridge substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects the historic integrity of the National Register-eligible Cold Spring Canyon Bridge. The Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

Response to comment #3: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).



Ann Brode
<ann.brode@54.org>
01/23/2011 03:33 PM

To <Matt_C_Fowler@dot.ca.gov>
cc
Subject cold springs bridge

Dear Sir,

I urge you to do all that you can to make sure that the historic nature of this bridge is preserved. I do believe there is a plan for a horizontal barrier that would protect safety and preserve the bridge view.

This means so much to the people who live here in Santa Barbara.

sincerely ,

Ann Brode

Response to comments from Ann Brode

Your opposition to the project has been noted.

A horizontal net barrier would have greater impacts on the National Register-eligible Cold Spring Canyon Bridge than the preferred alternative would. The bridge is eligible for its engineering qualities; the design and engineering of the substructure of the bridge are its most important character-defining features and the reason it has won engineering awards for its beauty. The design of the deck and railings are character-defining, but are less important than the substructure in conveying the engineering significance of the bridge.

It has been accepted by both the State Historic Preservation Officer and the Advisory Council on Historic Preservation that there are no “historic views” from the Cold Spring Canyon Bridge deck.

The installation of a horizontal net barrier would have adverse impacts to the bridge by obscuring portions of the substructure and by requiring a type of retrofit (physical modifications) that would change the fundamental engineering of the structure – the engineering that makes the bridge eligible.

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance

8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

In designing a suicide barrier, Caltrans’ efforts have focused on minimizing impacts to the bridge substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects the historic integrity of the National Register-eligible Cold Spring Canyon Bridge. The Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”



Michael Balaban
<mbalaban@impulse.net>
01/20/2011 08:16 AM

To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc Carolyn Balaban <cbalaban@impulse.net>
Subject Horizontal barrier for Cold Springs Bridge

We are long time residents of the Santa Ynez Valley. We fully support the "horizontal" option for a suicide barrier on the Cold Springs Bridge.

Sincerely,
Michael & Carolyn Balaban
2727 Mesa Verde Road
Santa Ynez, Ca.
93460
Sent from my iPhone

Response to comments from Michael and Carolyn Balaban

Your opposition to the project has been noted.

A horizontal net barrier would have greater impacts on the National Register-eligible Cold Spring Canyon Bridge than the preferred alternative would. The bridge is eligible for its engineering qualities; the design and engineering of the substructure of the bridge are its most important character-defining features and the reason it has won engineering awards for its beauty. The design of the deck and railings are character-defining, but are less important than the substructure in conveying the engineering significance of the bridge.

It has been accepted by both the State Historic Preservation Officer and the Advisory Council on Historic Preservation that there are no “historic views” from the Cold Spring Canyon Bridge deck.

The installation of a horizontal net barrier would have adverse impacts to the bridge by obscuring portions of the substructure and by requiring a type of retrofit that would change the fundamental engineering of the structure – the engineering that makes the bridge eligible.

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance

8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

In designing a suicide barrier, Caltrans’ efforts have focused on minimizing impacts to the bridge substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects the historic integrity of the National Register-eligible Cold Spring Canyon Bridge. The Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”



Leanne Schlinger
<leanne@santaynezvacationrentals.com>
01/24/2011 10:04 AM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Spring Canyon Bridge

Dear Mr. Fowler,

Thank you for taking the time to read my letter concerning the Cold Spring Canyon Bridge.

.....
Warmly,

Leanne S. Overton

Santa Ynez Vacation Rentals
Post Office Box 1704 | Santa Ynez, CA 93460
p 805.245.9667 | f 805. 456.1663
leanne@santaynezvacationrentals.com | www.santaynezvacationrentals.com

Please Note: * For your protection, our fax number is highly secured and all documents faxed to Santa Ynez Vacation Rentals (805-456-1663) will be delivered directly to this email address in a password protected PDF format. Please fax all documents as you would a normal fax.

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Caltrans cold spring bridge.doc

Leanne S. Overton
1651 Ballard Canyon Road
Solvang, CA 93463
Phone: 805-245-9667
leanne@santaynezvacationrentals.com

January 22, 2011

To: Matt Fowler

I have been a resident of Solvang for over 10 years and I also own a business in the Santa Ynez Valley (Santa Ynez Vacation Rentals). I have been reading and worrying about the construction of an unsightly barrier on the Cold Spring Bridge on Highway 154.

I strongly urge Caltrans to examine very carefully other architectural suicide barrier designs that would be more aesthetically pleasing to this historic and scenic bridge that leads into the beautiful Santa Ynez Valley and will have a negative impact to the scenic, unique beauty of one of the most beautiful highways in the area, Highway 154.

1

I ask strongly and urge Caltrans to consider other environmentally superior designs-alternative barriers that are available – namely a horizontal barrier. There are several variations – one of which was developed working with Caltrans engineers in Sacramento. Recently, such a barrier was chosen for the Golden Gate Bridge. Installation of a horizontal barrier along with raising the height of the substandard railing would meet the stated goals for the project while protecting the irreplaceable historic and scenic value of the Cold Spring Canyon Bridge.

2

Sincerely,

Leanne S. Overton



<angelaslater@grandmeadows.com>

01/22/2011 10:45 AM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Opposition to current Suicide Bridge design for Cold Springs Bridge

Dear Matt,

I do hope you/Caltrans will consider an alternative suicide barrier design being proposed for Cold Spring Bridge on scenic highway 154.

Sincerely, Angela Slater



Angela Slater

Founder

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Angela M Slater
1651 Ballard Canyon Road
Solvang, California 93463
Fax: 805-688-1312
Phone: 805-688-2826
angelaslater@grandmeadows.com

January 22, 2011

To: Matt Fowler

I have been a resident of Solvang for over 10 years. I have been reading and worrying about the construction of an unsightly barrier on the Cold Spring Bridge on Highway 154.

I strongly urge Caltrans to examine very carefully other architectural suicide barrier designs that would be more aesthetically pleasing to this historic and scenic bridge that leads into the beautiful Santa Ynez Valley and will have a negative impact to the scenic, unique beauty of one of the most beautiful highways in the area, Highway 154. 1

I ask strongly and urge Caltrans to consider other environmentally superior designs- alternative barriers that are available – namely a horizontal barrier. There are several variations – one of which was developed working with Caltrans engineers in Sacramento. Recently, such a barrier was chosen for the Golden Gate Bridge. Installation of a horizontal barrier along with raising the height of the substandard railing would meet the stated goals for the project while protecting the irreplaceable historic and scenic value of the Cold Spring Canyon Bridge. 2

Sincerely,

Angela Slater

Response to comments from:

- Leanne S. Overton
- Angela Slater

Your opposition to the project has been noted.

Response to comment #1: The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge.

The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity. For the Cold Spring Canyon Bridge, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

Caltrans fully identified and assessed impacts to views from the Cold Spring Canyon Bridge in its analysis of project effects to the structure as a historical resource. The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to

its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

Response to comment #2: The current railing along the bridge is considered standard for the construction period of the structure. The suggestion of raising the existing bridge railings in combination with constructing a horizontal net barrier would likely cause an additional adverse effect by introducing a visual element that diminishes the property’s historic integrity of design, feeling, and association. Increasing the height of the bridge rail would also not comply with the Secretary of Interior’s Standards for Rehabilitation, Standards 2 and 9. Thus, inclusion of this aspect of the suggested alternative would further decrease the ability of the horizontal net barrier alternative to reduce impacts to historic resources.



melissa ramsey
<mymuse.melissa@gmail.com>

01/04/2011 04:49 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Barriers input re Cold Springs Bridge

Dear Mr. Fowler:

Please leave Cold Spring Bridge exactly as it is. What would save far more lives are barriers dividing the road at spots where drunken idiots kill people who DONT want to commit suicide. That is a far better use of limited financial resources and much more easily achieved. Then we could use the money saved to better fund suicide prevention efforts, and put more chp officers on the roads.

Sincerely,

Melissa Ramsey
275 Rosario Park Road
Santa Barbara, CA 93105

Response to comments from Melissa Ramsey

Your opposition to the project has been noted.

As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).



Selma Rubin
<sosrubin@silcom.com>
01/23/2011 09:10 PM

To <Matt_C_Fowler@dot.ca.gov>
cc
Subject Cold Spring Bridge

Mr. Fowler,

I am submitting my comments to consider further a more appropriate design horizontal barrier system for the

the Cold Spring Bridge. I want a superior system similar to the one proposed by the State Historical Preservation

1

Officer for the Golden Gate Bridge. It is a superior system that will answer the Caltrans suicide barrier

proposal for Cold Spring Bridge. I have a great concern that the aesthetic and historical qualities be left for

public enjoyment. The tall vertical cage fencing barriers proposal is deeply flawed. I am asking you to stop

2

working on it and substitute the horizontal barrier system.

I would like a response from you in reference to my comments.

Thank you.

Sincerely,
Selma Rubin

Response to comments from Selma Rubin

Your opposition to the project has been noted.

Response to comment #1: A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Golden Gate Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and

Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting (physical modifications) the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The

Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Response to comment #2: Your opposition to the project has been noted.



Gerry Schroeder
 <gerryschroeder@gmail.com>
 >
 01/24/2011 02:34 PM

To <matt_c_fowler@dot.ca.gov>
 cc
 Subject Opposition to Suicide Barrier on Cold Springs Bridge

To Whom it May Concern:

My name is Gerald Schroeder and I am a psychologist in private practice in Santa Barbara, CA. I live in Solvang and commute to Santa Barbara by 154.

I AM AGAINST ANY SUICIDE BARRIERS ON THE COLD SPRINGS BRIDGE.

Unfortunately suicide is a fact of life. People kill themselves not bridges. People use medication to kill themselves, so according to your reasoning, we should do away with medication. Since people use guns to kill themselves, we should also do away with guns. And since people use automobiles to kill themselves, should we ban automobiles? Your plan is a complete waste of money and based on false assumptions. When people are truly committed to killing themselves, they will find a way.

1

If you are truly interested in suicide prevention, lend your support to people who are considering suicide in their lives.

2

If you are truly concerned about safety on 154, stop the aggressive large truck traffic which actually does kill people.

3

To make the road safer, strictly enforce laws against drinking and driving and stop intoxicated drivers who also kill people going to and from the Chumash Casino.

The bridge is beautiful as it stands. Don't make unnecessary changes to its structure and aesthetics.

Thanks for your consideration,

Gerald Schroeder, Ph.D.

Response to comments from Gerald Schroeder, PhD

Your opposition to the project has been noted.

Response to comment #1: The purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the 2009 Final Environmental Impact Report Summary and Chapter 1 for a full discussion of the project's Purpose and Need. Physical suicide barriers have been shown to be effective on bridges, as described in Chapter 2 of the 2009 FEIR. Our findings are consistent with the consensus of experts in the field of suicidology, as documented in Chapter 2 of the 2009 FEIR including the section titled "Difference of opinion regarding the effectiveness of physical suicide barriers vs. 'human barriers.'" "

Response to comment #2: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #3: California Highway Patrol in partnership with Caltrans is working towards improving truck safety on Route 154. This effort is outside the scope of the purpose and need for this project.



Patricia Simon
<prsimon417@gmail.com>
01/19/2011 04:13 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge Suicide Barrier

Dear Mr. Fowler:

Attached is my comment card referring to the proposed suicide barrier to be built at the Cold Spring Bridge in the Santa Ynez Valley.

I
Should there be any problem opening my attachment it states the following:

I AM OPPOSED TO CALTRANS' PROPOSED VERTICAL BARRIER AT COLD SPRINGS BRIDGE. I PREFER "NO-BUILD". HOWEVER, IF SOMETHING MUST BE ERECTED CHOOSE THE "HORIZONTAL NET BARRIER" INSTEAD.

1

I would also like to add that in these financially devastating times, I believe this is an absolutely wasteful and irresponsible use of substantial sums of money because of 1.2 deaths per year on the bridge! Better spent doing mental health programs in schools or alcohol treatment centers which address the roots of suicidal problems and would save hundreds of lives lost each year.

2

This is the height of foolishness.

Patricia Simon, LMFT

Information from ESET Smart Security, version of virus signature database 5801 (20110119)

The message was checked by ESET Smart Security.

<http://www.eset.com>



IMAGE1.JPG

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA **154**

2011

COMMENT CARD

NAME: PATRICIA SIMON
 ADDRESS: 905 CROFT LANE CITY: SOLVANG ZIP: 93463
 REPRESENTING: MYSELF

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the Comment Box or

Mail to: ~~Cathy Stettler~~ MATT C. FOWLER MATT.C.FOWLER@DOT.CA.GOV
 Senior Environmental Planner
 Department of Transportation
 50 Higuera St.
 San Luis Obispo, CA 93401

I would like the following comments filed in the record (please print):

I AM OPPOSED TO CALTRANS' PROPOSED VERTICAL BARRIER
@ COLD SPRINGS BRIDGE. I PREFER "NO BUILD" - HOWEVER
IF SOMETHING MUST BE ERECTED CHOOSE THE "HORIZONTAL
NET BARRIER" INSTEAD.

1

(Please respond by ~~June 24, 2008~~
 JANUARY 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: WE WATCH

Response to comments from Patricia Simon, LMFT

Your opposition to the project has been noted.

Response to comment #1: The No-Build Alternative does not meet the purpose of this project and therefore has not been identified as the preferred alternative.

The 2010 DSEIR discusses in depth several types of horizontal net barriers and their effects on the Cold Spring Canyon Bridge. Five categories of horizontal net barriers [2010 DSEIR, page 5] were considered as potential alternatives but eliminated from further discussion. Two refined designs were developed from these five categories of ideas: the Safety Net Alternative and the Cantilever Arc Barrier Alternative.

The Safety Net Alternative is developed in the Supplemental Report, which is included in the SEIR, Attachment 20. The Supplemental Report discusses in depth the impacts to the historic integrity of the structure as well as the evaluation of the adverse effects of the proposed Safety Net Alternative.

The Cantilever Arc Barrier Alternative is developed in the Feasibility Study included in the SEIR, Attachment 39. This design, along with the constraints, was developed in conjunction with the State Historic Preservation Officer. The Feasibility Study discusses the impacts to the structure, including the impacts to the historic integrity of the structure.

In summary, horizontal net barriers have been rejected for the following reasons:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.



Judi & Rick
<rjshow@gotsky.com>
01/22/2011 05:35 PM

To <Matt_C_Fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge

Friends of the Bridge is calling upon Caltrans to take the time needed to fully consider and select an appropriately designed horizontal barrier system for the Cold Spring Bridge, as proposed by the State Historic Preservation Officer and similar to the system than has been chosen for installation on the Golden Gate Bridge. A horizontal barrier system is a superior means of meeting all of the purposes of the Caltrans suicide barriers proposal for the Cold Spring Bridge without needlessly destroying the historic and aesthetic qualities for which it is widely renowned and treasured. We demand that Caltrans not cut corners in its haste to resume work on its deeply flawed and destructive tall vertical cage fencing barriers proposal.

. . . out of wonder into wonder . . . existence opens — Lao Tsu

Judi Stauffer & Rick Hubbard
rjshow@gotsky.com
805 688 5477

Response to comments from Judi Stauffer and Rick Hubbard

Your opposition to the project has been noted.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Golden Gate Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and

Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting (physical modifications) the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The

Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

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8. Increased costs associated with need to replace safety net.



Judi & Rick
<rjshow@gotsky.com>
01/23/2011 01:47 PM

To <Matt_C_Fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge

Dear Mr. Fowler,

As a resident of the Santa Ynez Valley, I respectfully request that Caltrans be instructed to take the time necessary to fully consider and select an appropriately designed horizontal barrier system for the Cold Springs Bridge.

My understanding is that the State Historic Preservation officer has also made this request, and that a similar horizontal barrier system will be installed on the Golden Gate Bridge.

It is my belief that while we have an obligation to promote safety, it must be balanced by other considerations. In this case, by historic and aesthetic factors must also be considered — particularly when a system exists, such as the horizontal barrier, that has been proven to be a superior deterrent for suicides.

The public needs transparent assurances that the best all-around solution will be approved and implemented. Allowing Caltrans to cut corners in its haste to resume work on an ill-conceived vertical-cage fencing barrier is a very poor substitute. We deserve much better.

I thank you for your time in reading this letter, and ask that you acknowledge its receipt. My contact information is below.

judi

judi stauffer
1610 cougar ridge road
buellton california 93427
805 688 5477 PHONE
rjshow@gotsky.com

Response to comments from Judi Stauffer

Your opposition to the project has been noted.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

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1. Unacceptable rescue response time
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5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Linda Taibe
 <taibecats@starband.net>
 01/11/2011 10:30 AM

To "Matt Fowler, Senior Environmental Planner"
 <Matt_Fowler@dot.ca.gov>
 cc

Subject Cold Spring Bridge

Dear Mr. Fowler:

Several years ago, when Cal Trans first began talking about putting a cover on the bridge to prevent suicide, I wrote a letter to the Santa Barbara News Press which was published, objecting to covering the bridge, first because of the cost and secondly, because if someone wants to commit suicide, they are going to succeed at some point in time. Our economy is far worse today than when this first came up. I feel that if Cal Trans wants to do something helpful, make those funds - \$648,000 available through the County Mental Health Department for people to get help through counseling and medication. Mental Health is an area that has been severely impacted by the recession, plus Santa Barbara County has a tremendous short fall of funds. You are probably wondering why I would presume to know about suicide. Well our family is a victim of suicide. My nephew, who was a straight A student at Point Loma University, committed suicide 5 years ago at age 21. He talked frequently about killing himself over a 2-3 year period, we had him hospitalized and in therapy. I always thought if someone talked about killing themselves, they would never do it and how wrong I was to assume that. It is something a family never recovers from - especially the parents of the child who has taken their life. I urge you to put this money to better use than covering the bridge, give it to Mental Health Dept. or fix our highways and roads, do something else with it. If someone wants to end their life, putting a cover on that bridge won't stop them!!

1
 2

Sincerely,

Linda L. Taibe

Response to comments from Linda L Taibe

Your opposition to the project has been noted.

Response to comment #1: As stated in the 2009 Final Environmental Impact Report, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 Final Environmental Impact Report (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #2: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.



Bob <rwellskuntz@cox.net>

01/19/2011 12:14 AM

Please respond to
<rwellskuntz@cox.net>

To <Matt_C_Fowler@dot.ca.gov>

cc

Subject Cold Spring Bridge - Opposition to Caltrans Proposal

Dear Mr. Fowler:

I am writing to express my strong opposition to the tall vertical cage fencing barriers proposed by Caltrans for the Cold Spring Bridge. I favor the installation of horizontal barriers of an appropriate design, along the lines of those recently approved for installation on the Golden Gate Bridge and proposed by the State Historic Preservation Officer.

Please acknowledge receipt of this e-mail note. Thank you.

Sincerely,

Robert W. Kuntz
183 Carlo Drive
Goleta, CA 93117-2050
805-964-4986
rwellskuntz@cox.net

Response to comments from Robert W Kuntz

Your opposition to the project has been noted.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Golden Gate Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and

Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting (physical modifications) the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The

Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Tim Gill <timgill@cox.net>

01/22/2011 06:33 PM

To <Matt_C_Fowler@dot.ca.gov>

cc

Subject Cold Springs Bridge needs a horizontal barrier system

I support "Friends of the Bridge" in calling upon Caltrans to take the time needed to fully consider and select an appropriately designed horizontal barrier system for the Cold Spring Bridge, as proposed by the State Historic Preservation Officer and similar to the system that has been chosen for installation on the Golden Gate Bridge. A horizontal barrier system is a superior means of meeting all of the purposes of the Caltrans suicide barriers proposal for the Cold Spring Bridge without needlessly destroying the historic and aesthetic qualities for which it is widely renowned and treasured. We demand that Caltrans not cut corners in its haste to resume work on its deeply flawed and destructive tall vertical cage fencing barriers proposal. Your existing plan may work but would be very unsightly. Why not consider a better aesthetic? When you look at the beauty of Cold Springs bridge, it seems clear that taking the extra effort is imperative. Thank you for your consideration.

Sincerely,

Tim Gill

Response to comments from Tim Gill

Your opposition to the project has been noted.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Golden Gate Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and

Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting (physical modifications) the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The

Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Anna Kokotovic
<annamk@cox.net>
01/23/2011 09:35 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject bridge

I am against building a barrier on the bridge. There are better ways of preventing suicide, including building better mental health services throughout the county. Use the funds to support community mental health services!

Anna M. Kokotovic
Psychologist

~~~~~  
Anna M. Kokotovic, Ph.D.  
Psychologist

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***Response to comments from Anna M. Kokotovic, PhD, Psychologist***

Your opposition to the project has been noted.

As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.



<fpdiani@aol.com>  
01/22/2011 08:41 PM

To <matt\_c\_fowler@dot.ca.gov>  
cc <fpdiani@cox.net>  
Subject "The Suicide Bridge"

January 22, 2011

**Some Considerations in order of importance, in our view, for the Suicide Prevention Modification to the Cold Springs Bridge:**

- 1) Prevention Effectiveness
- 2) Ease of recovering those who insist on making the final 'decision'
- 3) Safety of the recovery team
- 4) Ease and cost of design
- 5) Ease, safety to the installers and cost of implementation
- 6) Visual "amenities"

It is not clear from the previewed material how extensively the above considerations were studied. "We" tried to download the DSEIR, to get more details, but it took more than an hour to get about 65% of the PDF file – I gave up. Besides, I wanted mainly to see the 'alternatives studies section' but that did not appear to be separable from the mass of all the other material and was not easy to find in the PDF document.

1

It *appears* from the more "easily" available literature that only non-quantitative trade off material is available to the general public. Admittedly, since the decision appears at this writing to be a foregone conclusion, we did not scour the web in the search.

2

It *also appears* that the *apparent* "winner[s]" (there were photos of three almost identical constructs online) of the trade off studies, though not really pleasing to the eye, may well be the easiest and cheapest to implement. Whether this design will be the most effective deterrent ... only time will tell. A return to a more solid moral set of values in our society, in my opinion, would be most effective way to prevent suicides.

~~~~~

**F&F Diani
Goleta**

Response to comments from F&F Diani

Your comments on the project have been noted.

Response to comment #1: The supplemental document was available in multiple formats and multiple locations for review including the public library, on CD and on the Caltrans website. Copies of the document were also available on request for individuals that preferred an alternate format.

Response to comment #2: The 2010 DSEIR was circulated in response to a lawsuit from Friends of the Bridge and only contained those sections from the 2009 FEIR that needed to be updated. A full discussion of all alternatives considered was provided in the 2009 FEIR and incorporated by reference. The 2009 FEIR was also available in multiple formats, in multiple locations, for review including the Caltrans website and Santa Barbara area libraries. The alternatives section in the SEIR analysis was provided to present the additional variations that were analyzed in relation to the horizontal net alternative prior to approval of the 2009 FEIR. However, this alternative, including the variations discussed, was not found to be feasible.

WILLIAM HOWARD WITTAUSCH
ARCHITECT ♦ CIVIL ENGINEER
P.O. Box 267 SANTA BARBARA, CA 93102
TEL (805) 898-8766 FAX (805) 898-1177
howard@whwace.com

January 21, 2010

Matt Fowler, Senior Environmental Planner
California Department of Transportation District 5
50 Higuera Street
San Luis Obispo, CA 93401

Re: Cold Spring Canyon Bridge Suicide Barrier
Draft Supplemental Environmental Impact Report

Dear Mr. Fowler,

CALTRANS Draft Supplemental EIR (DSEIR) presented to the public in Santa Barbara on January 18, 2011 rejects one horizontal safety net option on the basis of hypothetical response scenarios, structural alterations, load limitations, liability, risk, and costs associated with this option. 1

State Highway 154 is the historic link between Santa Barbara County's South Coast and the Santa Ynez Valley and the Cold Spring Canyon Bridge provides a unique aesthetic and scenic experience. The proposed fence barrier would forever alter and adversely impact these qualities as well as it's historic importance as a significant example of mid-twentieth century Modernism and the work of California State's Division of Highways Bridge Department and American Bridge Division of United States Steel Corporation.

Studies show that a fence barrier can physically prevent suicide victims from jumping but may not actually reduce the number of suicides within a specific given community due to "displacement" - suicide in another location - or "substitution" - suicide by other means. Safety nets have been successfully installed on other bridges with positive results in terms of reducing suicides by jumping. Safety nets are proposed for the Golden Gate Bridge, the West Coast landmark gateway to San Francisco Bay. A safety net may actually reduce the number of suicides by interfering with a victims thought process and/or substituting minor injuries for certain death after jumping. 2

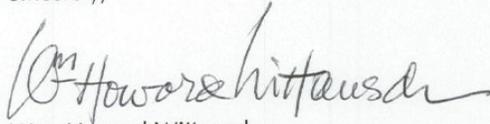
DSEIR does not acknowledge input from any mental health professionals or organizations with respect to suicide prevention. This omission seems odd in that the primary purpose of the project is to prevent/reduce the number of suicides at the subject location. Other than a preliminary budget estimate, DSEIR also does not document the findings that led to rejection of the safety net. As an architect and civil engineer, and as a member of the Santa Barbara County Historic Landmarks Advisory Commission (HLAC), omission of this input and documentation does not appear to be in the public interest. Please recall in its letter dated June 18, 2008 requesting that the safety net be considered, HLAC also restated its preference for the "no-build" option. 3

Matt Fowler
January 21, 2011
Page 2

This letter therefore supports Marc Chytilo's letter dated January 18, 2011 requesting an extension of the public review period as needed to allow time for CALTRANS to assemble the necessary supporting technical documentation for the Cantilever Arc Barrier Net (i.e. load analysis, structural calculations, preliminary budget estimates) or other safety net options, and for the public and HLAC to avail itself of "expert analysis" prior to the next public meeting.

4

Sincerely,



Wm. Howard Wittausch
Architect Civil Engineer

cc: John Woodward, Chair HLAC
Marc Chytilo, Attorney Friend of the Bridge

Response to comments from Wm Howard Wittausch, Architect Civil Engineer

Your opposition to the project has been noted.

Response to comment #1: The SEIR was prepared in accordance with the Judgment of the Superior Court of California for the County of Santa Barbara. In its Judgment, the court ruled that the DEIR impermissibly deferred the development of measures mitigating impacts to cultural and visual/aesthetic resources to the 2009 FEIR, thereby effectively precluding any public comment about or public participation in the development of such mitigation measures. The DSEIR was prepared and publicly circulated to comply with the court's Judgment and Writ issued thereon. The 2008 DEIR (pages 6-14) and 2009 FEIR (pages 7-15), incorporated by reference in the 2010 DSEIR, contains analyses of Alternatives, Environmentally Superior Alternative, Preferred Alternative, and Alternatives Considered but Eliminated From Further Discussion, etc.

The grid/mesh alternative is designed to be reversible, with minimum impacts to the historical fabric of the bridge, if the panels were to be removed (2009 FEIR page 37).

The safety net alternative was considered but rejected in the DEIR, FEIR (Section 1.4.6 Alternatives Considered but Eliminated From Further Discussion, pp 10-12), and 2010 DSEIR (Section 1.5.1, pp 5-7).

For clarity, the open forum public hearing for the 2010 DSEIR occurred in Santa Barbara on January 5, 2011, not on January 18, 2011.

Response to comment #2: The purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Draft Environmental Impact and FEIR Summary and Chapter 1 for a full discussion of the project's Purpose and Need. Physical suicide barriers have been shown to be effective on bridges, as described in Chapter 2 of the 2008 DEIR and 2009 FEIR. Our findings are consistent with the consensus of experts in the field of suicidology, as documented in Chapter 2 of the 2008 DEIR and 2009 FEIR.

As far as the DSEIR not acknowledging input from mental health professionals, this issue has been addressed in the 2009 FEIR, which is included in the SEIR by reference. The purpose of the DSEIR is discussed in Chapter 1 of the SEIR. Studies

by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the DEIR (pages 17-23) and FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

Response to comment #3: The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the DSEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Cold Spring Canyon Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would. As discussed above, views from the Cold Spring Canyon Bridge are not a character-defining element of the historical resource, and the impacts of the vertical barrier are therefore limited to diminishing the bridge's integrity of design, feeling, and association, as compared to impacts of a safety net barrier.

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

Response to comment #4: Comment noted.



<anacitrin@cox.net>
01/24/2011 03:39 PM

To <matt_c_fowler@dot.ca.gov>
cc <rich_krumholz@dot.ca.gov>
Subject DSEIR Comments - Cold Spring Canyon Bridge Suicide
Barrier

Mr. Fowler,

Attached please find a comment letter on the DSEIR for the Cold Spring Canyon Bridge Suicide Barrier, submitted on behalf of Friends of the Bridge. Kindly acknowledge receipt of this letter within the comment period by return email, or if you have any problems opening the document please let me know and I will resend it.

Thank you,

Ana Citrin
Law Office of Marc Chytilo
P.O. Box 92233
Santa Barbara, CA 93190
Phone: (805) 570-4190
Fax: (805) 682-2379

* * * * *

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* * * * *



LOMC CSB DSEIR Comment Letter 1-24-11 w. Exhibits 1-8.pdf

LAW OFFICE OF MARC CHYTILO

ENVIRONMENTAL LAW

January 24, 2011

Matt Fowler, Senior Environmental Planner
California Department of Transportation District 5
50 Higuera Street
San Luis Obispo, CA 93401

By email to matt_c_fowler@dot.ca.gov

RE: Cold Spring Canyon Bridge Suicide Barrier, Draft Supplemental Environmental Impact Report

Dear Mr. Fowler,

This office represents Friends of the Bridge ("Friends") in this matter. Friends successfully challenged the adequacy of Caltrans' environmental review of the Cold Spring Canyon Bridge Suicide Barrier Project ("Project"), resulting in Santa Barbara Superior Court Judge Anderle ordering the recirculation of a draft environmental impact report (EIR) for further public review. The crux of Judge Anderle's ruling was that Caltrans deprived the public of the legally required opportunity to review and comment on an adequate draft EIR. We are therefore quite disappointed that the Draft Supplemental Environmental Impact Report (DSEIR) again lacks relevant information critical to the public and responsible agencies' understanding of the Project and the feasibility of alternatives. In this letter we address the material flaws and omissions that we encountered in the DSEIR, and outline potential solutions that we urge Caltrans to implement to avoid any further CEQA inadequacies.

1. Request for Public Hearing Prior to Project Approval

When Caltrans' District 5 Director Krumholz certified the previous FEIR for the Project and approved the Project, there was no opportunity for the public to either review the FEIR, or communicate with the decisionmaker. Further, the prior FEIR was not released to the public until after the Project had been approved, allowing the public no opportunity to review the agency's responses to their comments on the draft, or communicate any additional concerns to Caltrans staff. The public also had no opportunity to review or comment on the agency's draft findings. This process is very different from the typical approval process before local agencies and many state agencies where the public has the opportunity to review the FEIR and draft findings, and address the decisionmaker at an approval hearing. Moreover, the public hearing on the former DEIR and on the DSEIR that occurred on January 5, 2011 allowed the public no opportunity to address the decisionmaker, or even to air comments in a formal setting such that individuals could listen to the comments of others. These circumstances all contributed to the pervasive feeling surrounding the last EIR that the public had been excluded from the process. The CEQA process appears, once again, to offer little more than a post hoc rationalization rather than an integral part of agency decisionmaking.

1

LAW OFFICE OF MARC CHYTILO
P.O. Box 92233 • Santa Barbara, California 93190
Phone: (805) 682-0585 • Fax: (805) 682-2379
Email(s): airlaw5@cox.net (Marc); anacitrin@cox.net (Ana)

Mr. Fowler
January 24, 2011
Page 2

Accordingly, we strongly urge Caltrans to a) release the FEIR at least 30 days prior to the Project approval hearing to, *inter alia*, enable the public to review Caltrans responses to their comments, b) release draft findings to the public at least 15 days prior to project approval, and c) hold an approval hearing at which members of the public may address Director Krumholz before any final action is taken.

2. Evaluation and Response to Comments

CEQA Guidelines § 15088 requires that Caltrans evaluate comments received on the DSEIR and provide a written response that “describes the disposition of significant environmental issues raised (e.g., revisions to the proposed project to mitigate anticipated impacts or objections).” “In particular, the major environmental issues raised when the lead agency’s position is at variance with recommendations and objections raised in the comments must be addressed *in detail* giving reasons why specific comments and suggestions were not accepted.” (CEQA Guidelines § 15088 (c) (*emphasis added*)). “There must be good faith, reasoned analysis in response. Conclusory statements unsupported by factual information will not suffice.” (*Id.*). The evaluation and response to public comments is an essential part of the CEQA process, and failing to comply with CEQA Guidelines § 15088 can be grounds for the issuance of a writ of mandate to set aside an approval decision. (Remy et al., *Guide to CEQA* (11th ed., 2007), p. 371; *Envtl. Prot. Info. Ctr. v. Johnson* (1985) 170 Cal. App. 3d 604, 627).

Friends expects that Caltrans will respond to the comments raised herein and comments raised by other members of the public and by government agencies with good faith reasoned analysis and the level of detail required by CEQA. Where, for example, specific questions are posed regarding alternatives to the grid-mesh barrier, we expect that the comment response will not merely repeat conclusory statements included in the DSEIR for Caltrans’ initial rejection of those alternatives. Further, as requested in section 1 above, we expect that Caltrans will make the FEIR available to the public with sufficient time for the public to review the responses to comments, and communicate with Caltrans staff and/or Director Krumholz regarding any issues raised by the responses.

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3. Impact Analysis

a. Flawed Visual/Aesthetic Impact Analysis

i. Failure to Evaluate the Project’s Consistency with the Corridor Protection Plan for Highway 154

Highway 154 is an Officially Designated State Scenic Highway. (DSEIR p. 10). The DSEIR and updated Visual Impact Assessment (VIA) included in the DSEIR describe the State’s Scenic Highway program as designated routes based on high-quality views of the natural landscape along the route and on the local governing body’s implementation of a Corridor Protection Plan. (DSEIR p. 10; DSEIR Attachment G, p. 4). The documents further describe the Corridor Protection Plan as

including policies and ordinances addressing land use, design review, billboards, earthwork and landscaping, and utility structures. (*Id.*) The VIA articulates several statements included in the State Scenic Highway program, including that “Caltrans works with appropriate agencies to ensure the protection of scenic corridors *to the maximum extent feasible.*” (VIA p. 4). The document does not however evaluate whether the Project is consistent with this Policy, and for reasons discussed in section 5.a and 4.a, below, it appears that the Project is inconsistent with this policy, given that feasible mitigation measures and/or alternatives could reduce or avoid impacts to the Highway 154 scenic corridor.

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Neither the VIA nor the body of the DSEIR articulate any of the policies or ordinances in the Corridor Protection Plan for Santa Barbara County. An example of policies adopted by Santa Barbara County as part of its Corridor Protection Plan include the Scenic Highway Element of the General Plan, the goals of which include “To enhance and preserve the valuable scenic resources located along roadways within the County” and “To help maintain the economic contribution of tourism to the County”. (Scenic Highway Element¹, p. 6). Additionally, the Scenic Highway Element provides that the guide to scenic highway implementation prepared by the State Department of Transportation defines a Rural Designated Scenic Highway as “a route that traverses a defined visual corridor within which *all natural scenic resources and aesthetic value are protected and enhanced.*” (*Id.* p. 5, *emphasis added*). The Scenic Highway Element also outlines the nature of the regulations and other controls applicable to designated State Scenic Highways, including the D Design Control Overlay District in the Zoning Ordinance. (*See Id.* p. 9). The DSEIR/VIA does not discuss the Project’s consistency with any of these applicable policies or ordinances.

The DSEIR is defective for failing to analyze the Project’s consistency with State Scenic Highway program policies and for failing to disclose and analyze State Route 154 Scenic Highway Corridor Protection Plan policies. The Responses to Comment must address these deficiencies with reasoned analysis and factual support and if the response reveals significant new information, CEQA requires recirculation of the DSEIR. (CEQA Guidelines § 15088.5).

ii. Understatement of Visual Impacts to the State Scenic Highway

The DSEIR severely understates the Project’s significant impact on the visual resources of the State Scenic Highway as a whole, and it contains contradictory statements regarding the quality of views from Cold Spring Canyon Bridge and their contribution to the scenic quality of Highway 154 generally. For example, the DSEIR states the following regarding views from Cold Spring Canyon Bridge as compared to views along the majority of State Route 154:

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The project crosses Cold Spring Canyon, which allows sweeping vistas of the Santa Ynez Valley and mountains beyond. Throughout the region, vegetation is a primary component of overall visual character. Along much of State Route 154, the topography and density of the

¹ Available at <http://longrange.sbcountyplanning.org/programs/genplanreformat/PDFdocs/ScenicHwys.pdf>

existing roadside vegetation blocks long-range views to and from the highway. In the vicinity of the project, however, the sloping topography and bridge elevation allow expansive views unhindered by roadside trees.

(DSEIR, p. 11). The “Summary of Visual/Aesthetic Impacts” that follows downplays the significance of views from the Bridge, stating, “high-quality views from the highway while not on the bridge would remain mostly intact”. (DSEIR p. 20). Clearly the *quality* of views from the majority of Highway 154 are distinct from the sweeping vistas from Cold Spring Canyon Bridge, as confirmed by the above reference on DSEIR p. 11, and the Project would obstruct one of few sweeping vistas available from Highway 154.

When properly evaluated, the Project has substantially greater visual impacts in the form of serious degradation of the overall visual quality of Highway 154. This substantial increase in the severity of the Project’s visual/aesthetic impacts is grounds for recirculation of the document pursuant to CEQA Guidelines § 15088.5 (a)(2). Further, this substantial increase in the severity of impacts to Highway 154 must be taken into consideration in the discussion and evaluation of potential alternatives to the Project that do not have this effect on the Scenic Highway, namely horizontal alternatives that position a barrier below the Bridge deck (see section 5, below) or mitigation measures that increase the transparency of the barrier (see section 4.a, below).

b. Flawed Cultural/Historic Impact Analysis

i. Failure to Identify and Evaluate the Potentially Significant Impact to the Integrity of the Bridge Concrete

The DSEIR states that one reason for the selection of the grid/mesh barrier is that it is “designed to be reversible, with minimal permanent impact to the historical fabric of the bridge structure if panels were to be removed.” (DSEIR p. 22). However, following the commencement of drilling into the Bridge deck concrete to install the grid/mesh barrier, members of the public observed serious crumbling and disintegration of the concrete indicating that more extensive permanent damage would be caused by the panel installation. (See Exhibit 1 – photos of the crumbling concrete). Members of the public also observed that apparently some holes had been patched, indicating that the damage to the bridge deck concrete is more extensive than the DSEIR describes (see Exhibit 2 – photos of patching). The historic and aesthetic impacts associated with drilling holes into the Bridge concrete must be disclosed and evaluated, and if the evidence reveals a new significant environmental impact the DSEIR must be revised and recirculated pursuant to CEQA Guidelines § 15088.5 (a)(1).

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ii. Failure to Identify and Evaluate Significant Historic/Cultural Impacts Associated with Views from the Bridge

The DSEIR impermissibly fails to disclose that the Project will have significant historical

impacts caused specifically by destroying views from the bridge deck. This affects the findings in the Supplemental Report that a safety net alternative similar to that proposed for the Golden Gate Bridge would diminish more aspects of the bridge's integrity than the vertical barrier and would be non-compliant with more of the Secretary of the Interior's Standards for Rehabilitation (DSEIR p. 31).

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A structure's setting clearly contributes in a substantial way to its historical significance (*see* CEQA Guidelines § 15064.5 (b)(1) (listing "relocation" and "alteration of the resource *or its immediate surroundings*" as constituting substantial adverse changes in the significance of an historical resource)) and this is particularly true in this case. The Bridge's setting is experienced mainly by the people travelling over the bridge, and construction of the grid/mesh barrier would severely degrade that experience.

Views from the Bridge are also clearly a "character-defining feature" of the Bridge that conveys its historic significance. The DSEIR concludes that views from the bridge are not considered character-defining features (p. 35), however the evidence does not support this conclusion. The DSEIR describes character-defining features as "those components that are part of its original design and overall design effect". (p. 28). The expansive view from the bridge, enabled by the deliberate design of the bridge deck and rails, is a component that is part of the Bridge's original design and overall design effect. Specifically the Historical resources Evaluation Report (HRER) supports this conclusion, stating as follows: "[a]t the time of the bridge's construction, views of the canyon from the bridge and surrounding countryside were considered to be among the most beautiful and impressive in California and considered an asset to the structure's design." (Pp. 15-16, citing F. H. Yoshino and R. L. Whitaker, *California Highways and Public Works*, Sept.-Oct. 1963, p.15.) Furthermore, the HRER states "[t]he Bridge Department did not consider a specially designed railing for this structure, although the decision to use the type that is on the bridge may have been seen to provide for improved views from the bridge." (Pp. 15, citing Whitaker, oral interview, March 12, 2007; Schulman, oral interview, March 19, 2007; *California Highways and Public Works*, pp.15 and 20). Additionally, the *California Highways and Public Works* article concludes, "The Cold Spring Canyon Bridge is one of the first major arch structures to use all welded sections. This feature combined with the unusual arch proportions, *the picturesque setting* and the generally pleasing appearance make this structure a notable addition to the California highway scene." (Exhibit 3, p. 20).

The historic impacts associated with destroying this character-defining feature of the Bridge must be disclosed and evaluated. Additionally, because the above evidence reveals a new significant environmental impact, DSEIR must be revised and recirculated pursuant to CEQA Guidelines § 15088.5 (a)(1). Once identified, the Project's significant historic impacts caused by destroying this character-defining feature of the Bridge undermines the DSEIR's conclusion that a horizontal barrier would have greater historic impacts than the proposed grid/mesh vertical barrier (see section 5, below), and a thorough analysis of the relative historic impacts of these alternatives must be undertaken.

4. Mitigation Measure Adequacy

a. Additional Visual/Aesthetic Mitigation Measures

The mitigation measures for the Project's significant visual/aesthetic impacts do not include any alternative *materials* that would reduce the visibility of the barrier. For example the use of transparent toughened glass or plexiglass/transparent acrylic material for the barrier panels is a feasible mitigation measure that would preserve motorists' views from the bridge and reduce the Class I visual and historic impacts associated with the grid/mesh and vertical barrier designs. This mitigation measure further would implement the goals for the barrier identified by the Aesthetic Design Advisory Committee (that the barrier be visually transparent). (DSEIR Appendix G, Appendix B (ADAC Meeting #2 Summary)). A feasible mitigation measure considerably different from others previously analyzed that would clearly lessen the significant environmental impacts of the project such as this transparent barrier proposal, is grounds for recirculation of another draft EIR if the project's proponents decline to adopt it. (See CEQA Guidelines § 15088.5 (2)).

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b. Ineffectiveness of Key Historic Mitigation Measure

One of the key historical mitigation measures described in the DSEIR and required pursuant to the Memorandum of Agreement is that Caltrans will photographically reproduce plans, elevations, and selected details from construction drawings in accordance with Historic American Engineering Record photographic specifications that are not deemed confidential for security reasons. (DSEIR p. 32). Recent developments have clearly indicated, however, that Caltrans believes all plans and engineering drawings are confidential for security reasons. Specifically, the City of Santa Barbara also requested detailed engineering data related to the alternate barrier designs, and to which Caltrans issued the following statement to the Santa Barbara News-Press "Due to sensitive public safety and security issues pertaining to homeland security, which have been in place for several years well before a suicide barrier was proposed, Caltrans is not releasing any detailed engineering information about the Cold Spring Canyon Bridge structure at this time." (See Exhibit 4). Also, Friends submitted a Public Records Act request to Caltrans requesting documents that assess the physical condition of the bridge and documents that relate to the engineering load and stress associated with the cantilever net alternative, and in response Caltrans stated that these documents "include sensitive information about the bridge's infrastructure that should not be disclosed due to public safety and security reasons" (see Exhibit 5 – Caltrans response to Public Records Act Request).

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If Caltrans is unwilling to disclose any detailed engineering information about the Bridge, the above referenced mitigation measure will not be effective. By contrast, if Caltrans is willing to release detailed engineering information to fulfill its mitigation obligation, there is no justification for withholding that same information from public review during the public review period for this DSEIR.

5. Alternatives Analysis

a. Improper Rejection of Horizontal Barrier Alternatives

Horizontal barrier alternatives would completely avoid one of the most significant environmental impacts of the Project: the obstruction of the unique panoramic view from the Bridge deck experienced by nearly six million people each year. However, the DSEIR rejects horizontal barrier alternatives without proper consideration of their environmental impacts relative to the grid/mesh vertical barrier, without disclosing that they have proven more effective at avoiding suicides, and by improperly assuming they will have significant historic impacts from substructure retrofitting without any evidence whatsoever that substructure retrofitting would be required. The DSEIR also addresses the feasibility of horizontal barrier alternatives in a combined fashion that ignores the fact that different horizontal barrier designs have different designs, costs, physical and operational constraints, and environmental impacts. Each of these issues is addressed in more detail below.

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A horizontal suicide barrier was recently approved for one of California's most iconic bridges, the Golden Gate Bridge. Specifically, on February 12, 2010, the Board of Directors of San Francisco's Highway and Transportation District certified the FEIR for the Golden Gate Bridge Physical Suicide Deterrent System. (<http://www.ggbsuicidebarrier.org/>). Although funding was not available at the time the Project was approved, the Board nonetheless voted unanimously to support "Alternative 3", a net system suspended below the Bridge deck. (*Id.*). Failing to at least evaluate one or more horizontal barrier designs as true project alternatives, given that neither of the alternatives evaluated in detail (grid/mesh and vertical picket) substantially reduce or eliminate any of the significant environmental impacts of the Project, severely thwarts the entire environmental review process.

i. Failure to Disclose that Horizontal Barriers Are More Effective than Vertical Barriers

The DSEIR fails to disclose that studies performed to date have demonstrated that a horizontal net-style alternative can actually be more effective than a vertical barrier. Specifically, Reich, T. et al, Securing a Suicide Hot Spot: Effects of a Safety Net at the Bern Muenster Terrace (The American Association of Suicidology, *Suicide and Life-Threatening Behavior* 35(4), August 2005) showed that a horizontal net installed at the Bern Muenster Terrace eliminated suicides, as during the study period six suicides occurred during a four-year period without safety net compared to zero suicides during the four year period with the safety net. By contrast, studies pertaining to the effectiveness of vertical barriers similar to the grid/mesh barrier show a reduction but not elimination of suicides. Specifically O'Carroll et al., Community Suicide Prevention: The Effectiveness of Bridge Barriers (*Suicide and Life Threatening Behavior*, 1993) determined that a vertical barrier installed at the Ellington Street Bridge reduced suicide rates from 25 suicides in over 7 years period without barrier compared to one suicide in just under 5 years with a barrier. Similarly Beautrais,

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Effectiveness of Barriers at Suicide Jumping Sites: A Case Study (*Australian and New Zealand Journal of Psychiatry*, 2001) determined that the removal of a vertical barrier at an unnamed Australian bridge increased the suicide rate from 3 suicides in a 4 year period with the barrier compared to 15 suicides in a 4 year period after removal of barrier.

This information should have been provided in the DSEIR to enable a full comparison of how well vertical and horizontal barriers effectuate the primary purpose of the Project.

ii. Failure to Differentiate Among the Safety Net Barrier Designs and Lack of Substantial Evidence Regarding their Infeasibility

The DSEIR discusses the “safety net barrier variations” together instead of differentiating among the various designs (see DSEIR pp. 6-7). Many of the stated bases for infeasibility apply to a net-style design but not a cantilevered design, and vice-versa. Additionally many of the bases for infeasibility are not supported by substantial evidence, as detailed below.

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1. Unacceptable Rescue Response Times

The concern that delays in rescue response times due to the Bridge’s remote location would allow a suicidal person to make their way to the edge and jump again is overstated given that any suicide net would actually *deter* people from jumping into the net in the first place, making any rescue a rare occurrence. This rationale is used in the Golden Gate Bridge Suicide Deterrent System FEIR to address concerns raised in public comment regarding delays in rescue, stating that “[b]ased on the success of nets at other suicide hotspots . . . delay associated with a net rescue would be a rare, non-recurring occurrence.” (Exhibit 6, p. 4-28). The deterrent effect of a safety net located a sufficient distance below the Bridge deck such that an individual contemplating jumping into it would face the prospect of injury rather than death is particularly strong. Friends not aware of a single instance where an individual has jumped or fallen into a safety net requiring rescue.

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The concern that an individual would make their way to the edge and jump again is utterly unfounded with respect to the Cantilever Net alternative because the design of the structure with a vertical barrier with inward curving edge would prevent the suicidal individual from jumping from edge. In this respect it combines the deterrent value of a net with the physical obstruction of a vertical barrier, making this alleged basis for infeasibility inapplicable to the Cantilever Net alternative.

2. Increased Danger to Individuals Attempting Suicide

Similar to the above, this basis for infeasibility also ignores the deterrent value of a horizontal net, and that the instance of an individual jumping into the net would be rare. The FEIR for the Golden Gate Bridge barrier considers the possibility of injury but does not consider it a basis for the infeasibility of the safety net. (Exhibit 6, p. 4-29). The safety net and cantilever net also have

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different pros and cons in this regard – an individual would be less likely to be injured falling 6 ft. into the cantilever net than falling 13 or 20 feet into the safety net, however the deterrent value of the safety net would also likely be greater, making any such instance even more rare.

3. Unacceptable Risk to Emergency Response and Rescue Personnel

Once again this basis for infeasibility ignores the deterrent value of a horizontal net, and the fact that rescue attempts would be rare. The FEIR for the Golden Gate Bridge barrier considers this scenario but does not consider it a basis for infeasibility and notes that any such risk could be minimized by periodic practice retrievals, and the use of a Under Bridge Inspection Truck to retrieve individuals from the net (that would be specifically purchased for that purpose as well as cleaning and maintaining the net) (Exhibit 6, p. 4-26 - 30). In the event that the UBIT used for the Cold Spring Canyon Bridge is not available for this purpose, Caltrans could consider purchasing a UBIT specifically for this purpose, as the Golden Gate Bridge Authority did. Alternatively, the horizontal net design could include a secured walkway under the bridge deck for use in clearing the net and to provide access for performing rescues, should that ever be necessary. An additional measure to improve the safety of emergency response and rescue personnel would be to raise the height of the existing Bridge railing.

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4. Increased Impacts to the Historic Substructure

The Cantilever Net alternative would be attached to the Bridge deck and, as such, the structure itself would not impact the historic substructure of the Bridge. Further, any impacts associated with retrofitting the substructure are purely speculative, as discussed at length in subsection iii, below. Both horizontal alternatives, moreover, avoid historic impacts associated with view blockage (see subsection 3.b.ii, above), lessening their overall impact to historic resources, and would substantially lessen the Project's visual impacts, discussed in subsection iv below.

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Additionally, altering the design could reduce impacts to the historic substructure associated with a safety net alternative. For example a net structure suspended 20 feet below the Bridge deck could be designed to follow the contours of the bridge arches such that it would be less than 20 feet at the center of the bridge and would not bisect the arches themselves. Contouring the structure along the top of the arch ribs may also allow some weight to be distributed onto the arches, increasing the structural support for the net structure and potentially eliminating the need for an extensive retrofit of the bridge substructure. Alternatively, a safety net structure positioned closer to the Bridge deck could also reduce impacts to the historic substructure, and if necessary the net structure could be designed to pivot, similar to the design adopted for the Golden Gate Bridge to provide clearance for Bridge inspections.

5. Liability Associated with an Attractive Nuisance

The attractive nuisance effect is highly speculative, and the use of the Debris Flow Barrier in Big Sur referenced in the Feasibility Study for the Cantilever Net alternative is not even remotely analogous given that that structure is near the ground, more easily accessible, and far less dangerous. Only the boldest of thrill seekers would contemplate jumping into a net structure located 400 feet above a canyon. The Golden Gate Bridge Barrier FEIR also considers the possibility that thrill seekers would purposefully jump into the net, but concludes that because horizontal nets have proven 100% effective in stopping people from jumping off those structures, that this risk is remote. (Exhibit 6, pp. 4-32- 4-33).

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6. Design Load Limitations

In the discussion of this basis for infeasibility, the DSEIR incorrectly states, "Construction of the barriers would also entail retrofitting the substructure." (DSEIR p. 7). Discussed in subsection iii below, the DSEIR does not establish this fact, particularly for the Cantilever Net Alternative. Additionally, the DSEIR fails to consider whether alternative materials for the net would be lighter and allow the design load to be increased while avoiding the need for retrofitting altogether. As discussed in subsection 8, below, the use of other materials may only be more expensive in the short term, as life cycle costs are less, and the actual long-term costs of the alternative are projected to potentially be less when the costs associated with repair, maintenance and/or replacement of the netting are considered.

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7. Unacceptable Risks Associated with Safety Net Maintenance

Due to the Bridge's remote location and rare use by pedestrians, and that no vegetation overhangs the Bridge span, and the fact that wind would likely dislodge debris on a regular basis, it is unlikely that debris accumulating in the net would be a significant problem. Moreover, periodic cleaning of the net could occur with the UBIT when the Bridge is inspected (as proposed for the Golden Gate Bridge Barrier – see Exhibit 6, pp. 4-26 – 4-27) or from any secure below-bridge walkway.

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8. Increased Costs Associated with Need to Replace Safety Net

Again, because of the deterrent value of a safety net, instances of an individual jumping into the net and causing it to deform would be rare. Additionally no vegetation overhangs the Bridge span, reducing the likelihood that heavy debris would fall into the net. Choosing a different material for the net could also reduce the cost of potentially replacing the safety net, and simultaneously could resolve concerns regarding design load limitations of the Bridge (see subsection 6, above). The Feasibility Study for the Cantilever Net Alternative describes the short term versus long-term costs associated with the net materials as follows:

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Steel was chosen as the preferred material because of its strength to weight ratio, its affordability and its relative durability. Other materials were considered but dismissed because of their relative economic infeasibility; namely other potential materials for the structure are a minimum of 4 times more expensive than steel . . . Although steel is the preferred material for the reasons stated above, the higher cost alternative material would have to be compared with the higher life-cycle cost of having a steel structure. Long-term maintenance costs of a steel structure at this location may exceed the initial high cost of alternative materials when considering a long-term cost benefit analysis.

(DSEIR Appendix H, Attachment 39, p. 2)

iii. Failure to Provide Sufficient Information Regarding Potential Retrofitting to Evaluate its Feasibility and Impacts

One of the reasons stated in the DSEIR for rejecting horizontal barrier alternatives is that they would have increased impacts to the historic substructure from retrofitting. (DSEIR p. 6). Specifically the document states that "Safety net installation would . . . require substantial retrofit of the substructure, which additionally diminishes the integrity of the historic property." (*Id.*) To support this contention the DSEIR cites to the Supplemental Report for the Safety Net Alternative and the Feasibility Study for the Cantilever Arc Barrier Net Alternative. The Supplemental Report however reveals the speculative basis for any conclusion regarding the extent and impacts of any retrofit required for the safety net alternative, stating that

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Although exact calculations regarding changes to the columns, towers, and arches have not been completed for the potential retrofit, it is likely that each of the elements of the substructure that are considered character-defining features of the bridge would need to be physically altered and that the slender qualities of those components would be changed, perhaps radically so.

(DSEIR Appendix H, Attachment 20, p. 18).

The Feasibility Study for the Cantilever Net Alternative is even more speculative regarding the need for substructure retrofitting, stating

In addition to a retrofit of the entire bridge deck and concrete rails, it is very likely that the substructure would also have to be retrofitted; however, detailed structural analysis for retrofitting the superstructure and substructure was not completed.

(DSEIR Appendix H, Attachment 39, p. 4).

Whether the Cantilever Net Alternative would require a retrofit of the Bridge deck only, or also a retrofit of the substructure, bears directly on the extent of the feasibility, cost, and impacts of the Cantilever Net Alternative. If only bridge deck retrofitting is required, the cost would be substantially less. Also, given the documented condition of the deck concrete (*see* section 3.b.i, above), it is possible that a retrofit of the deck and rails is required under for the grid/mesh barrier and maybe even for the no-project alternative. In this case bridge deck and rail retrofitting is not unique to the Cantilever Net Alternative and is an improper basis for its rejection. The impacts associated with a retrofit of only the bridge deck moreover are also substantially less than the impacts associated with a retrofit of the substructure, given that the DSEIR finds that “Some of the [] original design features (the substructure’s arch ribs, towers, columns, and girders, for example) are more significant than others (such as the standard-type railings and concrete road deck) in conveying the bridge’s significance” and that “[t]hese differences in relative significance are taken into account in assessing the proposed project’s effects/impacts on this historic property.” (DSEIR p. 28).

The seismic retrofit completed in 1997 did not diminish the historic integrity of the Bridge including concrete bolsters flanking the abutments and steel clips on the roadway girders at the top of the towers (HRER, p. 40). This retrofit also included alteration of the steelwork, the large thrust blocks at the arch springing and the “skewbacks”, the drilling of 21,000 holes, placement of new caissons and concrete grade beams, and other structural modifications. (Exhibit 7, document describing the 1997 retrofit prepared by the company that designed the retrofit; Exhibit 8, American Bridge article on 1997 retrofit; *see also* Finding of Effect, Appendix H, Attachment 4, pp. 16-17). Caltrans did not find the 1997 seismic retrofit project caused a significant impact to the bridge’s historical resources. The conclusion in the HRER that the 1997 seismic retrofit did not diminish the historic integrity of the Bridge clearly demonstrates that it is incorrect to presume that any retrofit of the substructure would have significant historic impacts, and that detailed information regarding the specific nature of any retrofit is required before any conclusions about its impacts can be reached. Accordingly Caltrans lacks substantial evidence for concluding that either the safety net alternative or the Cantilever Net Alternative would have significant historic impacts by virtue of any required retrofitting.

iv. Failure to Analyze the Visual/Aesthetic Impacts of Horizontal Barriers

CEQA requires that the EIR “include sufficient information about each alternative to allow meaningful evaluation, analysis, and comparison with the proposed project.” (CEQA Guidelines § 15126.6 (d)). The EIR only evaluates two project alternatives in detail, the Grid/Mesh and Vertical Picket alternatives, each of which have significant and unavoidable impacts in the areas of aesthetic/visual resources and cultural/historic resources. In the brief section explaining the rejection of the safety net alternatives, DSEIR opines that the safety net alternatives would have increased historic impacts relative to the Grid/Mesh alternative, however not only is this conclusion speculative as discussed in sections 3.b.ii and 5.a.iii *infra*, but it is only half of the picture. Specifically the DSEIR utterly fails to discuss the aesthetic/visual impacts of these alternatives. This failure obscures the fact that the safety net alternatives avoid the most significant of the Project’s aesthetic/visual

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impacts – the obstruction of the scenic views experienced by the nearly six million people that cross the bridge annually.

v. Failure to Consider Increased Achievement of Project Objectives from Horizontal Barriers

Horizontal safety nets would increase worker, pedestrian and law enforcement personnel safety by substantially reducing the consequences of an accidental fall from the bridge deck. Increased safety is a project objective that is enhanced differently and in some ways preferably to the proposed vertical fence alternative, such as for those performing maintenance outside the vertical barrier.

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vi. Preferred Horizontal Barrier Alternative

Friends believes that increasing the existing Bridge rail, combined with a horizontal net designed by modifying either the Safety Net or Cantilever Net to address certain feasibility concerns (as discussed above) is the best possible alternative. This alternative would avoid the Class I visual impact associated with obstructing views from the Bridge, and would have equal or reduced historic impacts compared to the Grid/Mesh alternative. Moreover this alternative would be equally or more effective at achieving the Project's purpose and need as the Grid/Mesh alternative, given that horizontal nets have proven 100% effective at deterring suicide and for the additional reasons set forth in section v, above. We urge Caltrans to disclose the relevant engineering information requested by Friends and by the City of Santa Barbara to enable the development and detailed evaluation of such a horizontal net alternative.

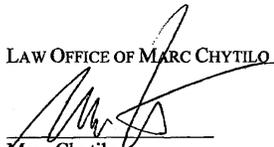
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6. Conclusion

For the reasons stated herein, the DSEIR is inadequate and requires substantial revision and recirculation for public review. We trust that these comments will receive a thorough response, and that Caltrans will not merely approve the Project without additional public process.

Sincerely,

LAW OFFICE OF MARC CHYTILO



Marc Chytilo

Attorneys for Friends of the Bridge

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Exhibits:

- Exhibit 1: Photos of the Crumbling Concrete on the Cold Spring Canyon Bridge
- Exhibit 2: Photos of Concrete Patching on the Cold Spring Canyon Bridge
- Exhibit 3: F. H. Yoshino and R. L. Whitaker, *California Highways and Public Works*, Sept.-Oct. 1963
- Exhibit 4: "Terror fears cloud barrier debate – Councilmen want transparency on net alternative", Santa Barbara News-Press, January 8, 2011.
- Exhibit 5: Caltrans response to Friends' Public Record Act Request, September 23, 2010
- Exhibit 6: Golden Gate Bridge Suicide Deterrent System FEIR, Chapter 4, selected pages
- Exhibit 7: Buckland & Taylor Ltd. Document re. Cold Spring Canyon Bridge seismic retrofit in 1997
- Exhibit 8: American Bridge article on 1997 retrofit

CC: Director Richard Krumholz

EXHIBIT 1







EXHIBIT 2







EXHIBIT 3

Cold Spring Canyon Arch

By F. H. YOSHINO, Resident Engineer, and R. L. WHITAKER, Project Designer



Motorists driving from Santa Barbara to Lake Cachuma and Santa Ynez by way of San Marcos Pass (State Sign Route 150) will soon be traveling across one of the 10 longest* steel

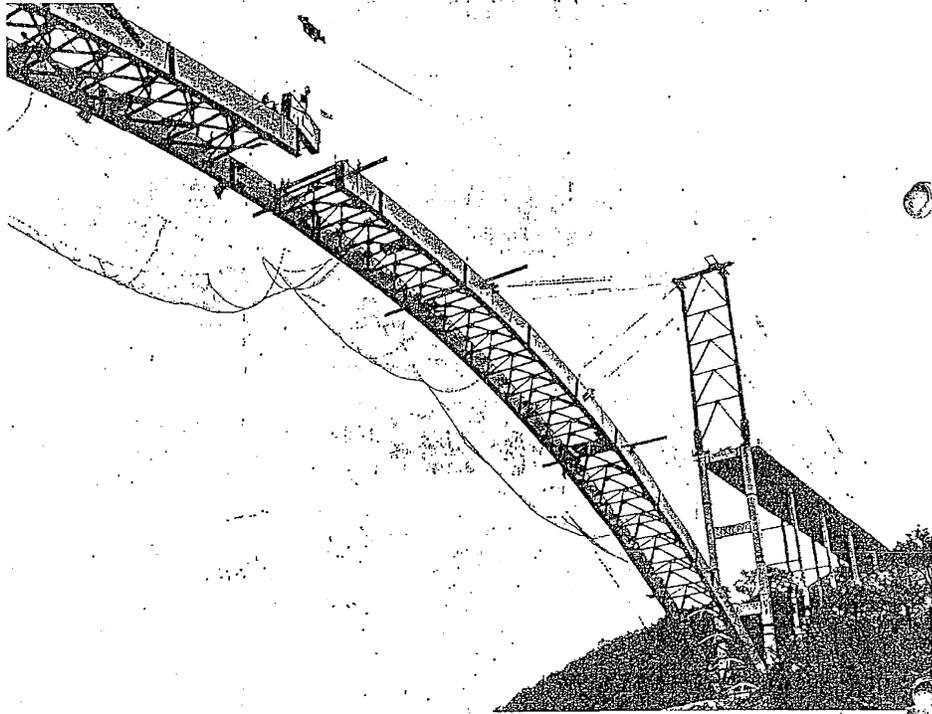
arch bridges in the United States.

* Reference World Almanac, 1963.

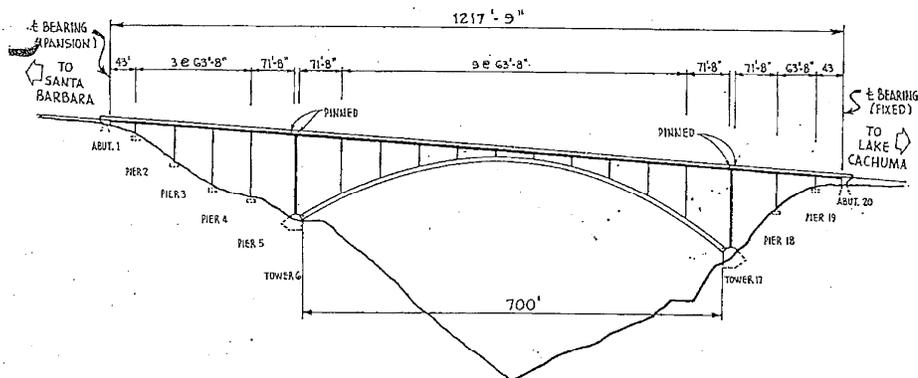
Route 150 follows in general a trail used by early-day travelers between Mission Santa Barbara and Mission Santa Ynez. In 1846 Lt. John Fremont and his troop crossed San Marcos Pass to surprise a Mexican Army contingent waiting in ambush at Gaviota Pass to the north. The trail was later used as a stagecoach route. Cold Spring Tavern, one of the original coach stops, is at the side of the present route about one-half mile upstream

from the bridge site. The original trail intersects the new alignment near the north end of the bridge. The trail is clearly defined and was used by survey crews to gain access to the bridge site.

Traffic on this scenic and historic route has increased over the years as the attractions of the Missions, Solvang, the quaint Danish community, and the recreational facilities of Lake Cachuma became more widely known.



The final segment of the arch is placed at the crown. Flags are attached to the closing segment in the traditional way of signifying the successful completion of the job. The arch was lowered until bearing was obtained at the crown by releasing pressure from the jacks at the base of the temporary towers.



A profile of the bridge showing some general dimensions and the numbering of the towers and piers.

The present road no longer provides adequate service for the traffic volume.

Part of Major Improvement

The bridge, which is located about 13.5 miles north of Santa Barbara, is part of a major improvement in alignment and grade of Route 150. About 1.5 miles of road are presently under construction in the vicinity of San Marcos Pass summit. The bridge eliminates approximately one mile of road which would otherwise curve in and out of the canyon. Savings in time and money were thus achieved. The panoramic view of the canyon and surrounding countryside from the bridge and approaches will surely rank as one of the more beautiful and impressive of California's many scenic splendors.

The bridge and road work are being performed under two separate contracts. At the time this article was written it was anticipated that the road work would be completed in February 1964. The bridge is scheduled to be completed in November 1963.

The bridge is 28 feet wide between arches and 1,218 feet long overall. Welded steel girders, floor beams, columns and two main towers support the concrete deck. The 700-foot arch span which rises 400 feet above the canyon floor carries 11 girder spans. The side spans flank the main span. The arch and main towers are supported by concrete skewbacks. The

approach columns rest on concrete piers. Concrete abutments support the end spans.

Curved Box-shaped Arches

All structural steel for this bridge was A-7 or A-373 type. One thousand four hundred forty tons of steel plate were formed into two gracefully curved box-shaped arch sections each nine feet deep by three feet wide. The box shape is made up of 2 web plates 15/16 inch thick joined by 2 flange plates of different thicknesses

ranging from 1½ to 3½ inches. Welding was specified for all shop splices and high-strength bolts were required for all field connections. The 2 ribs 26 feet apart are connected by trussed steel bracing. Steel pedestals were welded to the ribs to form column bases.

The arch was designed to be erected as three-hinged with provision for conversion to two-hinged following placing of girder steel. Deck concrete for the arch spans was required to be placed in a particular sequence which was designed to minimize unbalanced loadings during construction.

Design live loading was A.A.S.H.O. H20-S16 classification without reduction due to the length of span.

Each rib carries a dead load thrust of 2,600,000 pounds which increases to 3,000,000 pounds under design vehicle loads. The maximum design moment on each arch rib is about 12,000 foot-kips.

The arch is capable of carrying loads equal to 1.8 times design dead load plus live load, without permanent distortion of any member.

Arch Deflection

Under maximum uniform design loading the deflection of the arch at the crown will be approximately three inches and under unsymmetrical loading the maximum deflection will be approximately six inches at the one-quarter point.

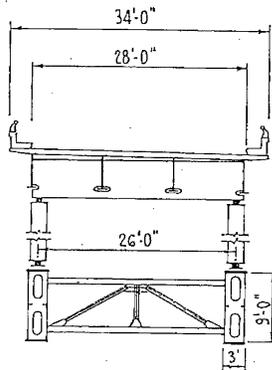
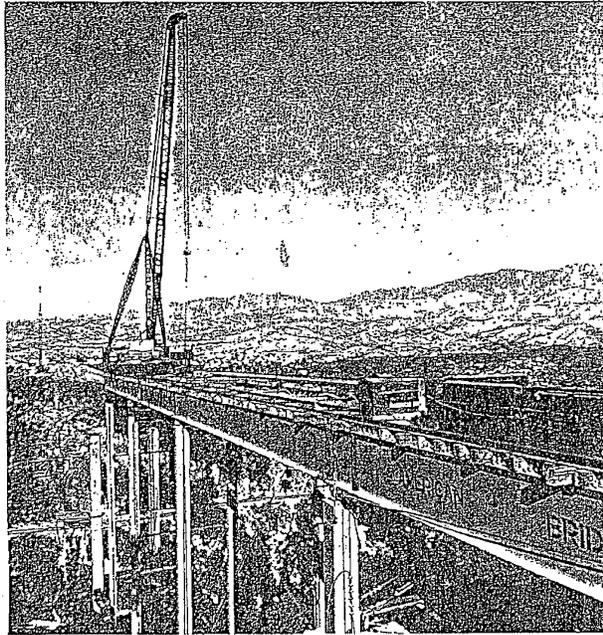


Diagram of a typical section of the bridge.

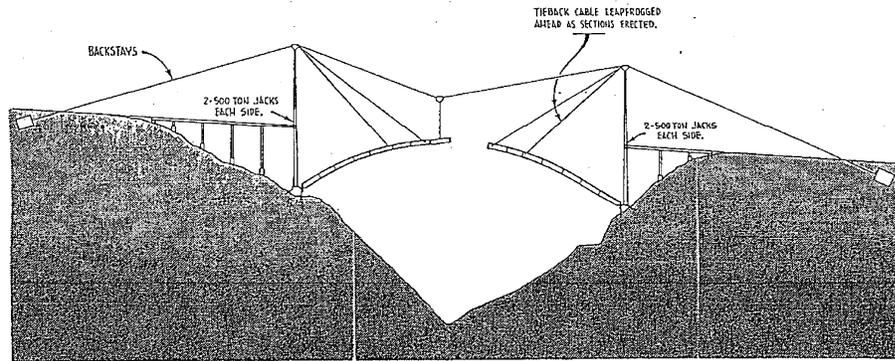


A view from Abutment No. 1 (toward Santa Barbara) showing spans 1 through 4 and the traveler crane in position to set steel for main tower 6 and the girders of span 5.

Two cables each $1\frac{1}{8}$ inches in diameter are concealed near the crown of each rib. These cables connect the rib to the deck. The deck is anchored to the abutment at the north end and therefore the cables tend to restrain the arch crown against translation thereby reducing arch deflections under unsymmetrical loadings.

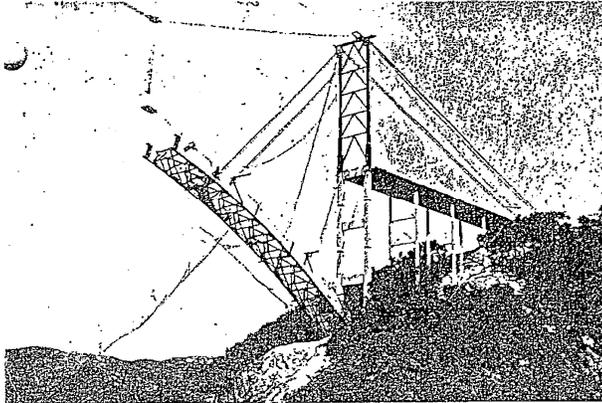
The ends of the arch rest on hinge pins eight inches round by three feet long. These pins are supported by steel castings which in turn rest on concrete skewbacks. The skewbacks transmit loads from the arch and main towers directly to bedrock which consists of massive bedded sandstone of the Gaviota Formation of the Eocene-Oligocene age.

Eight hundred ninety tons of steel plates were formed by welding into shapes for towers, columns, floorbeams, and girders. Each main tower is 134 feet high consisting of two four-foot square legs connected by one cross strut at the roadway level and two struts at about the one-third points. Each leg is anchored to the skewbacks by $29\frac{1}{8}$ -inch round prestressing rods. The rods are about 10 feet long. Each rod was required to be preloaded to 131,000 pounds in order to secure the tower to the base and prevent uplift under all design loadings. The towers act to support



ARCH ERECTION

This diagram shows a profile of the arch erection technique used on the bridge.



The four photos on this and the next page show a time sequence of the arch erection procedure.

girder loads and to carry lateral loads from the deck system to the foundation.

Columns Are Hinged

Twelve columns support the approach spans and 20 columns carry the spans on the arch. Each column is a 10-foot square, sealed, welded box section which is hinged at the floor beam and at the footing or top of arch to permit relative movement of the deck due to temperature changes or arch translation due to unsymmetrical loadings.

A slender column effect is obtained because of the column height which reaches 93 feet at pier 5.

The columns were designed with no bracing of any kind and were required to be fabricated full length without field splices to further enhance the effect of slenderness.

Each approach span column is supported on an individual concrete footing which in turn rests on bedrock similar to the material at the main arch support.

Four girders four feet four inches deep spaced nine feet four inches apart support the concrete deck. The deck and girders have joints at the abutments and towers only. Girder spans are continuous for vertical loadings for each approach span group and for each group of spans between towers. The entire deck was made continuous

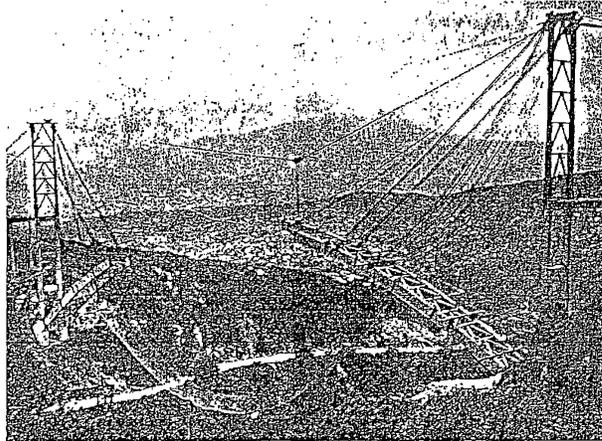
from abutment to abutment by utilizing a hinge detail at each tower which permits vertical rotation of the deck and girders but not horizontal rotations. Shear is transferred across the joints by a key detail, thus the deck and girders act as a horizontal continuous beam with the abutments, main towers and arch ribs acting as

supports to resist horizontal loads from wind or earthquakes.

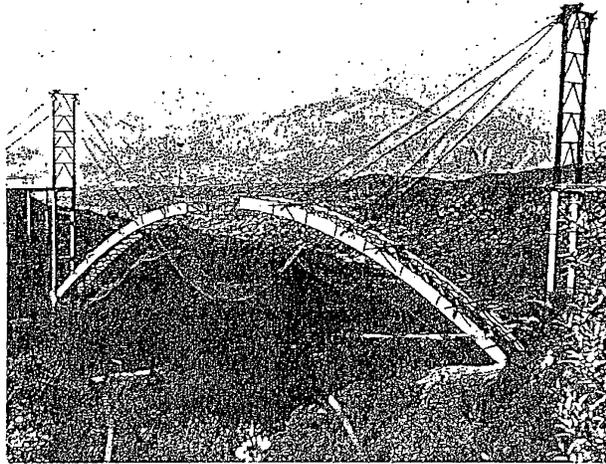
Slotted Floor Beams

Continuity for the girders across the floor beams was obtained by slotting the floor beams to match the girder flanges. This permitted installation of bolted field splice plates for girder flange connections. Girder web connections to the floor beams were made in the usual manner with angle connections to the floor beams.

Girders were made composite with the concrete deck. A steel lateral system was installed near the lower flange of the girders. Both the steel bracing and the deck act to resist horizontal loadings. The girders support a 7 1/4-inch concrete deck which is 28 feet wide between type II barrier railings. Each railing is three feet seven inches high with a two-foot-wide curb. The total width of deck is 34 feet. The concrete deck is reinforced longitudinally, as well as transversely, to act in conjunction with the girder bracing system to resist lateral loadings. Deck units were required to be placed in a sequence designed to reduce the effects of concrete shrinkage. The longitudinal reinforcing and composite action of the girders resists any tendency toward cracking.



Further segments have been added to the arch. The temporary erection towers and high-line rigging are in position on top of the permanent main tower at each end of the arch.



The arch is nearly completed. Note the safety nets under the arch.

Abutment No. 1 at the end nearest Santa Barbara rests directly on bedrock. Abutment No. 20 is supported on steel pilings which were driven to bedrock. The deck is tied to Abutment No. 20. Expansion takes place at Abutment No. 1.

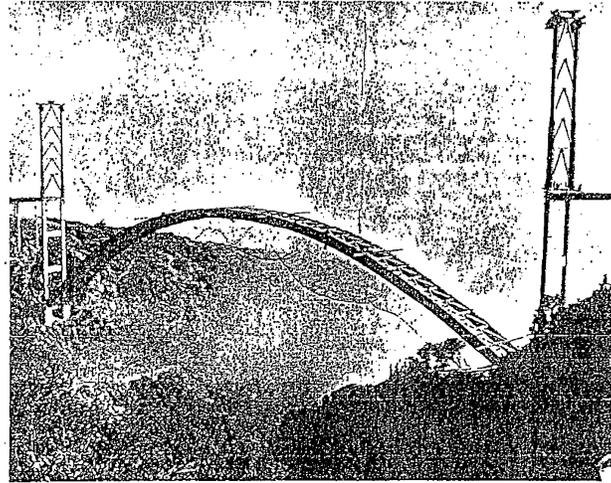
Abutment 20 supports both the vertical loads from girders and vehicles and the longitudinal loadings due to live load traction and loads from the cross tie cables at the arch crown.

Rugged Terrain

The accompanying photographs show the rugged terrain at the bridge site. Surveying to the high degree of accuracy necessary with this type of construction in this terrain posed problems not commonly encountered in bridge work. To assure the required accuracy, horizontal distances across the canyon were measured by five separate methods. They were: slope chaining, direct chaining, subtense bar, triangulation and by geodimeter measurement. Triangulation results were discarded because the short base lines and large vertical angles required introduced errors which could not be eliminated. Results of the remaining methods agreed to within $\frac{1}{2}$

inch in 740 feet or an accuracy of about 1:18,000.

A giant slingshot composed of two very elastic rubber bands, normally used for exercising, was used to put



The arch completed. All temporary support cables are removed and the arch is self-supporting.

the first line across the canyon. This was done in order to pull a 600-foot chain over the top of the dense growth of trees and brush. Level circuits were run along the existing road and across the canyon.

The contract was awarded to the United States Steel Corporation, whose American Bridge Division did the steel erection. In June 1962, Massman Construction Company of Kansas City, Missouri, moved on the site. They were the subcontractors for the excavation and concrete work. Foundation work began on the south slope, progressed downhill and across to the other side of the canyon.

Hand Tools Used Extensively

Due to inaccessibility of the footing sites very little heavy equipment could be employed for excavation and concrete work. Hand tools and air jackhammers were used extensively for excavation. Concrete was transported from transitmix trucks to footings and skewbacks by a machine which pushes the concrete through a pipe by air pressure.

Concrete was dry batched in Santa Barbara and transported in dump trucks to a transfer plant at the

Marcos Pass summit. At the transfer plant transit mix trucks were waiting to be belt loaded with the dry batch and to have water added. This arrangement made it possible for the contractor to have concrete at the worksite in the minimum amount of time, and also to have better control of the rate of delivery.

Grinding was required for the arch and tower bearing surfaces on the skewbacks. This was a major job considering that areas which were as much as 24 feet square had to be ground to within $\frac{3}{32}$ inch of a true plane. Grinding was done with portable electric-driven hand grinders. Results were checked with a scratch template.

The arch segments and arch lateral bracing were fabricated by the American Bridge Company at their plant in Gary, Indiana. All other structural steel was fabricated at the Maywood plant near Los Angeles. Steel was shipped by rail to Santa Barbara and trucked to the site.

Shop inspection of the work done at Gary was performed for the State by the R. W. Hunt Company, under provisions of a service agreement. The Division of Highways Materials and Research Department performed shop inspections for all other work.

Additional tests, sufficient to assure suitability of delivered materials, were performed by the materials laboratory at the request of the resident engineer during progress of the work.

Erection of structural steel began October 30, 1962. The south approach spans and tower were erected first, as shown in the photo on page 16. A traveler crane was assembled on span 1 and moved forward on rails to erect the next four spans and high-line tower. The traveler was then dismantled and reassembled on the other side of the canyon at abutment 20, and the same procedure repeated. Temporary guys and bracing were used at bents and at the top of girder flanges during erection to provide increased stiffness until the deck was in place.

High-line System

A high-line system was used to erect and support the arch during construction and to place superstructure steel on the arch. A high line consists es-

entially of a suspension bridge having special equipment for transporting loads along the main cables.

The 120-foot high-line towers were placed on top of the 134-foot high skewback towers. The function of the temporary towers was to carry the high line and to support the arch during erection. The high-line towers were free to rotate at the bottom in the direction of the centerline of bridge. A pair of 500-ton hydraulic jacks were put under each leg to provide a high degree of control over the vertical position of the towers. Both the erection towers and traveler crane have been used on similar jobs.

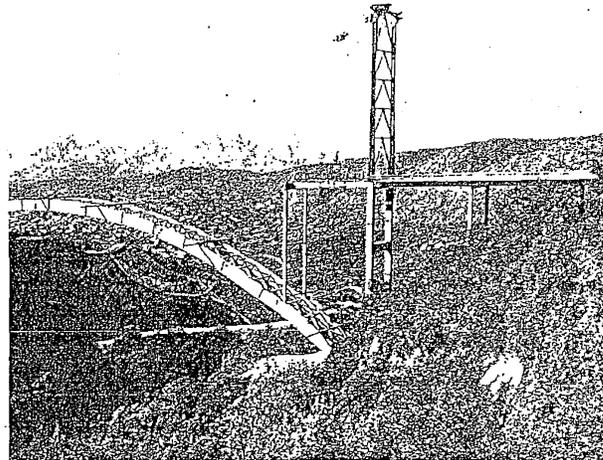
Five miles of cable were used for the high line, arch tiebacks, and backstay lines shown in the photographs. During operation, three hoists were used to control the moving cables. A carriage, riding on rail, atop the erection towers provided for transverse movement of the high line.

Rib sections were transferred directly from trucks parked on the existing road near the north skewback to the high line and then into position in the arch.

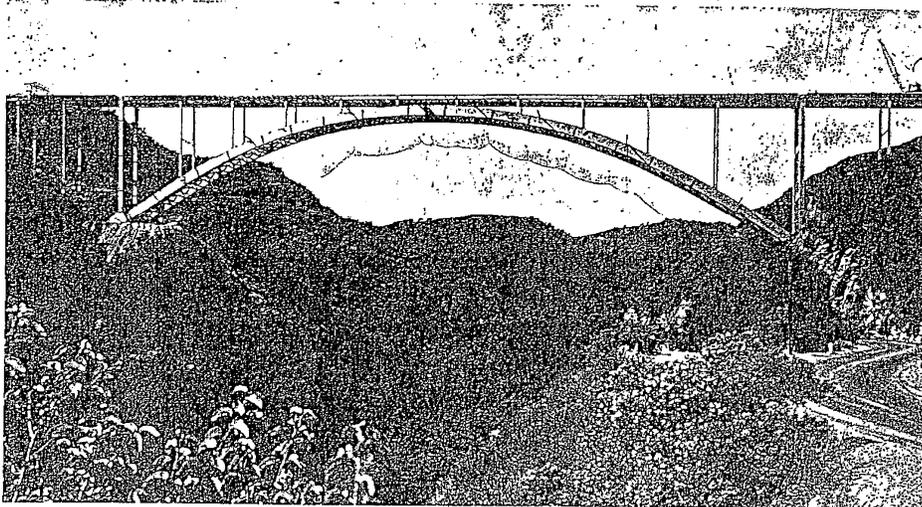
Communications between the contractors' key personnel were maintained by use of citizens band transmitter radios. The advantages of this radio system can be appreciated when it is recognized that each of the two hoist operators (one near each abutment) were hundreds of feet from the point of picking up the rib segments and seldom could see the load being lifted or set.

Tieback Tables

As succeeding sections of arch were placed outward from each skewback, tieback cables from the erection tower to rib were set near the end of the newly placed segment and the preceding tieback cables were released. This procedure was repeated until the last section of rib was in place at the crown. All field connections were made with high-strength bolts. The arch was intentionally erected higher than the intended final elevation so that by releasing pressure from the hydraulic jacks at the base of each erection tower the arch halves could be lowered until bearing was obtained at the crown hinge. This delicate and critical operation was completed on



The final operation of setting girder spans on the arch. Girder span 7 is shown in position. Upon completion of this operation all highline rigging and temporary bracing are removed.



The steel structure as seen from downstream. The deck and rolling remain to be completed. The bridge will be painted green as a final operation. The existing road can be seen in right foreground.

June 27, 1963. The "topping out" or in this case the closing operation was celebrated in the traditional manner by raising the "Stars and Stripes" with the final rib segment, signifying the successful completion of a difficult and noteworthy task. Erection of the rib segments was completed in 60 working days.

The arch became self-supporting when the two halves were lowered into bearing at the crown hinge. All tieback cables were then removed and erection of the columns, floorbeams and girders on the arch was begun. Each floor beam and column was raised and set as a unit. The four girders were then placed and bolted in each span. The final piece of structural steel was set on July 25, 1963. A total of 8 months and 25 days elapsed between setting the first and last pieces of structural steel. The crown hinge was then bolted rigid, as planned, thus converting the arch from three-hinged to a two-hinged type.

Accurate Within One Inch

Surveys on the arch after all structural steel was in place show that the arch curve actually obtained was accurate to within one inch in elevation compared to the theoretical elevations planned. This is within anticipated variations for this type of work. Final adjustments will eliminate all variations in the deck profile.

The high-line rigging and towers were removed during August 1963 and placing deck concrete on the arch spans was commenced. Deck concrete for the approach spans was placed while the arch erection was underway. All deck concrete was placed by labor crews using "Georgia Buggies."

Concrete for deck spans on the arch was placed in a special skip pattern specified by the designers. The purpose of this placing pattern was to keep the arch from deflecting excessively under the unbalanced loadings and also to minimize deck cracking resulting from normal shrinkage of the concrete. A specially designed

rolling frame was used for stripping deck falsework. This frame rolls on the completed deck and has two platforms which extend under the deck. The platforms are hinged in such a way that they can be raised or lowered by operators on the deck. This machine thus eliminated the need for conventional working platforms under the deck.

The bridge will be painted green as the final operation.

The contract bid cost was \$1,948,638 which includes about one-fourth mile of approach grading necessary to provide a working area for construction of the bridge. Three hundred working days were allotted for completion of the contract.

The Cold Spring Canyon Bridge is one of the first major arch structures to use all welded sections. This feature combined with the unusual arch proportions, the picturesque setting and the generally pleasing appearance make this structure a notable addition to the California highway scene.

EXHIBIT 4

SANTA BARBARA NEWS-PRESS

Terror fears cloud barrier debate - Councilmen want transparency on net alternative

SCOTT STEEPLETON, NEWS-PRESS CITY EDITOR

January 8, 2011 6:23 AM

Claiming that Caltrans is not being forthcoming about a "capture net" alternative to the 9-foot-7 \$4 million vertical metal fencing proposed as suicide barriers on Cold Spring Canyon Bridge, some members of the Santa Barbara City Council are seeking a full public vetting of the option by Caltrans and the Santa Barbara County Association of Governments.

At issue is whether the state's transportation agency is using claims of terrorism concerns to keep from the public information that could prop up a barrier design that runs contrary to the option Caltrans is clearly behind.

During Tuesday's council meeting — the first of 2011 — Dale Francisco and Frank Hotchkiss will ask colleagues to place an item on the agenda stating that the council "supports transparency in the design review of nets as a possible alternative for suicide prevention" at the bridge.

Mr. Francisco told the News-Press he's not opposed to a vertical barrier like the one being promoted by Caltrans, which has the backing of Santa Barbara County Sheriff Bill Brown and others. He just wants to ensure the historic bridge on State Route 154 is outfitted with the best possible and least intrusive barrier and he's not sure Caltrans is providing all the information necessary to make that determination.

Work on the vertical barrier was moving forward in July 2010 when Santa Barbara County Superior Court Judge Thomas Anderle brought it to a halt by way of a ruling in a lawsuit filed by the group Friends of the Cold Spring Bridge claiming Caltrans paid short shrift to a cantilever net alternative.

The judge revoked project approval after finding that Caltrans violated California Environmental Quality Act regulations in the public outreach process.

As a result, Caltrans is in the process of circulating an updated environmental report, with additional information on the net, to the public for review.

"After Anderle made his ruling, one of the things Caltrans did is they collaborated with Wayne Donaldson, the state historic preservation officer and President Obama's appointee as chairman of the national Advisory Council on Historic Preservation," Mr. Francisco told the News-Press Friday. "Wayne worked with two engineers from Caltrans and they came up with a cantilever arm capture net. It's a series of arms coming off the side of the roadbed every 10 feet with netting. It looks like the vertical barrier except on its side."

Such nets, said Mr. Francisco, have been used successfully around the world.

"People walk on the bridge, they look over and say, 'Oh, there's a net. I can't jump.' "

But after analyzing the net alternative, Caltrans concluded that it would place an unacceptable load on the bridge.

Aside from saying that the cost to upgrade the bridge's substructure to hold a net is cost-prohibitive, Caltrans officials have been unwilling to share the engineering calculations used to make that determination.

"What they claim is it's a (Department of) Homeland Security designated bridge and they can't share any information," Mr. Francisco said.

Not even with other government agencies, apparently.

Caltrans officials on Friday would not elaborate on any of the engineering data in question.

Instead, the agency issued the following statement to the News-Press: "Due to sensitive public safety and security issues pertaining to homeland security, which have been in place for several years well before a suicide barrier was proposed, Caltrans is not releasing any detailed engineering information about the Cold Spring Canyon Bridge structure at this time."

Later in the day, Caltrans District 5 spokesman Colin Jones contacted the News-Press to say the agency is evaluating its position.

"The statement is not the final word," he said.

A request for the engineering data has been filed by an attorney representing Friends of the Cold Spring Bridge.

Cold Spring Canyon Bridge was deemed a sensitive structure after 9/11, Mr. Jones said, although he could not point to a specific list of where that designation is contained.

Asked whether Caltrans was withholding the information pertaining to the cantilever net option because the agency favors the other option, Mr. Jones said no.

"What we're trying to do is balance the security issues with releasing the information to the public," he said. "It's really because the bridge has been designated a sensitive structure. It's the only one in our district. That's why we've been so circumspect on releasing the information, due to the security and public safety reasons."

If the information is truly sensitive, said Mr. Francisco, there is no need to make it available to the general public.

"I say, 'If you can't make that information public, why can't you share it with another agency, like the state officer of historical preservation?' " said Mr. Francisco. "I think you're going to be alright."

"If the net won't work, then fine, OK, end of story," he added. "But why not be clear and transparent about that?"

Nets are not a new idea in deterring suicide. A year ago, the Golden Gate Bridge, Highway and Transportation District voted to install a \$50 million net system at the historic structure.

The Golden Gate Bridge Physical Suicide Deterrent System Project includes the cost of final design; construction of the net, including replacing rolling maintenance scaffolds on the bridge to accommodate the net; construction engineering; environmental monitoring during construction; the purchase of a large snooper truck for retrieving individuals from the net; and the purchase of a small, sidewalk-sized snooper truck to remove litter and debris from the net.

Like the Cold Spring Canyon Bridge, which has a railing that's 32 inches tall, the Golden Gate Bridge has a low outside railing — 4 feet tall — providing little deterrence to people climbing onto it and jumping off. As a result, about two dozen jump to their deaths from the fabled Bay Area structure every year.

The district considered vertical barriers similar to the one favored by Caltrans for Cold Spring Canyon Bridge, but in the end chose the net. A report from the Golden Gate Bridge, Highway and Transportation District details the net as extending horizontally about 20 feet from the bridge.

"The support system for the netting would include cables that would pre-stress the netting to help keep it taut and not allow the wind to whip the netting."

Net supports would be painted International Orange to match the existing bridge structure, but the net material would be unpainted and uncoated stainless steel.

So far, \$5 million in federal grants has been allocated for the final design cost.

Sources for the rest of the money have yet to be identified.

Mr. Francisco said that since Santa Barbara is a member of the Santa Barbara County Association of Governments — a regional body that is responsible for prioritizing funding for highway projects — and since association funding "has made this project possible, it is important that we advocate for complete transparency in the design review process."

A memo from him and Mr. Hotchkiss in Tuesday's agenda packet states: "It is appropriate for the City Council to consider this matter because the current Caltrans design, which we have supported through prior SBCAG deliberations may in fact be inferior" to that recommended by Mr. Donaldson.

It would be Mayor Helene Schneider's responsibility, as the council's representative on the association of governments, to make the council's view known, if in fact the item were to pass.

"We hope that City Council will express its support for transparency in the interest of creating a true public consensus, and recommend this approach to SBCAG and to Caltrans," Mr. Francisco and Mr. Hotchkiss state in their memo.

e-mail: ssteepleton@newspress.com

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EXHIBIT 5

DEPARTMENT OF TRANSPORTATION

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PHONE (805) 549-3111
FAX (805) 549-3326
TDD (805) 549-3259
<http://www.dot.gov/dist05>



*Flex your power!
Be energy efficient!*

September 23, 2010

Law Office of Marc Chytilo
P.O. Box 92233
Santa Barbara, CA 93190

Dear Mr. Chytilo:

This letter follows up on your request to the California Department of Transportation (Caltrans) made under the California Public Records Act (CPRA: Government Code §6250-6276), received August 30, 2010.

1. Your first three request involve confidential information. Withholding these records outweighs the public interest of disclosure [Gov. Code Section 6254(a), 6254(k), 6254(aa), 6254(ab), 6255(a); Evidence Code Sections 1040, 1041]. Your first three requests are also related to pending claims and/or litigation [Govt. Code Section 6254(b)].

You have indicated that you would like access to or receive a copy of documents relating to the quality and condition of the concrete on the Cold Spring Canyon Bridge, documents that assess the physical condition of the bridge and documents that relate to the engineering load and stress associated the cantilever net alternative. These records, which include sensitive information about the bridge's infrastructure that should not be disclosed due to public safety and security reasons, are exempt from disclosure under the Public Records Act. At the very least, disclosure of these documents is contingent upon the execution of a Confidentiality Agreement. Moreover, records prepared for, or in anticipation of, litigation or claims against Caltrans are exempt from disclosure. The information requested also includes confidential communications between Caltrans and its attorneys (attorney-client privilege) and materials prepared by the attorneys and other employees or consultants working at the attorneys' direction (work-product).

2. Your third request is also vague and overly broad.

Your third request for documents "assessing the physical condition of the Cold Spring Canyon Bridge following inspection or otherwise" is vague and overly broad. The request conceivably covers multiple subject areas and divisions within Caltrans, and spans decades since the bridge was built. Requests for public records must accurately and sufficiently describe the records sought so that identification, location and retrieval can be accomplished. Your request does not reasonably describe identifiable records. The request is unduly vague and needs to be refined. We believe the magnitude of your request as submitted would be oppressive and unreasonably burdensome of Caltrans' resources.

3. In response to your fourth request, enclosed is a copy of a Bridge Maintenance Project Initiation Report.

Should you have any questions, please do not hesitate to contact me at (805) 549-3318.

Sincerely,

A handwritten signature in cursive script that reads "Donna Carter".

Donna Carter
California Public Records Coordinator
Enclosure

"Caltrans improves mobility across California"

EXHIBIT 6

Response 18

The District agrees that the netting material should be as lightweight as possible, immediately usable after an event and easy to maintain. Marine-grade stainless steel wire netting satisfies all of these criteria. The net will incorporate a grid between 4 and 10 inches, the actual size to be determined during final design.

The District prepared the *Golden Gate Bridge Suicide Deterrent System Operations, Maintenance and Emergency Response Report* in order to evaluate the effects of the proposed alternatives on maintenance, operations and emergency response activities. This report, which discusses the impacts and associated costs, is available on the project website: <http://www.ggbsuicidebarrier.org/studydocuments.php>

Comment 19

Commenters requested information on the maintenance of netting including: repainting; cleaning and removal of catch debris and garbage; associated costs.

[Marin Mental Health Board (7c); Lew-McCrigler (10); Boyd (17); Edison (31); Hull (42a); Lee (47); Nygren (62); Teng (75); Topor (76); Aro (91c)]

Response 19

The net will incorporate a grid between 4 and 10 inches, the actual size to be determined during final design. The larger size would allow many common items, such as cameras, to pass through the net and fall to the water similar to what happens if a camera is dropped today. A smaller grid would capture more debris.

In addition to pedestrians dropping items into the net, debris from the roadway may accumulate in the horizontal net system. The Bridge is located at a windy site and lightweight debris may be blown onto the net. However, this lightweight debris which has been transported into the net by wind may similarly be removed from the net by the wind.

The net is most visible from the sidewalks at the towers (see photograph to the right). Thus, along the majority of the length of the net, where it is not readily visible to the public, a once every three month cleaning interval would likely be adequate.

However, the approximately 200 foot long length nearest the towers would be very visible, necessitating that this area be more regularly cleaned. The required frequency of cleaning to satisfy public expectations of cleanliness is



unknown at this time, since there is no basis to estimate how quickly trash will accumulate in these segments of the net.

The snooper truck that would be used for emergency operations with the net can be used to clean debris from the net. However, the snooper for emergency operations requires a single lane closure. In order to avoid traffic impacts associated with trash removal the District will purchase a second, smaller sidewalk-sized snooper (see photograph to the right) for debris removal operations. The cost of the smaller snooper truck is also included in the project cost estimate. As previously discussed the use of snooper trucks near mid-span is limited. Alternate methods will be used for cleaning the nets at these locations.



The District prepared the *Golden Gate Bridge Suicide Deterrent System Operations, Maintenance and Emergency Response Report* in order to evaluate the effects of the proposed alternatives on maintenance, operations and emergency response activities. This report, which discusses the impacts and associated costs, is available on the project website: <http://www.ggbsuicidebarrier.org/studydocuments.php>

Comment 20

Commenter states that the net will rot faster than metal.

[Lew-McCrigler (10)]

Response 20

The net would be made from marine-grade stainless steel wire netting so it will be quite durable.

Comment 21

Commenter states that if Alternative 3 is built, rescue staff would need repelling training and cherry picking training, use of a truck equipped with an inverted "cherry picker" mechanism and basket, requiring closure of the sidewalk and travel lanes on the Bridge. This would result in catastrophic delays.

[California Highway Patrol (3b)]

Response 21

If an individual were to jump into the net, the District would need to rescue the individual from the net. In order to provide for the safe retrieval of such an individual, the District would purchase an under Bridge inspection truck (UBIT), which are some times referred to as "snooper trucks". The

snooper truck would be used to access and facilitate retrieval of jumpers from the horizontal netting along most of the length of the Bridge. Snooper trucks have a truck-mounted bucket-controlled basket that can be used for access beneath a bridge from the roadway. The District would purchase a snooper truck which operates within a single lane closure and that has a reach to span over the sidewalk and reach down to the net. Several manufactures make such a unit. One example is the Aspen A-62, manufactured by Aspen Aerials, Inc.



The equipment and procedures involved in deploying the UBIT are quite complex, so the District would have to periodically practice retrieval operations in order to be adequately prepared to retrieve someone if necessary.

It is important to note that the use of snooper trucks would be limited within approximately 300 feet of either side of mid-span. Rescue of victims from this area would require specialized and highly technical "suspended rescue" techniques. Operation of snooper trucks would also be prohibited during severe wind conditions. In these instances Bridge workers would utilize the same rescue techniques that are contemplated for the rescue of an injured Bridge worker. A small davit would be deployed on the sidewalk and a personnel basket lowered to the location of the individual in the net.

Traffic congestion and motorist delays are a possibility associated with a net rescue. The deployment of the snooper truck would require the closure of a traffic lane, reducing vehicular capacity on the Bridge during the incident. Depending on the time of day (lane configuration in place and traffic demand) this may result in significant delay to the motoring public. In addition, the Bridge sidewalk would need to be closed in the vicinity of the snooper truck during such an operation. Based on the success of nets at other suicide hotspots traffic congestion and delay associated with a net rescue would be a rare, non-recurring occurrence. The impact on pedestrian and vehicular traffic on the Bridge and surrounding highways is not anticipated to be catastrophic.

The District prepared the *Golden Gate Bridge Suicide Deterrent System Operations, Maintenance and Emergency Response Report* in order to evaluate the effects of the proposed alternatives on maintenance, operations and emergency response activities. This report, which discusses the impacts and associated costs, is available on the project website: <http://www.ggbsuicidebarrier.org/studydocuments.php>

Comment 22

The commenter notes that since the California Highway Patrol does not train its personnel in the skills needed nor maintain the sort of vehicles and equipment to accomplish a rescue, other emergency personnel or Bridge workers and special vehicles would be needed to be called to the scene.

[California Highway Patrol (3c)]

Response 22

It is recognized that the California Highway Patrol does not train its personnel in the skills needed nor maintain the sort of vehicles and equipment to accomplish a rescue. In order to provide for the safe retrieval of such an individual, the District would purchase an under bridge inspection truck (UBIT), which are sometimes referred to as "snooper trucks". The snooper truck would be used to access and facilitate retrieval of jumpers from the horizontal netting along most of the length of the Bridge. Snooper trucks have a truck-mounted bucket-controlled basket that can be used for access beneath a bridge from the roadway. The District would purchase a snooper truck which operates within a single lane closure and that has a reach to span over the sidewalk and reach down to the net. Several manufacturers make such a unit. One example is the Aspen A-62, manufactured by Aspen Aerials, Inc.

In these instances where a snooper truck could not be deployed Bridge workers would utilize the same rescue techniques that are contemplated for the rescue of an injured Bridge worker. A small davit would be deployed on the sidewalk and a personnel basket lowered to the location of the individual in the net.

It is anticipated that the rescue operation discussed above would be a rare occurrence based on the history of other net applications. However, the equipment and procedures involved are quite complex, so the District would periodically practice retrieval operations in order to be adequately prepared to retrieve someone if necessary.

Comment 23

Commenters question if a person jumping into the net (a fall of 20 feet) would survive or sustain serious injury or require immediate medical attention.

[California Highway Patrol (3a); Goldin (40a); Moody (55); Muller (58a); Evans (96b); Hernandez (100b); Wellman (82)]

Response 23

The net is intended to impede individuals from jumping, and the installation of similar systems elsewhere has proven to be effective in that regard. It is possible that an individual who fell into the net could experience injuries, and it is possible that those injuries could worsen while the individual awaits emergency personnel arriving on the scene.

Comment 24

The commenter notes that jumpers into the net may resist help, assault rescuers, or otherwise complicate and delay rescue efforts.

[California Highway Patrol (3d)]

Response 24

It is possible that an individual who jumped into the net could resist help from retrieval personnel. The District, however, has prepared the *Golden Gate Bridge Suicide Deterrent System Operations, Maintenance and Emergency Response Report* in order to evaluate the effects of the proposed alternatives on emergency response activities, including how to retrieve an individual located on the net. As discussed in Responses 21 and 22, the District would purchase an under bridge inspection truck (UBIT), which are some times referred to as “snooper trucks”, to most effectively retrieve an individual from the net. The snooper truck would be used to access and facilitate retrieval of jumpers from the horizontal netting along most of the length of the Bridge. In the instances where a snooper truck could not be deployed, Bridge workers would utilize the same rescue techniques that are contemplated for the rescue of an injured Bridge worker. The District would also periodically practice retrieval operation in order to be adequately prepared to retrieve someone if necessary.

Comment 25

Commenters express concerns for the safety/well being of jumpers, Bridge rescue personnel, and pedestrians and motorists using the Bridge during rescues.

[California Highway Patrol (3e); Barrett (12); Goldin (40a); Montarano (54); Bohman (93b)]

Response 25

The District agrees that the selection of the suicide deterrent system should consider the safety of the persons at risk of doing harm to themselves, as well as the safety of Bridge employees, public safety personnel, and the vast majority of the pedestrians and motorists who use and depend on the bridge for its intended transportation purpose. The District prepared the *Golden Gate Bridge Suicide Deterrent System Operations, Maintenance and Emergency Response Report* in order to evaluate the effects of the proposed alternatives on maintenance, operations and emergency response activities. Based on this evaluation and based on the success of nets as a suicide deterrent at other structures the District has determined that the net alternative provides the least overall risk.

Comment 26

Commenters request information on the protocols for responding when the net is engaged by a jumper including: how easily are the nets accessed; how the Bridge staff and response personnel would be notified that the net is

engaged; what would the impacts to the Bridge users; and note that there would be recurring costs.

[Marin Mental Health Board (7c); Lew-McCrigler (10); Barrett (12); Raise the Rails (36); Raise the Rails (42b); Mcgale (51); Topor (76); Aro (91a); Bohman (93a, 93b); Garcia (98)]

Response 26

Responses 21 and 22 describe the equipment and activities that would take place to retrieve individuals from the net. As noted in these responses specialized vehicle, called a “snooper” truck is necessary to access the net and would be brought in during a retrieval event. Two specially trained rescue workers would be lowered down to the net in a bucket to pull the person out. Existing surveillance measures will be maintained to identify when an individual has landed in the net.

During a retrieval operation from the net, authorities would shut down a lane of traffic and the pedestrian pathways. The impact on pedestrian and vehicular traffic on the Bridge and surrounding highways is not anticipated to be severe. Moreover, retrieval operations are not expected to be a common occurrence. The District prepared the *Golden Gate Bridge Suicide Deterrent System Operations, Maintenance and Emergency Response Report* in order to evaluate the effects of the proposed alternatives on maintenance, operations and emergency response activities. This report, which discusses the impacts and associated costs, is available on the project website:
<http://www.ggbsuicidebarrier.org/studydocuments.php>.

Comment 27

Commenters question if a determined person would be able to crawl out of the net and jump after landing there.

[Barrett (12); Blangsted (15); Brigan (18c); Castaneda (20c); Cherny (21); Cremen (25); Doerr-Kashani (30); Johnson (45); Mcgale (51); Muller (58b); Munjee (59); Myhre (61); O’Neill (63); Radel (65b); Taylor (74); Watkins (80); Winfrey (84); Andersen (88); Andersen (89b); Arnal (90); Aro (91a); Bloom (92); Brunt (94); Cauble (95); Evans (96a); Heaton (99); Hernandez (100a); Hutchison (101b); P (102); Peterson (103); Rudisill (105a); Shea (106); Taylor (107); Williams (108)]

Response 27

The purpose of the project is to reduce the number of injuries and deaths associated with individuals jumping from the Bridge. Currently, each year approximately two dozen people kill themselves by jumping from the Bridge. Although the number of injuries and deaths associated with people jumping from the Bridge will greatly decrease with the installation of the net, it is possible that an individual who fell into the net could crawl out to the edge of the net and jump to their death.

Alternative 3 (Net System), the Preferred Alternative, was developed based on several successful installations of nets as a suicide deterrent. The most famous such installation is the Muenster Terrace in Bern, Switzerland. At that location the net has been in place for ten years, and to date, nobody has jumped into the net.

Comment 28

The commenters question if it would be possible for someone to jump far enough out to by-pass the net.

[Moody (55); Resnick (66)]

Response 28

As noted in the Draft EIR/EA the net would be located approximately 20 feet below the sidewalk and extend horizontally approximately 20 feet from the Bridge. Given the horizontal distance of the edge of the net from the Bridge, it would be very difficult for someone standing on the Bridge to jump beyond the net. As noted in Response 27, nets installed elsewhere have created a substantial deterrent to individuals jumping from other suicide hot spots.

Comment 29

Commenters question the safety of the net and what might happen if the net fails.

[Fieber (37); Moody (55)]

Response 29

The net will be constructed of marine-grade stainless steel cable supported by struts, or beams, that extend out from the structure. These elements will be designed to support the anticipated loads (or weights) that are likely to occur during the life of the net structure. Marine-grade stainless steel wire netting was selected for the netting material to insure that it maintains adequate strength and provides a long service life in the harsh marine environment that exists at the Bridge.

Comment 30

Commenters note that "thrill seekers" and pranksters could purposefully jump into the net.

[Brigan (18c); Cremen (25); Edison (31); Goldin (40a); Muller (58a); Rudisill (105b)]

Response 30

It is possible that the net may attract thrill seekers that would purposefully jump into the net. Alternative 3 (Net System) was developed based on several successful installations of nets as a suicide deterrent. Where nets have been used in such a fashion, they have been 100 percent effective, because people have stopped jumping off those structures. It is anticipated

that the number of deaths associated with people jumping from the Bridge will greatly decrease with the construction of the net, which is consistent with the purpose of the project and the District criterion that the project must "impede" the ability of an individual to commit suicide by jumping from the Bridge.

Comment 31

The commenter questions what impacts the net might have on maintenance workers, i.e. would they be safer or placed at greater risk by moving the net around.

[Brigan (18d)]**Response 31**

The District prepared the *Golden Gate Bridge Suicide Deterrent System Operations, Maintenance and Emergency Response Report* in order to evaluate the effects of the proposed alternatives on maintenance, operations and emergency response activities. This report, which discusses the impacts and associated costs, is available on the project website: <http://www.ggbsuicidebarrier.org/studydocuments.php>.

The maintenance activity impacted by the net alternative is associated with work using the maintenance traveler. Alternative 3 (Net System) is designed to be able to be pulled up in sections to allow unimpeded movement of the maintenance traveler. The maintenance workers are protected by the railing when the net is raised and are protected by the traveler railing when in the traveler.

Comment 32

Commenters state that the pictures show views towards the Bridge but not from the Bridge and that the images do not portray how the net would look from different angles.

[Diehl (28); Swaminathan (72)]**Response 32**

Simulations of the build alternatives (including the net) were created from 14 different viewpoints, in order to provide the public and reviewing agencies visual references for each of the build alternatives, and the opportunity to assess their potential visual impacts. These simulations are included in the Draft EIR/EA in Section 2.2, Visual Resources.

Existing and simulated views towards the Bridge were provided from viewpoints:

- Viewpoint 1 - Fort Point
- Viewpoint 2 - Baker Beach
- Viewpoint 3 - North Fishing Pier

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COLD SPRING CANYON BRIDGE

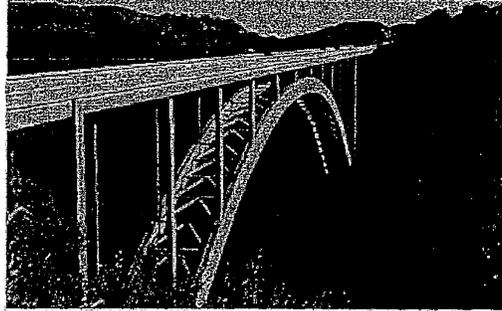
Location:
Near Santa Barbara,
California, USA

Description:
Two lane, steel arch bridge built in 1962. Soaring over the valley, it won the AISC Most Beautiful Steel Bridge (Long Span) Award in 1963.

Total length: 371 m (1214 ft.)

Main span: 213 m (700 ft.)

Owner and Client:
State of California



Scope of Work:

Buckland & Taylor Ltd. designed the seismic retrofit of California's longest span arch in the Phase II seismic retrofit program. Detailed analysis revealed that both the steelwork and the large thrust blocks at the arch springing, the "skewbacks", needed retrofit. However, because of environmental sensitivity, no access road was permitted to the skewbacks, so everything was designed based on access from the roadway deck only.

The retrofit scheme that we adopted made use of the existing continuity of the deck from end-to-end by adding CIDH restraining piles at the abutments, while allowing for expansion. The arch ribs and main towers were strengthened, and the skewbacks modified, including the addition of tie-downs for the arches. Completed in 1997.

Awards:

- 1999 - CELSOC Engineering Excellence Award of Merit was awarded to Buckland & Taylor Ltd. for the Cold Spring Canyon and Arroyo Quemado Arch Bridges Project. This project was one of only two Merit Award winners, which was chosen by judges for entry at the ACEC (American Consulting Engineers Council) level.
- 1999 - Buckland & Taylor Ltd. was awarded an ACEC Engineering Excellence Honor Award for the Cold Spring Canyon and Arroyo Quemado Arch Bridges Project.

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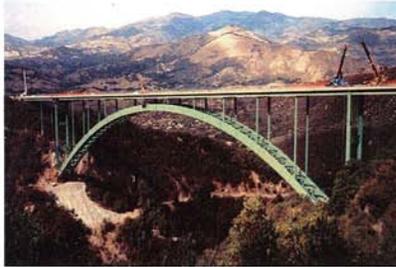


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Cold Spring Canyon Bridge - Seismic Retrofit



Completion Date:
February 08, 1998

This project involved the seismic retrofit of this deck arch bridge, originally constructed by American Bridge in 1962. The bridge features a 1,148-foot deck supported by twin steel box arch chords 9 1/2' deep by 3' wide. The twin arches are anchored in thrust blocks 700' apart at the base. Two towers form bents and rise from the thrust blocks to

support the bridge's outboard ends. All work was completed while the bridge was fully operational.

Major retrofit components included: New steel plates, bolted on the tops and bottoms of the twin boxes - plates reaching from the thrust blocks to points 80' upward on the arches. An all-bolted job, the plate reinforcement entailed the drilling of 21,000 holes; New 6-foot-diameter caissons were drilled 100' to anchor the abutments on each of their sides. On the valley side of each old abutment a new concrete grade beam was placed, supported at each end by a caisson. Huge rubber bumpers form cushions between the new grade beams and abutments; On the uphill sides of the old abutments, concrete blocks 13' x 20' in cross section and 34' long were placed to anchor the structure. Three caissons anchor each end of the blocks; Below, the thrust blocks were expanded with additional reinforced concrete. Some excavation was needed, and new concrete was doweled to the old; Two sets of 4 cables were drilled 125' down at each thrust block, tensioned and anchored with rock bolts, a total of 16 8-inch steel cables. At each tower, 4 cables were drilled straight down and 4 were drilled as an extension of the arch's diagonal; At the apex of the arch and at each tower, major steel bracing was connected to the underside of the deck to stiffen the structure against seismic loads.

There was no access to the south arch foundation and equipment and materials had to be lowered from the roadway approximately 130' above. Work on the roadway was performed on half the bridge at a time with traffic controlled in the remaining single lane.

[FULL VIEW]



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COLD SPRING CANYON BRIDGE

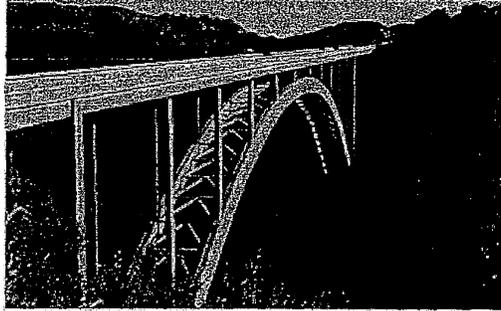
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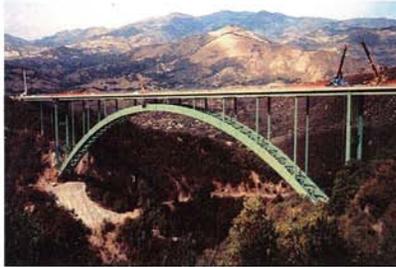


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Cold Spring Canyon Bridge - Seismic Retrofit



Completion Date:
February 08, 1998

This project involved the seismic retrofit of this deck arch bridge, originally constructed by American Bridge in 1962. The bridge features a 1,148-foot deck supported by twin steel box arch chords 9½' deep by 3' wide. The twin arches are anchored in thrust blocks 700' apart at the base. Two towers form bents and rise from the thrust blocks to

support the bridge's outboard ends. All work was completed while the bridge was fully operational.

Major retrofit components included: New steel plates, bolted on the tops and bottoms of the twin boxes - plates reaching from the thrust blocks to points 80' upward on the arches. An all-bolted job, the plate reinforcement entailed the drilling of 21,000 holes; New 6-foot-diameter caissons were drilled 100' to anchor the abutments on each of their sides. On the valley side of each old abutment a new concrete grade beam was placed, supported at each end by a caisson. Huge rubber bumpers form cushions between the new grade beams and abutments; On the uphill sides of the old abutments, concrete blocks 13' x 20' in cross section and 34' long were placed to anchor the structure. Three caissons anchor each end of the blocks; Below, the thrust blocks were expanded with additional reinforced concrete. Some excavation was needed, and new concrete was doweled to the old; Two sets of 4 cables were drilled 125' down at each thrust block, tensioned and anchored with rock bolts, a total of 16 8-inch steel cables. At each tower, 4 cables were drilled straight down and 4 were drilled as an extension of the arch's diagonal; At the apex of the arch and at each tower, major steel bracing was connected to the underside of the deck to stiffen the structure against seismic loads.

There was no access to the south arch foundation and equipment and materials had to be lowered from the roadway approximately 130' above. Work on the roadway was performed on half the bridge at a time with traffic controlled in the remaining single lane.

[FULL VIEW]



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Response to comments from Marc Chytilo, Law Office of Marc Chytilo, Attorneys for Friends of the Bridge

Your opposition to the project has been noted.

Response to comment #1: The open forum public hearings for both the 2008 Draft Environmental Impact Report (DEIR) and 2010 Draft Supplemental Environmental Impact Report (DSEIR) were conducted in accordance with the California Environmental Quality Act (CEQA) and Caltrans policies. The open forum format provided an opportunity for attendees to meet individually with Caltrans staff members in order to ask questions and discuss the project, view exhibits and handouts, and make environmental comments and formal statements on the CEQA document to the certified court reporter, in writing on comment forms, or after the hearing by U.S. Mail, email, or personal delivery.

Response to comment #2: Responses to comments and final project action will be made in accordance with CEQA and in compliance with orders of the court in the matter *Friends of the Bridge vs. Caltrans* (California Superior Court, County of Santa Barbara, Case No. 1338496).

Response to comment #3 3a i, Failure to Evaluate the Project's Consistency with the Corridor Protection Plan for Highway 154: As directed by the court, and consistent with CEQA, the Supplemental EIR and Updated Visual Impact Assessment fully discuss and disclose the project's consistency with the Santa Barbara County Comprehensive Plan, Land Use Element, Visual Resource Policy Number 2 [DSEIR page 10, Updated VIA page 4]. In addition, the Updated VIA specifically lists the County of Santa Barbara Comprehensive Plan Thresholds and Guidelines Manual criteria used for the County's determination of potential visual impacts as it relates to CEQA Appendix G [Updated VIA page 5].

In addition, the DSEIR and Updated VIA acknowledge and state that local concern and visual sensitivity to the project are rated high, based on review of Santa Barbara County planning policy and State Scenic Highway designation [DSEIR page 10, Updated VIA page 12]. This high level of viewer sensitivity is specifically cited as a reason for the determination that the project would result in a Significant Unavoidable visual impact.

The Updated VIA and DSEIR thoroughly analyze the visual effects of the project and fully disclose the project's effect on scenic views and visual quality. In the interest of full public disclosure, the Federal Highway Administration (FHWA) visual analysis methodology employed by the DSEIR and Updated VIA includes twelve photo-simulations of the project [DSEIR page 14-19, Updated VIA Figs. 2-13]. The adverse affect on existing visual quality is specifically cited as a reason for the determination that the project would result in a Significant Unavoidable visual impact [Updated VIA page 12].

The State Scenic Highway Program (2007) specifically states that “the designation of a route as an official scenic highway does not substantially alter the type of project proposed” [Updated VIA page 4]. The State Scenic Highway Program requires that projects be “evaluated for visual impact to scenic views as part of the environmental process” [Updated VIA page 4]. Consistent with the State Scenic Highway program, the Updated VIA and DSEIR fully evaluate, disclose and make findings based on the project's adverse impact to scenic views.

Response to comment #4 3a ii: Understatement of Visual Impacts to the State Scenic Highway: The Updated VIA and DSEIR identify Significant and Unavoidable Class I visual impacts resulting from the project, the most adverse finding of impact allowed under CEQA [DSEIR pages 21, 25; Updated VIA page 12]. The full extent and nature of the Significant and Unavoidable visual impact is fully discussed and disclosed in the Updated VIA and DSEIR. The visual impacts are clearly disclosed as:

Because of the expected high level of viewer sensitivity associated with the bridge and Highway 154, combined with the magnitude of visual change identified by the visual quality evaluation ratings, the project is anticipated to result in substantial adverse impacts to the visual environment. The impacts would be the result of:

The partial blockage of high-quality views from an Officially Designated State Scenic Highway; and

The visual incompatibility of the futuristic style barrier with the historic, somewhat industrial architecture of the existing bridge structure. [Updated VIA page 12]

The DSEIR and Updated VIA, based on application of FHWA analysis methodology, found that scenic vistas (including views from the Cold Spring Canyon Bridge) are not the sole determinant of visual quality along Highway 154. Rather, the varied

topography and native vegetation are also important contributors to the high visual quality of the corridor. The following summary description is found on page 11 of the DSEIR and page 10 of the Updated VIA:

The existing visual quality within the project area is high. This view quality is due primarily to the varied topography and native vegetation along the roadsides and adjacent hills. The exaggerated landform, curved road alignment and limited visibility of built elements outside of the roadway corridor also contribute to the existing visual quality. The alternating sweeping vistas of the Santa Ynez Valley and close-in views to the adjacent hillsides provide a dynamic viewing experience for the highway traveler.

The DSEIR and Updated VIA are consistent in their analysis and make an important distinction between views experienced while on the Cold Spring Canyon Bridge, and views experienced elsewhere on Highway 154. The DSEIR [page 20] and Updated VIA [page 11], state that:

The visual quality evaluation ratings done for the project show that a substantial change in visual resources would occur as a result of the proposed project. Although high-quality views from the highway while not on the bridge would remain mostly intact, the construction of a barrier would have an effect on as much as 70 percent of the existing view as seen specifically from the bridge deck.

This statement of fact describes the visual context of the project. The statement quantifies the adverse effect of the project while on the bridge itself, and also explains the important distinction that as seen from the remainder of Highway 154, existing views would not be affected. This visual distinction is important considering that the project would only affect approximately 0.2 mile (approximately 1/6th of one percent) of the total 32-mile mile length of the Officially Designated State Scenic Highway.

Even though the project would only affect approximately 1/6th of one percent of the total length of the view along the scenic highway, the DSEIR and Updated VIA identify the views from the Cold Spring Canyon Bridge as being of the highest quality. The high visual quality of the views is described as:

The Cold Spring Canyon Bridge offers some of the most memorable views along State Route 154 from the highway as well as from Stagecoach Road in the vicinity of the project [DSEIR page 11; Updated VIA page 10].

The views from the highway include the broad panoramas to the north and the wooded hillsides along the roadway to the south. The high quality of views from the roadway is emphasized by the elevated viewing position the bridge provides. [DSEIR page 11; Updated VIA page 3].

The effects on the high quality views as seen from the Cold Spring Canyon Bridge as disclosed are identified as one of the primary contributors to the Updated VIA and DSEIR finding of Significant and Unavoidable Class I visual impacts [DSEIR pages 21, 25; Updated VIA page 12].

Response to comment #5 3b i: Failure to Identify and Evaluate the Potentially Significant Impact to the Integrity of the Bridge Concrete: The grid/mesh alternative is designed to be reversible, with minimal permanent impact to the historic fabric of the bridge structure if the panels were to be removed in the future. The bridge substructure would be left entirely intact because the installation of the grid/mesh alternative would be along the existing concrete barriers flanking the bridge deck. The connection points are located on the outside of the concrete barrier, and all holes will be cored from the outside of the rail towards the inside.

Portions of the concrete rails that are visible to the traveling public do have locations where the concrete had already spalled (split or chipped) on its own, prior to the start of this project. There are also a few locations on the outer side of the rail where the concrete has spalled around the drilled holes. Such spalling is non-structural and is a minor and incidental part of working with existing concrete, including the concrete drilling associated with this project. To date, no patching has occurred as part of the current project, but the spalls will be patched prior to completion of construction.

Response to comment #6 3b ii: Failure to Identify and Evaluate Significant Historic/Cultural Impacts Associated with Views from the Bridge: Caltrans fully identified and assessed impacts to views from the Cold Spring Canyon Bridge in its analysis of project effects to the structure as a historical resource. The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register

Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

The assertion in the second full paragraph of page five of this comment letter regarding views from the Cold Spring Canyon Bridge is not supported by historical evidence, but rather is based on a misinterpretation of information. None of the evidence noted in this comment corroborates an intention on the part of the Division of Highways bridge engineers to design the bridge rails as a deliberate effort to afford expansive views from the bridge. Rather, as quoted from *California Highways and Public Works*, September-October 1963, page 15 (see Exhibit 3 of the comment letter), the bridge engineers mentioned in one sentence that views were a benefit of where the structure had been built, making no indication that the standard type railings used on the structure were employed to provide improved views. The six-page *California Highways and Public Works* article's discussion of engineering specifics, particularly about the substructure, and its explanation of the bridge's purpose to improve safety and travel speeds reflect the focus of the engineers' design. The quote in the letter taken from page 15 of the HRER citing the oral interview with Raymond Whitaker does not state that either he or other Division of Highways bridge engineers considered views from the bridge when choosing the standard railing type, but rather that the design allowed for views. Also misinterpreted in this comment letter is the mention of setting that is in the final paragraph of the *California Highways and Public Works* article where the importance of the structure's engineering and aesthetic design is summarized. The "picturesque setting" referred to in the paragraph in the article is coupled with a declaration of the structure's "pleasing appearance," drawing attention to the successful way the structure fits into the canyon — not the wider views of the Santa Ynez Valley from the bridge. As noted on page 14 of the HRER, steel arches were considered suitable and economic engineering choices for bridges that span steep canyons, as well as being aesthetically pleasing. The final paragraph of the *California Highways and Public Works* article reflects the importance of the bridge's engineering design and the value the engineers placed on views of the bridge, not from the bridge.

The surroundings of a historical resource are the areas adjacent to, and importantly associated with, the historical resource. Caltrans' analysis of the bridge's historic

integrity of setting presented in the HRER and Finding of Adverse Effect report is based on the explanation of the term in *National Register Bulletin 15: How to Apply the National Register Criteria for Evaluation*, wherein setting is defined as the “physical environment of a historic property” (page 45) (available online at: <http://www.nps.gov/nr/publications/bulletins/nrb15/>).

The way in which the Cold Spring Canyon Bridge is situated in its canyon was noted at the time of the bridge’s construction as part of its important aesthetic qualities. Alterations to areas in the environment adjacent to the structure, i.e., its immediate surroundings, have the potential to cause a substantial adverse change to the historical resource. This could occur if a new structure were to obstruct views of the historical resource. As noted in the Finding of Adverse Effect, the project to install the suicide barrier does not impact the bridge’s surroundings; the conclusion was accordingly drawn that the project did not diminish the bridge’s historic integrity of setting. As noted, historic evidence does not support the assertion that views from the bridge are character-defining in supporting its eligibility for listing in the National Register. The State Historic Preservation Officer (SHPO) has concurred in, and the Advisory Council on Historic Preservation (ACHP) did not object to, Caltrans’ eligibility conclusions that identified the structure’s character-defining features or Caltrans’ findings of effect that addressed how the project would impact the Cold Spring Canyon Bridge’s historic integrity (Attachment 20 of the DSEIR).

The comment under this heading fails to recognize that a horizontal safety net alternative would diminish the Cold Spring Canyon Bridge’s historic integrity of design, materials, and workmanship in ways that the preferred alternative does not because a safety net alternative could not be constructed in a manner that is reversible. A safety net alternative would consequently meet fewer of the Secretary of the Interior’s Standards for Rehabilitation than the preferred alternative does. The analysis regarding this issue is presented in the Supplemental Report (Attachment 20, pages 17-19, of the SEIR).

As-built drawings of the Cold Spring Canyon Bridge, which included the standard Type II railings used on the structure, were reviewed by architectural historians for the HRER (and are cited therein). These drawings do not support the assertion that views were part of the “deliberate design” considered in the decision to use the Type II railings. The bridge design sheets show various standard railings including railings that were taller, shorter, and narrower than the Type II barrier railings chosen. They did not choose the shortest or smallest standard railing. As noted on page 15 of the

HRER, the bridge engineers chose the railing to fulfill physical and safety requirements of a highway barrier that would successfully be incorporated into the design working with the careful load calculations of the substructure and deck.

Response to comment #7 4a: Additional Visual/Aesthetic Mitigation Measures:

A variety of materials were considered by the design team as well as by the Aesthetic Design Advisory Committee. Glass and Plexiglas panels were considered [DSEIR, Appendix G, VIA, Appendix G, Meeting #2 Summary Cold Spring Canyon Bridge ADAC] for the suicide barrier design but they do not meet the criteria to stay within wind load tolerances. Wind load is an important factor in bridge design due to the stress and instability that wind can create on the structure. Wind tunnel testing confirmed that the acceptable solid ratio varies between 12 and 23%. The solid ratio is defined as the total area of the solid components of the barrier divided by the total area of the barrier. The findings of this study are discussed in the *Phase 1 Wind Studies Report, Environmental Studies and Preliminary Design for a Suicide Deterrent System Contract 2006-B-17 by the Golden Gate Bridge Highway and Transportation District, dated May 24, 2007 (www.ggsuicidebarrier.org)*.

Response to comment #8 4b: Ineffectiveness of Key Historic Mitigation

Measure: The heading for this comment is “Ineffectiveness of Key Historic Mitigation Measures,” but the comment is not about this issue. Rather, the comment is related to access to engineering information about the bridge. The Memorandum of Agreement (MOA) stipulates that Caltrans is to reproduce plans and drawings for the Historic Architectural Engineering Record (HAER) documentation (MOA Stipulation III.A.2.).

The type of drawings / plans that are typically included in HAER documentation is different from the detailed engineering data related to load and stresses that have been requested by Friends of the Bridge. The HAER documentation includes general layout and plans that supplement the written description of the bridge and its significance, showing dimensions and general configuration of a structure. Such plans are not deemed confidential for security purposes.

In addition, please note that, in reference to the heading for this comment, Caltrans provided the ACHP an opportunity to review a draft of the HAER report and other draft documents stipulated in the MOA. Charlene Dwin Vaughn, the ACHP Assistant Director for Federal Permitting, Licensing, and Assistance Section, Office of Federal Agency Programs, responded in a letter on October 18, 2010, stating that

(t)he quality of the work exhibited in all three documents is exemplary. These documents create a lasting record of the historic Cold Spring Canyon Bridge as an elegant and beautiful engineering marvel. The booklet and interpretive displays are informative and well-illustrated, and will be a resource for local and state residents. We commend you for a job well done.

Response to comment #9 5a: Improper Rejection of Horizontal Barrier

Alternatives: The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the DSEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Cold Spring Canyon Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and

Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would. As discussed above, views from the Cold Spring Canyon Bridge are not a character-defining element of the historical resource, and the impacts of the vertical barrier are therefore limited to diminishing the bridge's integrity of design, feeling, and association, as compared to impacts of a safety net barrier.

Response to comment #10 5a i: Failure to Disclose that Horizontal Barriers Are More Effective than Vertical Barriers: The safety net alternative was considered but rejected in the DEIR, FEIR (Section 1.4.6 Alternatives Considered but Eliminated From Further Discussion, pp 10-12), and DSEIR (Section 1.5.1, pp 5-7).

Research indicates that having a barrier in place is an effective deterrent for individuals who wish to attempt suicide by jumping from bridges. There is evidence that suicides by jumping off tall structures are highly impulsive acts, sometimes with only seconds between the impulse and the jump. Barriers of all types make jumping more difficult and buy time for reconsideration or intervention by others. Studies show that a vertical barrier is an effective suicide and jump deterrent (Pelletier, A.R. *Injury Prevention* 2007 13(1): 57-9; Beautrais, Annette L.; Sheree J. Gibb; David M. Fergusson; L. John Horwood; Gregory Luke Larkin. "Removing bridge barriers stimulates suicides: an unfortunate natural experiment". *Australian and New Zealand Journal of Psychiatry* 2009 43 (6): 495-497; Sinvor, Mark; Anthony J. Levitt. "Effect of a barrier at Bloor Street Viaduct on suicide rates in Toronto: natural experiment". *British Medical Journal* 2010; 341: c2884.)

As discussed above, the rejection of the horizontal safety net alternative was not due to its level of effectiveness in deterring suicide attempts, but because it was found to not be feasible on the Cold Spring Canyon Bridge for the various reasons cited in Response to comment #9 above, and elaborated upon in Responses to comments #12-#18 below.

The vertical barrier is more effective in reducing emergency personnel to exposure of risks than the horizontal net barrier. Furthermore, a horizontal net barrier system capable of holding more than one person would have greater impacts to the qualities under which the Cold Spring Canyon Bridge is considered to be eligible to the National Register and, additionally, have a greater cost. Therefore, the vertical barrier meets the project's purpose and need better than the horizontal net barrier systems.

Response to comment #11 5a ii: Failure to Differentiate Among the Safety Net Barrier Designs and Lack of Substantial Evidence Regarding their Infeasibility:

As described in the FEIR and DSEIR, the safety net alternative, including the five variations of the horizontal net barrier discussed, were considered but rejected as not feasible for the Cold Spring Canyon Bridge Suicide Barrier project. The measures of feasibility were analyzed for all variations and found to be very similar for all of them, including the Cantilever Net barrier variation as discussed below.

Response to comment #12 5a 1: Unacceptable Rescue Response Times: As discussed in the DEIR, FEIR and DSEIR, the remote location of the bridge can result in lengthy emergency response times. In addition, individuals who fall into a net at night may not be seen for an extended period of time, which may allow them additional time to make their way to the edge of the net structure or cantilever net structure and attempt to jump again before rescue crews arrive. While delay associated with a net rescue at the Golden Gate Bridge is considered rare, this rare instance of delay is related to various factors and circumstances associated with the Golden Gate Bridge. These factors include the Golden Gate Bridge's urban setting and 24-hour surveillance systems, including video, lighting, and the constant presence of California Highway Patrol, Caltrans maintenance forces and the public. An Under Bridge Inspection Truck is also exclusively assigned to the Golden Gate Bridge. None of these factors or circumstances are in place at the Cold Spring Canyon Bridge.

Response to comment #13 5a 2: Increased Danger to Individuals Attempting Suicide: For the reasons stated above in Response to comment #11 and in the DEIR,

FEIR and DSEIR, the safety net alternative was considered but rejected because it was determined to not be feasible for the Cold Spring Canyon Bridge Suicide Barrier project.

This feasibility measure is based on the assumption that an individual has already jumped into the net. The general deterrent value of horizontal net barriers is not relevant to individuals that have already jumped and are caught in the metal netting. These individuals may be injured, and if not seen for an extended period of time, may still be exposed to cold, heat, wind, rain, and further psychological trauma as described in the DEIR (page 11), FEIR (page 12) and DSEIR (page 6) discussions. This would be true for all horizontal net barrier variations including the cantilever net variation.

In addition, on page 4-29 of Exhibit 6 of your comment letter (Golden Gate Bridge response to comments), the Golden Gate Authority acknowledges that “individuals who fell into the net could experience injuries, and it is possible that those injuries could worsen while the individual awaits emergency personnel arriving on scene.” There is no contrary evidence to dispute either the Authority’s response in Exhibit 6 of the comment letter, or Caltrans’ reason for rejecting the safety net alternative as disclosed in numerous project documents for this project.

Response to comment #14 5a 3: Unacceptable Risk to Emergency Response and Rescue Personnel: For the reasons stated above in Response to comment #11 and in the DEIR, FEIR, and DSEIR, the safety net alternative was considered but rejected as not feasible for the Cold Spring Canyon Bridge Suicide Barrier project. A safety net would not meet the project’s Purpose and Need [(DEIR pages 1-5), FEIR (pages 1-6), and incorporated in DSEIR by reference (page 3)], which states in part:

2. Reduce the exposure to risks for emergency personnel such as law enforcement officers or search and rescue teams when attempting to prevent persons from jumping off the bridge, and reduce the number of recoveries that need to be performed following a suicide jump from the bridge.

As stated in the DEIR (page 10), FEIR (page 11) and DSEIR (page 6), during a rescue operation, emergency personnel may be pulled over the existing rail. While attempting to maintain balance in the unstable net, using technical rescue equipment, securing/subduing a distraught, uncooperative, violent person, and then hoisting them to the top of the bridge, is unacceptable risk to rescue personnel. Search and Rescue Team members are not in law enforcement or trained to confront potential combatants

and Sheriff's Deputies are not trained in the specialized field of search and rescue, and depend on the Search and Rescue Team.

Adding a "secured walkway under the bridge deck" or deciding to "raise the height of the existing Bridge railing" would have additional adverse impacts on the historic, aesthetic/visual, and engineering of the Cold Spring Canyon Bridge (see response to comment #23), and would not serve the project's Purpose and Need. Refer to response #18, below for discussion of the purchase and use of an Under Bridge Inspection Truck (UBIT) at the Cold Spring Canyon Bridge.

Moreover, one of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to "Reduce the exposure to risks for emergency personnel..." In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff's office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

Response to comment #15 5a 4: Increased Impacts to the Historic Substructure:

The DSEIR discusses in depth several types of horizontal net barriers and their effects on the Cold Spring Canyon Bridge. Five categories of horizontal net barriers (DSEIR, page 5) were considered as potential alternatives but eliminated from further discussion. At the request of the State Historic Preservation Officer (SHPO), two refined designs were developed from these five categories of ideas: the Safety Net Alternative and the Cantilever Arc Barrier Net Alternative.

The Safety Net Alternative, similar to that proposed for the Golden Gate Bridge, is developed in the Supplemental Report, included as Attachment 20 of the Supplemental EIR. The Supplemental Report discusses in depth the impacts to historic integrity of the structure as well as the evaluation of the adverse effects of the proposed Safety Net Alternative. It addresses the weight that would be added to the bridge, the dimensions of the structural elements that would be required to construct the net alternative, and how this alternative would alter the overall design of the bridge, including the retrofitting requirements to support the safety net. As discussed in the Supplemental Report, the Safety Net Alternative would require Caltrans to

construct an entirely new steel structure to frame the panel sections of net and attach the new structure to the columns, towers, and arch ribs in the substructure and tie the panels back to the concrete railing on the superstructure. The Safety Net Alternative would not only diminish the bridge's historic integrity of design, feeling, and association, it would diminish the bridge's historic integrity of materials and workmanship in ways that the vertical barrier alternatives do not.

The Cantilever Arc Barrier Net Alternative is developed in the Feasibility Study, included as Attachment 39 of the SEIR. This design, along with the constraints, was developed in conjunction with the SHPO. The Feasibility Study addresses the weight that would be added to the bridge, the dimensions of the structural elements that would be required to construct the cantilever arc barrier net, cost estimate, load limitations, and liability of the design. Retrofitting of the substructure, bridge deck, and the existing concrete rails was required if the system was developed to support anything more than a single load. As discussed in the Feasibility Study, one of the design constraints was that the net structure would avoid being tied back to the steel substructure so that retrofitting the substructure to meet the required load system would not be necessary. In other words, the Caltrans engineers and the SHPO designed the Cantilever Arc Barrier Net so that attachment locations were restricted to the existing concrete bridge deck and concrete rails (Attachment 39, page 1).

With the constraint of attaching the cantilever arc barrier net to the existing concrete bridge deck and rails, however, the design can support only a single load (500 pounds) (Attachment 39, page 3). Design standards require that we design the system for greater than a one-person load (dead load + live load).¹ The moment generated by the combined weight of the cantilever and additional personnel (dead load + live load) would be too great on the existing concrete bridge rails and bridge deck, which were not designed to support such loads. Therefore, from an engineering standpoint, the Cantilever Arc Barrier Net Alternative is not feasible without extensive physical modifications to the bridge. As discussed in the Feasibility Study (Attachment 39, page 3), a design for the Cantilever Arc Barrier Net Alternative that would support the required load would entail altering the superstructure, including replacing the existing concrete rails and the entire bridge deck. Construction of the barriers would also very likely entail changes to the substructure. This would lead to greater permanent and irreversible structural changes to the Cold Spring Canyon Bridge,

¹ Dead loads, also referred to as permanent loads, are the self weight of the components of the structure. Live loads are intermittent or variable loads during the life of the structure. For example, traffic on the bridge would be considered a live load on the bridge structure.

further decreasing the bridge's historic integrity and adversely affecting those qualities that make the bridge eligible for listing on the National Register of Historic Places (DSEIR Attachment 39, page 4).

Construction of a net alternative would not lessen impacts to the historic integrity of the Cold Spring Canyon Bridge. A net alternative may avoid "view blockage" but, as noted above, views from the bridge are not considered to be among the character-defining qualities that make this structure eligible for listing in the National Register.

Varying the placement of a safety net has also been evaluated by Caltrans in an attempt to minimize impacts to the historic substructure. In addition to considering a safety net located 20 feet below the bridge deck, a safety net 13 feet below the bridge deck was also considered (DSEIR, Attachment 20, Figure 4). Attaching the safety net at 13 feet below the bridge deck would place the net near the top of the arch, whereas attaching the safety net at 20 feet below the bridge deck would place the net just below the arch ribs in the center of the structure. Whether located 13 feet or 20 feet below the bridge deck, however, a safety net still results in similar impacts to the historic integrity of the bridge, as discussed above and as addressed more thoroughly in the Supplemental Report.

All safety net alternatives were designed so that one or more net panels could be raised during maintenance operations by articulating at the bridge and safety net frame connection point (DSEIR Attachment 20, under Impacts to Historic Integrity, and Figure 4). This design is to accommodate the Under Bridge Inspection Truck.

The net variation suggested in the comment – a barrier net structure that follows the contours of the bridge arch – would only marginally change the impact that other suggested net alternatives would have on the bridge's historic integrity of design. While focused on lessening impacts to the bridge's main span arch, the suggestion does not take into account the contribution that the bridge's columns and towers play in the overall substructure design and their role as character-defining features of the bridge. Furthermore, it is not clear whether such a design would be feasible or effective. The suggested variation is similar to the safety net proposed to be built 13 feet below the bridge deck, which has already been discussed and evaluated in the Supplemental Report and withdrawn from further consideration.

As discussed above, the impact to the bridge from construction of a horizontal net barrier would diminish its historic integrity. The horizontal net barrier alternative would also be less compliant with the Secretary of Interior's Standards for

Rehabilitation than the preferred alternative because a horizontal net barrier alternative could not be built in a reversible manner. The requirement for retrofitting the structure is identified in the responses to comments.

Response to comment #16 5a 5: Liability Associated with an Attractive

Nuisance: Caltrans owns and operates 12,168 bridges and inspects an additional 11,790 locally owned bridges throughout the State of California. As such, Caltrans is the State’s expert on liability issues related to highway structures, including liability issues associated with the Cold Spring Canyon Bridge. Caltrans has photographic evidence of individuals climbing the Cold Spring Canyon steel arch from the footing of the structure; on one particular occasion, individuals reached the top of the arch. People have been known to play musical instruments as they walk along the length of the bridge.

An additional liability and maintenance issue is graffiti. Cold Spring Canyon Bridge is already a target for graffiti, as depicted in the pictures below (see Figures 2-14 and 2-15). Graffiti will become a larger issue should there be additional access to the bridge available with the design and installation of a horizontal net barrier.



Figure 2-14 Graffiti on the Cold Spring Canyon Bridge



Figure 2-15 Graffiti on the Cold Spring Canyon Bridge

The Golden Gate Bridge Barrier FEIR acknowledges that a safety net could be an attractive nuisance (page 4-32); the document states that “it is possible that the net may attract thrill seekers that would purposefully jump into the net”. The Golden Gate Bridge Authority is better able to respond to issues associated with thrill seeking activity due to the urban setting of the bridge and the fact that there is 24-hour video

surveillance, elaborate lighting as well as the constant presence of Caltrans maintenance staff and assigned California Highway Patrol. The large number of people using pedestrian facilities along both sides of the structure also provides a deterrent to thrill seekers. The Golden Gate Bridge has a much different setting and surroundings when considering these types of issues. In contrast, the Cold Spring Canyon Bridge is located in a remote, rural setting. There is no permanent maintenance staff or 24-hour law enforcement assigned specifically to the Cold Spring Canyon Bridge; neither is there lighting or pedestrian facilities on the Cold Spring Canyon Bridge.

As stated in the Feasibility Study [DSEIR, Attachment 39], the probability of individuals or groups gathering on the Cantilever Arc Barrier Net is very high and of great concern to Caltrans. This concern also extends to the Safety Net Alternative. The Chief Law Enforcement Officer of the County, Sheriff Bill Brown, also shares these concerns and, for these reasons, is not in favor of horizontal net type designs. Please see his comment included in this document (in Section 2.0 Santa Barbara County Government and Commission).

The Sheriff's office has also since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

Response to comment #17 5a 6: Design Load Limitations: The statement on page 7 of the DSEIR, "Construction of the barriers would also entail retrofitting the substructure" is correct when the dead load (the weight on the structure that would result from installation of a horizontal safety net-type barrier) and live load (the weight of materials or human beings that may fall or climb into the net) are considered together. Caltrans did consider the use of lightweight materials for the construction of the Cantilever Arc Barrier Net [DSEIR, Attachment 39, page 2]. These would help reduce the dead load; however, the dead load is not the driving reason for the need to modify the superstructure and substructure. It is the live load that drives the need for the structure to be altered. As discussed in the Feasibility Study [DSEIR, Attachment 39, page 3], the Cantilever Arc Barrier Net design could only support a single live load, which is equivalent to one person, without substantially changing the structure through thickening of the bridge deck and rails and as well as the steel members of the substructure. Refer also to Response to comment #15, above.

Response to comment #18 5a 7: Unacceptable Risks Associated with Safety Net Maintenance: Trash, litter and graffiti are issues for Caltrans maintenance crews and we anticipate that trash and litter would get caught in a safety net. Removal of these materials would require the use of an Under Bridge Inspection Truck (UBIT), which is used for bridge inspections across the State of California and only brought to the Cold Spring Canyon Bridge site every two years. Use of the UBIT would not be a viable option for maintaining or cleaning a safety net at Cold Spring Canyon Bridge on a regular basis.

It is our understanding that the Golden Gate Bridge District was recommended to purchase a UBIT for exclusive use at the Golden Gate Bridge if the safety net option is constructed there (www.ggbsuicidebarrier.org). The Golden Gate Bridge UBIT would be deployed should an individual need to be retrieved from the net. The Golden Gate Bridge also has catwalks in the bridge's truss that are used for bridge inspections; such catwalks are not considered feasible for the Cold Spring Canyon Bridge. Many man-hours are required for training and practicing retrieval operations to perform this task. A lane closure on the Golden Gate Bridge will be required during the operation of the UBIT. A lane closure is also required on the Cold Spring Canyon Bridge when the UBIT is deployed. An example of the effort that would be required to be ready to conduct rescue operations on the Golden Gate Bridge is in the following text, which is provided from the Golden Gate Bridge Study:

It is anticipated that a rescue operation would be a rare occurrence based on the history of other net applications; however, the cost and operational impacts of being prepared for such an operation would not be negligible. The equipment and procedures are quite complex and the District would have to periodically practice retrieval operations in order to be adequately prepared to retrieve someone if necessary. Assuming retrieval operations are practiced once a month, require six staff (2 ironworkers, 2 operating engineers and 2 Bridge personnel) and lasts two hours, this corresponds to 12 hours or 720 minutes of lost productivity each month which corresponds to 32.8 minutes of lost productivity per day (720 minutes/month, 22 days/month).

The jurisdiction for rescue operations at the Cold Spring Canyon Bridge is the Santa Barbara County Sheriff-Coroner. Therefore, a UBIT would have to be purchased, operated, maintained, and housed by the County Sheriff-Coroner Department. This is unlikely since the County Sheriff-Coroner Department is not trained or licensed for heavy equipment, such as a UBIT. The UBIT would also have to be housed at a

location near the Cold Spring Canyon Bridge, which would require the construction of an equipment barn in the vicinity of the project site.

Due to the remote location of the Cold Spring Canyon Bridge, and the expense of purchasing (\$700,000), operating, and maintaining the UBIT, this is not a practical solution to address the increased maintenance resulting from an addition of a safety net to the Cold Spring Canyon Bridge.

Response to comment #19 5a 8: Increased Costs Associated with Need to Replace Safety Net: When an individual drops onto the safety net, the net will deform. Deformation of the net thus makes it difficult for a person to climb out. Therefore, the deformation of the net is an important deterrent against individuals jumping again, from the net structure. Regardless of the material, the net would have to be replaced each time a person jumps into the net structure, which adds to the maintenance cost.

Response to comment #20 page 11, iii: Failure to Provide Sufficient Information Regarding Potential Retrofitting to Evaluate its Feasibility and Impacts: The DSEIR, pages 16 to 19, discusses impacts the net alternative would have on the Cold Spring Canyon Bridge, including which character-defining structural and design components would likely be affected by such an alternative. These components include the bridge's form, plan, and proportions, along with the spatial relationship of its components and the arrangement of spaces between the bridge's various essential physical features. These features include the columns, towers, and arch ribs, all of which would be impacted by construction of a net alternative and be further impacted if retrofitting required alteration to those components. Such alteration could include addition of materials, encasing of substructure members, addition of cross bracing between columns and/or towers, and changes to the spaces and proportions of the bridge.

As discussed in Response to comment #15, the impact to the bridge from construction of a horizontal net barrier would diminish its historic integrity. The horizontal net barrier alternative would also be less compliant with the U.S. Secretary of Interior's Standards for Rehabilitation than the preferred vertical barrier alternative because the horizontal net alternative could not be built in a reversible manner.

Modifying the superstructure - the existing concrete bridge deck and concrete rails - is not required for the grid/mesh alternative or the no-build alternative. Please see the discussion in regards to the concrete quality for Response to comment #5. In regards

to the speculation that the Cantilever Arc Barrier Net design may or may not require physical modifications, please refer to the answer regarding the design load limitations for the Cantilever Arc Barrier Net design, Response to comment #17.

As discussed in the Supplemental Report (DSEIR Attachment 20, under Impact to Historic Integrity), Caltrans has concluded that the installation of a Safety Net Alternative would mean that most, if not all, substructure components of the bridge would need to be physically altered in order to carry the additional dead and live loads of the nets and to withstand the wind and seismic loads. The alterations could require Caltrans to encase substructure members, covering the original steel structure, and could require Caltrans to construct cross bracing under the bridge deck between the columns.

Conclusions drawn in the HRER indicate that Caltrans considered the Cold Spring Canyon Bridge's aesthetic qualities when it designed the 1997 seismic retrofit (which took place before the bridge had been determined eligible for listing on the National Register). While it is possible that a theoretical retrofit for an undetermined purpose may not cause a substantial adverse change to the historical resource, as is suggested in this comment, the changes required to modify the structure for construction of the net alternative do pose that potential impact, as discussed. When considering the additional live and dead loads that the bridge structure would be subject to with the addition of the net alternative, alteration requirements of the structure can be significantly different than a seismic retrofit. The SHPO concurred in, and the ACHP did not object to, Caltrans' finding that the 1997 seismic retrofit did not diminish the bridge's historic integrity.

Response to comment #21 pages 12-13, comment iv: Failure to Analyze the Visual/Aesthetic Impacts of Horizontal Barriers: The FEIR and DSEIR discuss several types of safety nets and horizontal net barriers, and their impacts on the Cold Spring Canyon Bridge. Five categories of horizontal net barriers [page 5, DSEIR] were considered as potential alternatives but eliminated from further discussion because they were found not to be feasible. Since these alternatives were found not to be feasible, further analysis regarding potential visual impacts as a result of safety nets and horizontal net barriers were not warranted.

Response to comment #22 page 13, comment v: Failure to Consider Increased Achievement of Project Objectives from Horizontal Barriers: One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to "Reduce the exposure

to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal net alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

Response to maintenance of the horizontal net alternatives is provided in Response to comments #18 and #19. The maintenance requirements for both the Cantilever Arc Barrier Net design and the Safety Net design would be far greater than those required for the vertical mesh barrier alternative. Both horizontal net alternatives would require graffiti removal, trash removal, replacement of steel mesh panels when they have been damaged, and increased inspection responsibilities, including inspection of the articulating bolts for the safety net alternative. The number of required lane closures will increase with the increased maintenance and inspection activities, which will increase costs and impact to the traveling public.

Response to comment #23 page 13, comment vi: Preferred Horizontal Barrier Alternative: As described in the 2008 DEIR, 2009 FEIR, and 2010 DSEIR, safety net/horizontal net barrier alternatives were considered but rejected as not feasible for the Cold Spring Canyon Bridge Suicide Barrier project.

For the reasons discussed in Response to comment #15 above, construction of a horizontal net barrier would not reduce impacts to historic resources. The extensive retrofit that would very likely be required to install a horizontal net barrier would result in an increase in the impacts to the historic substructure of the bridge.

In addition, the suggestion of raising the existing bridge railings in combination with constructing a horizontal net barrier would likely cause an additional adverse effect by introducing a visual element that diminishes the property’s historic integrity of design, feeling, and association. Increasing the height of the bridge rail would also not comply with the Secretary of Interior’s Standards for Rehabilitation, Standards 2 and 9. Thus, inclusion of this aspect of the suggested alternative would further decrease the ability of the horizontal net barrier alternative to reduce impacts to historic

resources. Response to comment #22 also addresses issues in regards to raising the height of the existing bridge railings.

Additionally, aside from the impacts to historic resources, increasing the height of the bridge rail would adversely affect views from the Cold Spring Canyon Bridge on this Officially Designated State Scenic Highway.

Matt C. Fowler
Senior Environmental Planner
Caltrans
50 Higuera St.
San Luis Obispo, Ca 93401

Mr. Fowler,

Regrettably I missed your meeting on Tues. 1/5/11 regarding the suicide barriers contemplated for the Cold Springs bridge.

I read the press coverage looking for the one really salient fact regarding this project. As hardhearted as it may sound, I question where the state will borrow the funds to construct this barrier. The state does not have the funds to cover the weekly unemployment payments so even the paltry few millions needed to pay for this project will also have to be borrowed. If we are capable of justifying this multimillion dollar project to save an average of around one jumper a year, how many millions should we spend to fence the railroad tracks in Santa Barbara to save one teenager walking along the tracks distracted by his ipod. The financial crisis in California is real and demands some pragmatic decision making when it comes to spending. I do not think this project is worthy when we have so many other more critical needs.

Bill (payer) Garnett
Santa Barbara



Response to comments from Bill Garnett

Your opposition to the project has been noted.

Local Recovery Act funds, which are federal funds, have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.



Celeste Barber
<celestebarber52@verizon.net>

01/03/2011 10:24 AM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Letter to Caltrans re Cold Springs Bridge, Jan 2011

Dear Mr. Fowler,

Unfortunately, we cannot attend this coming Wednesday's public hearing in Santa Barbara regarding Cold Springs Bridge. My husband and I oppose the CalTrans proposal to construct a suicide barrier there. I have attached our letter to this email and request that it be presented at the January 5th hearing.

Please confirm back that you have received both this email notification and the attachment.

Thank you,



Celeste Barber Letter to Caltrans re Cold Springs Bridge, Jan 2011.docx

January 3, 2011

Caltrans
50 Higuera Street
San Luis Obispo, CA. 93401
Attention: Matt Fowler

Re: Public Hearing on Cold Springs Bridge, 1-5-11

Dear Mr. Fowler,

We had planned to attend the public hearing set for this coming Wednesday in Santa Barbara to speak against the proposed suicide barrier for Cold Springs Bridge. However, we will be out of town this week. I would like our comments to be included in the public record at that time.

My husband and I oppose the building of a suicide barrier on Cold Springs Bridge. We believe that such a barrier offers little remedy to those individuals who tragically believe suicide is the only option left for them. First, compare the number of suicides from the bridge to the total number countywide. According to Caltrans figures, 54 people have died there since 1964, averaging 1 death per year to date. Compare that to the total number of suicides countywide over a recent three-year period from 2007 through 2009: 122 deaths (Sheriff's Department statistics).

1

Furthermore, anyone who contemplates suicide from Cold Springs Bridge is not doing so on the spur of the moment. This bridge is located well outside a populated area, and one must drive a good distance. Suicide from Cold Springs Bridge is not a rash act but a calculated action of desperation. Compare this to those who have committed suicide on our railways – often with easy access and usually in populated areas. Sadly, on the two trips that I have made with Amtrak to Orange County in the past 15 years, there were rail suicides both times, the most recent this past October in Moorpark. And the following day, another suicide at the Camarillo train station. Yet there are few constraints to railway access.

It would be truly sad to lose this treasure. The bridge offers breathtaking views of the Santa Ynez mountain range as travellers descend along Highway 154. The bridge itself is impressive with its simple, clean lines. Furthermore, if Caltrans is so concerned about saving lives, why not invest their limited funds to correcting the deadly intersection at 154 and 246. There have been horrific accidents at this spot for too long, and Caltrans has yet to resolve the problem. This intersection requires a stoplight. I also recommend that Caltrans install warning stoplights about 200 feet from the intersection on all four sides – similar to the Canadian warning lights we have seen on our visits to British Columbia.

2

In closing, we resent being forced to plea for this bridge. It's as if those of us who wish to preserve the beauty of Cold Springs Bridge are doing so heartlessly and with no regard for human life. But we believe that Caltrans's proposal will do little to nothing to prevent suicides. Please leave Cold Springs Bridge the treasure that it is.

Yours truly,

Celeste and Kenneth Barber
1221 San Andres Street
Santa Barbara, CA. 93101
(805) 965-2274
barber@sbcc.edu

Response to comments from Celeste and Kenneth Barber

Your opposition to the project has been noted.

Response to comment #1: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #2: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

In regards to the State Route 154/246 intersection, Caltrans has a safety project for this location that we are currently developing. The alternatives being considered for the 154/246 intersection include a roundabout and a signalized intersection.



Robert Bernstein
 <alist@swt.org>
 01/13/2011 03:40 PM

To Matt Fowler <matt_c_fowler@dot.ca.gov>
 cc
 Subject Cold Spring Bridge Public Comment

Matt Fowler, Senior Environmental Planner

Thank you for taking public comment on the Cold Spring bridge proposal.
 Please reply with an acknowledgement that this has been received for comment.

"The Great Interstate Barrier" is how Caltrans' Highway 101 was presented
 to us when I arrived as a graduate student at UCSB back in the early 1980s.

The truth of that image has only grown over the years. Caltrans built this
 road slashing through the heart of our community with no regard to how
 people were supposed to get from one side to another. Young people
 regularly run across the freeway, including a girl who was killed doing so
 not long ago.

Why do I bring this up in a public comment period for a Cold Spring Arch
 Bridge project?

To highlight what I consider to be severely misplaced priorities. Caltrans
 has created a hazard that it has refused to mitigate for decades. There is
 not a single dedicated pedestrian/bicycle crossing anywhere in Goleta.
 There used to be crossings shared with motor vehicles, but these have
 become so hazardous to cyclists and to most pedestrians as to be almost
 unusable.

1

I would like to know why Caltrans is willing to spend millions of dollars
 allegedly to save lives of people who want to end their lives... when it
 refuses to spend a penny to protect the lives of those who are desperately
 trying to stay alive crossing the barrier you have constructed through our
 town?

2

I am very sensitive to the needs of those who want to end their lives. I
 lost a very dear friend to suicide. He would never have been deterred in
 ending his life by a fence on one bridge. He was failed by a society that
 treats people as disposable.

3

Your failure to provide crossings across your Great Interstate Barrier for
 decades is a perfect example of what creates that sort of despair in our
 society.

To repeat: I would like to know why Caltrans is willing to spend millions
 of dollars allegedly to save lives of people who want to end their lives...

4

when it refuses to spend a penny to protect the lives of those who are desperately trying to stay alive crossing the barrier you have constructed through our town?

Why don't you build some pedestrian/bicycle freeway crossings in Goleta first and then get back to us on these other projects?

Best wishes,
Robert

Robert Bernstein
448 Mills Way #B
Goleta, CA 93117
alist@swt.org

Response to comments from Robert Bernstein

Your opposition to the project has been noted.

Response to comment #1: In regards to access for pedestrians and cyclists across Route 101 in the City of Goleta, the new Cathedral Oaks Overcrossing and Overhead is currently under construction. The Cathedral Oaks Overcrossing and Overhead replaces the Hollister Avenue Overcrossing and will accommodate both pedestrians and bicycles on the new structure, which will provide access over Route 101 and the railroad. The City of Goleta is also working on a proposed overcrossing between Stroke Road and the Cathedral Oaks interchange.

Response to comment #2: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #3: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties). Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #4: Please see Response to comment #2, above.



Andre Lobanoff
<andrelobanoff@yahoo.com>

To <matt_c_fowler@dot.ca.gov>

cc

01/24/2011 07:48 AM

Subject cold springs bridge

Dear Matt - Although I realize a lot of time and effort and consideration is going into this project, I believe it is a fools errand to utilize a verticle barrier to stop suicides. People who are in this much anguish **will find a way to end their life one way or another.** I believe a wiser use of money would be in the support of clinics and services for more people in mental or emotional crisis. If that is not an option, at least spare the thousands of us who love the view from the bridge the verticle option. I have been driving this road for 40 years and am a good tax paying, home owner in this area with rights that should be acknowledged.

Thank you,
Patricia Murphy
Ballard, Ca.

1

2

Response to comments from Patricia Murphy

Your opposition to the project has been noted.

Response to comment #1: Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

Response to comment #2: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.



Kimbris Brady
 <kimbris@sbmailworks.com>
 Sent by:
 <kimbris.brady@gmail.com>

To <Matt_C_Fowler@dot.ca.gov>

cc

Subject Cold Springs Barrier

01/03/2011 12:17 PM

Dear Sir:

I use the Cold Springs bridge every day on my commute. I have one major concern that I have not seen addressed in any of the published reports about this project.

Basically, I am concerned about the quality of materials to be used for the barrier. The bridge - constructed many decades ago - still looks like a new bridge. The railings are not rusting, corroded or bent. The bridge is always graffiti free, and with the present barrier affords good visibility as you approach either end. If I see an obstruction ahead, I start to brake well before entering the bridge decking in the cold months.

Will all the materials to be used in the barrier have the same longevity (read this as lifespan) as the bridge itself. I am concerned that the barrier will be made of galvanized iron, and steel mesh both of which will look old within 10 years. At this point has Cal Trans included sufficient maintenance funds to completely replace the barrier - or will we be saddled with a degrading eyesore?

1

Are all barrier components constructed of stainless steel? I realize aluminum would be too susceptible to damage, and galvanized will rust. Just look at other barriers along the major California corridors.

I would also like to be assured that no funds for this project are derived from highway dollars - defined as sales taxes imposed to improve roads or gasoline taxes to maintain roads. (from either local, State or Federal funding sources)

2

I would fully support a barrier if all the funds for the project came from an existing budget source for the treatment of mental illness. This is a health issue. Very few - if any - of the deaths that have occurred are the result of an involuntary accident involving

vehicles, bicyclists or pedestrians. The deaths are a premeditated event planned by an individual who needs help and counseling.

The barrier will only encourage these individuals to seek an alternative course of action when in fact they really need help.

3

Thanks for your time and thoughtful consideration of these ideas. I can not attend the meeting this week, but you can present these comments to the panel and all attendees - so that others may comment on them.

--

Kimbris Brady
V 805.964.2969
F 805.964.3530
SB Mailworks
601 Pine Avenue
Goleta, CA 93117
www.sbmailworks.com
kimbris@sbmailworks.com

Response to comments from Kimmis Brady

Your opposition to the project has been noted.

Response to comment #1: The existing structure is comprised of steel, is in good condition, and not subject to a marine environment. The proposed grid/mesh barrier would be constructed with galvanized steel. The lifespan of the barrier will match or exceed the lifespan of the structure but will require maintenance, which is anticipated.

Response to comment #2: Originally, the proposed project was to have been built under Caltrans' Safety Improvement Program. However, at the request of the California Transportation Commission, Caltrans investigated alternate funding sources other than the State Highway Operation and Protection Program (SHOPP). As a result, it has been determined that the money needed to construct the barriers will now come from Local Recovery Act funds. The Recovery Act funding has been designated specifically for this project. In terms of utilizing funds from a budget source for the treatment of mental illness to build the barrier, Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #3: Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in the 2008 DEIR (pages 21-23) and 2009 FEIR (pages 22-24).

As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Jan 2, 2011

To whom it may concern,
I'm writing to protest
the barrier on the
Cold Spring Bridge. The
money could be better spent
I believe, in education &
preservation. My own dear
beloved brother, Martin
Gjergson, jumped from the
bridge on Oct 23, 1982. I'll
never get over it. The current
orange barrier is an eyesore
& suicide magnet I believe.
It's a beautiful bridge
with spectacular views.
Just leave it alone.

Thank you,

Lina Gjergson Gjerga
(805) 566-9992

1

2

Response to comments from Tina Jorgensen Jouglia

Your opposition to the project has been noted.

Response to comment #1: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #2: The orange fencing you mentioned was temporary construction safety fencing, and has been removed.

P. O. Box 865
Los Olivos, CA 93441
805.686.9122
sybilcline@gmail.com

January 10, 2011

Caltans
50 Higuera Street
San Luis Obispo, CA 93401

Attention of Matt Fowler

To Whom It May Concern:

Please add my name and opinion to the chorus of votes *AGAINST* the suicide barriers on the Cold Spring Arch Bridge in the Santa Ynez Valley.

In today's negative economy it seems folly to spend huge numbers of dollars on creating such an eyesore in a questionable effort to save one life a year (on average). I feel there are many more ways in which to save more lives per dollar expenditure.

Given the controversy on both sides of the question, it would seem to be a fair request to put the issue to a public vote.

Thank you for your consideration.

Sincerely,



Sybil K. Cline

1311 SALINAS PLACE UNIT 4
SANTA BARBARA, CA 93103
(805) 966-1301

January 19, 2011

Matt Fowler, Sr. Planner
Cal Trans
50 Higuera St.
San Luis Obispo, CA 93401

Topic: Cold Springs Barrier, Con

Dear Matt,

I have been following the discussions on the proposed suicide barrier on the Cold Springs Bridge and want to voice my concerns and go on record as being opposed to the barrier. I prefer Cal Trans fix the pot holes in the freeway system.

I believe a barrier would be a huge waste of money and hope that the project will not move forward and no more money and/or time will be spent debating it.

Sincerely,



Bruce E. Emmens



Julie Rayden
<jul.ray@hotmail.com>
01/12/2011 08:14 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Spring Bridge suicide bridge project

Mr. Matt Fowler, Sr. Environmental Planner
CalTrans
50 Higuera Street
San Luis Obispo, CA 93401

Dear Mr. Fowler,

I was unable to attend the community meeting held a week ago at San Marcos High School, and wanted to express my opinion on the plans to build (or continue building) a suicide barrier on the Cold Spring Bridge. I'm very opposed to the building of this barrier and would like to have you record my opposition to the building of this barrier.

Not only do I believe tax payer funds could be used for better uses at this point in time, but I do not believe this will be a deterrent to suicide. Lastly, this barrier will be an aesthetic nightmare on a gorgeous bridge and property. That is obviously not the sole reason not to build the barrier, but something to think about while undergoing the process.

Thank you for putting my comments into this process.

If you need to reach me about this issue, my address and phone number are below.

Sincerely,

Julie Rayden
11 Arroyo Vista Drive
Goleta, CA 93117
85-685-3410

Response to comments from:

- Sybil Cline
- Bruce E. Emmens
- Julie Rayden

Your opposition to the project has been noted.

As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

PAULA C. EMMENS

1311 SALINAS PLACE UNIT 4
SANTA BARBARA, CA 93103
H (805) 966-1301
W (805) 681-4332
CELL (805) 403-4774

January 19, 2011

Matt Fowler, Sr. Planner
Cal Trans
50 Higuera St.
San Luis Obispo, CA 93401

Topic: Cold Springs Barrier, Con

Dear Matt,

I have been following the discussions on the proposed suicide barrier on the Cold Springs Bridge and want to voice my concerns and go on record as being opposed to the barrier. First, is California saddled with a huge looming deficit. This is not the time to be expending these funds. Secondly, I was a witness to a jumper in downtown Santa Barbara a few years ago and know that jumping off a parking structure is just as deadly as the bridge. If someone is going to commit suicide, let's not move it to where our children can see it. And third, the bridge is beautiful. Please do not take that away from us. This is our community and we love the bridge.

1

2

I believe this to be a huge waste of money and hope that the project will not move forward and no more money and/or time will be spent debating it.

Sincerely,



Paula Emmens

Response to comments from Paula Emmens

Your opposition to the project has been noted.

Response to comment #1: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #2: Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

5680 PEMBROKE CT
SANTA BARBARA, CA 93111
JAN. 6, 2011

CALTRANS
50 HIGUERA ST.
SAN LUIS OBISPO, CA 93401
ATTN: MATT FOWLER

I AM WRITING TO CAST MY VOTE
AGAINST THE BRIDGE EXTENSION, OR
"SUICIDE-PREVENTION" BARRIER, SO-CALLED.

PEOPLE HAVE A RIGHT TO DIE WHEN
THEY CHOOSE, STOP SPENDING OUR
TAXES TRYING TO PRECLUDE SUICIDES.

1

2

NANCY FREEMAN
NANCY FREEMAN

Response to comments from Nancy Freeman

Your opposition to the project has been noted.

Response to comment #1: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #2: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Dec. 16, 2010

Mr. Fowler:
 According to today's issue of the "Independent," Dantz Borkura's weekly
 giveaway paper, you're the go-to man regarding public input on the very
 controversial issue of installing a suicide barrier on the Cold Spring Canyon
 Bridge.

I am totally against wrecking and ruining a perfectly aesthetic beauty
 of both that bridge and incomparable views it affords as is. I've so
 enjoyed that part of Hwy. 154 so often in the past, I can no longer do so
 because I'm disabled & almost totally homebound because of difficulty in
 getting wheelchair, from parking. (Those views are indelible parts of my
 memories now, but I hurt to think others may never have that same
 experience.

As with most costly misguided political ventures, millions of dollars will be
 wasted plus the likelihood of even more money due to cost over-runs & wrong
 "guess'timates." Incurably depressed people will simply find other ways to
 commit suicide. Some men here in Dantz Borkura jumped off the foredeck, parking
 lot, 3rd floor, some stand in front of oncoming trains, cross heavily trafficked
 highways, etc. spend tax dollars repairing freeways, ramps, overpasses, etc.

City will be the public doing what I want
 it's supposed to be done, I will feel better

1

2

Response to comments from Jo Neugent

Your opposition to the project has been noted.

Response to comment #1: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #2: Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.



Marc McGinnes
<mcginnes@es.ucsb.edu>
01/24/2011 04:50 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Comment re SDEIR

Sir:

This comment I make as an individual and not as a representative of or on behalf of Friends of the Bridge. Our attorneys are filing comments on behalf of that organization.

I will be brief. Here goes:

What begins not at all well tends to continue on in that way and to conclude poorly. The history of this deeply flawed and fiscally irresponsible proposal to needlessly deface the Cold Spring Bridge is a case in point. The term **BOONDOGGLE** is perfectly descriptive of what Caltrans has created and of how Caltrans has carried on throughout the long slog from Nevin Sams' derelictions to where we are today.

1

It has been a pleasure to confront such manifest malfeasance by Caltrans. It will be my pleasure to continue to do so.

J.Marc McGinnes, J.D.
Santa Barbara

Kindly acknowledge your receipt of this comment.

Response to comments from J. Marc McGinnes, JD

Your opposition to the project has been noted.

Response to comment #1:

Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.



Patti Rogers
<patnron@verizon.net>
01/07/2011 04:30 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge Barrier

Mr. Fowler:

I believe that, if a person wishes to commit suicide, they will find somewhere to do it. Putting a barrier on Cold Springs Bridge will not prevent a suicide.... It will only force the person to find some other way to do it, which they will. All the Barrier does it satisfy the local residents and officials that it doesn't happen "In My Backyard".

1

If a Barrier is inevitable, then I strongly suggest that it be painted to go with the bridge structure and the environment green. This would help keep it from being so obtrusive, as white is not as much a part of the environment. Also, the structure below is already a nice green, so the mesh would "blend in" rather than "stand out (like a sore thumb)".

2

Thank you for considering my suggestions,

**Pat Rogers
Buellton, CA
patnron@verizon.net**

Response to comments from Pat Rogers

Your opposition to the project has been noted.

Response to comment #1: Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

Response to comment #2: Suggestion noted.

December 18, 2010

To:
Mr. Matt Fowler, Senior Environmental Planner
Caltrans
50 Higuera St.
San Luis Obispo, Ca 93401

From:
Lisa Ann Kelly & Family
1724 Olive Avenue
Santa Barbara, Ca 93101-1021
phone/fax: 805. 569-1714

Re: Cold Spring Bridge Suicide Barrier Project on State Route 154

Dear Mr. Fowler:

My family and I are grateful to have this opportunity to comment on the proposal to erect a suicide barrier on Cold Spring Bridge. This is an issue we have had great interest in from its inception.

Having followed the controversy (pro and con) for quite some time, we then took the time to peruse the DSEIR at our Central Library. Seeing the document has not helped to change our belief that the proposed barrier is costly and completely unnecessary.

1

We are writing this letter to you today to add our voices to those who oppose the erection of any such barrier to the Cold Spring Bridge. We feel certain that you have heard the many valid arguments in opposition. Please take heed and desist in your pursuit of this project.

Thank you for your time and consideration in the reading of our letter.

Sincerely,

Lisa Ann Kelly & THREE FAMILY MEMBERS

Response to comments from Lisa Ann Kelly and three family members

Your opposition to the project has been noted.

Response to comment #1: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.



Van Bivans
<vbivans@hotmail.com>
12/31/2010 11:41 AM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold springs bridge

I do Not support the barriers on the bridge. If someone wants to kill them self, they will find a way. It has nothing to do with the bridge. Leave the bridge alone.

Response to comments from Van Bivans

Your opposition to the project has been noted.

Response to comment #1: Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

Kyle Abello
<kabello@ucsd.edu>
01/21/2011 12:51 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Kyle Abello
Solvang

nsil afromsky
<vino1620@yahoo.com>
01/20/2011 06:01 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
nsil afromsky
santa Barbara

Bradley Alpers
<brad@alpersconstruction.com>
01/19/2011 05:57 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Bradley Alpers
Santa Ynez, CA 93460

Brian Asselstine
<brianasselstine@gmail.com>
01/19/2011 01:32 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Brian Asselstine
Santa Ynez

Susan Asselstine
<hdcsyv@gmail.com>
01/19/2011 01:30 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Susan Asselstine
Santa Ynez

"Sally E. Austin"
<sallyaustin@cox.net>
01/19/2011 10:30 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Sally E. Austin
Carpinteria

Cynthia Bacon
<dogpaws758@gmail.com>
01/22/2011 09:22 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Cynthia Bacon
five miles from the Cold Springs Bridge

Catherine Balch
<Tbalch@Mac.com>
01/22/2011 09:44 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Catherine Balch
Santa Barbara

John Balch
<Jbalch9241@aol.com>
01/22/2011 09:45 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
John Balch
Santa Barbara

Brenda Barrett
<unuzul@yahoo.com>
01/19/2011 10:52 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Brenda Barrett
Santa Barbara, CA

Barbara Bartolome
<SunnySBBarbara@gmail.com>
01/19/2011 06:39 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Barbara Bartolome
By E Olive and E Figueroa Sts, downtown SB

Cobina Beaudette -Wellman
<scottyandturtle@hotmail.com>
01/20/2011 12:42 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Cobina Beaudette-Wellman
Buellton

Elisabeth Beeler
<lizbeeler@aol.com>
01/24/2011 01:45 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Elisabeth Beeler
Solvang, CA 93463

erin berenson
<erinberenson@sbcglobal.net>
01/20/2011 09:36 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
erin berenson
santa ynez

Cindy Bergen
<cindybergen@verizon.net>
01/24/2011 07:54 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Cindy Bergen
Solvang

Michael Bergen
<michaelbergen@verizon.net>
01/24/2011 07:52 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Michael Bergen
Solvang

Lance Birk
<lance@lancebirk.com>
01/19/2011 06:18 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Lance Birk
santa Barbara

Teasha Blackman
<solangblackman@verizon.net>
01/20/2011 06:32 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Teasha Blackman
Solang

William Blackman
<solangblackman@verizon.net>
01/20/2011 06:33 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
William Blackman
Solang

Barbara J Boltjes
<bjb_rt44@hotmail.com>
01/21/2011 07:36 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Barbara J Boltjes
Midwest

Guy Bray
<guybray@yahoo.com>
01/21/2011 08:24 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Guy Bray
Santa Barbara

Theresa Bruhn
<theresa.bruhn5@gmail.com>
01/19/2011 08:30 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Theresa Bruhn
Santa Ynez

Olga Brunello
<obrunello@verizon.net>
01/22/2011 07:58 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Olga Brunello
Santa Barbara

Theresa Butler
<snappyt4@msn.com>
01/19/2011 12:31 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Theresa Butler
santa ynez, ca

Elise Cane -Van Enoo
<docoma6@gmail.com>
01/18/2011 03:25 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Elise Cane-Van Enoo
Solvang

elizabeth chapple
<elizabethchapple@gmail.com>
01/19/2011 06:17 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
elizabeth chapple
santa Barbara

john chapple
<chapple@verizon.net>
01/19/2011 06:15 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
john chapple
santa Barbara

kathy cleary
<kcleary@cfginc.us>
01/19/2011 03:41 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
kathy cleary
los olivos

Sybil Cline
<sybilcline@gmail.com>
01/19/2011 08:29 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Sybil Cline
Santa Ynez Valley

Neil Cline
<neilcline@earthlink.net>
01/19/2011 10:39 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Neil Cline
Los Olivos Ca.

Lunn courson
<lfcb3c@wildblue.net>
01/23/2011 08:34 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Lunn courson
Buellton

adriana daily
<dailyap@gmail.com>
01/23/2011 12:22 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
adriana daily
santa ynez,ca

"Dr. Patrick Daily "
<drpatdaily@gmail.com>
01/23/2011 12:20 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Dr. Patrick Daily
SANTA YNEZ, CA

"Carol H. Davidson"
<chdchat@cox.net>
01/24/2011 01:34 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Carol H. Davidson
Santa Barbara

Yves de Bievre
<ybeaver@verizon.net>
01/20/2011 01:32 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Yves de Bievre
hope ranch annex

Bruce De Feyter
<bdefeyter@hotmail.com>
01/19/2011 09:46 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Bruce De Feyter
Goleta

Mary DeSmidt
<mrdesmidt@yahoo.com>
01/22/2011 04:19 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Mary DeSmidt
Painted Cave

Richard DeSmidt
<mrdesmidt@yahoo.com>
01/22/2011 04:00 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Richard DeSmidt
Painted Cave

Christine DiPego
<chris.dipego@gmail.com>
01/18/2011 09:55 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Christine DiPego
santa ynez

Gerald DiPego
<jerrydipego@gmail.com>
01/18/2011 09:36 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Gerald DiPego
Santa Ynez Valley

Mike Elliott
<auwinger1200@yahoo.com>
01/21/2011 12:02 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Mike Elliott
Sacramento, CA

"William F. Elliott"
<biliott2@yahoo.com>
01/21/2011 08:31 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
William F. Elliott
Merced, CA

Bruce Emmens
<bemmens@cox.net>
01/20/2011 08:56 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Bruce Emmens
Santa Barbara

Joye Emmens
<joye@emmens.us>
01/22/2011 05:30 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Joye Emmens
Ventura

Paula Emmens
<beemanonthebeach@yahoo.com>
01/20/2011 09:39 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Paula Emmens
Santa Barbara City

Puck Erickson-Lohnas
<cpe@arcadiastudio.com>
01/19/2011 05:44 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Puck Erickson-Lohnas
Los Olivos

Greg Farrell
<g1farrell@hunges.net>
01/22/2011 09:57 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Greg Farrell
Santa Barbara

Richard & Diane Fly
<dd2fly@gmail.com>
01/19/2011 01:21 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Richard & Diane Fly
Santa Ynez

Evan Gantz
<eg623@cox.net>
01/21/2011 07:52 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Evan Gantz
Santa Barbara

Susan Gantz
<susan.gantz@gmail.com>
01/19/2011 12:16 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Susan Gantz
Santa Barbara (city)

Elvina Geauque
<tax1savers@aol.com>
01/21/2011 01:50 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Elvina Geauque
Santa Barbara

"Nancy R. Gilman"
<rox814@gmail.com>
01/21/2011 09:11 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Nancy R. Gilman
Santa Barbara CA 93105

D.Giorgio
<d4solvang@comcast.net>
01/23/2011 12:10 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
D.Giorgio
Santa Ynez Valley, Ca.

Kathleen M Gordon
<mochickies@mac.com>
01/20/2011 07:43 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Kathleen M Gordon
Solvang/Santa Ynez

Michael gordon
<Bmgmike@aol.com>
01/20/2011 07:40 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Michael gordon
Santa ynez

David Gowing
<drgowing@gmail.com>
01/19/2011 08:49 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
David Gowing
Solvang

Alma Gray
<almareesegray@gmail.com>
01/20/2011 11:21 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Alma Gray
Santa Barbara

Martha Gray
<magray@silcom.com>
01/20/2011 11:21 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Martha Gray
Santa Barbara

Annie Grove
<Groveannie0@gmail.com>
01/19/2011 01:25 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Annie Grove
Los Olivos

Bill Grove
<billgrove@prusb.com>
01/19/2011 12:22 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Bill Grove
Los Olivos, CA

Jo Grove
<lm4calling@aol.com>
01/20/2011 03:36 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Jo Grove
Los Olivos

Betsy Hadley
<betz@mhadley.com>
01/19/2011 11:10 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Betsy Hadley
Santa Ynez

Mike Hadley
<mhadley@silcom.com>
01/19/2011 11:09 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Mike Hadley
Santa Ynez

Joan Hall
<joan.hall@btinternet.com>
01/24/2011 02:09 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Joan Hall
Warwickshire England

Bob Hamber
<hamberr@netscape.net>
01/24/2011 02:00 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Bob Hamber
Noleta

Laura Hanberg
<lhanberg@impulse.net>
01/19/2011 02:39 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Laura Hanberg
Solvang

john harness
<joharne1@aol.com>
01/18/2011 08:22 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
john harness
los olivos

Jane Harrah
<majharrah@cox.net>
01/21/2011 01:37 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Jane Harrah
Santa Barbara

Mary Harris
<mbharris@yahoo.com>
01/19/2011 01:13 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Mary Harris
Santa Ynez

Richard Harris
<riverock@silcom.com>
01/19/2011 02:40 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Richard Harris
Santa Ynez

David Hemming
<David@1valley.com>
01/18/2011 09:20 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
David Hemming
Solvang, CA

Mark Hemming
<mark.mhic@verizon.net>
01/18/2011 06:43 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Mark Hemming
Solvang

Eric Hemming
<ehemming@gmail.com>
01/18/2011 09:00 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Eric Hemming
Foothill Ranch, CA

suellen hilzer
<mshelley@cox.net>
01/21/2011 10:59 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
suellen hilzer
santa barbara ca

Jean Hoch
<BlondeVenus@gmail.com>
01/21/2011 10:51 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Jean Hoch
Santa Barbara

Christa Ivy
<harvey@silcom.com>
01/22/2011 09:56 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Christa Ivy
Santa Ynez

Jarvis G Ivy
<jgivy@verizon.net>
01/20/2011 09:00 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Jarvis G Ivy
Santa Ynez

Lawrence H Ivy
<harvey@silcom.com>
01/22/2011 09:55 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Lawrence H Ivy
Santa Ynez

Peggy J Ivy
<zomaroonie@earthlink.net>
01/20/2011 09:01 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Peggy J Ivy
Santa Ynez

"Angela W. Janes"
<abwjanes@gmail.com>
01/20/2011 09:26 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Angela W. Janes
Santa Ynez, CA

"Sandra A. Jankowski "
<sandyjrose@verizon.net>
01/19/2011 11:19 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Sandra A. Jankowski
Santa Ynez Valley

Josiah Jenkins
<Josiah@Jedlickas.com>
01/21/2011 12:30 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Josiah Jenkins
Santa Barbara & Santa Ynez

Bob Jennings
<Bobjennings@coldwellbanker.com>
01/19/2011 01:51 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Bob Jennings
Santa Ynez

Jane Jervis
<jwjervis@sprynet.com>
01/19/2011 05:12 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Jane Jervis
Malibu, CA

Linda Johns
<Linda@coastalbreeze.net>
01/20/2011 08:03 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Linda Johns
Orange County

Chris Johnson
<cfj2000@gmail.com>
01/19/2011 12:41 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Chris Johnson
Solvang

R Jones
<rejones.sr@gmail.com>
01/23/2011 09:13 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
R Jones
Santa Barbara, CA

Floyd Keinath
<IMTOMMIXONE@VERIZON.NET>
01/19/2011 04:07 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Floyd Keinath
SOLVANG, CA

"Kenneth T. Kelley"
<kenatttr@thirdrail.com>
01/19/2011 04:56 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Kenneth T. Kelley
Santa Ynez, CA 63460

Kevin
<kroberson@gmail.com>
01/19/2011 05:58 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Kevin
Goleta

Joseph Klein
<kleinsbsa@verizon.net>
01/19/2011 12:36 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Joseph Klein
Santa Barbara, CA

bill krauch
<billkrauch@aol.com>
01/23/2011 09:55 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
bill krauch
Santa Ynez

christine krauch
<christinekrauch@aol.com>
01/23/2011 09:57 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
christine krauch
Santa Ynez

edw l kushner
<edk5306@aol.com>
01/19/2011 02:24 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
edw l kushner
Solvang

Lenard Landreth
<len.landreth@gmail.com>
01/22/2011 03:30 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Lenard Landreth
Lake Marie

Karen Langley
<karenlangs@yahoo.com>
01/18/2011 05:23 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Karen Langley
Solvang

Leigh Layman
<Leighlee@wildblue.net>
01/23/2011 02:44 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Leigh Layman
Solvang

Rick Layman
<cowboycolonel@wildblue.net>
01/23/2011 02:47 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Rick Layman
Solvang

Thomas Le Pley
<tom@losolivos.com>
01/19/2011 04:06 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Thomas Le Pley
Santa Ynez

Iken Lord
<ikenlord@yahoo.com>
01/19/2011 01:21 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Iken Lord
santa Barbara

Martha Lorenz
<lorenz.martha@gmail.com>
01/20/2011 08:03 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Martha Lorenz
Santa Ynez Valley - Solvang

Leatrice Luria
<leeluria81@gmail.com>
01/21/2011 06:41 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Leatrice Luria
Montecito

Jayne Martin
<cbicar@comcast.net>
01/19/2011 01:30 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Jayne Martin
Los Olivos

Kelly McGill
<ridingmcg@mac.com>
01/23/2011 10:38 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Kelly McGill
Solvang, Ca

Michael McGill
<mmcgill1@mac.com>
01/23/2011 10:39 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Michael McGill
Solvang, CA

Molly McGill
<mollyrides2008@yahoo.com>
01/23/2011 10:39 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Molly McGill
Solvang, CA

Sandra McGrath
<ssandymcgrath@aol.com>
01/19/2011 08:32 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Sandra McGrath
Goleta

Cathie McHenry
<cathie@hwy246.net>
01/18/2011 08:47 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Cathie McHenry
Santa Ynez

Dana McMahon daughter of
Peg Ivy
<mcmahonfamily13@yahoo.com>
01/20/2011 07:12 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Dana McMahon daughter of Peg Ivy
Santa Ynez, Ca

Joseph Meehan
<jpm54@earthlink.net>
01/21/2011 09:10 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Joseph Meehan
Solvang

Mary Melville
<mermelville@yahoo.com>
01/21/2011 10:20 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Mary Melville
Solvang

Jerry Merrell
<merrellonline@verizon.net>
01/22/2011 01:40 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Jerry Merrell
Santa Ynez, CA

Paula Merrell
<merrellonline@verizon.net>
01/22/2011 01:39 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Paula Merrell
Santa Ynez, CA

Barbara Meyer
<barbara.meyer@hotmail.com>
01/19/2011 04:39 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Barbara Meyer
Solvang

"Brandy Miller "
<kenbrandy@verizon.net>
01/21/2011 12:41 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Brandy Miller
Solvang, CA

Kendall Mills
<kendallmills@mac.com>
01/21/2011 08:34 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Kendall Mills
Solvang

Bobbi Minne
<clouseau007@gmail.com>
01/21/2011 10:50 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Bobbi Minne
Santa Barbara

Roberta Minne
<rjm215@cox.net>
01/21/2011 10:48 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Roberta Minne
Santa Barbara 93105

Roy Moffett
<i.forgot@verizon.net>
01/23/2011 12:30 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Roy Moffett
Santa Maria, Ca 93454

D L Morgan
<teddiesandrains@aol.com>
01/19/2011 03:59 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
D L Morgan
Santa Ynez

" Linda Morgan "
<teddiesandtrains@aol.com>
01/19/2011 04:00 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Linda Morgan
Santa Ynez

Steve Muellner
<smuellner@aol.com>
01/19/2011 12:56 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Steve Muellner
Santa Ynez, Ca

janice nagler
<naglerje@aol.com>
01/19/2011 07:19 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
janice nagler
Solvang

richard nagler
<naglerich@gmail.com>
01/17/2011 10:56 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
richard nagler
Solvang

CHRISTINA NELSON
<cknelson@pacificstaff.com>
01/19/2011 12:56 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
CHRISTINA NELSON
Santa Ynez Valley

Chris Nielsen
<nielsen5@comcast.net>
01/22/2011 06:24 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Chris Nielsen
Solvang, CA

Susan Noble
<aunteeq@aol.com>
01/19/2011 07:39 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Susan Noble
Solvang

June Oesterling
<juneoesterling@gmail.com>
01/21/2011 10:45 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
June Oesterling
Goleta

Stephen Orosz
<steve@oegsite.com>
01/21/2011 04:42 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Stephen Orosz
Santa Ynez Valley

Martha Osborne
<martha-osborne@hotmail.com>
01/24/2011 01:53 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Martha Osborne
Santa Ynez, Ca

Patti Ottoboni
<pattio1@mac.coM>
01/21/2011 01:45 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Patti Ottoboni
Santa Barbara

Allan Pacela
<apacela@pmc.us>
01/19/2011 03:34 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Allan Pacela
Solvang

Steve Palmer
<slpalmer1@verizon.net>
01/24/2011 01:16 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Steve Palmer
Los Olivos

Phoebe Patterson
<jpinsolvang@msn.com>
01/23/2011 10:46 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Phoebe Patterson
Solvang

Dennis Paulazzo
<dennis.paulazzo@gmail.com>
01/21/2011 09:46 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Dennis Paulazzo
Santa Barbara, CA

Donna Payne
<dpayne@cfiemail.com>
01/21/2011 12:23 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Donna Payne
Santa Barbara County, Summerland CA

John H Peckham
<jhpeckham@aol.com>
01/19/2011 04:20 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
John H Peckham
Solvang

Linda S Peckham
<Peckham729@verizon.net>
01/19/2011 04:23 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Linda S Peckham
Solvang

Kayla Perez
<kaylaperez958@gmail.com>
01/22/2011 09:14 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Kayla Perez
Sant Ynez

Dawn Perrine
<dawn@montyroberts.com>
01/20/2011 09:10 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Dawn Perrine
Solvang

Charles Pessley
<chuck.latour@gmail.com>
01/20/2011 10:08 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Charles Pessley
Santa Barbara

Diane Petras
<diane@hwy246.net>
01/19/2011 01:41 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Diane Petras
Santa Ynez

Betty Polansky
<bettypolansky@aol.com>
01/24/2011 12:23 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Betty Polansky
Solvang, Ca

Tom Polansky
<twpolansky@aol.com>
01/24/2011 12:25 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Tom Polansky
Solvang, CA

Linda Preston
<ltheprestons@aol.com>
01/23/2011 01:45 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Linda Preston
Solvang, CA

Timothy Pryko
<timaneta@gmail.com>
01/21/2011 11:37 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Timothy Pryko
Santa Barbara

Joan M Reden
<greengoddess7@gmail.com>
01/19/2011 08:44 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Joan M Reden
Santa Ynez Valley

Steven Reden
<stevenreden@yahoo.com>
01/19/2011 07:50 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Steven Reden
Santa Ynez Valley

Sara Reden
<sreden@gmail.com>
01/20/2011 09:03 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Sara Reden
Santa Ynez

Dave Retz
<dretz@comware.us>
01/17/2011 12:05 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Dave Retz
Solvang

Robert
<robdafoe@gmail.com>
01/19/2011 06:02 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Robert
santa Barbara

Evalyn Robins
<evalynrobins@hotmail.com>
01/21/2011 04:40 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Evalyn Robins
Goleta

Dan Rohr
<dprohr@gmail.com>
01/19/2011 12:58 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Dan Rohr
Santa Ynez

"rose "
<chapple.md@verizon.net>
01/19/2011 06:18 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
rose
santa Barbara

"Kelly A. Rose"
<kelly.rose1@verizon.net>
01/19/2011 11:18 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Kelly A. Rose
Los Olivos - - Santa Ynez Valley

Lee Rosenberg
<rosey3333@gmail.com>
01/18/2011 09:11 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Lee Rosenberg
Solvang

linda rosenberg
<rosey222@gmail.com>
01/17/2011 06:08 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
linda rosenberg
solvang California

LOUIS RUBINSTEIN
<louruby34@yahoo.com>
01/24/2011 11:35 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
LOUIS RUBINSTEIN
solvang, ca

"MARCIA RUBINSTEIN "
<louruby34@yahoo.com>
01/24/2011 11:35 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
MARCIA RUBINSTEIN
solvang, ca

Ernie Ruiz
<teri_ruiz@att.net>
01/21/2011 01:54 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Ernie Ruiz
Nipomo

Teri Ruiz
<teri_ruiz@att.net>
01/21/2011 01:54 PM To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Teri Ruiz
Nipomo

Joan Rutkowski
<joanrutkowski@cox.net>
01/17/2011 06:59 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Joan Rutkowski
Santa Barbara

KEITH SAARLOOS
<Keith@saarloosandsons.com>
01/22/2011 05:40 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
KEITH SAARLOOS
Los Olivos

oscar sanchez
<omapsanchez@verizon.net>
01/20/2011 11:58 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
oscar sanchez
Buellton

John Sanger
<RCSanger@aol.com>
01/20/2011 10:56 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
John Sanger
Santa Ynez, Calif.

Randi Sanger
<RCSanger@aol.com>
01/20/2011 10:55 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Randi Sanger
Santa Ynez

Lillian Seldeen
<roxanna@yahoo.com>
01/21/2011 10:50 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Lillian Seldeen
Solvang

Sharon
<sbarnoddess@aol.com>
01/19/2011 01:38 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Sharon
Santa Ynez

Marlene and Jim Sheldon
<nonnasparks@verizon.net>
01/19/2011 05:32 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge

Dear Mr. Fowler:

I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.

Sincerely,
Marlene and Jim Sheldon
Solvand

"G. B. Shepherd"
<shepherd@west.net>
01/19/2011 06:33 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge

Dear Mr. Fowler:

I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.

Sincerely,
G. B. Shepherd
Santa Ynez

Donna Small
<dsmall2@earthlink.net>
01/18/2011 10:25 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge

Dear Mr. Fowler:

I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.

Sincerely,
Donna Small
Santa Ynez, CA

Tom Small
<tsmall@earthlink.net>
01/18/2011 10:24 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge

Dear Mr. Fowler:

I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.

Sincerely,
Tom Small
Santa Ynez, CA

sharon smith
<sharksmyth@yahoo.com>
01/19/2011 12:04 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
sharon smith
santa Barbara

Steve Smith
<ssmithts@gmail.com>
01/05/2011 01:40 PM
To "matt_c_fowler@dot.ca.gov" <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge Barrier
I support implementing a barrier on the Cold Springs Bridge as a measure to help prevent suicides.
Sincerely
Steve Smith

Tana Sommer-Belin
<tana@tanassommer.com>
01/21/2011 01:09 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Tana Sommer-Belin
Santa Barbara 93103

RAndy Stabin
<randys15@verizon.com>
01/19/2011 08:32 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
RAndy Stabin
Santa Barbara

"Vickie Stabin "
<vickieshu@hotmail.com>
01/19/2011 08:33 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Vickie Stabin
Santa Barbara

Jon Stephen
<L79chevelle@yahoo.com>
01/19/2011 10:45 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Jon Stephen
Solvang, CA

Janet Stevenson
<jas.stevenson@verizon.net>
01/19/2011 03:19 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Janet Stevenson
Santa Barbara

MaryPat Sweeney
<rmpsweeney@me.com>
01/22/2011 07:08 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
MaryPat Sweeney
santa Barbara

Heather and Bernie Taupin
<htroundup@me.com>
01/19/2011 05:38 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Heather and Bernie Taupin
Santa ynez

D Tescher
<suurre@gmail.com>
01/23/2011 01:19 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
D Tescher
ventura county

Iris Timmerman
<ayame@impulse.net>
01/21/2011 08:15 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Iris Timmerman
Lompoc, Ca

Philip Vacca
<calvaccas@yahoo.com>
01/23/2011 01:59 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Philip Vacca
Solvang

Annelies Villarreal
<travelgal540@yahoo.com>
01/19/2011 02:55 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Annelies Villarreal
Santa Barbara

Douglas Waldron
<douglas@thewaldrons.com>
01/17/2011 09:54 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Douglas Waldron
Solvang

candy waldron
<candy@thewaldrons.com>
01/19/2011 12:32 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
candy waldron
Santa Ynez/Solvang

Brandon Ware
<brandonware9@gmail.com>
01/21/2011 04:31 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Brandon Ware
Orange County

Delaney Ware
<delaneyhays@mac.com>
01/21/2011 04:32 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Delaney Ware
Los Angeles

Eloise Ware
<Eloise.Ware@gmail.com>
01/20/2011 06:19 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Eloise Ware
Visit the area often

Susette Warynick
<susettew@earthlink.net>
01/24/2011 10:49 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Susette Warynick
Santa Barbara

larry white
<larry@inlglass.com>
01/18/2011 07:34 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
larry white
santa ynez

Harold Williams
<hwilliams@getty.edu>
01/19/2011 11:46 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Harold Williams
Santa Ynez

Brian Wolf
<bwolf@hwy246.net>
01/19/2011 04:32 PM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Brian Wolf
Solvang, but from Santa Barbara

Dellie Woodring
<califdell@aol.com>
01/19/2011 10:20 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Dellie Woodring
California

John Yamasaki
<Yamifamily@aol.com>
01/20/2011 08:28 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc

Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
John Yamasaki
Goleta

karla zuehlke
<karlaz@impulse.net>
01/20/2011 09:58 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>

cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
karla zuehlke
santa ynez

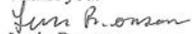
Neil Zuehlke
<neil@materialfabricators.com>
01/20/2011 10:02 AM
To "Matt C. Fowler" <matt_c_fowler@dot.ca.gov>
cc
Subject Opposing Vertical Barrier on Cold Spring Bridge
Dear Mr. Fowler:
I am opposed to the proposed vertical barrier on the Cold Spring Bridge and I support the horizontal barrier.
Sincerely,
Neil Zuehlke
Santa Ynez, CA 93460

Matt Fowler
Caltrans
50 Higuera St.
San Luis Obispo, CA 93410

Please keep Cold Springs Bridge as it is.

If you are forced to add structure, please design the structure to minimize the visual profile of the bridge and its additional structure.

Thank you.


Lorin Bronson
Lompoc CA 93436



Jerry Bellamy
<jerry@bellamyservices.com>

To <matt_fowler@dol.ca.gov>

cc

12/16/2010 02:45 PM

Subject Do NOT change Cold Springs Bridge from the way it is now.....

Matt:

Please do not allow Caltrans to change the current configuration of the Cold Springs Bridge. Do not make any changes.

Rev Jerry Bellamy
3070 Lucinda Lane
Santa Barbara, CA 93105
home and wedding tele: 805-687-4248
website www.santabarbaraweddingminister.com
email address for brides and grooms: jerry@bellamyservices.com

Response to comments from:

Kyle Abello <kabello@ucsd.edu>; nsil afromsky <vino 1620@yahoo.com>; Bradley Alpers <brad@alpersconstruction.com>; Brian Asselstine <brianasselstine@gmail.com>; Susan Asselstine <hdcsyv@gmail.com>; Sally E. Austin <sallyaustin@cox.net>; Cynthia Bacon <dogpaws758@gmail.com>; Catherine Balch <Tbalch@Mac.com>; John Balch <Jbalch9241@aol.com>; Brenda Barrett <unuzul@yahoo.com>; Barbara Bartolome <SunnySBBarbara@gmail.com>; Cobina Beaudette-Wellman <scottyandturtle@hotmail.com>; Elisabeth Beeler <lizbeeler@aol.com>; Erin Berenson <erinberenson@sbcglobal.net>; Cindy Bergen <cindybergen@verizon.net>; Michael Bergen <michaelbergen@verizon.net>; Lance Birk <lance@lancebirk.com>; Teasha Blackman <solvangblackman@verizon.net>; William Blackman <solvangblackman@verizon.net>; Barbara J Boltjes <bjb_rt44@hotmail.com>; Guy Bray <guybray@yahoo.com>; Theresa Bruhn <theresa.bruhn5@gmail.com>; Olga Brunello <obrunello@verizon.net>; Theresa Butler <snappyt4@msn.com>; Elise Cane-Van Enoo <docoma6@gmail.com>; elizabeth chapple <elizabethchapple@gmail.com>; john chapple <chapple@verizon.net>; kathy cleary <kcleary@cfginc.us>; Sybil Cline <sybilcline@gmail.com>; Neil Cline <neilcline@earthlink.net>; Lunn courson <lfcbar3c@wildblue.net>; adriana daily <dailyap@gmail.com>; Dr. Patrick Daily <drpatdaily@gmail.com>; Carol H. Davidson <chdchat@cox.net>; Yves de Bievre <ybeaver@verizon.net>; Bruce De Feyter <bdefeyter@hotmail.com>; Mary DeSmidt <mrdesmidt@yahoo.com>; Richard DeSmidt <mrdesmidt@yahoo.com>; Christine Dipego <chris.dipego@gmail.com>; Mike Elliott <auwinger1200@yahoo.com>; William F. Elliott <billiott2@yahoo.com>; Bruce Emmens <bemmens@cox.net>; Joye Emmens <joye@emmens.us>; Paula Emmens <beemanonthebeach@yahoo.com>; Puck Erickson-Lohnas <cpe@arcadiastudio.com>; Greg Farrell <g1farrell@hunges.net>; Richard & Diane Fly <dd2fly@gmail.com>; Evan Gantz <eg623@cox.net>; Susan Gantz <susan.gantz@gmail.com>; Elvina Geauque <tax1savers@aol.com>; Nancy R. Gilman <rox814@gmail.com>; D.Giorgio <d4solvang@comcast.net>; Kathleen M Gordon <mochickies@mac.com>; Michael Gordon <Bmgmike@aol.com>; David Gowing <drgowing@gmail.com>; Alma Gray <almaresegray@gmail.com>; Martha Gray <magray@silcom.com>; Annie Grove <Groveannie0@gmail.com>; Bill Grove <billgrove@prusb.com>; Betsy Hadley <betz@mhadley.com>; Mike Hadley <mhadley@silcom.com>; Joan Hall <joan.hall@btinternet.com>; Bob Hamber <hamberr@netscape.net>; Laura Hanberg <lhanberg@impulse.net>; john harness

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Your opposition to the project has been noted.



Bob Crowe
<bob.crowe@verizon.net>
01/06/2011 05:14 PM

To <matt_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge barrier

1/6/2011

I didn't make it to the meeting last night but wanted to go on record as being opposed to the Vertical Barrier on the bridge. This is a scenic highway designated by the State for the reason of its beauty. They don't allow signage or bill boards etc. to obscure the view. This barrier falls into this category. I have been using this bridge since 1974 when I moved from LA to the Santa Ynez Valley and this view from the bridge was my first breath taking view of the valley and lake as I headed toward Rt.246 on RT.154. All my friends that visit here comment on that view as they cross the bridge. It is a landmark that should be protected. I propose the same barrier they are planning for the Golden Gate Bridge, a horizontal metal mesh net below the bridge. This would give the potential jumper second thoughts and he would move on. Leave our view intact. Thank you. Bob Crowe, 1090 Mustang Drive, Santa Ynez, Ca. 93460. 805-688-7698

Response to comments from Bob Crowe

Your opposition to the project has been noted.

The State Scenic Highway designation for Highway 154 extends along the route's entire 32-mile length. Scenic vistas (including views from the Cold Spring Canyon Bridge) are not the sole determinant of visual quality along Highway 154 nor its Scenic Highway designation. Rather, the varied topography and native vegetation are also important contributors to the high visual quality of the corridor. The project would only affect approximately 0.2 mile (approximately 1/6th of one percent) of the total 32-mile mile length of the Officially Designated State Scenic Highway.

The State Scenic Highway Program (2007) does specifically prohibit the installation of new outdoor advertising (billboards) for the purpose of minimizing visual blight. In addition, the State Scenic Highway Program states that “the designation of a route as an official scenic highway does not substantially alter the type of project proposed” (Updated VIA, page 4). The State Scenic Highway Program does not prohibit the construction of necessary highway improvements such as this project. The Scenic Highway Program does-require that projects be “evaluated for visual impact to scenic views as part of the environmental process” (Updated VIA, page 4). Consistent with the State Scenic Highway Program, the Updated VIA and DSEIR fully evaluate, disclose and make findings based on the project’s adverse impact to scenic views.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge’s historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge’s columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge’s integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Golden Gate Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting (physical modifications) the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of

Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance

8. Increased costs associated with need to replace safety net.

COMMENT ON THE DRAFT SUPPLEMENTAL ENVIRONMENTAL REPORT OF DECEMBER 2008 FOR THE COLD SPRING CANYON
BRIDGE SUICIDE BARRIER ON THE CHUMASH HIGHWAY AT COLD SPRING CANYON BRIDGE 05-SB-154-PM 22.9/23.1
05-0P9100 SCH# 2008011060

Thank you for giving me the opportunity to comment on this *Draft Supplemental Environmental Impact Report [DSEIR]*, a charming, discontinuously paginated, 593-page document—cheerily released for the Holidays—whose appendices have attachments and whose attachments, like fleas with fleas, have attachments... My previous *Comment* [reprinted, warts and all, in the *DSEIR*, Appendix F, pp.72-78] on the *Draft Environmental Impact Report* whose format was a hybrid between that required by the National Environmental Policy Act [NEPA] and that required by the California Environmental Quality Act [CEQA], took its theme from NEPA 42 U.S.C. §§4321, *et seq.*, to show that the *DEIR* did not offer a sufficiently complete analysis so that the decisions based on it could be fully informed and well-considered. Because the *DSEIR* has been prepared under court order to comply with CEQA, my theme this time comes from the CEQA Guidelines §15151, and will show that the *DSEIR* has not been prepared with a sufficient degree of analysis to provide enough information to intelligently evaluate the the significant adverse impacts on the Visual/Æsthetic Resources and on the Cultural Resources, nor with a sufficient degree of analysis to provide enough information to intelligently evaluate the proposed mitigations of these adverse impacts. In the language of the CEQA Guidelines, the *DSEIR* is inadequate, incomplete, and is not a good faith effort at full disclosure.

According to the Superior Court's Post-Judgement Order on Recirculation:

The public must be fully advised about the impacts as a whole, in order to be able to comment on the mitigation measures adopted with respect to those impacts. [p.15]

Because the preferred Grid/Mesh Alternative has not been built, extensive use has been made of computer-modeled photo-simulations to assess the impacts of this type of barrier on the visual environment, and to inform decision makers within and without the government, as well as the public at large, of these impacts. However, there is a crucial error in these photo-simulations of the Grid/Mesh Alternative which makes the grid/mesh appear therein much more transparent than it will if actually installed.

The *DSEIR* speaks of "the 'stacking effect' when closely spaced vertical pickets are viewed from an oblique angle" [pp.21 &53]. However, not just pickets, but any series of verticals will appear to get closer and closer together as the viewing angle (measured from the vertical nearest the viewer) becomes more and more oblique, that is, as the verticals recede further and further toward the horizon from the given viewpoint. This elementary principle of perspective, for example, accounts for the telephone poles along a straight stretch of highway appearing to get closer and closer together as the road proceeds further and further into the distance; eventually the telephone poles appear to "stack up" and nothing can be seen between them any longer. The closer together the telephone poles are actually situated, the sooner they will "stack up." This "stacking effect" is also described somewhat differently in the *Updated Visual Impact Assessment [DSEIR, Appendix G, p.7]*:

...[T]he extent to which the barrier would affect views would depend on the viewing-angle through the barrier.... Views from the front of the vehicle would see the barrier at a more acute angle, which would result in the barrier elements appearing closer together and blocking a greater percentage of the existing view through it. The barrier would appear increasingly more opaque as the view-angle became more acute {"Acute," that is, when measured from the farthest vertical, not the nearest one—TEF}

Since this "stacking effect" applies to any series of verticals, it will not only apply to the pickets as shown in the photo-simulations, and to the posts holding the grid/mesh panels as shown in the photo-simulations, both of which appear to get closer and closer together as they recede toward the horizon, but also to the vertical wire elements of the grid/mesh itself, which, since they are actually much closer together (only 2 inches apart in the final design) than either the pickets (6 to 8 inches apart) or the posts (8 to 10 feet apart) will "stack up" much sooner to produce complete opacity long before either the pickets or especially the posts holding the grid/mesh, will "stack up."

The photo-simulations, however, do not show the grid/mesh getting progressively—and at a much faster rate than the posts—more and more opaque because of the “stacking effect” of the grid/mesh’s own more closely spaced verticals, but instead show the grid/mesh retaining the same transparency all along the bridge until the “stacking effect” of the posts alone no longer allows the grid/mesh to be visible. A considerably greater portion of the view will be blocked by the total opacity of the grid/mesh itself, and much sooner, than is shown in the photo-simulations.

The incorrect rendering of the grid/mesh in the photo-simulations should have been obvious to any of the architects, engineers, designers, and so forth, at Caltrans, who should certainly know what a line of telephone poles looks like along a highway, and who should also know the rudiments of perspective drawing; but if they had only used an architectural drawing program on their computers, the program itself would automatically have depicted the grid/mesh in proper perspective—getting progressively more opaque faster than the posts— unless the grid/mesh was added in later, a deliberate action which suggests, in the language of the CEQA Guidelines §15151, that the photo-simulations of the Grid/Mesh Alternative were not presented in a good faith effort at full disclosure.

These faulty and misleading photo-simulations were used to make the Visual Quality Evaluation as reported in the original [see Figures 4 & 7], and now the updated, *Visual Impact Assessment* [see *DSEIR*, Appendix G], were published in the widely disseminated *DEIR* [Figures 2-3 & 2-7] sent to both the State Historic Preservation Officer and the Advisory Council on Historic Preservation, appeared in the *Finding of Adverse Effect* [Figures 4 & 7], were presented to the Aesthetics Design Advisory Committee, and in much enlarged versions were a prominent feature of the public forum I attended on June 9, 2008 at the Faulkner Gallery in the Central Branch of the Santa Barbara Public Library, my introduction to this profoundly misguided project.

In my *Comment* [dated 6/24/08] on the *DEIR*, I explained [¶3, my p.2, *DSEIR*, p.73, *FEIR*, p.356], using the standard term “perspective foreshortening,” this crucial error in the photo-simulations of the grid/mesh photo-simulations about which Caltrans, in their response to this paragraph of my *Comment* wrote:

The use of photo-simulations is an industry standard and a best practice method of understanding the potential appearance of a proposed project. Computer-modeling software is used to increase accuracy of the renderings.
[*FEIR*, p.363]

Did Caltrans then correct this serious error in the grid/mesh photo-simulations to increase the accuracy of the renderings so that all who viewed these photo-simulations could best understand the potential appearance of the preferred Grid/Mesh Alternative? No, they did not. Instead, in their response [*FEIR*, p.364] to this objection of my *Comment*, they merely quoted from the textual discussion found in the original *Visual Impact Assessment* [*VIA*, ¶3, p.7] that I, myself, had referenced to document the discrepancy between their text [not found in the *DEIR* or the *FEIR*] and their photo-simulations, and, in bad faith [see CEQA Guidelines §15151], again printed the same inaccurate photo-simulations in the *FEIR* [Figures 2-3 & 2-6]!

Not only are the same seriously inaccurate photo-simulations of the Grid/Mesh Alternative reprinted twice in the *DSEIR* [Figures 2-3 & 2-6, pp.15 & 17, respectively; and in the *Updated VIA*, *DSEIR*, Appendix G, Figures 4 & 8], but continuing in “worse faith,” Caltrans has newly released photo-simulations of the Grid/Mesh Alternative with Mitigation/Minimization Measures Applied [*DSEIR*, Figures 2-11 & 2-12, pp.23 & 24, and in the *Updated VIA*, *DSEIR*, Appendix G, Figures 5 & 9] from Viewpoints 1 & 2, that do not accurately render the so-called “stacking effect,” not of the posts holding the grid/mesh, but of the vertical elements of the grid/mesh itself, which again do not show the increasing opacity of the grid/mesh which remains transparent all along the bridge until the “stacking effect” of the posts alone no longer allows the grid/mesh to be visible. The potential appearance of the proposed, preferred Grid/Mesh Alternative with the so-called mitigation/minimization measures applied is falsely rendered by the photo-simulations which make the grid/mesh look transparent instead of quickly converging to complete opacity. So little care went into the new photo-simulations that the one from Viewpoint 2 [*DSEIR*, Figure 2-12, p.24] shows the tops of at least four vehicles traveling east in thin air outside the barrier on the side of the bridge for the westbound traffic! Both the old seriously inaccurate

photo-simulations and the new seriously inaccurate photo-simulations, both in much enlarged versions, were a prominent feature of the Open Forum Public Hearing for the *DSEIR* I attended on January 5, 2011 at the Cafeteria of San Marcos High School in Santa Barbara, and the two and a half years since my introduction to this profoundly misguided project has not mitigated or minimized the bad faith of Caltrans.

As mentioned above in the first paragraph, my *Comment* on the *DEIR* has been reprinted in the *DSEIR* [pp.72-78] to include Caltrans's response [*DSEIR*, p.79], reprinted *verbatim* from pages 362-363 of the *FEIR*, concerning the project's inconsistency with the Santa Barbara County Comprehensive Plan prohibition on a project's intruding into the skyline. However, the first paragraph of Caltrans' response is directed to my objection that there was no analysis whatsoever in the *DEIR* (or the original *VIA*) of three more factors which will make the proposed barrier even more obstructive: 1) the effects of headlights, 2) the effects of shadows, and 3) the effects of moisture from rain and fog.

With respect to the first, Caltrans' insufficient response, I now realize, after having read the *FEIR*, was partly a defense of their checking the "No impact" box of the CEQA Checklist under Aesthetics (d): "Would the project create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?" [*FEIR*, p.76]. Though traffic headlights will illuminate the barrier in an often distracting intermittent rhythm from Viewpoints 2 and 3, I am most concerned with a well-known consequence on Viewpoint 1 of how the eye works: when the grid/mesh is lit by headlights, or at certain times of day by sunlight, because of the difference in illumination between the grid/mesh and the much darker total landscape (the skyscape, the landscape, and their interface) on the other side of it, only the brighter object will be seen, that is, the grid/mesh and not the total landscape. Aren't the experts at Caltrans familiar with the homely nighttime experience of an outside light making a window screen opaque, while an inside light preserves its transparency? It is interesting to note that in this paragraph of their Response [*DSEIR*, p.79; *FEIR*, p.362] and only here, does Caltrans admit that the "low reflectivity finish" [*DSEIR*, pp.23 & 54] will make the barrier darker! Nothing the Aesthetics Design Advisory Committee, however, could come up with, short of redesigning the human eye, will prevent headlight illumination of the barrier from obscuring the wonderful views from the bridge at night that I briefly described in paragraph three of my *Comment* [*DSEIR*, pp.72-73]. Headlights, moonlight, and starlight, for example, will not get along in the presence of the proposed barrier.

With respect to the second factor, that is, the potential distracting effects of a network of shadows onto the bridge deck or one's windshield, or as potentially making parts of the barrier more obstructive, I still do not find any evidence that any analysis has been done. The relevant, and of course, already misleadingly inaccurate, photo-simulations seem to represent a mythic high-noon in which the barrier casts no shadows onto the deck...

With respect to the third factor, the effects of the moisture from rain and fog, Caltrans absurdly responds by merely ruling out the formation of films of water between the interstices of the grid/mesh through the action of surface tension [*DSEIR*, p.79]! In my *Comment* [*DSEIR*, p.73, first ¶], I was asking for a sufficient analysis of what will happen when the barrier gets wet in the rain, in the fog, or even from dew. Do the experts at Caltrans want to maintain that the posts, rails, and grid/mesh (from #8 wire, thicker than is sold at the Home Improvement Center in the city of Santa Barbara) will not get wet in the rain or fog or dew? Or has the Aesthetics Design Advisory Committee come up with (but not disclosed) an even darker water-repellent coating that will prevent the barrier from ever getting wet? Caltrans is mute on this, as on so many other topics. There is the potential that when the barrier gets wet, the surfaces, even with a low-reflectivity finish, will glisten in light and be more distracting, and that collected water droplets could blow onto one's windshield in a distracting manner, and so forth. There is not, of course, even an inadequate analysis to control speculation, for example, on what effects moisture could have on both the structural integrity of the barrier and the bridge, effects which could, for example, increase the periodicity and costs of maintenance for both.

I would like to point out again [as I did previously in my *Comment*, *DSEIR*, p.78, ¶3], since costs feature in the *DSEIR* [for example p.7, last bullet point, and extensively in Appendix H] as ruling out other Alternatives than the preferred one, that in Table 1.1, Comparison of Alternatives, of the *FEIR* [p.10], actual figures for construction costs of the two Alternatives considered there (the Grid/Mesh and Vertical Alternatives) are compared to the maintenance cost of the No-Build

Alternative for which no actual figure is given. If costs are going to be used to decide between Alternatives, they should be comparable. This table should not only include the actual costs of maintaining the bridge with no barrier, but also the maintenance costs of the other Alternatives, not just their construction costs. In the previous paragraph above I raise the issue that it is possible that a barrier, in addition to its own maintenance costs, may also, for a variety of reasons, difficulty of access, for example [above it was the effects of barrier water on the non-barrier parts of the bridge], increase the costs of standard maintenance of the bridge. One of the repeated complaints of the State Historical Preservation Officer [for example, *DSEIR*, Appendix H, Attachment 42] was that Caltrans did not fully itemize costs.

I have so far mainly been concerned with the inadequacy, incompleteness, and bad faith effort at full disclosure of the *DSEIR* in analyzing the impacts as a whole that the Grid/Mesh Alternative will have on Visual/Aesthetic Resources before mitigation. The proposed mitigations of this Preferred Alternative are supposed to be design refinements by the Aesthetics Design Advisory Committee [ADAC]:

The design committee's recommendations did not change the fundamental design of the barrier, but helped refine detailed aspects of the barrier's design. [*DSEIR*, p.53]

The characteristics of the fundamental design of the Grid/Mesh Alternative include both what are called the Common Design Features of the Build Alternatives and what are called the Unique Features of the Build Alternative in question [listed in the *FEIR*, pp.7-8], and it would be those Features which the ADAC would refine to reduce their impacts. Thus the Mitigation/Minimization Measures of the Preferred Alternative listed in the *DSEIR* [twice on pp.22-23 & 53-54] resulting from refinement would correspond to either a Common Design Feature or a Unique Feature of the Grid/Mesh Alternative, both of whose impacts should already have been sufficiently analyzed in the *FEIR* [as well as depicted in the photo-simulations before the applied mitigation]. For example, the first listed Mitigation Measure [*DSEIR*, p.22 & p.53] includes the 2-inch square openings of the grid/mesh which correspond to the 1-to -2-inch opening of the grid/mesh included as a Unique Feature of the Grid/Mesh Alternative [*FEIR* p.8], and whose view blockage was deemed a significant impact in the *FEIR*. The 2-inch opening is a mitigating design refinement presumably because a 2- inch opening would have less of an impact on the visual resource than a 1-inch opening.

However, in trying to reconstruct the doings of the ADAC from the inadequate meeting summaries provided in the unpaginated Appendix B of Appendix G [fleas with fleas] of the *DSEIR*, I came across item 5 of Meeting #5 [at which only three members out of seven from the local community were in attendance; absenteeism without replacement by an alternate was an inadequacy of the meetings (all the community members were present for only the first one)]:

5. Access preventer (end treatments) non-climbable. Photos of the existing conditions at the ends of the bridge were provided. End treatments slope down to end. Where should fence end? Access preventer should be located 1 to 2 bays in from the end of the barrier. Make the barrier as short as possible with 2 bays going past the maximum height of 20 to 25 feet was suggested. Access preventer should be a fan shape to match curve of barrier possibly, be located on the vertical post, and be aware of the bridge expansion joints. Discussed various barrier dimensions.

And item 6 from Meeting #6 [the final meeting of the ADAC, 3 from community absent]:

6. End treatment or access preventer design ideas were discussed.

And finally what are called the "action items" from Meeting #6:

Action items: 1) We will provide the team the following information via email as the final follow up for the ADAC Committee: • Appearance and location of the access preventers • Explore options for terminating the barrier ends, such as providing a transition to full height, etc • Location of the beginning and end of barrier relative to the bridge deck

The so-called "access preventer" is listed neither as a Common Design Feature nor a Unique Feature of the Build Alternatives, nor does it have a corresponding Mitigation Measure, nor does one or more appear in any of the photo-simulations, nor is it mentioned anywhere in the *DEIR* or the *FEIR*, and yet there will apparently be one of these fan-shaped things from 20 to 25 feet high (?) on both sides at both ends of the bridge! If these imaginably hideous structures are not

Features, Common or Unique, of the Build Alternatives, they cannot be design refinements that mitigate the impacts of those Features on the Visual/Aesthetic Resources. It would seem that they themselves would need to be mitigated, but because the email information which is supposed to show the appearance and location of these structures [For a decision of the ADAC after their final meeting?] is not part of the DSEIR (as it should be), these mysteriously ugly access preventors, if nothing else, are just another example of Caltrans' bad faith whose height, or rather depth, is surely more than 25 feet, fan-shaped or not. And if the preferred Grid/Mesh Alternative is not climbable from the deckside, or at least, not conveniently so [DSEIR, pp.22 & 53], why should access to it on the other side be prevented? To deter thrill seekers?

To return to the Mitigation Measures, consider the last Measure [DSEIR, p.23 & 53], the low-reflectivity finish. This corresponds to the last Common Design Feature of the Build Alternatives [FEIR, p.8] which is, a low reflective finish! Is a low-reflective finish a design refinement mitigating a low reflective finish [perhaps the hyphen is the mitigating factor]? There appears to be little difference in the finish of the posts, rails, and grid/mesh in the photo-simulations before and after mitigation measures have been applied. What did the ADAC do to come up with this Mitigation Measure? Examining the sketchy meeting summaries I came up with item 2 of Meeting #4 [2 community members absent]:

2. Presentation of design sketches and actual grid/mesh samples. Discussion of rod and grid/mesh dimensions, colors/materials, black, galvanized, acid washed galvanized, stainless steel, models should be tested in real life situation, how to make the barriers better aesthetically but still be effective, make the opening sizes as large as possible but still effective, vertical 4 or 6 inch between vertical posts, grid/ mesh size (1-2 inches), and a reduction in horizontal rods.

Item 2 of Meeting #5 [planned as the last one, 4 community members absent]:

2. Discussed design and colors. Simulations with revised sizes and design were provided. Vertical support members' connection with the existing structure was revised as shown in the simulations. Committee members stated that bonderizing in dark brown or gray colors looks old, galvanized finish looks better. The grid/mesh design will be easier to look through than the vertical pickets. The best finish and color choice is dependent upon which side of the structure one is looking out, since the backdrop on one side of the structure is the densely forested green canyon whereas the other side is the sky and mountain panorama view. The panorama view is the top priority and the barrier on each side of the structure should be consistent with the same in color, finish and design.

And items 4 and 5 from Meeting #6 [the final one, 3 from community absent]:

4. The ADAC members unanimously agreed that the grid/mesh alternative had the least visual impact. Try to match the color of the tube rail. With a bonderized finish, reflectivity is reduced as compared to powder coat so it is recommended to use a bonderized finish with low reflectivity. Keep the least length of the barrier with the end treatments on the bridge side of the bolsters.
5. Colors and finish were discussed.

Well, what happened to black and acid wash galvanized (or is that the same as galvanized later)? Where did grey come from? A bonderized finish was recommended [by whom?] and a galvanized finish looks better than bonderizing in dark brown or gray. Both sides of the grid/mesh should be the same color even though what finish and color is chosen for one side will not be as good for the other side. However, the color of the original tube rail of the bridge is supposed to be matched. But what color and finish were chosen and by whom? Do the photo-simulations of the Grid/Mesh with mitigation measures applied show the final choice? As I pointed out above, Caltrans' Response to my *Comment* claims that:

The barrier finish will be darkened to reduce reflectivity from both headlights and from the sun. [DSEIR, p.79]

Is that the bonderized finish? How can it match the original tube rail if the finish is darkened? But what color is it? And will it be worse to look through it to the "densely forested green canyon"? It is difficult to understand what is supposed to be a mitigation process by reading the meeting summaries, and the list of Mitigation Measures that supposedly resulted from this process is too general to be adequate, let alone complete.

When it comes to the "mitigated" relationship between the posts and the rails discussed at Meeting #3, the reduction of one obstructive factor seems to involve an increase in another, and it is not clear why the ADAC gives different weights to the different factors. For example, the ADAC seems concerned with reducing the diameter of the top rail even though it means that the intermediate rails (through which the view is seen) have to be thicker. Again, the ADAC seems concerned to reduce the number of posts (because of the "stacking effect," though not taking account that effect as it applies to the much more closely spaced verticals of the grid/mesh itself) even if it results in adding more intermediate rails. What is the rationale for these choices? A comparison of the [inaccurate] photo-simulations of the Grid/Mesh Alternative for Viewpoints 1 and 2 with and without mitigation measures applied shows a top rail the same diameter with and without mitigation [though for some reason (another error?), the original bridge railing is thicker in the unmitigated photo-simulation from Viewpoint 1], but 2 more intermediate rails in the mitigated version which apparently allow for fewer posts, approximately one every other bridge rail support. Taking into consideration the "stacking effect" of the grid/mesh verticals which will produce opacity sooner than the "stacking" of the posts, the visual obstruction of more intermediate rails could be worse than more posts, but the inaccurate rendering of the grid/mesh in the photo-simulations does not allow for an informed decision on the optimum relationship between posts and rails. More and thicker rails might offer greater purchase for climbing, too, because of increase in the "ladder-like" configuration" [FEIR, p.11] of the barrier. The "irregular intervals between the existing bridge-railing supports" [FEIR, p.38; DSEIR, pp.23 7& 54] on the order of a two foot discrepancy are not apparent in the photo-simulations or in what are supposed to be [undoctored?] photos of the existing site. Wouldn't the installation of the posts extending 6 feet above the original bridge railing and in alignment with the bridge railing supports not tend to emphasize this irregularity? And wouldn't evenly spaced posts (with a less expensive standard panel size) staggered in relation to the comparatively short bridge railing supports tend to disguise the irregular intervals because of the much greater dominance of the grid/mesh structure as a whole with its regular "rhythm"? In the mitigated version, the posts will only line up with every other bridge support anyway, and the "counter-rhythms" of the bridge supports in the staggered version might even add interest to the depressing wire cage.

But these finicky design refinements of the inadequately explained and incompletely documented activities of the ADAC cannot mitigate the substantial and significant impacts from view blockage (often complete opacity depending on the acuteness of the viewing angle) and visual intrusion that at least 70% [How this figure was arrived at has so far been unexplained] of the high quality views from the bridge along an Officially Designated State Scenic Highway will suffer if the preferred Grid/Mesh Alternative is built. That is because the substantial and significant impacts result from the Common Design Features of the Build Alternatives combined with the Unique Features of the Grid/Mesh Alternative before the so-called design refinements of the ADAC are applied to either. Tinkering with these Features (or Constraints, that is, conditions that the project must meet [DSEIR, Appendix G, Updated VIA, Appendix B, Item 4, Meeting #2 Summary]) will not substantially or significantly alter these impacts.

My Comment was included in the DSEIR [pp.72-78] so that Caltrans could include their Response [p.78] to my objection that the Grid/Mesh Alternative was incompatible with the Santa Barbara County Comprehensive Plan Land Use Element Policy prohibiting projects from intruding into the skyline as the Preferred Alternative will. Caltrans' answer is that their documents did not claim that this Alternative would not violate this policy [And I suppose that their documents do not claim that their photo-simulations are not deliberately misleading!]. However, in the CEQA Checklist of the FEIR [p.81] the "No impact" box is checked under the Land Use and Planning section, b):

Would the project conflict with any applicable land use plan, policy, or regulation of any agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?

Upholding the incompatibility and checking that box can only mean that the Santa Barbara County Comprehensive Plan Land Use Element is not applicable to this project. Indeed, in the new material added to the DSEIR [p.10, 2.1 Visual Aesthetics, Affected Environment, corresponding to the same section of 2.1.2 of the FEIR, pp.26-28] Caltrans writes that

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State Route 154 (The Chumash Highway) is “not under the jurisdiction of the local planning authority,” but that, nevertheless, this inapplicable policy is “an indicator of the general level of community sensitivity regarding the aesthetic character of the region and of the project area,” a community sensitivity which Caltrans callously disregards in their desire to cage-in the community users of a scenic bridge over which there are an average of 5,840,000 trips a year. However, in the preceding paragraph Caltrans, after explaining that the Chumash Highway through the project area is an Officially Designated State Scenic Highway, goes on to say:

The State Scenic Highway Program designates routes based on high quality views of the natural landscape along the route, and on the local governing body’s implementation of a Corridor Protection Plan. [DSEIR, p.10]

It would seem, then that if the Chumash Highway is a State Scenic Highway, the local planning authority would have jurisdiction over it by way of its Corridor Protection Plan, and that if the project will have unmitigable substantial and significant impacts on “some of the most memorable views views along State Route 154” [DSEIR, p.11], that the relevant box to check under Land Use and Planning b) of the CEQA Checklist should have been “Potentially significant impact.” Or are there no land use plans, policies, or regulations of the Corridor Protection Plan that are applicable to the project? If there are not, if the scenic views of a designated highway can be destroyed by the very agency responsible for maintaining the highway, what can the meaning of “protection” be here? Perhaps it means that billboards cannot be erected along the highway corridor, but Caltrans can put them on the bridge. Maybe they should, as a third build Alternative, and use the rental money for a unique suicide barrier that would not require the misuse the funds deviously allocated for the proposed project. And what role did the Santa Barbara County Association of Governments and the Santa Barbara Board of Supervisors play in furthering this project if it was “not under the jurisdiction of the local planning authority”? Caltrans has been making the most of legal loopholes that must be larger than 2 inches in diameter.

Judging from the bulk of Appendix G of the DSEIR, destroying some of the most scenic views of a scenic highway is less important than preserving a debatable set of so-called character-defining features of a bridge that was only 43 years old—7 years younger than the 50 years usually required—when the Cold Spring Canyon Bridge became eligible for listing in the National Register of Historic Places. What should now be referred to as the Chumash Highway had already been an Officially Designated State Scenic Highway for 35 years when the bridge received its premature eligibility on Jan. 1, 2004:

The State of California designated SR154 from Santa Barbara into the Santa Ynez Valley as a Scenic Highway in 1968. In addition, the nonprofit organization Scenic America rated a 32-mile section on SR154 from Santa Barbara through the Santa Ynez Valley as one of the top ten most scenic highways in the United States. [DSEIR, Appendix H, Attachment 1, *Historical Properties Survey Report (HPSR)*, Attachment 3, *Historical Resources Evaluation Report (HRER)*, p.30, referring to *Santa Barbara News-Press*, January 3, 1995, 35A; Division of Highways, *Scenic Highway Corridor Survey*, 1968; and Santa Barbara Route 154 file at Caltrans Transportation Library. The scenic route designation signing occurred November 22, 1968. (Feas on fleas, and the CD ROM is not really indexed—TEF)]

Why should preserving the historical integrity of 43-year-old, compromised skewbacks [DSEIR, Appendix H, HRER, p.31] be more important than preserving the panoramic views from the bridge along a State Scenic Highway? What are deemed the most significant character-defining historic features of the bridge, “the steel arch, columns, towers and other elements of the bridge’s substructure, which were designed specifically for the the Cold Spring Canyon location” [DSEIR, p.35], cannot be seen by those people making the 5,840,000 vehicle trips over the bridge a year:

While traveling across the bridge on top of the deck, the bridge arch and super-structure cannot be seen. In addition, the roadway is comparatively straight approaching the bridge from both directions, which doesn’t allow opportunities to see the lower part of the structure {that is, the substructure which includes both the arch and what supports the deck above the arch, the superstructure—TEF} from the roadway elsewhere on State Route 154. As a result, the only bridge elements visible from the highway itself are the paved lanes, bridge rails, guardrail at each end of the bridge rail, and signs {this last not really a “bridge element”—TEF}. [DSEIR, p.11]

In most cases, it would be illegal stop and look at the bridge substructure (including some of the arch and its superstructure) from Viewpoint 2:

It should be noted that this area is signed for emergency parking only and not legally available for sight-seeing opportunities of the bridge or the surroundings. Guardrail along the other three approaches to the bridge prevents parking and limits side views of the bridge from those locations. [DSEIR, p.12]

Therefore, the so-called character-defining features in question—400 feet up— can only be peeked at through oak trees after stopping at some select pull-outs on Stagecoach Road, or by craning one's neck at an extreme angle in a convertible while driving (dangerously) along the bottom of the canyon on Stagecoach Road [DSEIR, p.12]. Sightseeing passengers might have a better time of it along Stagecoach Road, but not on the bridge itself, though Caltrans is curiously uninterested in such passengers, having ignored the request in my *Comment* [DSEIR, p.73, my p.2, ¶4] to include passenger viewpoints in their photo-simulations. Caltrans also did not disclose anywhere in their massive documents the average number of vehicle trips along Stagecoach road—with, or without, passengers.

Both the drivers and the passengers in vehicles making an average 16,000 trips a day over the bridge can safely see the fantastic panoramic views of the total landscape (that is, the landscape, the skyscape, and their interface), but what they can see of the bridge itself are, of the debatable character-defining features, those debatably deemed of lesser significance (because not designed specifically for this location): the concrete deck and the railings [DSEIR, p.35]. This emphasis on design specificity in determining the significance of the so-called character-defining features is not explained. Nor is what is meant by design specificity. The steel arch is deemed one of the most significant character-defining features, and yet steel arches were used in many other bridges at the time.

The Bridge Department used well-established structural components—including steel arch, welded steel components, and reinforced concrete deck—but executed the overall design in a manner that captures aspects of the highest quality of work in bridge engineering in California at that time. [HRER, p.38]

However, the deck, on a par with the arch in this respect, is deemed a character-defining feature of lesser significance. The dimensions and proportions of both the arch and the deck, after chosen as structural components for the design of the bridge, would, of course, have to be calculated for this particular bridge. Is the arch deemed of greater significance because those calculations would be more complex? The concrete in the deck, of course, was the kind of concrete used at the time—but so was the steel in the arch. The properties of the steel available at the time in fact restricted the possible design types for the bridge.

The Cold Spring Canyon Bridge was also designed and built prior to other innovations and structural testing that are now prevalent in bridge construction. These include innovations in steel manufacturing that have allowed other structural types to be used for long spans, including "weathering steel" developed in the mid-1960s and "high-performance steel," which was developed in the early 1990s. The latter type has greater strength, is lighter in weight, and has greater atmospheric resistance than conventional steel. [HRER, p. 25, referring to Shulman, oral interview, March 19, 2007; Whitaker, oral interview, March 12, 2007; Hood, oral interview March 28, 2007; Whitaker, oral interview, March 12, 2007; Robert A.P. Sweeney, Chairman of the Committee on Steel Bridges, "Steel Bridges," Transportation for a New Millennium. Available from the Transportation Research Board, "Millennium Papers" <http://www.trb.org/TRB/publications/MillenniumPapers.asp> (accessed January 2007).]

Neither the concrete used for the deck, nor the steel for the arch were designed specifically for the bridge. And yet integrity of the materials used in a historic property is one of the criteria used for evaluating impacts to that property [DSEIR, p.29]. An alteration of those materials, whether specifically designed or not, would constitute an adverse effect. It would seem that using an undefined concept of design specificity to determine the significance of the so-called character-defining features of the bridge gives lesser significance to just those features of the bridge which would be most effected by the Alternative chosen by Caltrans, and gives the most significance to just those features which would be most effected by Alternatives rejected by Caltrans, for example, the Safety Net Alternative. One of the reasons for rejecting the latter Alternative is the following:

Increased impacts to the historic substructure—Installation of a safety net involves drilling holes in the face of the bridge and adding and suspending a large horizontal metal net below the bridge deck. This would diminish the

bridge's historic integrity by permanently altering the appearance of the bridge's most significant character-defining feature, its substructure. [DSEIR, p.6]

Alteration of the substructure was also one of the reasons for rejecting the Cantilever Arc Barrier Net Alternative proposed by the State Historic Preservation Officer:

The bridge cannot withstand the additional load that would be associated with a suicide attempt if the Cantilever Arc Barrier Net were on the bridge. Although the barrier could support at least one individual, the barrier would not be able to withstand the weight associated with a rescue team. A design for a greater load would entail retrofitting the structure, including replacing the existing concrete rails and bridge deck. Construction of the barriers would also likely entail retrofitting the substructure. This would lead to greater permanent and irreversible structural changes to the Cold Spring Canyon Bridge; further decreasing the integrity and historic qualities that make the bridge eligible for listing on the National Register of Historic Places. [DSEIR, Appendix H, Attachment 39, *Feasibility Study* {FS}, p.4]

Caltrans attempts to explain why the bridge was evaluated for National Register eligibility prematurely:

Cold Spring Canyon Bridge is the only cultural resource and the only historic property present in the project's Area of Potential Effect. An assessment of the proposed project's effects on the bridge is therefore required. In general, cultural resources that are not yet 50 years old are not evaluated for National Register eligibility. Although the Cold Spring Canyon Bridge was at the time only 43 years old, it was formally evaluated in 2007 in connection with the barrier project. Caltrans made this decision because the bridge is a notable structure, because it is central to the proposed project, and because sufficient time had elapsed since the bridge was built to allow an assessment of its place in the historic record. [DSEIR, Appendix H, Attachment 20, *Supplemental Report* {SP}, pp.1-2]

The *HREER* explains that as late as 2006, the bridge was still considered too young for evaluation:

Furthermore, Caltrans has, to date {prior to this report—TEF}, considered the bridge to not be eligible for listing in the NRHP based on the department's original historic bridge inventory in the 1980s. This was because the structure was less than fifty years old at that time. Caltrans also did not evaluate the structure during the recently completed historic bridge inventory update (2002-2006) because the inventory evaluated bridges built in or before 1959. The Cold Spring Canyon Bridge does not appear to have been previously evaluated for its eligibility for listing in the NRHP or CRHR. [HREER, p.2]

By the time Caltrans initiates the premature process for determining the bridge's eligibility, they have already decided on a barrier and that it would be built on the deck. Hence the necessity for controlling the significance of the so-called character-defining features to allow Caltrans to build what they want. This process was already underway by February 2007 [see letter from Christopher McMorris to the County of Santa Barbara Historic Landmarks Advisory Commission dated Feb. 2, 2007; DSEIR, Appendix H, *HPSR*, Attachment 3, *HREER*, Appendix C, Attachment 5, first letter {fleas with fleas}]; the *DEIR* was released in May 2008!

Let's get back to the deck with the panoramic views of the total landscape and an uncluttered view of one of the lesser character-defining features of the bridge: the railing. This was not specifically invented [DSEIR, p.35] for the bridge:

The railings on the bridge are a Division of Highways standard type of their period, called type II barrier railings. Each railing is 3 feet, 7 inches tall and has three-foot-wide barrier curbs in front of them. [HREER, p.33, reference to Jahlstrom, "Report of Completion for the construction of Cold Spring Canyon Bridge, County of Santa Barbara," June 17, 1964; *California Highways and Public Works*, September-October 1963, 15-17.]

It was a "well-established structural component" chosen to go with the other elements of the total design. Since it is a type II, presumably there was a choice at least between two types, if not more; what the differences are between these types is not disclosed. The railing, type II or not, is not a pedestrian safety railing, but rather one to prevent automobiles from going off the bridge in an accident:

...[T]he bridge was not designed for pedestrian use. It has no sidewalks, and the existing 2-foot-wide by 10-inch high concrete curb is an integral part of the bridge safety railing. The top of the existing safety rails is 3 feet 7 inches from the roadway and 2 feet, 7 inches above the concrete curb. [FEIR, p.12]

There isn't enough room to walk between the cars and the edge of the bridge, and the railing is too low and thick for pedestrians. Why did the designers of the bridge not choose to make it pedestrian safe? What was the basis for this decision?

The Bridge Department did not consider a specially designed railing for this structure, although the decision to use the type that is on the bridge may have been seen to provide for improved views from the bridge. [HRER, p.15, referencing Raymond L. Whitaker, oral interview, March 12, 2007; Shulman, oral interview, March 19, 2007; F. H. Yoshino and R.L. Whitaker, *California Highways and Public Works*, September-October 1963, 15 and 20.] {Didn't Mr. McMorris directly ask about this in his interviews?}

That means that the views from the bridge were considered in the original design of the bridge! To make the design changes necessary to accommodate pedestrians, including a walkway at a greater distance from the traffic and a different kind of (standard?) railing, would have compromised the spectacular views from the bridge of the total landscape.

Shulman maintains that the magnitude of the site, including its picturesque location, demanded a structure to complement the setting. [HRER, p.14]

As construction was coming to completion, Whitaker assessed the structure's aesthetic qualities, describing the "two gracefully curved box-shaped arch rib sections" of the bridge and listing the structure's important qualities include its all-welded sections, "unusual arch proportions," picturesque setting, and "generally pleasing appearance" as contributing to its being a "notable addition to the California highway scene." [HRER, p.15, referring to *California Highways and Public Works*, September-October 1963, 15 and 20.]

Associate engineer Marvin A. Shulman was largely responsible for the design of the bridge, but he left the department prior to completion of its design. Raymond L. Whitaker completed the engineering on the structure and signed the project plans as the project designer. [HRER, p.12]

What could the two main designers of the bridge have been referring to by "its picturesque location" and "its picturesque setting"? To steep terrain covered with poison oak and dropped clipboards at the foot of the columns? They were referring to the panoramic views from the bridge that they did not want to obscure with a railing suitable for pedestrians and other bric-a-brac on the deck. I repeat: the picturesque views from the bridge were taken account of in the original design in the deliberate decision not to provide for pedestrians by using a different railing. This bears repeating to counteract the reiteration by Caltrans that the views from the bridge were not part of the design process:

As discussed in the project HRER, the views from vehicles on the bridge were not part of the design of the structure, but were considered a benefit of the chosen design that included the short standard-type railing. [DSEIR, Appendix H, *Finding of Adverse Effect (FAE)*, p.20]

Views from the bridge were a benefit following construction, but were not part of the design process and are not considered to be a character-defining element of the bridge. [FAE, p.21]

Let's clarify this attempt at obfuscation on consecutive pages of the same document. Concerning the definition of character-defining features, the DSEIR says:

The character-defining features that make the Cold Spring Canyon Bridge eligible for the National Register of Historic Places are those components that are part of its original design and overall design effect, including the arch ribs with their cross bracing, the towers and columns, floor beam girders, skewbacks, abutments, railings, and road deck. [DSEIR, p.28]

According to the quotation above from page 20 of the FAE, views from vehicles on the bridge are supposed to be a benefit of the chosen design, that part of the design which included the choice of the railings; which would mean that the views from the bridge are part of its overall design effect, thus falling under the second part of the definition of a character-

defining feature as given in the quotation above from the *DSEIR*. Caltrans does not disclose the legal basis for limiting character-defining features of a historical property to those features of its original design and overall design effect, nor does it define what constitutes the design process. It was shown above that without such a definition, which features of a historic property qualify can be manipulated to achieve previously desired outcomes on the part of the agency using the historic evaluation process for its own purposes other than so-called preservation.

That there are relevant features of a historic property that can be subject to adverse impacts under the Criteria of Adverse Effect [*DSEIR*, p.29] that are not the result of an unstated, idiosyncratically conceived, and ambiguously applied definition of original design is clear from the fact that two of the categories under those Criteria are the setting and the location. The setting and the location of the Cold Spring Canyon Bridge are not part of the original design of the structure!

When I argued in my *Comment* on the *DEIR* [*DSEIR*, p.74, my p.3, ¶12] that the views from a historic property are part of its location and setting by imagining the consequences of the Washington monument and the Eiffel tower changing places so that the views from the former would be of Paris and the views from the latter would be of Washington, D.C., the Response of Caltrans [*FEIR*, p.365] was make the erroneous claim that the views from the bridge were not part of the design process, and refer me to their Response #1 [*FEIR*, p.158] to the letter of Kellam de Forest, Chairman of the Pearl Chase Society [*FEIR*, p.157] who refers to a previous letter from the Pearl Chase Society [not included in the *FEIR*] which pointed out "the historic significance of the Bridge" and asked "that the visual beauty of this architectural gem and the vistas from its motor-way be preserved." Caltrans Response describes their initiation of the (premature) eligibility for listing in the National Register of Historic Places and summarizes:

Many of the features of the bridge that make it eligible for the National Register of Historic Places cannot be seen while you are driving on the bridge itself. The most significant historical aspects of the Cold Spring Canyon Bridge can only be viewed from below the bridge; that view would remain virtually unchanged. The physical alteration to the historic fabric of the bridge itself would be limited to bolt holes to attach the barrier. [*FEIR*, p.158]

This more evidence of the premeditated tactic of Caltrans to use the eligibility listing as a pretext for artificially ruling out the "vistas from the motor-way" as being part of the historic significance of the Bridge as the Pearl Chase Society considered them to be.

A person does not have to be a member of the Pearl Chase Society to realize that "vistas" are a feature of any property, historical or not. Any visual preservation ordinance prohibiting blockage of views assumes that the views subject to litigation under such widespread ordinances are feature of the property of the owner. If one's ocean view was blocked by a neighbor's building a grid/mesh barrier that obstructed 70% of the view, under such an ordinance, the owner of the property with the view could sue. Unfortunately, when Caltrans is sued, taxpayers pay for both sides!

Finally, even if the vistas from the bridge were not part of the manipulative, *ad hoc* definition of what counts as the design process, those vistas have been seen from the bridge since it was built, as Caltrans admits.

At the time of the bridge's construction, views of the canyon from the bridge and surrounding countryside were considered to be among the most beautiful and impressive in California and considered an asset to the structure's design. [*HREER*, pp.15-16, referring to *California Highways and Public Works*, September-October 1963, 15.]

The vistas from the bridge would fall under the integrity criteria, in addition to setting and location, of association and feeling. When the bridge was designed, it didn't have any historic features at! When it bridge was built, it wasn't 50 years old because it was brand new! Its age of 43 at the time of its premature eligibility evaluation was not a part of the original design process as defined in bad faith by Caltrans, either. Historic features accrue to property over time.

Although the bridge is not yet 50 years old, its significance can be viewed with historical perspective: the structure illustrates a defined period of bridge engineering and architecture that reflects the refined development of steel arch bridge technology and the aesthetic of the post-World War II Modern era. [*FEIR*, p.40]

That the bridge is an illustration of a defined period was not part of the design process of the bridge, nor was the bridge an illustration of this period when it was built because the period of which it became an illustration had not elapsed!

The Cold Spring Canyon Bridge illustrates the maturation of bridge design that encompasses an aesthetic influenced by Modern-era design of the World War II period. It demonstrates a collaborative effort in the Bridge Department that encompasses the engineering and architectural values of its period. The simple geometry, graceful lines, slender components, and open uncluttered appearance are indicative, as noted above, of the era in which the bridge was designed and built—a period that can now be viewed with sufficient historical perspective. [HREER, p.26]

Unlike the almost-too-obvious-to-mention picturesque vistas from the bridge that its designers deliberately chose to enhance and not burden with extraneous visual “clutter,” the so-called “Modernism” of the final design of the bridge could not have been on their minds then, because this “Modernism” can only be seen “with sufficient historical perspective.”

While the engineers did not recognize this atmosphere as working in the “Modernist” aesthetic, their efforts to design an open, uncluttered, graceful steel arch design corresponded to the tenets of mid-twentieth century Modernism that sought economy and efficiency of form, functional and material honesty, and structural balance that was harmonious with its setting. [HREER, p.28]

The majority of those who, in the picturesque setting of the bridge, make the 5,840,000 vehicle trips a year, cannot literally or figuratively see the “Modernism” of the bridge’s design, but they can see the panoramic views from the bridge of the total landscape, but not if Caltrans continues to force this misguided project to completion. These panoramic views, remember, were one of the primary reasons for the Chumash Highway’s State Scenic Highway designation only 5 years after the bridge was built, because what is scenic doesn’t need the benefit of a 43-year historic perspective to decide.

The mitigation proposed for what the *DSEIR* has to admit anyway are significant adverse impacts to the integrity of a historic property, cannot be significantly mitigated by photographs [of the poor quality of the fanciful photo-simulations?], copies of plans, and so forth [excepting those “confidential for security reasons”—which those are, cannot be disclosed “for security reasons”] and other interpretive “clutter.” If such impacts could be significantly mitigated in this way, why did the County spend so much money restoring the damaged murals in the Santa Barbara Courthouse? Photographs should have been sufficient. Why are they spending so much money to “restore” [make a new copy] of the fountain of this building? They could put an interpretive display in its place... Yes, this will be a precedent for mitigating cell phone towers on Half Dome and El Capitan [views of which are supposed to have a “possible” visual quality rating of 7 (unlike the mere 5.2 of Viewpoint 1 before a barrier) according to the *FEIR*, p.363] with interpretive displays featuring tiny, low quality photos of those landmarks before desecration... Of course the photos are not supposed to be pitiful stand-ins for the panoramic views from the bridge to that spectacular total landscape, no, they are merely pitiful stand-ins for various views of the so-called significant character-defining features of the bridge itself. Since the *DSEIR* has not disclosed an example of such a photo, it is hard to imagine [Will any automobiles that may appear be on the road itself, or somewhere in outer space?] what they will be like. Perhaps just the same old photos Caltrans has been recycling for years, now, of the existing views of the bridge, like Figure 2-5 or better (for those without convertibles and neck trouble), Figure 3-5 of the *DSEIR*.

The *DSEIR* is presents an insufficient analysis of both the significant adverse impacts and their proposed mitigations. The *DSEIR* is inadequate, incomplete, and not a good faith effort at full disclosure. It is not in full compliance with CEQA or the courts orders. In so far as one can understand what the mitigations are, they will not be substantial or significant.

Because the *DSEIR* includes the purpose of the proposed project, as follows:

1. Reduce the number of suicides at the Cold Spring Canyon Bridge.
2. Reduce the risks to emergency personnel such as law enforcement officers or search and rescue teams when attempting to prevent a suicide or when recovering a body. [*DSEIR*, Appendix H, Attachment 20, *SP*, p.1]

I would like to comment on the second, first, to remove one of the “warts” in my *Comment* on the *DEIR* [*DSEIR*, p.78, my p.7, ¶2]. According to the Santa Barbara County Sheriff’s office, there have been approximately 162 suicide-related calls at the bridge in the last 8 years [that is, since May 2008; *FEIR*, p.5] to which law enforcement personnel have responded. Because nothing more about these incidents has been disclosed, one can assume that since during those 8 years there was on average a single suicide a year from the bridge, the reponse of law enforcement prevented 19 suicides a year from the bridge [which figure corrects the “8” in my *Comment*], and without a single documented injury. Far from being

unsafe, law enforcement officers on the bridge have been successful! And, depending on whether or not the proposed grid/mesh barrier can be "inconveniently" climbed, perhaps more successful than the barrier will be. A purely anecdotal video of one incident involving law enforcement almost having an accident while trying to save a potential suicide is not sufficient evidence that their presence on the bridge in such circumstances is dangerous. This would be like saying the job of life guard is dangerous because of a single video showing a life guard nearly drowning while trying to save a non-swimmer. In the case of life guards, that rarely happens because they are properly trained to avoid getting the typical grappling behavior a drowning person. That suggests that the officers involved in the video-taped incident were not properly trained, as indeed, the testimony of a fireman in the *FEIR* alleged. But to then close off a beach to swimmers to reduce the risks to life guards would be absurd, as is the second purpose of the proposed project. As far as the search and rescue teams' safety goes, poison oak and sprained ankles and knees [for which no statistics at all have been disclosed by Caltrans] are minor injuries typical of merely hiking in the chaparral and are slight risks the search and rescue teams have willingly shouldered (and presumably injuries that their training would keep to the minimum).

Concerning the first part of the purpose, let me say that my aunt on my father's side of the family, an uncle on my mother's side of the family, my grandmother on my mother's side (on her second attempt here in Santa Barbara), both my wife's parents in a local double-suicide, and others in my life, have all committed suicide. But forcing means restriction on the public at large, instead of only on those at risk for suicide, is totalitarian oppression that is not in keeping with a free and democratic society. The vast majority of the community (among them, perhaps a tiny fraction at risk for suicide) who make the 5,840,000 trips over the bridge a year and experience the beauty of those panoramic views of the total landscape from the bridge should not be deprived of a good which helps to make their life worth living, in order to potentially deter a factional minority that would better be helped by caring intervention before a crisis situation arises. I personally know someone who was from suicide at the bridge and went on to substitute a different, less public, means which was not successful only because those who were close to this person were aware enough to intervene in time.

Tracy Fernandez
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Response to comments from Tracy Fernandez

Your opposition to the project has been noted.

Response to comment #1: The DSEIR and Updated VIA fully disclose, in both text and photo-simulations, that regardless of alternative, the project would result in some optical “stacking.” This effect would create the perception of increased view blockage caused by the barrier as the viewer travels across the bridge and looks straight ahead. The Updated VIA plainly states that:

With each alternative the barrier would appear most transparent when viewed perpendicular to the barrier, such as from the side windows of a vehicle, because the individual elements of the barrier would occupy the least amount of visible space relative to the view beyond. Views from the front of the vehicle would see the barrier at a more acute angle, which would result in the barrier elements appearing closer together and blocking a greater percentage of the existing view through it. The barrier would appear increasingly more opaque as the view-angle became more acute. (Updated VIA page 8)

In addition, the DSEIR clearly discloses that the barrier’s opacity (regardless of alternative) would result in the following adverse effect:

(T)he construction of a barrier would have an effect on as much as 70 percent of the existing view as seen specifically from the bridge deck.” (DSEIR page 20)

The optical stacking-effect for both project alternatives can be clearly seen in the photo-simulations (DSEIR Figures 2-3, 2-4, 2-5 and 2-6).

Importantly, the DSEIR and the Updated VIA clearly state that the proposed barriers’ view blockage (opacity caused by optical stacking) is not the sole cause of visual impact. The DSEIR states that:

The visual quality evaluation identified two distinct potential visual effects the barrier would have: 1) the view blockage (or opacity) caused by the barrier; and 2) the visual detraction to the existing setting caused by the barrier itself. (DSEIR page 20)

The DSEIR and Updated VIA point out that, separate from and regardless of the relative optical stacking effect of each alternative, the visual evaluations determined that:

(T)he vertical pickets would themselves be distinguishable elements that would draw attention to the barrier. The vertical pickets would not blend with the background and would be seen more as distinct architectural features that define the barrier.”

(DSEIR page 20), and that “The mesh barrier would be the less noticeable of the two alternatives because the mesh itself would tend to recede and visually blend with the background.” (DSEIR page 20)

Although not required by CEQA Guidelines or FHWA visual impact assessment methodology, photo-simulations are included in the DSEIR and Updated VIA in the interest of full public disclosure. The photo-simulations are based on engineering and architectural data and utilize the latest industry-standard computer modeling applications. The photo-simulations are accurate depictions of the proposed project.

In addition to the photo-simulations, the DSEIR and Updated VIA analysis methodology utilized full-scale physical model segments of the barriers that were built and viewed from various angles and distances, as well as from within moving vehicles. Members of the ADAC observed and analyzed these full-scale models and found, based on this empirical evidence that:

The Aesthetics Design Advisory Committee concurred that the grid/mesh alternative would result in less view blockage than the vertical picket alternative because it would avoid the “stacking” effect created when closely spaced vertical pickets are viewed from an oblique angle. (DSEIR page 22, Updated VIA page 12)

Response to comment #2: The project proposes no new source of lighting on or around the bridge. The minor amount of potential glare which may have resulted from the barrier will be minimized by the application of a low-reflectivity finish, described in the mitigation measures (DSEIR page 22, VIA page 13). The required low-reflectivity finish proposed by the project will minimize potential glare at all times of day and night and all seasonal conditions.

During rainy or foggy conditions, everything will be wet, including pavement, vehicles, windshields and windows, existing bridge rail and the surrounding landscape. The wetness of the proposed barrier will be visually consistent with the wet condition of everything else within view. Direct observation of several of the dozens of other bridges in Santa Barbara and San Luis Obispo Counties during rainy conditions that have fencing or barriers along the bridge rails indicate that any potential glare related to headlights would be negligible or non-existent and result in no visual impact.

The proposed openings in both project alternatives are too large for surface tension to allow water to bridge between adjacent members, therefore no increased volume of blowing water would result. Direct observation of several of the dozens of other bridges in the central coast of California during rainy and foggy conditions that have fencing or barriers along the bridge rails show that no visual impacts would occur as a result of water collecting on the proposed barrier.

Views of the night sky would remain intact with implementation of the project. As evidenced in the photo-simulations, the proposed barrier would mostly affect views of the surrounding landscape, and the sky, during both night and days, would be mostly unaffected. The project proposes no new sources of lighting on or around the bridge which would potentially affect nighttime views.

An existing lack of shadows on the bridge deck was not identified by the DSEIR as a factor contributing to the high quality of views from the bridge. The thinness of the proposed barrier elements (particularly the Grid/Mesh alternative) would minimize shadowing. Direct observation of several of the dozens of other bridges in Santa Barbara and San Luis Obispo Counties that have fencing or barriers along the bridge rails indicate that shadowing would be negligible or non-existent and would result in no visual impact.

Response to comment #3: The termination of the barrier at the ends of the bridge was analyzed by the design team and the ADAC. An array of 7 small-diameter rods will be placed on the outside of the barrier, approximately 20-25 feet in from the end of the barriers to prevent access to the outside of the barrier. The rods will be not be noticeable as placed on the outside of the barrier and will have no affect on visual quality or findings as defined by the DSEIR and VIA.

Response to comment #4: Based on several field visits, the design team and the ADAC determined that darkening or coloring the proposed barrier would cause the barrier to be much more noticeable and blend in less with the visual setting. The proposed mitigation measure requiring a low-reflectivity finish (DSEIR page 22, VIA page 13) was selected by the ADAC because, in addition to being less reflective, it would be more visually-neutral in the landscape, considering both hillside and sky backdrops. This would allow the barrier to somewhat recede visually causing less interference to the scenic vista.

Response to comment #5: The design team and the ADAC determined that the visual benefits of having half as many vertical posts (approximately every 20-feet

versus every 10 feet) outweighed the visual disadvantage of having one additional horizontal rod, required to maintain panel rigidity.

Response to comment #6: The Updated VIA and DSEIR identify Significant and Unavoidable Class I visual impacts resulting from the project, the most adverse finding of impact allowed under CEQA (DSEIR pages 21, 25; Updated VIA page 12). The full extent and nature of the Significant and Unavoidable visual impact is fully discussed and disclosed in the Updated VIA and DSEIR. The visual impacts are clearly disclosed as:

Because of the expected high level of viewer sensitivity associated with the bridge and Highway 154, combined with the magnitude of visual change identified by the visual quality evaluation ratings, the project is anticipated to result in substantial adverse impacts to the visual environment. The impacts would be the result of:

The partial blockage of high-quality views from an Officially Designated State Scenic Highway; and

The visual incompatibility of the futuristic style barrier with the historic, somewhat industrial architecture of the existing bridge structure. (Updated VIA page 12)

The DSEIR and Updated VIA, based on application of FHWA analysis methodology, found that scenic vistas (including views from the Cold Spring Canyon Bridge) are not the sole determinant of visual quality along Highway 154. Rather, the varied topography and native vegetation are also important contributors to the high visual quality of the corridor. The following summary description is found on page 11 of the DSEIR and page 10 of the Updated VIA:

The existing visual quality within the project area is high. This view quality is due primarily to the varied topography and native vegetation along the roadsides and adjacent hills. The exaggerated landform, curved road alignment and limited visibility of built elements outside of the roadway corridor also contribute to the existing visual quality. The alternating sweeping vistas of the Santa Ynez Valley and close-in views to the adjacent hillsides provide a dynamic viewing experience for the highway traveler.

The DSEIR and Updated VIA are consistent in their analysis and make an important distinction between views experienced while on the Cold Spring Canyon Bridge, and

views experienced elsewhere on Highway 154. The DSEIR (page 20) and Updated VIA (page 11), state that:

The visual quality evaluation ratings done for the project show that a substantial change in visual resources would occur as a result of the proposed project. Although high-quality views from the highway while not on the bridge would remain mostly intact, the construction of a barrier would have an effect on as much as 70 percent of the existing view as seen specifically from the bridge deck.

This statement of fact describes the visual context of the project. The statement quantifies the adverse effect of the project while on the bridge itself, and also explains the important distinction that as seen from the remainder of Highway 154, existing views would not be affected. This visual distinction is important considering that the project would only affect approximately 0.2 mile (approximately 1/6th of one percent) of the total 32-mile mile length of the Officially Designated State Scenic Highway.

Even though the project would only affect approximately 1/6th of one percent of the total length of the view along the scenic highway, the DSEIR and Updated VIA identify the views from the Cold Spring Canyon Bridge as being of the highest quality. The high visual quality of the views is described as follows:

The Cold Spring Canyon Bridge offers some of the most memorable views along State Route 154 from the highway as well as from Stagecoach Road in the vicinity of the project. (DSEIR page 11; Updated VIA page 10)

The views from the highway include the broad panoramas to the north and the wooded hillsides along the roadway to the south. The high quality of views from the roadway is emphasized by the elevated viewing position the bridge provides. (DSEIR page 11; Updated VIA page 3).

The effects on the high quality views as seen from the Cold Spring Canyon Bridge as disclosed are identified as one of the primary contributors to the Updated VIA and DSEIR finding of Significant and Unavoidable Class I visual impacts (DSEIR pages 21, 25; Updated VIA page 12).

Response to comment #7: Properties that are listed in, or considered eligible for listing in, the National Register of Historical Places are not necessarily highly visible to the public. Archaeological sites, underground missile silos, buildings in remote

areas, and buildings in areas with restricted access are examples of historic properties that are not easily viewed or not viewed by large numbers of people, yet are still eligible properties. Visibility does not in itself confer or augment eligibility. The ease of viewing the Cold Spring Canyon Bridge's substructure, or the number of people who avail themselves of the opportunity to view the substructure, is not a factor in determining the bridge's significance.

Response to comment #8: The individual materials and structural components used in the construction of the Cold Spring Canyon Bridge were not unique; rather, these elements were combined in a particular design, at a particular scale, in a particular architectural engineering aesthetic, in a particular location, and using a particular welding technology in a way that resulted in an exceptionally successful and beautiful bridge form. Caltrans determined that the Cold Spring Canyon Bridge is eligible under National Register Criterion C for its engineering qualities, and not for the views it happens to provide the traveling public. This determination has been accepted by both the State Historic Preservation Officer and the Advisory Council on Historic Preservation that there are no "historic views" from the bridge deck. The design and engineering of the substructure of the bridge are the most important character-defining features of the Cold Spring Canyon Bridge. The design of the deck and railings are character-defining, but are less important than the substructure in conveying the engineering significance of the bridge. The State Historic Preservation Officer concurred with Caltrans' eligibility determinations, including eligibility under National Register Criteria C and Criteria Consideration G, and under the California Register equivalents of these criteria.

Response to comment #9: Caltrans properly made the decision to evaluate the Cold Spring Canyon Bridge when it was only 43 years old because it was obviously a noteworthy structure and because it was central to the proposed project. *National Register Bulletin 15: How to Apply the National Register Criteria for Evaluation*, and *National Register Bulletin: Guidelines for Evaluating and Nominating Properties that Have Achieved Significance Within the Past Fifty Years* both provide guidance on the evaluation of more recent properties. Caltrans determined that the Cold Spring Canyon Bridge possessed the "exceptional importance" necessary to meet the standards for National Register eligibility under Criteria Consideration G, for properties that have achieved significance within the past fifty years. In our eligibility determination, Caltrans stated, "The structure's significance can be viewed with historical perspective because the structure illustrates a defined period of bridge engineering and architecture in California that, while influential to subsequent bridge

engineering and design, reflects the refined development of steel arch bridge technology and the aesthetic of the post-World War II Modern era.”

Response to comment #10: As-built drawings of the Cold Spring Canyon Bridge, which included the standard Type II railings used on the structure, were reviewed by architectural historians for the HRER (and are cited therein). These drawings do not support the assertion that views were part of the “deliberate design” considered in the decision to use the Type II railings. The bridge design sheets show various standard railings including railings that were taller, shorter, and narrower than the Type II barrier railings chosen. They did not choose the shortest or smallest standard railing. As noted on page 15 of the HRER, the bridge engineers chose the railing to fulfill physical and safety requirements of a highway barrier that would successfully be incorporated into the design working with the careful load calculations of the substructure and deck.

Similarly, in an article celebrating the construction of the bridge that appeared in the September-October 1963 issue of the Division of Highways’ journal *California Highways and Public Works* (discussed in the DSEIR Appendix H, Attachment 1, Historical Resources Evaluation Report, p. 15), the bridge engineers mention in one sentence that views were a benefit of where the structure had been built, making no indication that the standard type railings used on the structure were employed to provide improved views. The six-page *California Highways and Public Works* article’s discussion of engineering specifics, particularly about the substructure, and its explanation of the bridge’s purpose to improve safety and travel speeds reflect the focus of the engineers’ design.

Response to comment #11: In the *California Highways and Public Works* article referred to in Response to comment #10, above, the “picturesque setting” is coupled in the article with a declaration of the structure’s “pleasing appearance,” drawing attention to the successful way the structure fits into the canyon — not the wider views of the Santa Ynez Valley from the bridge. As noted on page 14 of the Historical Resources Evaluation Report, steel arches were considered suitable and economic engineering choices for bridges that span steep canyons, as well as being aesthetically pleasing. The final paragraph of the *California Highways and Public Works* article reflects the importance of the bridge’s engineering design and the value the engineers placed on views of the bridge, not from the bridge.

Caltrans' analysis of the bridge's historic integrity of setting presented in the HRER and Finding of Adverse Effect report is based on the explanation of the term in *National Register Bulletin 15: How to Apply the National Register Criteria for Evaluation*, wherein setting is defined as the "physical environment of a historic property" (page 45) (available online at: <http://www.nps.gov/nr/publications/bulletins/nrb15/>).

The way in which the Cold Spring Canyon Bridge is situated in its canyon was noted at the time of the bridge's construction as part of its important aesthetic qualities. Alterations to areas in the environment adjacent to the structure, i.e., its immediate surroundings, have the potential to cause a substantial adverse change to the historical resource. This could occur if a new structure were to obstruct views of the historical resource. As noted in the Finding of Adverse Effect, the project to install the suicide barrier does not impact the bridge's surroundings; the conclusion was accordingly drawn that the project did not diminish the bridge's historic integrity of setting.

Response to comment #12: The mitigation measures presented in the signed Memorandum of Agreement are not intended to fully compensate for the proposed project's adverse impacts to the Cold Spring Canyon Bridge.

In addition, please note that Caltrans provided the federal Advisory Council on Historic Preservation (ACHP) an opportunity to review a draft of the Historic American Engineering Record report and other draft documents stipulated in the Memorandum of Agreement. Charlene Dwin Vaughn, the ACHP Assistant Director for Federal Permitting, Licensing, and Assistance Section, Office of Federal Agency Programs, wrote to Caltrans on October 18, 2010, "The quality of the work exhibited in all three documents is exemplary. These documents create a lasting record of the historic Cold Spring Canyon Bridge as an elegant and beautiful engineering marvel. The booklet and interpretive displays are informative and well-illustrated, and will be a resource for local and state residents. We commend you for a job well done."

As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects the historic integrity of the Cold Spring Canyon Bridge and focuses on the aspects that make the bridge eligible for listing in the National Register. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."



Mercedes Eichholz
<mercedese@cox.net>
01/23/2011 11:42 AM

To <Matt_C_Fowler@dot.ca.gov>
cc 'Marc McGinnes' <mcginnes@es.ucsb.edu>
Subject Cold Springs Bridge

I am writing to try to save our bridge from being mutilated. When I moved to California in 1980 I crossed the bridge for the first time and seeing the valley from there I decided this is where I want to live till I die. Well, I later had to move to town because of my non-profit work, but I still would be there with my four horses, who have since gone to horse heaven. I do know this that when one decides to commit suicide no barrier will stop them. They will find a way and all you do won't help. I was once married to Justice William O. Douglas, the greatest environmentalist in the government and he would fight this barrier with all his might. This barrier is a total waste of taxpayer monies that could be used for something constructive.

Please consider this request. Cordially, Mercedes H. Eichholz

Response to comments from Mercedes H Eichholz

Your opposition to the project has been noted.



Katharine Ellis
<katharine.ellis@gmail.com>
01/23/2011 09:35 PM

To <matt_c_fowler@dot.ca.gov>
cc

Subject Cold Springs Bridge

Dear Mr Fowler,

I am a resident of the San Francisco Bay Area who recently lived in Santa Barbara County for one year. As part of my job for the local chapter of the American Red Cross, I drove almost every week between Santa Maria and Santa Barbara on 154. Every time I crossed Cold Springs Bridge I noticed two things: (1) it affords one of the most stunning natural landscapes in the region, and (2) it would be sickeningly easy to stop the car, get out, and jump. I am a suicide survivor. A classmate and close friend jumped from the Golden Gate Bridge in 2003 and died. For two years after, I volunteered on a suicide hotline and heard from hundreds of callers, many of whom were calling to prevent a suicidal impulse. They wanted to be helped. Suicide is absolutely preventable. While I lived in Santa Barbara, the subject of a Cold Springs barrier came up in conversation with a coworker. With all I had experienced, I couldn't believe how intensely this longtime county resident objected to a barrier of any kind. I didn't realize at the time that he was far from alone in his opinion.

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The objectors must be blissfully ignorant of the complete shock of such a life-changing loss. They probably also believe a lot of the myths about suicide which you have no doubt heard. "People will find another way"? This is simply not true, and literature abounds to show it. (See page 6, "Restricting Access to Methods of Suicide", http://www.glendon.org/content/common/attachments/barriers/restricting_access_to_methods_of_suicide.pdf).

Would a bridge barrier in particular be useful? I'm sure I don't need to tell you that it will. Among many other studies (<http://www.bridgerail.org/still-an-issue/why-the-net-will-work>), I refer you to page 26 of the 2010 Progress Review of the American Society for Suicide Prevention, available here: http://www.sprc.org/library/ChartingTheFuture_Fullbook.pdf. It states, "According to recent research both in the U.S. and worldwide, suicide prevention barriers on bridges have been shown to be effective at reducing suicide in some circumstances . . . Despite the evidence that bridge barriers save lives, the high cost of installation remains a barrier to wider use, as does public objections to their aesthetics."

I understand that a vertical barrier will create infrastructure issues, and arouses aesthetic concerns. These are understandable hurdles. But it is inhumane and unacceptable that this problem has been left alone for so long with a result of no barrier. If a vertical barrier is untenable, please follow the example of the Golden Gate Bridge District. Make a horizontal barrier happen before the next person jumps.

2

Sincerely,

Kate Ellis

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Restricting Access to Methods of Suicide

Rationale and Evaluation of this Approach to Suicide Prevention

Keith Hawton

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Abstract. Restriction of access to means for suicidal behavior, especially dangerous methods, is a key element in most national suicide prevention strategies. In this paper the rationale for this is discussed, including the fact that suicidal impulses are often brief, that availability of a method may influence both the occurrence and outcome of a suicidal act, and that if a favored means becomes less available it does not always result in substitution by another method. Examples of evidence for the effectiveness of restricting availability of suicidal methods on subsequent suicidal behavior are presented, plus the supporting findings from studies of long-term survivors of serious suicide attempts in which only a minority have gone on to die in subsequent suicide attempts. Finally, factors likely to determine the effectiveness of modifying access to means for suicide are considered, together with the main elements that need to be addressed in evaluation.

Keywords: methods of suicide, restriction of access, firearms, gas, paracetamol, evaluation

Introduction

Restricting access to methods that might be used for suicidal acts is a key element in national suicide prevention strategies (e.g., Department of Health, 2002; US Department of Health and Human Services, 2001; United Nations 1996). Why is this? There are several reasons. In this paper the rationale for this approach is reviewed, including evidence supporting it, the principles of where it is likely to be effective are discussed, and approaches to evaluation of this approach are explored.

Rationale

Suicidal behavior can result from many factors, including, for example, psychiatric disorder (especially depression and substance abuse), psychological characteristics such as aggression and impulsivity, life events and problems, exposure to role models in a person's family and social environment and in the media, and genetic and biological factors (Hawton and van Heeringen, 2000). However, at the point at which a person feels hopeless and potentially suicidal, access to specific methods for suicidal behavior can be crucial. Indeed, this may be the key factor that influences translation of suicidal thoughts into an actual suicidal

act. Most importantly, the nature of the method that is available may have a vital influence on the outcome, particularly where an act is impulsive – then the person engaging in suicidal behavior is likely to use the means most easily available to them. If the method has a high risk of being fatal (e.g., use of firearms, self-poisoning with pesticides) then there is a strong risk that the act will result in death, whereas if the method is less likely to be lethal (e.g., certain psychotropic agents) then the act is more likely to result in survival (Eddleston et al., 2005).

Other factors will also influence choice of method. For example, males generally favor more violent methods (e.g., guns or hanging) whereas females tend to favor less violent methods (e.g., overdoses of medication). This pattern, however, will also be influenced by availability. For example, males will often have greater access to certain violent means, whereas females tend to have more access to medication. Also environmental factors will be relevant, such as living in rural areas, where pesticides may, for example, be readily available, or in cities, where there are more likely to be, for example, tall buildings that may be used for jumping (Lin & Lu, 2006). Furthermore, where a method is readily available to members of a society then that method is likely to be seen as more acceptable as a means of suicide. This would apply to firearms in the USA, although availability may not be the sole explanation.

In order to appreciate the potential impact of reducing availability of the means of suicide it is important to be

aware of the concept of periods of suicide risk. For most people who become suicidal the period of real risk is relatively brief, lasting in some individuals for even just a few minutes or a few hours. In others it may last days, but rarely longer. This is very important in understanding the role of altering the availability of methods in prevention, in that if access to a dangerous means of suicide is restricted at times of risk then survival until the end of these periods is more likely.

The nature of psychological processes that may underlie suicidal acts are also relevant to the discussion of the role of availability of means for suicidal behavior. Although not extensively studied, survivors of near-lethal suicide attempts can be a valuable source of such information. De Moore, Plew, Bray, and Snars (1994) conducted such a study in Australia when they investigated 33 mostly young survivors of self-inflicted firearm injuries admitted to a general hospital after surviving their acts. Few of them had major psychiatric disorders, the most common diagnosis being adjustment disorder (i.e., a major stress response to life events or circumstances). Nearly a third abused alcohol. The most frequent reason given by the individuals for the suicide attempts was interpersonal conflict with a partner or family member. There was often a background of longer-term problems. Most of the patients had shared their suicidal ideas with others before their attempts, but only in the period just before the shooting. Thus, there was not usually evidence of long-term suicidal ideas. The actual shooting was relatively impulsive in the vast majority of cases. Most importantly, in all cases firearms were readily available in the home, and availability was usually the reason given for the choice of this method for the act. Thus, an impulsive response to an acute interpersonal crisis and availability of a firearm in the household were key features leading to suicidal acts by shooting. There was a similar finding in an American study of 30 patients treated in a trauma center for life-threatening self-inflicted gunshot wounds (Peterson, Peterson, O'Shanick, & Swann, 1995). Presumably, if a gun had not been available at least some of the individuals in these studies would have made suicide attempts by another method, but these may have been potentially less lethal. Indeed, the psychological characteristics of the self-harming behavior in the samples in these two studies were very similar to those of people who take overdoses in the UK (Hawton & Catalan 1987), in whom the case fatality rate is relatively low (e.g., Gunnell et al., 1997).

There are, however, several important questions to address in considering why restricting access to dangerous means of suicidal behavior is important. The first is whether availability of specific methods of suicidal behavior affects suicide rates. A second question is whether prevention of suicide during periods of risk is effective in the long term, or whether people simply find some other way of committing suicide. Finally, there is the question of whether there is evidence that deliberately restricting availability of a dangerous means of suicide is effective, first, in reducing the use of that method for suicide, and, second, in terms of affecting overall suicide rates.

Does Availability of Method Affect Suicide Rates?

Probably the most convincing example of the effect that availability of a dangerous method of suicide can have on overall suicide rates, and the potential impact of change in the method's availability comes from the UK. This was when the domestic gas supply underwent a gradual change from toxic coal gas to nontoxic natural (North Sea) gas during the late 1950s through the early 1970s. This followed a period of increasing suicide rates after the Second World War. Natural gas was introduced region by region. Kreitman (1976) estimated the mean percentage of carbon monoxide, the toxic agent in coal gas, in the UK throughout this period. The decrease in the carbon monoxide content began in 1958. At the beginning of the period of change-over, suicide by carbon monoxide poisoning (usually by a person putting their head in a gas oven) was the most common method of suicide in the UK, with just under half of all suicides being by this method. As the carbon monoxide content of gas supplies decreased there was a steady reduction in suicide deaths involving carbon monoxide in England and Wales. The decrease paralleled the reduction in carbon monoxide content of gas supplies. While there was a small increase in other methods, the overall net effect was a very large reduction in suicide rates in both genders; the overall suicide rate decreasing by a third. A similar pattern was observed in Scotland. Thus, the loss of many thousands of lives through suicide appears to have been prevented by this single measure.

The coal-gas story strongly suggests that availability of a dangerous method of suicide influences risk of completed suicide. What other evidence is there for this? Here are two examples of studies relating to firearms, in both of which a case-control design was used. In the first study, Brent and colleagues (1991) investigated three groups of individuals. The first group was a consecutive series of 47 adolescent suicides from Western Pennsylvania, 69% of whom had used firearms in their fatal acts. The second group included 47 suicide attempters admitted to psychiatric units, matched for sex and age and also race and country of origin with the members of the first group. None of the individuals in the second group had used guns in their attempts. The third group also consisted of psychiatric inpatients, but these were individuals who had reported never having had suicidal ideas. Similar matching criteria were used in the selection of this group. Within the group of 47 suicides the use of a firearm for suicide was highly correlated with the presence of a firearm in the home. Thus, where firearms were available in the home 29 out of 34 (85.3%) individuals used guns for suicide, whereas in those where a firearm was not available in the home only 1 out of 13 (7.7%) used a firearm for suicide.

Brent and colleagues compared those who died by suicide with each of the two control groups separately regarding firearm availability in the home. Among those dying

by suicide, a firearm was available in the household in 72.3% of cases compared with just 37.0% of the suicide attempters (odds ratio = 4.5, 95% CI 1.9–10.8). Among the control psychiatric inpatients 38.3% had a firearm available in the home (suicides vs. controls: odds ratio = 4.2, 1.8–10.0). These differences were even more marked for handgun availability in households, with 55.3% of suicides having a handgun available in the home compared with 19.6% of the suicide attempters (odds ratio = 5.1, 2.0–12.9) and 17.0% of controls (odds ratio = 6.0, 2.3–15.6). There were some differences between the suicides and the attempters in terms of the proportions with a diagnosis of depression and also the degree of suicidal intent involved in the suicidal act based on scores on the circumstances section of the Beck Suicidal Intent Scale (Beck, Schuyler, & Herman, 1974). Brent and colleagues, therefore, repeated their comparisons regarding presence of firearms in the home after controlling for these two factors. The difference in availability of firearms between the suicides and the attempters was still significant (odds ratio = 2.1, 1.2–3.6). A diagnosis of conduct disorder was somewhat more frequent in the psychiatric controls than in the group of suicides so the analysis regarding firearm availability in the home was repeated controlling for this factor. Again, a significant difference remained (odds ratio = 2.2, 1.4–3.5). The authors of this study concluded that availability of guns in the home appeared to increase the risk of suicide among adolescents.

In the second study, Kellermann and colleagues (1992) also used a case-control design. They compared 438 people who died by suicide in their homes with control individuals from the general population in Shelby County, Tennessee, and King County, Washington. The controls were matched with the suicides for gender, race, and age. After controlling for various other factors that differed between the two groups (living alone, psychotropic medication, having been arrested, substance abuse, and not graduating from high school), those dying by suicide significantly more often had firearms in the home (adjusted odds ratio = 4.8, 2.7–8.5). In their report the authors concluded: "... ready availability of firearms appears to be associated with an increased risk of suicide in the home ... People who own firearms should carefully weigh the reasons for keeping a gun in the home against the possibility that it might someday be used in a suicide."

Is Prevention of Suicide During Periods of Risk Effective in the Long Term?

A crucial question about restricting availability of methods of suicide is whether prevention of suicide during a period of acute risk helps to prevent suicide in the long term, or whether it makes no difference in that a person will seek out and find an effective method in due course (as people sceptical of

suicide prevention efforts might suggest). As noted above in relation to the coal-gas story in the UK, there was little evidence of an immediate compensatory increase in the use of other methods of suicide following the reduction in suicides by carbon monoxide poisoning. Two specific examples will be provided that provide strong evidence that survival of acute periods of risk is effective. These come from follow-up studies of survivors of serious suicide attempts involving methods that result in death for most people who use them.

O'Donnell, Arthur, & Farmer (1994) studied people who jumped in front of underground (subway) trains in London and survived. Surprisingly, a substantial proportion of individuals do survive what is clearly a method with apparent high risk of fatality. This is because there is a fairly deep well between the rails and also because the electric rail has been placed furthest from the platform. However, all surviving jumpers report believing with certainty that jumping would kill them. O'Donnell and colleagues studied a consecutive series of 94 individuals who survived jumping in front of underground trains during the 3-year period 1977–1979. They followed each of them up for at least 10 years and found that seven had died by suicide and two from probable suicide. Therefore, nine out of the 94 persons died by (likely) suicide, a suicide rate of 9.6%. Three of these died by again jumping under subway trains. All the suicides and probable suicides occurred in the first 3 years and 7 months of the follow-up period. There was no evidence of increased risk beyond this time.

The second example comes from a follow-up study of 515 people who were restrained from jumping from the Golden Gate Bridge in San Francisco between 1937 and 1971 (Seiden, 1978). Jumping from the bridge is nearly always fatal. During a follow-up period with a median of 26.7 years, only 25, or 4.9%, of the would-be jumpers died by suicide. Eight of these died by jumping from a bridge, all but one from the Golden Gate Bridge. These suicides usually occurred soon after the episodes in which jumping was prevented. In a comparison group of suicide attempters admitted to San Francisco General Hospital in 1956–1957 who were followed up for 15 years, the suicide rate was 7.1%. Thus, persons who came close to suicide by jumping from the Golden Gate Bridge had a relatively low long-term suicide rate.

It, therefore, appears that the majority of individuals who survive an extremely serious suicide attempt do not go on to die by suicide. Also, such individuals do not necessarily turn to another method.

Factors Likely to Determine Whether Modifying Access to Means for Suicide Will Be Effective

Several factors are likely to influence the effectiveness of a new policy of restriction. One is the popularity of the method

that is targeted. Clearly the more popular the method the more substantial might be the effect in terms of overall rates of suicidal behavior, as illustrated by the coal gas story. Another factor concerns the danger of the method, with restriction of access to more dangerous methods of suicide (e.g., firearms, pesticides, carbon monoxide) likely to have a greater effect on rates of death by suicide than changed availability of less dangerous methods. A further factor concerns the characteristics of the individuals and their behavior involved in use of a particular method. Where acts are characterized by impulsivity and use of a method available in the household then success of a strategy is more likely. A fourth factor is the extent to which the strategy can be implemented. A fifth factor is likely to be the availability of similar methods that might appeal to those who would have used the targeted method. Interestingly, some time after the success of the coal gas story there was subsequently a gradual increase in deaths by carbon monoxide self-poisoning from car exhausts, a trend that was then reversed with the introduction of catalytic converters (Kendell, 1998; Amos, Appleby, & Kiernan, 2001). Finally, the success of a scheme will depend on its being maintained.

Evaluation of Schemes for Changing Access to Methods of Suicide

It is crucial to carefully evaluate any strategy in which change in access to a method of suicidal behavior is introduced. The following are principles that should be applied in the evaluation:

1. The baseline period for which data are obtained before the change in access should be quite long, preferably several years. This will provide greater sensitivity to detecting change and also help identify underlying trends that will need to be taken into account in any analyses.
2. The period of postchange evaluation should also be quite long, say at least 2 or 3 years. Shorter periods may not enable a change in rate of suicide to be detected.
3. The implementation of the strategy should be assessed, i.e., the extent to which it is being applied and maintained.
4. The evaluation should include examining data on:
 - a. Suicidal behavior with the method that is being targeted; and
 - b. Use of other methods of suicidal behavior. This preferably includes both fatal and nonfatal acts. This will allow not only the impact of the strategy on the specifically targeted method to be assessed, but also whether there is a substitution of method (and perhaps a less dangerous one).
5. In the statistical analyses, account should be taken of underlying trends, both in overall suicide rates prior to the initiative and in use of specific methods of suicidal behavior.

The following is a description of the background, rationale, implementation, and evaluation of a specific example of changing access to means, namely analgesics used for self-poisoning. Between the 1970s and 1990s, rates of both fatal and nonfatal self-poisoning with analgesics sold over the counter, especially paracetamol (acetaminophen), increased dramatically in the UK. By the mid-1990s, between 35% and 50% of all overdose presentations to general hospitals involved paracetamol (Bialas et al., 1996; Hawton, Fagg, Simkin, Bale, & Bond, 1997), the majority being in very young people. Approximately 220–250 deaths were the result of paracetamol poisoning, mainly the result of liver necrosis (O'Grady, 1999). Interviews with patients who had taken nonfatal paracetamol overdoses showed that many had used this method of overdose because of the ready availability of the medication in the household, most acting on impulse (Hawton et al., 1995). Interestingly, many patients thought (incorrectly) they would become unconscious as an immediate result of the overdose. Several potential approaches to prevention were considered (Hawton, 2002). Reduction in pack sizes, with strict enforcement of the maximum amount of tablets that could be bought per purchase, was chosen as the primary approach, with additional changes to warnings on packs about the danger of overdose. These changes were introduced as legislation in September 1998. The legislation included aspirin as well as paracetamol because of concerns about possible substitution by aspirin overdose, which is also dangerous.

During the first year following the legislation there were significant changes in sales data, showing that pack size had been reduced and for paracetamol this had been compensated for by increased numbers of packs being bought. The net effect of this would have been that reduced amounts would have been available in households most of the time (Hawton, 2002). There were significant reductions in large overdoses of both paracetamol and aspirin, markedly fewer liver transplants caused by paracetamol overdose, and, most importantly, a sizeable reduction in deaths from both paracetamol and aspirin overdoses (Hawton et al., 2001). Further evaluation of the impact of the legislation has shown that these changes have largely persisted, and with only limited evidence of substitution of methods involving another over-the-counter analgesic, ibuprofen, but without this itself causing deaths (Hawton et al., 2004).

The impact of this measure has clearly been substantial. It has been estimated that it may have resulted in the prevention of approximately 200 deaths from analgesic poisoning during the 3 years after the legislation was introduced (Hawton et al., 2004).

Conclusions

Acute suicide risk is generally brief. Suicidal behavior, including actual suicides as well as attempts, is often impulsive. Most survivors of potentially lethal suicide attempts

do not appear to have a very high long-term risk of suicide. There is clear evidence that availability of method influences method choice for suicide. Also changes in the availability of popular methods of suicide, as shown in the change from coal gas to natural gas in the UK, can have substantial effects on suicides rates.

Another implication of the evidence presented in this contribution is that it is essential that access to dangerous methods of suicidal behavior is an integral part of clinical assessment of depressed or suicidal individuals. Dangerous methods should, as far as possible, be removed from the homes of those at risk. There should also be a public education policy regarding the risks associated with having dangerous methods of suicide available in the home. Such a policy should, however, be designed with care, particularly with regard to the potentially negative impact that the provision of such information can have on those who are already suicidal. Finally, serious efforts should be made to substantially reduce the availability of specific methods of suicide, especially from households.

It is quite clear that tackling availability of specific methods of suicide in individual countries must be an important element in national suicide prevention strategies. While not addressing the underlying causes of suicide, it is a policy that may have significant impact on overall suicide rates.

Acknowledgments

Some of the content of this paper is based, by permission of Oxford University, on a chapter by the author: "Restriction of access to methods of suicide as a means of suicide prevention" in *Prevention and Treatment of Suicidal Behaviour: from Science to Practice*, edited by Hawton, K. (2005). Oxford: Oxford University Press.

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About the author

Keith Hawton is Professor of Psychiatry and Director of the Centre for Suicide Research at Oxford University Department of Psychiatry, and Consultant Psychiatrist at the Warneford Hospital in Oxford. For 30 years he and his research group have been conducting investigations concerning the causes, treatment, prevention and outcome of suicidal behavior.

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Why the Net Will Work



There are several reasons why the safety net on the Golden Gate Bridge will stop the jumping there and save lives. These are related both to the design of the net, the nature of suicide at the Bridge, and the nature of suicide in general.

Critics of the net—and most any other proposal—raise a number of objections. At the core of this criticism is a belief that once someone decides to commit suicide, they cannot be stopped. Surely, if that were the case, someone who attempts suicide once and survives would simply try again. The truth is, such persons are at high risk for

suicide and some die in follow-up attempts. Yet nationally, 90 percent of all people who survive a first suicide attempt do not subsequently die by suicide.

Given time and treatment, a person in a suicidal state can get past the impulse to die, and get on with life. And that's exactly what has happened with people who survived an attempt at the Gate. Cornelia Van Ireland, the first to jump from the Bridge and

survive (in 1940), went on to live out her life. Ken Baldwin jumped in the early '80's and has had a long career as a schoolteacher since. Kevin Hines, whose story is detailed here, recovered from his injuries, got married and is an active mental health advocate. Like the national data, about 90 percent of people who survive a jump from the Golden Gate Bridge do not later die by suicide. And a study of people stopped during a Bridge suicide attempt found that 94 percent were still alive—for an average of 26 years—or had died of natural causes.

People in a suicidal state can get past it, get the help they need and survive. And the chance of that survival has a great deal to do with ease of access to lethal means. Sometimes the simplest changes—like packaging drugs in single dose containers rather than bulk packs—can decrease suicides. Proper use of gun-locks has reduced suicides in gun-owning households. Locking up poisonous pesticides cuts down suicides. The list goes on.

The bridge experience, as reported in Maine, Washington D.C., Canada, the U.K., Australia and elsewhere is the same. Restrict easy access and lives are saved. In none of these examples is there any evidence of people simply using another bridge once a known suicide bridge has a proper railing or net. In the D.C. example, the next nearest bridge is only blocks away.

The bottom line is really pretty simple—every example of restricting easy access to lethal means is associated with saving lives.

This leaves only questions about the Golden Gate Bridge proposal itself—will the net actually work? Certainly, the international experience tells us yes. The Swiss system, on which the Golden Gate Bridge proposal is based, has stopped suicide jumps so effectively that no one has even tried the system. The Golden Gate Bridge net—like the system in Bern—will hang some 20 feet below the pedestrian deck. A jumper would fall two stories before hitting the net, likely causing some serious injury. And the natural sag of the net—with an injured individual in it—will make climbing out very difficult. In addition, the plan includes a rescue service to pull any injured individual from the net. The available evidence tells us the net will work.

Next Steps

With the determination that the net will have no significant impact on the environment, and funding to complete the design work — only one major hurdle remains before we will see the suicides at the bridge stopped — the \$50 million needed to build the project.

Providing the information and background needed to secure these funds is the work of the Bridge Rail Foundation. To request information on how you can help - Please contact us via email - info@bridgerail.org - or through our online form.

Contact Us

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3020 Bridgeway #179
Sausalito, CA 94965

Connect with us at:



Also attached: “Charting the Future of Suicide Prevention.” A copy of this booklet is included with the comment letter from Dayna Whitmer.

Response to comments from Kate Ellis

Response to comment #1: Your support for the vertical barrier alternative is noted.

Response to comment #2: The horizontal safety net alternative was evaluated in the DEIR, FEIR, and Supplemental EIR, however it was not considered feasible on the Cold Spring Canyon Bridge. The safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Laurence Hauben
<laurencehauben@gmail.com>
>

01/02/2011 07:45 PM

To <Matt_C_Fowler@dot.ca.gov>

cc Marc McGinnes <bridgefriends@hotmail.com>

Subject Re; cold Spring Bridge Barrier should respect the beauty and history of the site

Dear Mr. Fowler:

As a long time resident of Santa Barbara, I feel very strongly that the beauty of the views from historic Cold Springs bridge must be respected when implementing suicide prevention barriers.

I have lived in Santa Barbara since 1983, and even though I have driven over San Marcos Pass hundreds of times, the views from Cold Springs Bridge remain as stunning as the first time I gazed upon their majesty.

Ugly cage-like vertical mesh would deface this magnificent view, lowering the quality of life for local residents, and are simply unthinkable.

Horizontal barriers, combined with raising the railing to meet current standards would accomplish the goal of suicide deterrence while respecting the historic significance of this unique site.

I hope that Caltrans will respect the wishes of our community and implement a suicide prevention barrier that does not ruin our views.

Thank you and best regards,

Laurence Hauben
814 San Roque Road
(805) 259-7229
LaurenceHauben@gmail.com

Response to comments from Laurence Hauben

Your opposition to the project has been noted.

The DSEIR determined that views from the Cold Spring Canyon Bridge are of the highest visual quality (DSEIR, page 11). As a result, the DSEIR requires that measures be implemented to protect those views to the greatest extent feasible. Efforts to minimize adverse visual effects included the formation of an Aesthetic Design Advisory Committee (ADAC) for the purpose of making recommendations regarding the appearance of the barrier, and to lessen the project's adverse visual effects. The committee was composed of Caltrans staff and local community members, including a representative of the Santa Barbara County Historic Landmarks Advisory Commission, architects, landscape architects, and County Public Works and Planning staff. Aesthetic Design Committee recommendations are incorporated into the minimization/mitigation measures for the project (DSEIR, pages 53 and 54).

The DSEIR also identifies significant visual impacts resulting from the project due in part to its affect on views specifically from the Cold Spring Canyon Bridge. Scenic vistas (including views from the Cold Spring Canyon Bridge), however, are not the sole determinant of visual quality along Highway 154 nor its Scenic Highway designation. The varied topography and native vegetation are also important contributors to the high visual quality of the corridor. The State Scenic Highway designation for Highway 154 extends along the route's entire 32-mile length. The project would only affect approximately 0.2 mile (approximately 1/6th of one percent) of the total 32-mile mile length of the Officially Designated State Scenic Highway.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of

the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

The current railing along the bridge is considered standard for the construction period of the structure. The suggestion of raising the existing bridge railings in combination with constructing a horizontal net barrier would likely cause an additional adverse effect by introducing a visual element that diminishes the property's historic integrity

of design, feeling, and association. Increasing the height of the bridge rail would also not comply with the Secretary of Interior's Standards for Rehabilitation, Standards 2 and 9. Thus, inclusion of this aspect of the suggested alternative would further decrease the ability of the horizontal net barrier alternative to reduce impacts to historic resources.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to "Reduce the exposure to risks for emergency personnel..." In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff's office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.



Steve Walker
<swalker@linkline.com>
12/16/2010 10:03 AM

To <matt_fowler@dot.ca.gov>
cc
Subject Fw: Hwy. 154 bridge

----- Original Message -----

From: [Steve Hausz](#)
To: matt_fowler@dot.ca.gov
Sent: Thursday, December 16, 2010 9:57 AM
Subject: Hwy. 154 bridge

Please add my comment in opposition to the barrier.

Lives, regrettably, are lost in so many ways. If one looks at all the ways one could meet their demise, where Caltrans has influence over the conditions and circumstances, surely there are many other opportunities to pursue projects with a much higher ratio of lives saved per dollar spent. While suicide leaves any normal person with a feeling of helplessness and evokes a strong emotional response, I feel it is important to be pragmatic and not base funding decisions on emotional justifications.

Sincerely,

--

[Steve Hausz, Architect](#)

829 De La Vina Street
Suite 300
Santa Barbara, CA 93101-3295

Phone 805-962-7478
Cell 805-886-2513
Fax 805-899-2900

Response to comments from Steve Hausz, Architect

Your opposition to the project has been noted.

Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties). The money needed to construct the barriers will come from Local Recovery Act funds. The Recovery Act funding has been designated specifically for this project and cannot be used for any other project.



Amy Hermann
<amybrookehermann@gmail.com>

01/24/2011 02:53 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject I oppose suicide barrier on Cold Springs Bridge

To Whom It May Concern:

I commute from Solvang to Santa Barbara four days a week for work, via 154.

I oppose the construction of a suicide barrier on Cold Springs Bridge for several reasons:

1. If someone is determined to kill him or herself, he or she will find a way. Constructing this barrier will only change the way someone kills themselves, not prevent suicide... therefore this project is a wasteful use of money and resources.

2. The bridge is a beautiful landmark. Please leave it as it is - it is a work of art and offers stunning views. A suicide barrier will not blend in with the architecture of the bridge and the natural beauty of the area. It will be an EYE-SORE.

Thank you for your consideration,

Amy Hermann

Response to comments from Amy Herman

Your opposition to the project has been noted.

Response to comment #1: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #2: Comment noted.



John <imjohnm@gmail.com>

01/02/2011 08:05 PM

To <Matt_C_Fowler@dot.ca.gov>,
<bridgefriends@hotmail.com>

cc

Subject Bridge meeting

Hi Matt,

I hope at the meeting Jan 5, you can offer valid reasons to ignore building a similar invisible horizontal barrier as scrutinized and approved by so many experts on the Golden Gate Bridge.

John
Santa Barbara



John <imjohnm@gmail.com>

01/03/2011 08:07 AM

To <Matt_C_Fowler@dot.ca.gov>,
<bridgefriends@hotmail.com>

cc

Subject Bridge

Hi Matt,

>

> I hope at the meeting Jan 5, you can offer valid
> reasons to ignore building a similar invisible
> horizontal barrier as scrutinized and approved by
> so many experts on the Golden Gate Bridge.

>

> John McCauley
2700 Exeter pl
> Santa Barbara 93105

Response to comment letters from John McCauley

Your comment has been noted.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Golden Gate Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and

Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting (physical modifications) the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The

Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Daniel Jost
<djost.1983@gmail.com>
01/13/2011 08:21 PM

To <Matt_Fowler@dot.ca.gov>
cc
Subject Public Comment on Suicide Barrier for Cold Spring Canyon Bridge

Dear Matt,

I would like to submit the following comment regarding the suicide barrier at the Cold Spring Canyon Bridge

This July, a new study came out in the British Medical Journal, suggesting that suicide barriers do not actually have any effect on the rate of jumping suicide within the surrounding area. They do not save lives. Instead, they merely stop people from jumping at a specific location. You can see the text of the report online at <http://www.bmj.com/content/341/bmj.c2884.full>. Given this new information, and the weakness of the previous evidence supporting suicide barriers, which the BMJ article explains, I would encourage you to cancel plans for building a barrier on the Cold Spring Canyon Bridge.

1

While it is essential that we do everything within our means to help those in need, wasting thousands of dollars in taxpayer money on an ineffective design that will take away a cherished view would be a tragedy. If the government insists on wasting its money, the least it can do is waste it in a way that preserves the existing beauty of the environment and build a horizontal net barrier that will not interrupt views. If a horizontal barrier is not possible, no barrier should be built.

2

Daniel Jost

Effect of a barrier at Bloor Street Viaduct on suicide rates in Toronto: natural experiment

Mark Sinyor, resident physician,^{1,2} Anthony J Levitt, psychiatrist in chief²

¹Department of Psychiatry, University of Toronto, ON, Canada

²Department of Psychiatry, Sunnybrook Health Sciences Centre and Women's College Hospital, 2075 Bayview Avenue, Toronto, ON, Canada M4N 3M5

Correspondence to: M Sinyor mark.sinyor@utoronto.ca

Cite this as: *BMJ* 2010;341:c2884 doi:10.1136/bmj.c2884

ABSTRACT

Objective To determine whether rates of suicide changed in Toronto after a barrier was erected at Bloor Street Viaduct, the bridge with the world's second highest annual rate of suicide by jumping after Golden Gate Bridge in San Francisco.

Design Natural experiment.

Setting City of Toronto and province of Ontario, Canada; records at the chief coroner's office of Ontario 1993-2001 (nine years before the barrier) and July 2003-June 2007 (four years after the barrier).

Participants 14 789 people who completed suicide in the city of Toronto and in Ontario.

Main outcome measure Changes in yearly rates of suicide by jumping at Bloor Street Viaduct, other bridges, and buildings, and by other means.

Results Yearly rates of suicide by jumping in Toronto remained unchanged between the periods before and after the construction of a barrier at Bloor Street Viaduct (56.4 v 56.6, $P=0.95$). A mean of 9.3 suicides occurred annually at Bloor Street Viaduct before the barrier and none after the barrier ($P<0.01$). Yearly rates of suicide by jumping from other bridges and buildings were higher in the period after the barrier although only significant for other bridges (other bridges: 8.7 v 14.2, $P=0.01$; buildings: 38.5 v 42.7, $P=0.32$).

Conclusions Although the barrier prevented suicides at Bloor Street Viaduct, the rate of suicide by jumping in Toronto remained unchanged. This lack of change might have been due to a reciprocal increase in suicides from other bridges and buildings. This finding suggests that Bloor Street Viaduct may not have been a uniquely attractive location for suicide and that barriers on bridges may not alter absolute rates of suicide by jumping when comparable bridges are nearby.

INTRODUCTION

It is well recognised that restricting access to a means of suicide may delay or even prevent suicide among vulnerable people.¹ This principle has been shown successfully in the United Kingdom with the implementation of relatively simple strategies such as switching to carbon monoxide-free sources of gas,² restricting pack sizes of paracetamol (acetaminophen) and salicylates,^{3,4} and fitting cars with catalytic converters.⁵ In both Canada and New Zealand firearm

related suicides decreased after the introduction of legislation for gun control,^{6,7} although some evidence suggests that these reductions were matched by increases in suicides by other means, such as jumping.^{8,9}

Barriers to prevent jumping have been established at the Empire State Building, the Eiffel Tower, and several bridges worldwide.^{10,11} Recent arguments in favour of barriers on bridges used for suicide stem from studies in the 1970s that assessed the survivors of suicide attempts at Golden Gate Bridge in San Francisco, the bridge with the world's highest annual rate of suicide by jumping.¹⁰ In one study, four of six survivors said that they would not have attempted suicide at any location other than Golden Gate Bridge and all six favoured the construction of a barrier at the bridge.¹² In another study, only 6% of 515 people who had been prevented from jumping off Golden Gate Bridge had subsequently completed suicide.¹³ Despite this evidence, 74% of respondents to a US telephone survey believed that most or all people prevented from jumping off Golden Gate Bridge by a barrier would find another way to complete suicide.¹⁴ Studies examining the introduction of suicide barriers at Memorial Bridge in Augusta, Maine¹⁵ and Clifton Suspension Bridge in Bristol, England¹⁶ as well as the introduction of a safety net at Muenster Terrace in Bern, Switzerland¹⁷ showed reductions in mean numbers of suicides of 0.6, 4.2, and 2.5 persons per year respectively at each location. Each article examined the change in rates of suicides by jumping from nearby bridges or buildings and concluded that little, if any, substitution of location occurred. However, these studies lacked statistical power because of the relatively small yearly decreases in numbers of suicides at each bridge as well as low rates of suicide in general. No study of a suicide prevention barrier has shown a statistically significant drop in overall rates of suicide in the vicinity.

For more than a decade it has been debated whether a barrier at Bloor Street Viaduct would be effective at preventing suicides in Toronto, Canada. Since the construction of the viaduct in downtown Toronto in 1918, at least 400 people have jumped to their deaths from the bridge.¹⁸ The 40 m high viaduct spans two major roads, is 490 m long, is double decked, and has an arched design with five lanes of traffic above a subway.¹⁹ With about 10 suicides annually from 1993

to 2002 (baseline data from this study), Bloor Street Viaduct had the dubious distinction of being the second most popular bridge for suicide studied in the world after Golden Gate Bridge.¹⁰ The barrier at Bloor Street Viaduct, named the “luminous veil,” was constructed between April 2002 and June 2003. The barrier is about 5 m high and consists of thousands of thin steel rods spaced closely together and supported externally by an angled steel frame.²⁰ It is not known whether the barrier has had any impact on Toronto’s overall rate of suicides and on the rate of suicides by jumping. We examined coroner’s data before and after the construction of the barrier, to determine if suicide rates had changed and whether or not people substituted Bloor Street Viaduct for different locations or means of suicide.

METHODS

We examined records at the chief coroner’s office of Ontario covering all suicides in Ontario from 1 January 1993 to 30 June 2007. To be included in the data collection the death had to be ruled a suicide by the coroner’s office according to the standard of a high degree of probability. Given the large number of charts, it was not possible to examine deaths classified under different categories such as accident, homicide, or undetermined cause of death. Staff at the coroner’s office provided a spreadsheet listing all the cases coded as suicides in Ontario for each year in the study. As it takes about two years for a case to be closed, complete data for 2007 were available only in 2009. The following information was included in the spreadsheet: date of suicide, age, sex, region, municipality, and cause of death, such as a fall or jump from a height, hanging, or shooting. We grouped the suicides into four categories: all suicides in Ontario (excluding Toronto), all suicides in Toronto, suicides in Toronto by jumping (where jumping implied from a height, therefore people who jumped in subways were excluded), and suicides in Toronto by means other than jumping. To determine whether the suicide was associated with a bridge or building we examined the charts for all suicides in Toronto coded as a fall or jump from a height. We also obtained the name and location of the bridge associated with the suicide.

The barrier at Bloor Street Viaduct was under construction from April 2002 to June 2003 (correspondence between chief coroner’s office and Mike Laidlaw, one of the engineers of the barrier). Accordingly we classified the nine years from 1993 to 2001 as being before the barrier and the four years from 1 July 2003 to 30 June 2007 as being after the barrier. To determine if those who jumped resided in Toronto or had travelled from outside the city, we obtained the postal codes of home residences for 1999-2001 and for 1 July 2003 to 30 June 2007.

The population of Ontario and Toronto was obtained from census data held by Statistics Canada for the years 1996, 2001, and 2006.²¹ We used these data to correct suicide rates for population over time. Linear population growth was assumed for the periods

1996-2001 and 2001-6. We estimated population growth by extrapolating backwards from 1993-6 and forwards from 2006 to June 2007.

Statistical analysis

To examine differences between suicide rates before and after the barrier we carried out Poisson regression analyses. We analysed demographic data using two tailed, independent sample *t* tests for continuous variables and two sided χ^2 tests for categorical variables. Data on postal codes were analysed using a one sided χ^2 test under the assumption that more people would travel from outside the city to complete suicide at Bloor Street Viaduct (before the barrier) than at other bridges in the period after the barrier. We considered a P value less than 0.05 to be statistically significant.

RESULTS

Annual rates of suicide by jumping in Toronto remained unchanged before and after the construction of the barrier at Bloor Street Viaduct (56.4 v 56.6, P=0.95; table 1, fig 1). A mean of 9.3 suicides took place annually before the barrier and none after the barrier (P<0.01; fig 2). After the barrier the annual rate of suicides by jumping from bridges other than Bloor Street Viaduct increased significantly (8.7 v 14.2, P=0.01) and from buildings increased non-significantly (38.5 v 42.7, P=0.32). In 2002, the year when construction of the barrier commenced (but was not complete), 63 suicides by jumping occurred in Toronto of which 30 were from bridges and 19 from the Bloor Street Viaduct.

Both the overall rate of suicides in Toronto and the rate of suicides by means other than jumping decreased by 28 suicides per year in the period after the barrier (all Toronto suicides per year: 253.4 v 225.4, P=0.05; Toronto suicides per year by other means: 197.0 v 168.8, P=0.04). The decrease in overall rate of suicide in Toronto bordered on statistical significance whereas the decrease in the rate of suicides by other means was significant. The overall annual rate of suicides in Ontario (excluding Toronto) also decreased significantly in the period after the barrier (836.4 v 752.5, P=0.01).

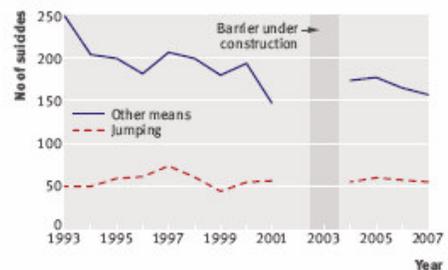


Fig 1 | Suicides in Toronto by jumping or other means before (1993-2001) and after (July 2003-June 2007) construction of a suicide prevention barrier at Bloor Street Viaduct: corrected per capita to suicides in 1993 population (not standardised for age)

Table 1 | Poisson regression analysis of annual suicide rates by jumping and other means in Ontario and Toronto before (1993-2001) and after (July 2003-June 2007) construction of a suicide prevention barrier at Bloor Street Viaduct, Toronto

Subgroup	Mean No of annual suicides pre-barrier		Mean No of annual suicides post-barrier		Regression coefficient	Standard error	P value	Incidence rate ratio (95% CI)
	Observed	Corrected*	Observed	Corrected*				
Ontario (excluding Toronto)	880.1	836.4	887.5	752.5	-0.11	0.04	0.01	0.90 (0.83 to 0.98)
Toronto (total)	261.2	253.4	241.8	225.4	-0.12	0.06	0.05	0.89 (0.79 to 1.00)
Suicide method								
Jumping	58.2	56.4	60.8	56.6	0.00	0.08	0.95	1.00 (0.87 to 1.17)
Location of jump:								
Building	39.7	38.5	45.8	42.7	0.10	0.10	0.32	1.11 (0.90 to 1.36)
Bridge	18.6	17.9	15.3	14.2	-0.23	0.19	0.22	0.79 (0.55 to 1.15)
Bloor Street Viaduct	9.6	9.3	0	0	-2.92	0.89	<0.01	0.05 (0.01 to 0.31)
Other bridges	9.0	8.7	15.3	14.2	0.49	0.19	0.01	1.64 (1.13 to 2.39)
Other means	203.0	197.0	180.8	168.8	-0.15	0.08	0.04	0.86 (0.74 to 0.99)

*Corrected per capita to suicides in 1993 population; not standardised for age.

†Incidence rate ratio of suicides after barrier compared with before barrier; df=11.

Demographics

People in Toronto who used means other than jumping to complete suicide after the barrier were older than those who completed suicide by other means before the barrier (48.0 years *v* 45.8 years, $P < 0.01$; table 2). Overall in Toronto no other statistically significant differences in age or sex were evident between the periods before and after the barrier. People who completed suicide by jumping in Toronto tended to be younger than those who did so by other means both before and after the barrier. Furthermore, among people who completed suicide by jumping, those who used bridges tended to be younger and were more predominantly male than those who used buildings.

Of the 57 people who completed suicide by jumping from Toronto bridges from 1999-2001 (before the barrier), only two were known to live outside the city and both jumped at Bloor Street Viaduct (table 3). Of the 61 people who completed suicide by jumping from Toronto bridges from July 2003 to June 2007 (after the barrier), nine lived outside the city. More people travelled from outside the city to jump from bridges after the barrier than before the barrier ($P = 0.049$).

DISCUSSION

The “luminous veil” was constructed at Bloor Street Viaduct in Toronto to prevent suicides. To be fully successful, it needed to prevent suicides at Bloor Street Viaduct with no reciprocal increases in suicides by jumping at other locations or by other means. The barrier did accomplish the first part of this goal with no suicides occurring at Bloor Street Viaduct during the study period July 2003-June 2007, after the barrier’s construction. Something about the barrier’s architectural design, its aesthetic quality, or the publicity surrounding its construction was sufficient to dissuade people from considering suicide at that location. This result is in keeping with previous work showing that barriers help to prevent suicides at the location where they are placed.^{15-18,22,23} The overall rate of suicide in Ontario decreased significantly in the period after the barrier’s construction. A similar trend was observed in

Toronto, with a decrease in the overall suicide rate that bordered on significance. This decrease in Toronto’s overall suicide rate by 28/year was accompanied by a statistically significant decrease in the same number of suicides per year by means other than jumping. No reduction occurred in the annual suicide rate by jumping in Toronto. Indeed, annual suicides from other bridges in Toronto showed a statistically significant increase, by 5.5 after the barrier (a 63% increase from the annual rate of 8.7 before the barrier). When this figure is compared with the 9.3 fewer annual suicides at Bloor Street Viaduct after the barrier, it might be speculated that most people who would have jumped at Bloor Street Viaduct chose other bridges instead. Increases in suicides by jumping from buildings may account for the remainder, although these increases did not reach statistical significance, perhaps because of the small numbers involved.

There are several possible explanations for why rates of suicide by jumping did not decrease in Toronto after the barrier was erected. One is that suicide barriers on bridges are not effective in decreasing overall suicide rates because people may substitute a bridge with a barrier for a different location, such as another

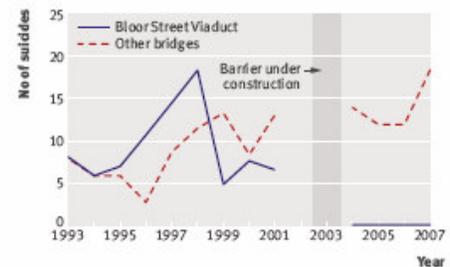


Fig 2 | Suicides in Toronto by jumping from Bloor Street Viaduct and other bridges before (1993-2001) and after (July 2003-June 2007) construction of a suicide prevention barrier at Bloor Street Viaduct: corrected per capita to suicides in 1993 population (not standardised for age)

bridge or a building. No study of a suicide barrier has shown a statistically significant drop in overall suicide rates in the vicinity. The removal of a suicide barrier on Grafton Bridge in Auckland, New Zealand was associated with a significant increase in suicides.²² However, suicides by jumping from other locations decreased by the same number, resulting in an unchanged total rate of suicide by jumping.²² That barriers on bridges would be effective was suggested by studies carried out in San Francisco; however, these studies were of people who had either jumped from Golden Gate Bridge and survived²² or contemplated suicide or made a suicidal gesture at the bridge that came to the attention of the police or were brought to a hospital.²³ These two groups may be qualitatively different from people who have been prevented from jumping at a location as a result of a physical barrier. Although some have argued that barriers on bridges are effective at preventing suicide,²⁴ the evidence in the literature in favour of such barriers when there are other available buildings or bridges is weak, in part due to the scarcity of bridges with pre-barrier suicide rates of sufficient magnitude to make statistical calculations plausible.

A second explanation for the effect observed here is that barriers decrease rates in some instances but that in Toronto specific circumstances led to the barrier failing to decrease overall suicide rates by jumping. The argument for putting a barrier on a notorious suicide bridge as a prevention tool is based on the assumption that people contemplating suicide have a preference for that bridge over others in the area. "Suicide magnet" may be a particularly apt term and has been used to describe suicide bridges in the sense that magnets have the ability to exert different amounts of pull and, presumably, the more pull a magnet exerts the less interchangeable it is with other locations. The evidence presented here shows that despite being the second most frequently used bridge for suicide, Bloor Street Viaduct was a relatively weak magnet. Factors that might make a bridge a stronger "magnet" are ease of pedestrian access, perceived lethality of a jump, and unique geographical features such as being over water.

Bloor Street Viaduct is easily accessed, jumps are highly lethal, and it is constructed over two major roads. However, numerous other bridges in Toronto fit this description. Other considerations are the bridge's aesthetic quality and that of the surrounding environment. Although Bloor Street Viaduct might have been viewed as an impressive structure when it was completed in 1918, it is not aesthetically grand by 21st century standards and nearby bridges are of similar scale and afford a similar view. A final consideration is the notoriety of the bridge. Although Bloor Street Viaduct is sufficiently notorious to have become a minor pop culture reference, notably in a song by the Barenaked Ladies²⁵ and a novel by Michael Ondaatje,²⁶ it is not a cultural icon like Golden Gate Bridge. Images and news about Bloor Street Viaduct are not ubiquitous in Toronto compared with Golden Gate Bridge. A survivor of a suicide attempt in San Francisco noted that for him "it was the Golden Gate Bridge or nothing,"²⁷ but whether anyone would make such a bold statement about Bloor Street Viaduct is debatable. One indicator of whether a bridge might hold a degree of importance for people contemplating suicide is if they choose to travel large distances to jump at that location. For example, when Gateway Bridge was opened in Brisbane, Australia, it became a "suicide magnet" after a well publicised suicide at the bridge's opening ceremony. Notably, 100% of people who died by jumping at that location had travelled there from outside the city compared with only 38% of people who jumped from a nearby bridge.²⁷ This pattern was not, however, observed with Bloor Street Viaduct. Indeed, postal code data show that significantly more people travelled from outside the city to jump from other bridges in the period after the barrier than to jump from Bloor Street Viaduct in the period before the barrier ($P=0.049$).

A third explanation is that the barrier at Bloor Street Viaduct could have decreased rates of suicide by jumping under different circumstances. It is important to note that the barrier was a standalone intervention. It has been argued that optimal suicide prevention programmes involve comprehensive strategies to provide

Table 2 | Demographic characteristics of people who completed suicide by jumping or other means in Toronto before (1993-2001) and after (July 2003-June 2007) the construction of a barrier at Bloor Street Viaduct

Characteristics	Location of suicides by jumping			Suicide by means other than jumping: D	Statistical comparison*
	Building : A	Bloor Street Viaduct: B	Other bridges: C		
Mean (SD) age (years):					
Before barrier	43.5 (1.8)	38.1 (12.5)	38.6 (15.6)	45.8 (17.4)	D>A>B>C
After barrier	45.8 (17.8)	—	38.5 (13.6)	48.0 (17.2)	D>A>C
Before versus after barrier*	NS	—	NS	Significant	—
No (%) of males:					
Before barrier	357 (59.4)	86 (79.3)	81 (78.5)	1827 (70.7)	B=O>D>A
After barrier	183 (64.5)	—	61 (72.1)	723 (70.1)	C>D>A
Before versus after barrier*	NS	—	NS	NS	—

NS=not significant.
*Significant if $P<0.05$.

Table 3 | Location of last known residence of people who died by jumping from Toronto bridges before (1993-2001) and after (July 2003-June 2007)*† construction of a suicide prevention barrier at Bloor Street Viaduct

Location of residence	No (%) completing suicide by jumping before the barrier (n=57)		No (%) completing suicide by jumping from other bridges after the barrier (n=61)
	Bloor Street Viaduct	Other bridges	
Toronto	17 (29.8)	23 (40.4)	41 (67.2)
Suburbs or beyond	2 (3.5)	0 (0)	9 (14.8)
No fixed address/unknown	4 (7.0)	11 (19.3)	11 (18.0)

*Postal codes were available for this analysis only from 1999.

†More people travelled from outside the city in the period after the barrier: $\chi^2=3.8$, $df=1$, $P=0.049$.

education, combat stigma, and improve accessibility of services to people contemplating suicide.²⁸ It is unclear whether a different result would have been observed if the barrier at Bloor Street Viaduct had been part of a more comprehensive suicide prevention programme. Furthermore, at least one prominent newspaper article published shortly after the barrier's construction speculated that it failed to prevent suicides because people were jumping at other locations.²⁹ In September 2003, the article reported that someone who had contemplated suicide at Bloor Street Viaduct subsequently jumped from a nearby bridge on discovery of the barrier. While results of the present study would seem to agree with the article's assertion, it is possible that the article itself may have influenced people contemplating suicide to consider other bridges. Moreover, it could have contributed to a widespread public belief in the inevitability of suicide, which may have further dissuaded suicidal people from seeking help. The article and other media reports on suicide may have influenced suicide rates and could have contributed to the observation that rates of suicide by jumping did not change in the period after the barrier. Interestingly, during the study 1998 and 2002 were the years with the highest number of suicides at Bloor Street Viaduct (n=19); respectively, the year of the coroner's inquest into suicide at Bloor Street Viaduct and the year the barrier was constructed. This suggests that, at least in those instances, publicity may have influenced patterns of suicide by jumping in Toronto.

Whether the barrier at Bloor Street Viaduct has had an impact on factors other than rates of completed suicide is outside the scope of this research. None the less, other reasons may exist as to why people might want to prevent suicides at a particular location. Several of the charts reviewed for this study noted vehicular trauma to the bodies of the deceased. This underscores the fact that there may be social consequences to people jumping on to busy roads, including psychological or physical morbidity as well as mortality risk to bystanders as a result of motor vehicle collisions, damage to property, disruption to travel networks, and impact on the economy. By eliminating all suicides at Bloor Street Viaduct in the period after the barrier, the barrier prevented such negative social consequences. Similar negative outcomes may, however, have increased at other bridges and buildings.

Strengths and limitations of the study

The barrier at Bloor Street Viaduct is an opportunity for a fascinating natural experiment. It provides what is perhaps the best scenario for testing whether such barriers are effective because the bridge had the second highest yearly rate of suicides after Golden Gate Bridge and because, unlike other locations such as San Francisco, no bridges in Toronto span large bodies of water, meaning that essentially all suicides by jumping in Toronto come to the attention of the coroner and are recorded. Furthermore, demographic data for people who completed suicide by jumping from Bloor Street Viaduct (median age 36, 79% male) were similar to data recently reported for people who jumped at Golden Gate Bridge (median age 40, 74% male).²¹

As in any natural experiment, however, this research has many uncontrolled variables. Firstly, despite the relatively high rate of suicides by jumping at Bloor Street Viaduct, the absolute numbers may have been too low to achieve adequate power in a study of this kind. Secondly, despite the relative comprehensiveness of the chief coroner's records, it is possible that suicide rates by all causes were overestimated or underestimated in the period before or after the barrier owing to incompleteness or inaccuracy of records. Thirdly, the coroner's records might be prone to bias because people found dead beneath certain bridges or after falling from any bridge or building are more likely to have been ruled as having died by suicide than by causes such as homicide or unintentional death. Finally, it is possible that an ecological fallacy is operating. Suicide in itself is a rare event and suicides by jumping are uncommon to an even greater extent. Despite the remarkably stable number of suicides by jumping in Toronto before the barrier, the possibility that rates of suicide at other bridges increased after the barrier for reasons other than substitution of location cannot be discounted. These reasons might include chance fluctuations in rates, economic changes, social changes, or other interventions to restrict the means of completing suicide. It is conceivable that the barrier led to a reduction in suicides but that this was masked by one or more of these uncontrolled variables.

Conclusions

No suicides occurred at Bloor Street Viaduct in the four years after the construction of a barrier; however, suicide rates by jumping in Toronto were unchanged because of a statistically significant increase in suicides by jumping from other bridges and a non-significant increase in suicides by jumping from buildings. This suggests that the availability of Bloor Street Viaduct was not an essential element for people contemplating suicide by jumping in Toronto. We speculate that a different result may be observed if a bridge holds a more powerful influence on suicidal people. This may be the case for Golden Gate Bridge, for example, although further evidence is needed. A safety net might be installed at Golden Gate Bridge in the near future,²⁴ so research similar to the present study might be possible in San Francisco. However, logistical challenges may be

Response to comments from Daniel Jost

Your opposition to the project has been noted.

Response to comment #1: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #2: Your opposition to the project has been noted.

As discussed in the 2008 DEIR pages 10 and 11, the 2009 FEIR pages 11 and 12, and Draft Supplemental EIR pages 5-7 and Appendix H Attachments 20 and 39, the net alternative was not considered feasible. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.



Linda <lkast6945@aol.com>

01/20/2011 07:20 AM

To <matt_c_fowler@dot.ca.gov>

cc

Subject cold spring bridge

Dear Mr. Fowler,

It has come to my attention that there is an alternative to the suicide barrier being planned for the Cold Spring Bridge in Santa Ynez. The bridge has provided those who wish to end their life with a tool to do so and those who wish to enjoy the gifts of life a tool to do so. We who love the view of the magnificent valley can still have that view while saving those who wish to end their life.

A horizontal barrier will serve both purposes. It has been used here in the state of California to great success .Please check out the Golden Gate Bridge in San Francisco.

I urge you to use a horizontal barrier instead of a vertical barrier on Cold Spring Bridge and satisfy all who live and travel to this beautiful part of California.

Sincerely,
Linda Kastner
6945 Happy Cantyon
Santa Ynez Ca 93460

Linda Kastner

Response to comments from Linda Kastner

Your opposition to the project has been noted.

As discussed in the 2008 DEIR (pages 10-11), the 2009 FEIR pages 11 and 12, and Draft Supplemental EIR pages 5-7, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Caltrans has reviewed information developed for the Golden Gate Bridge Barrier. The Golden Gate Bridge is a different type of bridge and is also in a completely different setting. When considering the different types of issues inherent for a horizontal net barrier design, as discussed above, versus the vertical grid/mesh design, the vertical grid/mesh is the preferred alternative.



carol kenyon
<scfacc@wildblue.net>
01/20/2011 10:15 AM

To <Matt_C_Fowler@dot.ca.gov>
cc
Subject Cold Spring Bridge vote

Dear Mr. Fowler,

Please support a vote by SBCAG advising Caltrans to immediately withdraw its support for the installation of tall vertical cage fencing barriers on the Cold Spring Bridge. Installation of horizontal barriers of an appropriate design, along the lines of those recently approved for installation on the Golden Gate Bridge and proposed by the State Historic Preservation Officer is a much more appropriate solution. This

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design system meets all of the purposes stated by Caltrans in support its project, and it is a superior alternative for it avoids the significant adverse impacts to the historic and aesthetic qualities for which the bridge is widely renowned and appreciated.

Thank you.

Carol Kenyon

Response to comments from Carol Kenyon

Your opposition to the project has been noted.

Response to comment #1: SBCAG (Santa Barbara County Association of Governments) has not withdrawn its support for the proposed grid/mesh alternative.

Response to comment #2: A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Golden Gate Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above

the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting (physical modifications) the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation

responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Penny Knowles
<Penny2CA@aol.com>
01/20/2011 03:59 PM

To <matt_c_fowler@dot.ca.gov>
cc 'Marc McGinnes' <mcginnes@es.ucsb.edu>
Subject Cold Springs Bridge

Dear Mr. Fowler:

I am one of those citizens of the Santa Ynez Valley who has been aghast by Caltrans' proposal to erect unsightly vertical barriers on either side of the Cold Springs Bridge. I do recognize that jumping off the bridge is an easy choice for those who are driven to suicide, and it is desirable to prevent this from happening, but there must be a better alternative to the vertical barriers.

I did consult the Draft Supplemental EIR at the Solvang Library, and was dissatisfied with the nominal consideration that was given there to horizontal netting. There was only one design alternative proposed and that was dismissed as being impractical from an engineering standpoint. No mention is made of other solutions to this problem devised for other landmark bridges such as the Golden Gate Bridge, where suicide attempts are clearly much greater than the Cold Springs Bridge.

I noticed in the EIR (p. 31) that the State Preservation Office and the State Advisory Committee on Preservation urged Caltrans to consider a cantilevered barrier net as a preferred solution. If the one you analyzed seemed unfeasible, then why not look into other designs?

Please study this a little further before you commit our precious taxpayer dollars to such an unfortunate solution.

E.P. Knowles
1446 Calzada Avenue
Santa Ynez, CA 93460

Response to comments from EP Knowles

Your opposition to the project has been noted.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Golden Gate Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and

Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting (physical modifications) the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

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In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The

Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Ethel Larrabee
<e.larrabee@verizon.net>
01/20/2011 09:46 AM

To <matt_c_fowler@dot.ca.gov>
cc WE Watch <info@we-watch.org>
Subject Cold Spring Canyon Bridge barrier

Dear Mr. Fowler,

Please give serious consideration to the aesthetics of a barrier on the Cold Spring Canyon Bridge. Probably a horizontal barrier, as W.E. Watch has advocated, would be appropriate, if any is necessary.

Ethel Larrabee
1145 Dove Meadow Rd
Solvang, CA 93463

Response to comments from Ethel Larrabee

Your opposition to the project has been noted.

Aesthetics are an important part of the project design. As mentioned in the DSEIR (pages 21 and 22) and the Updated VIA (page 12), an Aesthetics Design Advisory Committee (ADAC) was established regarding the appearance of the barrier and to make recommendations to lessen the project's adverse visual effects. The ADAC was composed of Caltrans staff and members from the local community including a representative of the Santa Barbara County Historic Landmarks Advisory Commission, architects, landscape architects, and County Public Works and Planning members.

As discussed in the 2008 DEIR, the 2009 FEIR and DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the five design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Leigh Layman
<leighlee@wildblue.net>
01/22/2011 01:07 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Spring Bridge

Dear Mr. Fowler,

It is my understanding that you are the person who I should make my concerns and opinions to ... Lucky you!

As a native Californian and a Santa Ynez resident for over 35 years I am someone who has driven over Cold Springs Bridge 100s of times since childhood, I am very opposed and saddened buy the possibility of a vertical barrier being constructed on this scenic bridge. It would be a major eyesore on what is one of the most beautiful views in our area.

If any barrier at all needs to be constructed, please, please consider another option. I have heard that the horizontal barrier is very effective and is less visually offensive.

I also understand that the time frame for making our comments to you is almost up and I know there are many valley residents who feel as I do and have not yet voiced their feelings opposing the horizontal barrier.

If you need a written letter as well as this email, please let me know as I want my feeling to count when you make your decision.

Leigh Layman
Rocking L Ranch

Response to comments from Leigh Layman

As discussed in the 2008 DEIR, the 2009 FEIR and the DSEIR, the safety net alternative was not considered feasible at the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the five design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Chuck
<chucklepkowsky@verizon.net>

01/05/2011 08:03 AM

To <matt_c_fowler@dot.ca.gov>

cc

Subject suicide barriers on Cold Springs Bridge

Dear Mr. Fowler:

I am writing as a licensed psychologist regarding the debate over the need for suicide barriers on the Hwy 154 Cold Springs Bridge.

I am aware that many of my colleagues are supporting the initiative to place suicide barriers on the bridge. As I understand it, this effort has been spearheaded by Lisa Firestone and the Glendon Association. I am writing to voice a dissenting opinion.

Unfortunately, the data do not appear to support the practical utility of erecting a suicide barrier on the bridge. You are probably aware that during the two or three weeks that a temporary barrier was installed, two people found their way around the barrier and successfully suicided by leaping off the bridge.

Erecting the proposed barrier -- a sort of chain-link tunnel over the bridge -- is a costly eyesore and frankly appears to be a poor investment.

Best wishes,

Charles M. Lepkowsky, Ph.D.

Response to comments from Charles M Lepkowsky, PhD

Your opposition to the project has been noted.

The comment references a “temporary barrier” that was installed for two or three weeks, however that barrier was not the Vertical Barrier that this project proposes to install. The “temporary barrier” was in fact temporary construction safety fencing designed to protect construction crews, it was not designed to prevent people from deliberately jumping off the bridge.

Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.



Roland and Bridget Lewin
<rblewin@cox.net>

01/07/2011 10:44 AM

To <Matt_C_Fowler@dot.ca.gov>

cc

Subject Hwy 154 Bridge

Dear Matt,

Please read my letter regarding the Highway 154 bridge.

Thank you very much,
Bridget Lewin



Hwy 154 bridge issue.doc

Dear Matt Fowler (Senior Environmental Planner),

I am a long term resident of Goleta and though I was not able to make it to the public meeting regarding the Highway 154 bridge, I would like to let you know my thoughts.

Suicide prevention need not obstruct one's view of beauty.

As a high school teacher, I have prevented suicide in two instances. Both cases required focused attention, community resources, and respect for all concerned. Therefore, I appreciate your intentions.

Through this process of discussion of how to prevent suicide on the bridge, Cal Trans has focused needed attention on how/why people may choose to end their lives through suicide at all.

Cal Trans and Environmental Planners NOW need to creatively consider how to meet the needs of the entire community. **Preserving lives and preserving beauty.**

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Can this be done? Absolutely!

How many lives have been saved by the expansive view of the valley?

Imagine, the woman who, after a very difficult and challenging day at work, is driving home from Santa Barbara to Solvang. Upon cresting the hill, after mentally going through all the turmoil of her day, she looks out to see the expansive valley. As she crosses over the bridge (without mesh or anything to block her view), she realizes and feels the awe that that view so clearly communicates to us all. Through that view, and the lack of obstruction, she feels the awe that only nature can provide. She is humbled. She is revitalized, she feels the awe and inspiration necessary to seek the solutions required to solve her problem. She feels refreshed. Upon entering her home, she is calm, joyful, and ends up sitting down at her computer with her inspired solution, and writes the email that solves the difficulty at work. Through this process, she not only saves her own "life," her solutions also ripple out to affect the lives of her colleagues and community. In this case, the bridge saved the quality of her life and the lives of her colleagues.

Each time I have crossed that bridge, and looked out to that view, I have been inspired and deeply affected by its beauty, power, and grace. That bridge is a testament to how incredibly creative engineers and planners can be!

Cal Trans and Environmental Planners can lead the way in solving this seeming dilemma, by keeping the view unobstructed while at the same time engineering a solution that prevents a person ending their life through suicide.

Upon reading the January 6th newspaper article in the SB News Press, I noted a solution that I had also thought about—the use of a steel net below the bridge. In addition, it would seem that cameras, speakers, information with numbers to call, etc. would be more helpful in actually dealing with the deeper issues of suicide than simply a fence.

2

We, as a community, can creatively implement a solution that **PRESERVES LIVES THROUGH PRESERVING BEAUTY**. I have faith it can be done! It will be another testament to collective action, integrity, creativity, and truly solving a community problem.

Thank you for all your efforts to find a true solution.
Let's do **ALL WE TRULY CAN** to meet everyone's needs.

Respectfully,
Bridget Lewin

Response to comments from Bridget Lewin

Your opposition to the project has been noted.

Response to comment #1: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #2: As discussed in the DEIR, the 2009 FEIR and DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the five design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

In addition, the 2008 DEIR and 2009 FEIR for the Cold Spring Canyon Bridge project which are incorporated by reference, did consider a “human barrier” alternative which included cameras, signs and call boxes, however that alternative was not found to be feasible on its own without the addition of a physical barrier. Please see pages 13-15 of the FEIR for additional details. Our findings are consistent with the consensus of experts in the field of suicidology, as documented in Chapter 2

of the 2009 FEIR including the section titled “Difference of opinion regarding the effectiveness of physical suicide barriers vs. ‘human barriers.’”



anniemail
<anniemail@earthlink.net>

01/10/2011 12:02 PM

Please respond to
anniemail
<anniemail@earthlink.net>

To <Matt_C_Fowler@dot.ca.gov>

cc

Subject Cold Springs Bridge

Dear Mr. Fowler:

I am adamantly opposed to use of a vertical barrier on the historic Cold Springs Bridge.

Other options- Nets, horizontal barriers, suicide call box on site- Must be addressed.

Anne Milgrim

Response to comments from Anne Milgrim

Your opposition to the project has been noted.

As discussed in the 2008 DEIR, the 2009 FEIR and DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the five design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
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5. Liability associated with an attractive nuisance
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7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

In addition, the DEIR and FEIR for the Cold Spring Canyon Bridge project, which are incorporated by reference, did consider a “human barrier” alternative which included cameras, signs and call boxes, however that alternative was not found to be feasible on its own without the addition of a physical barrier. Please see pages 13-15 of the FEIR for additional details.



Kendall Mills
<kendallmills@me.com>

12/17/2010 01:55 PM

To <Matt_Fowler@dot.ca.gov>

cc Chris Mills <m5r@mac.com>, Paul Matthies
<pmatthies@aol.com>, Jim Cassidy <jcquailridge@aol.com>

Subject Cold Spring Bridge suicide barrier

Dear Mr. Fowler, My wife and I have lived in Solvang for over 38 years, and we have driven over the Cold Spring Bridge thousands of times. Even though we are sad that too many people have used the bridge as the gateway to their next life, we love the bridge as it is now. We appreciate seeing the beautiful Valley without a 9.7-foot-high gridmesh wire fence compromising our view. We have experienced suicide in our family, and we doubt that the wire fence will reduce suicides in our County. Ken and Lloyd Mills

Response to comments from Ken and Lloyd Mills

Your opposition to the project has been noted.

As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).



Tracey Rich
<tracey@whitelotus.org>
01/20/2011 11:50 AM

To <Matt_C_Fowler@dot.ca.gov>
cc
Subject

White Lotus Foundation

2500 San Marcos Pass
Santa Barbara, CA 93105

Email: tracey@WhiteLotus.org Web: www.WhiteLotus.org
Telephone: 805-964-1944 Fax: 805-964-9617

Date: Thursday, January 20, 2011

To: Matt C Fowler, Caltrans

Message:

Dear Matt Fowler,

I am a resident on the San Marcos Pass and am writing to express my deep care and concern over the handling by Caltrans of the Cold Spring Bridge suicide barrier. I am also director of the White Lotus Foundation, a retreat and conference center on the San Marcos Pass that hosts guests from around the world. The current plans to erect a vertical barrier are not only in direct opposition to what has been voiced by our local community, but the plans are a travesty against the very nature that defines our Santa Barbara County. The Cold Spring Bridge is an extraordinary, historic monument which will be desecrated by the currently planned barrier. The bridge and its views serve as the glorious gateway to the unique Santa Ynez Valley, a noted signature of Santa Barbara County.

Since our voices were not heard to defeat this proposal all together then **I support an appropriate horizontal barrier similar to what is to be installed on the Golden Gate Bridge.** Information now shows that a below-roadway horizontal barrier is equal to, if not better, at preventing suicides than a vertical barrier and is an applicable solution that would preserve our Cold Spring Bridge. I am respectfully urging Caltrans to change their current design to one statistically proven to save lives while saving this historic and elegant structure. This will preserve our access to forever enjoy and appreciate the precious, natural environment that

surrounds us. I urge you to take the appropriate action on our community's behalf and correct this disrespectful mistake.

Respectfully,

Tracey Rich
Trustee, Director



[clip_image002.gif](#)

Response to comments from Tracey Rich

Your comments have been noted.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Golden Gate Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and

Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting (physical modifications) the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The

Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Linda Ryles
<windmillryles@msn.com>
01/23/2011 11:46 AM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Spring Bridge

It is requested that Caltrans to take the time needed to fully consider and select an appropriately designed horizontal barrier system for the Cold Spring Bridge, as proposed by the State Historic Preservation Officer and similar to the system than has been chosen for installation on the Golden Gate Bridge.

A horizontal barrier system is a superior means of meeting all of the purposes of the Caltrans suicide barriers proposal for the Cold Spring Bridge without needlessly destroying the historic and aesthetic qualities for which it is widely renowned and treasured.

We demand that Caltrans not cut corners in its haste to resume work on its deeply flawed and destructive tall vertical cage fencing barriers proposal.

This is a wonderful area to live and commute, please do not destroy our God given beauty.

Please also respond with a return notification of receipt of this e-mail. thank you

Linda Ryles
Solvang, Ca

Response to comments from Linda Ryles

Your opposition to the project has been noted.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Golden Gate Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and

Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting (physical modifications) the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The

Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



"Scudder, Henry"
<henry.scudder.66@my.csun.edu>

01/11/2011 05:25 PM

To <Matt_Fowler@dot.ca.gov>

cc

Subject Cold Springs Bridge

Attn: Matt Fowler
Senior Environmental Planner
Dear Sir,
We feel that a Horizontal Suicide Barrier is the much superior option to preventing suicides from the Cold Springs Bridge.
Please implement this solution to preserve the view from the Bridge.
Thank You,
Henry and Constance Scudder
Solvang, CA



"Kathy \"Pim\" Snow"
<katydid5@cox.net>

01/24/2011 10:43 AM

To <Matt.C.Fowler@dot.ca.gov>

cc

Subject S.B.Bridge

I strongly recommend use of the horizontal barrier (chosen for the Golden Gate Bridge after much study) which would solve the suicide problem as well as maintain the beauty of this magnificent Bridge

HARRY R. SNOW
3775 Modoc Rd. #12
S.B. 93105



Cheryle Teats
<cheryleteats@yahoo.com>
01/11/2011 05:18 PM

To <Matt_Fowler@dot.ca.gov>
cc
Subject Cold Spring Canyon Bridge

Mr Fowler,

I believe the horizontal net is the best idea for a suicide barrier. I think it is fair to say that the job of sheriff comes with some amount of danger. I don't think that is a valid argument. The eye sore of a vertical barrier would very sad.

Thank you

Cheryle Teats



Rudi Van Enoo
 <rvanenoo@mac.com>
 01/19/2011 10:35 PM

To <matt_c_fowler@dot.ca.gov>
 cc
 Subject Comment on bridge



Wewatch comment card.pdf

Cold Spring Canyon Bridge
 SUICIDE BARRIER 154

2011

COMMENT CARD

NAME: ELISE CANE-VANENOO

ADDRESS: 680 Ranch Alisal CITY: SOLVANG ZIP: 93463

REPRESENTING: myself

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the Comment Box or

Mail to: **Cathy Stettler** **MATT C. FOWLER**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera St.
 San Luis Obispo, CA 93401

matt_c_fowler@dot.ca.gov

I would like the following comments filed in the record (please print):

I am opposed to the VERTICAL BARRIER proposed
by CALTRANS. I DO SUPPORT the construction
of a HORIZONTAL BARRIER.

(Please respond by ~~June 24, 2008~~
 JANUARY 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

©2008.05.22 Caltrans, cje_eeb



Laura Verdi
<lverdi@sbcglobal.net>
01/23/2011 04:58 PM

To <matt_c_fowler@dot.ca.gov>
cc
Subject Cold Springs Bridge Barrier

Dear Mr. Fowler,

I understand we are approaching the deadline for comments and feedback associated with the security as well as the type of barrier that will be installed on the Cold Springs bridge. I want to share with you my deepest concern about the barrier and express that I'm supporting the horizontal barrier installation. I understand the horizontal barrier will be a little more pleasing to the eye and will certainly be able to address the current issues associated with protecting barrier. Thank-you for your time.

Sincerely,

Laura L. Verdi
1565 Calzada Ave.
Santa Ynez, CA 93460

Response to comments from:

- Henry and Constance Scudder
- Harry R. Snow
- Cheryl Teats
- Elise Cane-Van Enoo
- Laura L. Verdi

Your opposition to the project has been noted.

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible at the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Virginia
<virginiamarket@cox.net>

12/10/2010 09:08 PM

To <matt_fowler@dot.ca.gov>

cc

Subject cold springs bridge suicide barrier

i am totally against ruining the looks and views of this important bridge because of an ill-formed notion that we can engineer the human existence. this is like trying to help dieters by banning the sale of M&M's when there is a whole candy store full of other options.



Bill Tracy
<tracgeology@aim.com>

12/27/2010 08:45 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Cold Spring Canyon Bridge

Hi Matt, I'm writing to oppose the proposed construction of the suicide barrier on the bridge. Please do not build this project. Thank you. Bill Tracy 805-886-1591

Bill Tracy
work 805/681-5694

Response to comments from:

- Virginia Markel
- Bill Tracy

Your opposition to the project has been noted.



William Watson
<thewatson@gmail.com>

To <matt_c_fowler@dot.ca.gov>

01/24/2011 03:48 PM

cc

Please respond to
<ww@computer.org>

Subject Cold Spring Canyon Bridge Comment

I attended the public meeting at San Marcos on January 5th and got a lot of information on the subject of the barrier project.

I was hoping I could get stats on the number of suicides in the district over the years and particularly those that occurred at the bridge. My idea was to see if the number of suicides has increased over the years or whether the number of suicides has just migrated to the bridge. I do not have those numbers as of this deadline unfortunately.

The bottom line for me is that the solutions proposed by CalTrans are extremely obtrusive for a rather ill-defined benefit. There is a clear need to raise the height of the railing such that people (police/rescue folk) do not accidentally fall off the bridge but that does not require the huge barrier proposed. If the numbers show that the suicide rate in the district has not risen dramatically over the years, then I would say that the barrier would only displace the suicide attempts and ultimately not achieve anything to prevent these unfortunate situations.

1

As much as I sympathise with the families and rescue folk who have to deal with the aftermath of suicide attempts, I do not see that a massive barrier is going to alleviate the suicide rate and thus the unique visual impact of the bridge would be lost for no good reason.

I keep on hearing that there might be less intrusive solutions that were not presented at the San Marcos meeting. I definitely got the impression that CalTrans has made up its collective mind to do one thing if only because such a huge amount of time and money has been invested already in the project. I urge that a much less visually impactful solution be found that would actually solve the problems at hand.

2

Thanks for the opportunity to say my piece,
WW

William Watson, ww@computer.org, 805 967 6976
179 Nogal Drive,
Santa Barbara, CA 93110

Response to comments from William Watson

Your opposition to the project has been noted.

Response to comment #1: The current railing along the bridge is considered standard for the construction period of the structure. In addition, the suggestion of raising the existing bridge railings would likely cause an additional adverse effect by introducing a visual element that diminishes the property's historic integrity of design, feeling, and association. Increasing the height of the bridge rail would also not comply with the Secretary of Interior's Standards for Rehabilitation, Standards 2 and 9. Empirical evidence indicates that raising the height of the existing railing sufficiently to provide improved safety for police/rescue as suggested may adversely affect high quality views from the bridge. The Sheriff's office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties). Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

Response to comment #2: As discussed in the 2008 DEIR, the 2009 FEIR, and the DSEIR, the net alternative was considered but rejected because it was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the five design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide

3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



<lisawelms@verizon.net>

01/06/2011 11:03 AM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Cold Spring Canyon Brif=dge Suicide Barrier Project

Hello,

I rarely write public officials or entities, but feel strongly that I should add to any debate over the project.

I travel the 154 frequently and often choose this route over the 101 freeway simply for aesthetic reasons. This portion of the "San Marcos Pass" is idyllic and very representative of what's wonderful about the central coast. While most bridges and structures detract from the natural beauty, the Cold Springs bridge is a beautiful piece of architecture and fits into the environment quite well.

Placing "Suicide Barriers" would greatly affect the beauty of this area, a major reason that this route is also referred to as a California Scenic Route. A person who is determined to commit suicide will. Preventing this one avenue, the bridge, as an option will not decrease the suicide rate. And the expense to taxpayers is unwarranted.

Lisa Welms

Response to comments from Lisa Welms

Your opposition to the project has been noted.

As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.



James West
<jimwest@yahoo.com>

01/14/2011 03:34 PM

To <Matt_C_Fowler@dot.ca.gov>

cc

Subject Cold Springs Bridge

To: California Dept of Transportation:

I do not have the time to come to Santa Barbara from Santa Ynez to attend the meetings held to review the current plans, so I must voice my opinion by email.

PLEASE DO NOT BUILD THE BARRIER as currently planned!!

1- how is it possible that taxpayer dollars are being spent on a questionable resource without being put to a public vote or some other appropriate method of actual public approval?
2- why is the only design actively being pursued the one that will destroy the visual beauty of this landmark? If some government entity has determined that money NEEDS to be spent to do this, why not spend enough to perform this function without destroying the beauty of the bridge (such as a horizontal barrier)?

I URGE you to consider dropping the plans for this barrier or, if we are being RAILROADED into spending the money, do it in a way that does not destroy our beautiful landmark!

Jim West
Santa Ynez, California

Response to comments from Jim West

Your opposition to the project has been noted.

Response to comment #1: Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties). The money needed to construct the barriers will come from Local Recovery Act funds. The Recovery Act funding has been designated specifically for this project and cannot be used for any other project.

Response to comment #2: As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible at the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Ganga White
<gw@whitelotus.org>
01/06/2011 02:23 PM

To <Matt_C_Fowler@dot.ca.gov>
cc <bridgefriends@hotmail.com>
Subject ColdSpring Bridge

Dear Mr. Fowler,

I am the president of White Lotus Foundation, a world renowned conference center and retreat on San Marcos Pass Road. The retreat has been here for over 40 years and I have lived on the Pass for over 30 years. I drive the Coldspring Bridge on a daily basis.

It would be an unconscionable, unforgivable, and gross error to cage in this historic and beautiful bridge. We have hundreds of guests and visitors each year from all over our State, country, and even the world. We regularly suggest they drive the Pass and over the bridge to the Vista Point to enjoy the experience and the views. We also suggest they take Coldspring Road to see the bridge from that angle, as well as from below, and we have received innumerable compliments and expressions of gratitude for suggesting that ride to see that stunning beauty. Some of the best views of the Paradise valley are beheld while traversing the bridge. I'm sure you are also aware that many commercials and movies have been filmed on that bridge.

1

I actually feel that suicides would not be prevented by bridge barriers—only relocated. However, if a barrier is required, I would like to state in no uncertain terms that the only alternative to consider is the horizontal barrier, as was chosen for our State's Golden Gate Bridge.

2

Please reconsider caging this beauty! Please use another option, such as the horizontal barrier.

3

Respectfully yours,

Ganga White
Founder/President
White Lotus Foundation
2500 San Marcos Pass
Santa Barbara, CA 93105
805.964.1944

Response to comments from Ganga White

Your opposition to the project has been noted.

Response to comment #1: The DSEIR acknowledged the high visual quality of the Highway 154 corridor and the sweeping views of the valleys below. The DSEIR identified significant visual impacts resulting from the project due in part to its affect on views, specifically from the Cold Spring Canyon Bridge. Scenic vistas (including views from the Cold Spring Canyon Bridge) are not the sole determinant of visual quality along Highway 154 nor its Scenic Highway designation. The varied topography and native vegetation are also important contributors to the high visual quality of the corridor. The State Scenic Highway designation for Highway 154 extends along the route's entire 32-mile length. The project would only affect approximately 0.2 mile (approximately 1/6th of one percent) of the total 32-mile length of the Officially Designated State Scenic Highway. Although some of the existing view as seen while crossing the bridge would be affected, many other opportunities (such as the formal Vista Point approximately one-half mile west of the bridge) would remain along the route for public enjoyment of the high quality scenic views.

The DSEIR determined that as seen from Stagecoach Road below, the project would hardly be noticeable because of the distance and view-angle, and that because of the barrier's location on top of the bridge, views of the historic steel arch and substructure would not be adversely affected.

Response to comment #2: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #3: A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in

the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Golden Gate Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting (physical modifications) the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the

net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would.

The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

In addition, as discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide

3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



<marycontrary00@aol.com>

01/22/2011 06:42 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject bridge

The most beautiful view of of the Santa Ynez Valley is coming over the Cold Springs Bridge!

There seems to be an alternative to the proposed building of a suicide barrier that will keep everyone happy.

I support the W.E. Watch letter.

Mary Williams
Solvang

Response to comments from Mary Williams

Your opposition to the project has been noted.

Aesthetics are an important part of the project design. As mentioned in the DSEIR (pages 21 and 22) and the Updated VIA (page 12), an Aesthetics Design Advisory Committee (ADAC) was established regarding the appearance of the barrier and to make recommendations to lessen the project's adverse visual effects. The ADAC was composed of Caltrans staff and members from the local community including a representative of the Santa Barbara County Historic Landmarks Advisory Commission, architects, landscape architects, and County Public Works and Planning members.

It is not clear from this comment which alternative is being referred to. However, if it is in reference to a horizontal net barrier, as discussed in the 2008 DEIR, the 2009 FEIR, and the 2010 DSEIR, the net alternative was considered but rejected because it was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the five design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Eric Wilmanns
<eric@bw-environmental.com
>

To <Matt_C_Fowler@dot.ca.gov>
cc

01/03/2011 12:14 AM

Subject Coldsprings Bridge

Dear Mr. Matt Fowler,

I'm writing in support of Caldrons consideration of a horizontal suicide barrier for the cold springs bridge on hwy 154 out of San Marcos Pass. Honestly I felt no barrier was not a viable option! Our culture makes guns and we do engineer usable solution to maintain their safe use in culture. I was personally fine with the other option as I see it as a bridge and not a work of art. But if more parties can be served with better outcomes for troubled individuals via this course - so much the better.

Eric Wilmanns, MSE
Principal
Brown and Wilmanns Environmental, LLC
805-729-3168
eric@bw-environmental.com

Response to comments from Eric Wilmanns, MSE

Your opposition to the project has been noted.

As discussed in the DEIR, the 2009 FEIR, and the 2010 DSEIR, the net alternative was considered but rejected because it was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the five design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.



Duncan Wright
<duncan59@gmail.com>

01/05/2011 06:04 PM

To <matt_c_fowler@dot.ca.gov>

cc

Subject Pleading for no fencing on the Hwy 154 bridge.

Dear Mr. Fowler,

Don't fence me in! And please don't fence in beauty or nature .

Fences on viaducts and bridges over California's wide canyons might temporarily save a very few lives, but natural beauty enhances every life that passes by .

Every human considers suicide occasionally, not as a plan but as a passing thought . It is an essential part of the freedom of living autonomously .

A few people do occasionally act on the impulse to take their own life which is tragic . But is controlling the environment really the way to respond to this ?

I would think that spending the money on a hotline or suicide counselors would be a greater plan. Give these depressed and desperate individuals an option to suicide , don't fence everything in even more than it is already. We cannot control every human impulse with architecture and engineering. What people need is compassion and attention , dollars spent on support resources, not unsightly fences stretched across this most wonderous vision , that in its natural design offers hope .

Please consider my contribution to this debate .

Sincerely,

Duncan Wright

Coordinator of counseling services at a Santa Barbara non-profit, working at a residence built and managed by the SB Housing Authority to provide transitional housing out of homelessness .

(805) 452-5171

Response to comments from Duncan Wright

Your opposition to the project has been noted.

Both the 2008 DEIR (pages 11-14) and the 2009 FEIR (pages 13-15 and 24-25), which are incorporated by reference, did consider a “human barrier” alternative which included cameras, signs and call boxes. The “human barrier” alternative was eliminated for reasons discussed in these documents, in the Alternatives Considered but Eliminated From Further Discussion sections.

However, a separate project sponsored by the Santa Barbara County Association of Governments, would install crisis phones at the two nearest call boxes to the bridge, the crisis phones cannot be located on the bridge deck for safety reasons. Signs would say, “In Crisis? We Care Please Call Us” (see the 2009 FEIR at page 128).

Section 5.0 Comment Cards from Public Hearings

Cold Spring Canyon Bridge
SUICIDE BARRIER 
2011

COMMENT CARD

NAME: SAM S. ALFANO

ADDRESS: 71 LASSEN DR CITY: SANTA BARBARA ^{CA.} ZIP: 93111

REPRESENTING: _____

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
Senior Environmental Planner
Department of Transportation
50 Higuera Street
San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov

The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

SEE ENCLOSED

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

January 19 , 2011

Mr. Matt C. Fowler
Senior Environmental Planner
Department of Transportation
50 Higuera Street
Sam Luis Obispo, Ca. 93401

Dear Mr. Fowler,

Enclosed are 5 signed petitions totaling 91 signatories from residence in the Santa Barbara area who request that Caltrans move forward with the construction of the suicide prevention barriers.

Also enclosed are numerous articles taken from the Santa Barbara News Press relating to the issue.

There are those I am sure who would like to prolong the construction indefinitely through various side issues. It is imperative that the barriers be constructed as soon as possible to avoid further tragic loss of life and the endangerment of the lives of emergency personnel as they respond to the bridge site trying to constrain jumpers.

Sincerely,



Sam S. Alfano
Retired Outdoor Recreation Management Officer
Los Padres National Forest

805-967-0595

r123range1@cox.net

FRIENDS OF THE BRIDGE FOR LIFE

PETITION:

WHEREAS -- 55 PEOPLE HAVE JUMPED TO THEIR DEATHS SINCE THE COLD SPRINGS BRIDGE WAS BUILT IN 1963

WHEREAS --THE "FRIENDS OF THE BRIDGE" ARE OPPOSED TO RESTRAINING PEOPLE FROM JUMPING OFF THE BRIDGE

WHEREAS-- THE "FRIENDS OF THE BRIDGE" ARE PUTTING A HIGHER VALUE ON THE POSSIBLE LOSS OF AESTHETIC VALUE OVER THE LOSS OF HUMAN LIFE .

WHEREAS- WE AS A HUMANE SOCIETY OWE THOSE IN DISTRESS EVERY EFFORT TO CONTINUE ON WITH THEIR LIVES

WHEREAS-- IT IS INCONCEIVABLE THAT THERE ARE THOSE IN OUR SOCIETY WHO PLACE THE COST OF THE BARRIER OVER THE VALUE OF HUMAN LIFE

WHEREAS - CAL TRANS AND SBCAG HAVE THE FUNDING TO ERECT SUICIDE PREVENTION MEASURES

WHEREAS- THE LIVES OF CHP, SHERIFF, FOREST SERVICE, RESCUE PERSONNEL AND MOTORISTS ARE PUT AT RISK AS A RESULT OF SUICIDE ATTEMPTS AT THE BRIDGE

WHEREAS -AS EACH DAY PASSES BY WITHOUT THE PREVENTION MEASURES ANOTHER LIFE MAY BE LOST

NOW THEREFORE THE UNDERSIGNED HEREBY URGENTLY REQUEST THAT CAL TRANS AND SBCAG CONSTRUCT THE SUICIDE PREVENTION BARRIER WITHOUT FURTHER DELAY:

	NAME	DATE
1	Sister Linda Mulvihill	9/4/10
2	Sister Rose R. Statta	9/6/10
3	Aa Maere Russell Carr	9/11/10
4	Marie Do	9/13/10
5	Joan Tuomey	9/14/10
6	Palmer	9/14/10
7	James M. Allen	9/15/10
8	Stephen Montross	9/16/10
9	James a. Holm	9/17/10
10	Luisa Escalante	
11	Pat Waters	
12		
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18		
19		
20		

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NOW THEREFORE THE UNDERSIGNED HEREBY URGENTLY REQUEST THAT CAL TRANS AND SBCAG CONSTRUCT THE SUICIDE PREVENTION BARRIER WITHOUT FURTHER DELAY:

	NAME	DATE
1	Lee Alfano	8-16-10
2	Michael J. Smith	8/16/2010
3	Michael Smith	8/16/2010
4	Edward R. Harris	8/16/2010
5	William Fisher	8/16/2010
6	James Horland	8/16/2010
7	Paul Bayne	8/16/2010
8	Bernard Tothum	8/16/2010
9	Roger L. Bagnasco	8/16/2010
10	Joseph R. Danga	8/16/2010
11	Salomon Sandoz	8-16-2010
12	James H. Brasley	8-16-2010
13	Michael J. Fitzhugh	8-16-2010
14	Samuel C. Poy	8-16-2010
15	Tom Schmitt	8-18-2010
16	Eleanor Paulazzo	8-18-2010
17	John H. Hadden	8-18-2010
18	Elvira Cruz	8/18/2010
19	Bob Oke	8-18-10
20	Virginia H. Meyer	1/10/12

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- WHEREAS- WE AS A HUMANE SOCIETY OWE THOSE IN DISTRESS EVERY EFFORT TO CONTINUE ON WITH THEIR LIVES
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NOW THEREFORE THE UNDERSIGNED HEREBY URGENTLY REQUEST THAT CAL TRANS AND SBCAG CONSTRUCT THE SUICIDE PREVENTION BARRIER WITHOUT FURTHER DELAY:

	NAME	DATE
1	<i>Yvonne Ramirez</i>	1-10-11
2	<i>Sylvia Ludquist</i>	1-10-11
3	<i>Olga R. Cruz</i>	1-10-11
4	<i>Edomonte Sanchez</i>	1-10-11
5	<i>Chris Pys</i>	1-10-11
6	<i>Raphael Calderon</i>	1-10-11
7	<i>Stephen L. Johnson</i>	1-10-11
8	<i>David Varela</i>	1/13/11
9	<i>Theresa Stricker</i>	"13/11"
10	<i>Walt Huest</i>	1/13/11
11	<i>Jeff Bar</i>	1/13/11
12	<i>Cynthia & Peyton</i>	1-13-2011
13	<i>Garrett M. Van Wert</i>	1-16-2011
14	<i>John Van Wert</i>	1-16-2011
15	<i>Connie Maynahan</i>	1-16-2011
16	<i>Mark Dwyer</i>	1/17/2011
17	<i>Francisco Cabre</i>	
18	<i>David X. Fardette</i>	1-17-2011
19	<i>J. Alvarado</i>	1-17-2011
20	<i>At Boyer</i>	1-17-2011

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	NAME	DATE
1	Tom J. Alfano	8/18/10
2	Engha Finckhart	8/18/10
3	Shelley D. Saris	8/18/10
4	Dan E.anski	8/18/2010
5	Paul Sobellaid	8/18/2010
6	George A. Levin	08/18/2010
7	Ruthanne P. Bortnowicz	08/18/2010
8	Al George Hoogman	08/18/2010
9	Sister Linda Mulvehill	8/23/10
10	Aglvia Lindquist	8-23-10
11	Marie Modzik	8-23-10
12	Dulles	8-23-10
13	Amita Salaman	8-23-10
14	John Meagher	8-23-10
15	Hal Thompson	8-23-10
16	Joe Bauer	8/23/10
17	Donna Klueber	23 AUG 2010
18	Lucille N. Aubrey	8/23/10
19	Bucher Q. King	1/10/11
20	Madelyn Ratcliff	1/10/11

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NOW THEREFORE THE UNDERSIGNED HEREBY URGENTLY REQUEST THAT CAL TRANS AND SBCAG CONSTRUCT THE SUICIDE PREVENTION BARRIER WITHOUT FURTHER DELAY:

	NAME	DATE
1	William Fuhrer	1-17-2011
2	Edward J. Barner	1-17-2011
3	Patrick James Donohoe	1-17-2011
4	John Mizaloff	1-17-2011
5	Steve Bickel	1-17-2011
6	Carl A. Valsuela	1-17-2011
7	Janice	1/17/2011
8	Joy Kelly	1/17/2011
9	BART WADLEY	1/17/2011
10	PATRICIA DARTON	1/17/2011
11	John Winters	1/17/2011
12	William Lawson	1/17/2011
13	Sybil Adelquist	1/17/2011
14	[Signature]	1/18/2011
15	Grace Larson	1/18/2011
16	Jim DEWEY	1/18/2011
17	Bill Faust	1/18/2011
18	Stephanie Stark	1/18/2011
19	Azim Teadjana	1/18/2011
20	Christie Campos	1/18/2011



Terrorism not part of suicide barrier

I may be dense, but I don't see how terrorism is involved in the decision to place a suicide barrier on the Cold Spring Canyon Bridge. There are two obvious reasons that seem not to be considered.

First, elimination of another suicide site is one step in the right direction. That the bridge has been the scene of *so many* deaths makes its elimination even more valid.

The second is that a net to catch jumpers is of questionable value. How far below the bridge deck would it be built? How would a jumper be prevented from leaping off the net? Would a second, lower net be needed?

If the net is built 100 feet below the bridge level, it is questionable as to how a jumper would survive the jump. And if he or she is successful, the remaining distance to the ground would still be fatal.

Nets would have to be built on both sides of the bridge and therein lies the problem of overloading the bridge structure. What if a jumper misses the net? Will his or her next of kin sue the county or state?

The argument that eliminating the bridge as a suicide site will not stop suicides is fallacious. Of course it won't, but it is a site more often used than any other local site.

Come on, City Council members (*by the way, is the bridge in the city?*), get out of the way of a decision.

For a change, let common sense prevail.

Donald R. Aubrey
Santa Barbara

34

Bridge barrier may give suicide pause

In assuming some letter writers who oppose the Cold Spring Canyon Bridge barrier never have had a family member or friend commit suicide.

I have. The hurt and heartache never go away.

If we can prevent one suicide, the bridge barrier is worth it. Perhaps it will give a potential suicide time to think things over and not do it.

Not only will the barrier save

“

CHOICE WORDS

You are supposed to be watching the road while driving — not the scenery. The view is spectacular at either end of the Cold Spring Canyon Bridge.

Pat Beattie

”

that life but possibly the lives of the people who have to retrieve the body.

You are supposed to be watching the road while driving — not the scenery. The view is spectacular at either end of the bridge.

And, no, I am not part of a special-interest group.

Pat Beattie
Santa Barbara

Rename bridge to honor Reagan

The people of Santa Barbara need to honor President Ronald Reagan. What better time to honor him than what would have been his 100th birthday?

To honor him, we need to rename the Cold Spring Canyon Bridge the Ronald Wilson Reagan Memorial Bridge.

Mr. Reagan was an apostle of freedom, protector of Israel and liberator of Poland, Czechoslovakia, Hungary, Romania, Bulgaria and East Germany;

restorer of the U.S. presidency, conqueror of Soviet communism, victor of the Cold War, champion of the dignity of man and hero of the American people. He was a true American hero.

On June 21, 1973, he said: "There is no question that we have failed to live up to the dreams of the founding fathers many times and in many places. Sometimes we do better than others, but all in all the one thing we must be on guard against is thinking that because of this, the system has failed. The system has not failed. Some human beings have

failed the system."

In 1926, when Mr. Reagan was employed as a lifeguard at Lowell Park in Dixon, Ill., he was

credited with saving 77 lives in the seven summers he worked there. He once said: "No crisis is beyond the capacity of our people to solve, no challenge too great. It is up to us to work together for progress and humanity so that our grandchildren, when they look back at us, can truly say that we not only preserved the flame of freedom, but cast its warmth and light further than those who came before us."

"Peace is the highest aspiration of the American people. We will negotiate for it, sacrifice for it, we will never surrender for it, now or forever."

Let's rename this bridge in honor of a great American, Ronald Reagan never would have approved of a costly and unsightly barrier to our beautiful bridge.

Edward Stanley Blomfield lives in Santa Barbara.

VOICE FROM SANTA BARBARA

Edward Stanley Blomfield

TUESDAY, DEC. 14, 2010

25

Santa Barbara
Independent
INDEPENDENT.COM

volume 24, number 240, August 19-26, 2010

Criss-Cross

I rode my bicycle over the Cold Spring Bridge once, many years ago. But I realized that the wind from passing cars could easily force

me sideways, and the railing was so low I could easily go over the side. I never rode across the bridge after that.

The bridge is about 1,100 feet long, and at speeds of 55 to 60 mph, that allows only about 11 to 14 seconds (or less) to look at the view. A driver on this narrow two-lane road would be unwise and unsafe to gaze at that view even momentarily. There is an off-road pullout near the bridge going west. A perfect and quiet and safe spot to stop and see the beautiful valley.

From reading the anti-barrier postings, I have come to the conclusion that what we have here is a bias against people who become depressed — and, as often happens, temporarily overwhelmed with a feeling of hopelessness and a monumental emotion of being worthless. The effort required to jump is minimal and the act is frequently impulsive. Yet depression is usually transitory and very treatable.

Someone said something about the effort being representative of a "nanny government." I would remind them that before we had a "nanny government," we had no laws to protect workers, children, or the elderly, and most of the money was in very few hands. Perhaps the so-called Friends of the Bridge would volunteer to help the Sheriff's Search and Rescue team retrieve the remains after the tragic suicides. It is a not-so-pleasant duty, even traumatic.

It is imperative to finish the suicide-barrier project. This is about saving our humanity. The Friends of the Bridge could better spend their energy and time working for the environment and for life.

— Tony Tonkin, S.B.

24

Misuse of Forest

Being a Knight of Columbus compels me to speak out on the 55 tragic deaths, by suicide, at Cold Spring Bridge since 1964. One of the essential tenets of the Knights of Columbus is to be of service to those in the community who are most in need. In addition, as a retired recreation management officer for the Los Padres Forest, it distresses me that the forest has become a place where distraught individuals are taking their lives.

The Friends of the Bridge say these individuals will find other methods if suicide barriers are installed. This stance is extremely problematic. Taking the most callous view of it, if individuals attempted to take their lives elsewhere, it would certainly minimize the impact on the lives of CHP, sheriff's deputies, and Forest Service personnel who have to respond with red lights and sirens to each and every report of a possible suicide attempt at the bridge. In addition, rescue personnel risk their lives as they climb in extremely rough terrain to remove the bodies of those who have gone over the side. It is not a pleasant experience to remove a body that has fallen 154 feet.

Without barriers, suicide prevention at the bridge can only be accomplished with 24-hour surveillance. Recently an 18-year-old girl went there intending to take her life. It was a coincidence of fate that deputy sheriffs were driving by and able to convince her not to take the fatal jump.

It is imperative that the prevention measures be constructed as soon as possible.

—Sam Alfano, S.B.

16

YOU SHOULD KNOW

Resources are available for anyone contemplating suicide. The National Suicide Hotline is 1-800-ICIDE. Another number to call for help is the Safe Alternatives for Treatment of Youth Hotline at 1-334-2777. Santa Barbara County Crisis Recovery Emergency Services RES/ACCESS is available at 1-868-1649.

Man jumps from bridge

THURSDAY, MARCH 11, 2010

SAN MARCOS PASS A man said to be in his 70s jumped to his death Wednesday from the Cold Spring Canyon Bridge.

Witnesses told authorities the man drove up at about 6:15 p.m., got out of his car and jumped. Traffic on westbound State Route 154 was backed up because of a two-car collision in the Windy Gap area, hindering emergency crews responding from the South Coast.

Motorists on the bridge also encountered a backup as the victim's car sat idle. The California Highway Patrol removed the victim's car from the roadway at about 6:30 p.m. and reopened the traffic lanes.

Search and rescue personnel were on scene at about 6:45 p.m.

A Sheriff's Department rescue helicopter aided in the search for the victim, which began as darkness fell. It was called off at about 6:50 p.m. after the body was located some 400 feet below the span.

In 2009, at least seven people jumped to their deaths from the bridge. Wednesday's was the first of 2010. Caltrans has a plan to add so-called "suicide barriers" to the 47-year-old structure.

— Scott Steeg

5

SUNDAY, AUGUST 2, 2009

Bridge barrier restricts rights

According to Dr. Lisa Firestone of the Glendon Association, "restriction of means is one of the most effective strategies to prevent loss of life."

Restricting access to knives, scissors, glassware, neckties, belts, etc., may be appropriate for a patient in a mental-health facility, but to impose the same restrictions on the public at large would be a violation of their basic rights. Likewise, building a semi-opaque barrier with sorely needed public funds on a public bridge over which there are more than 5 million safe trips a year would violate the public's right to unobstructed views from this beautiful and historic bridge.

Restricting public access to our beaches as a deterrent to suicide by drowning would follow the same repressive illogic. To ask whether one's fun in the sun is worth a human life only muddies the waters.

Tracy Fernandez
Santa Barbara

Life more dear than any panoramic view

The recent letter to the editor, "Why not build suicide platform?" was appalling. How dare the author make light of a tragic situation?

Where is the compassion? No one wants to feel that way. Their lives get so bad that they can see no other way out; they just want to stop feeling the pain.

Try to start helping those in need instead of tearing them down. We must come together to prevent suicide. If that requires placing a barrier on the Cold Spring Canyon Bridge to save a life, then I'm sorry to take away someone's precious view for 2 minutes while driving over the bridge.

Is life not more precious than this ridiculous argument over a barrier?

Lisa Vanderlaan
Santa Barbara

Friends, family, faith save lives

I read with interest a July 24 letter to the editor by Sam S. Alfano, management officer and safety officer, Los Padres National Forest, and agree with all his arguments in favor of suicide barriers. Too many Ameri-

cans do not or will not understand the suicide mentally; maybe too materialistic. Suicide attempts are preventable.

Empathic family and true friends can and will play a key role in helping the severely depressed or folks with illnesses like bi-polar. When this almost happened to me, God was on my side. Those who get help are no longer a burden on anyone.

William Crawley
Lompoc

Obstacle to suicide could turn tide

The moment before a suicidal person pulls the trigger, steps into traffic or jumps off a bridge, that person's mind slips into a state of insanity. Their one and only thought is to put an end to their torment and pain. If there's an obstacle, i.e., a barrier or interference to stall them, it may be enough to snap them back into reality before it's too late.

To say a suicidal person is determined to kill himself or herself one way or another is an erroneous rationale being used by the barrier opponents. Studies show many survivors of suicide attempts — falls from high places — regretted their decision right after they jumped. Given a second chance at life, they never attempted suicide again.

We all know the barrier is ugly, but the bucolic, pristine panorama "The Friends" are trying to preserve has been marred by the 47 dead people. You can't preserve something that's been destroyed.

The sorrow one suffers from losing someone precious to suicide never goes away. Stop sniffing around for little legal loopholes and put up the barrier.

M.M. Hamey
Goleta

Authorities look at ways to stop public suicide

By **CRISTIAN SALAZAR**
ASSOCIATED PRESS

NEW YORK — Eighteen-year-old Tyler Clementi typed his intention to millions on the Internet. "Jumping off the gw bridge sorry." His body was found days later floating in the Hudson River beneath the George Washington Bridge.

Chef Joseph Cerignola, a contestant on the reality cooking show "Kilchen Nightmares," also jumped from the iconic bridge in the past two weeks. His restaurant was mired in debt, though beginning to make a comeback.

In March, Yale University student Cameron Dabaghi jumped from the Empire State Building's 86th-floor observation deck. He had written a note beforehand saying he was sorry and would be jumping from either the George Washington Bridge or the totemic skyscraper.

Those who choose to end their lives in public, dramatic fashion often pick landmarks — from the George Washington Bridge overlooking Manhattan and the Palisades, to the Golden Gate Bridge, with its sweeping views of the Pacific Ocean and the San Francisco Bay.

Authorities are looking at how to prevent the public deaths with everything from concrete barriers, suicide hot line phones or safety nets hanging from bridges.

The measures would have made a difference for Kevin Hines, who survived a leap from the 746-foot Golden Gate Bridge in 2000.

"I would never have jumped off that bridge," if he found obstructions in the way, he said.

In New York, few city landmarks with the potential to become suicide hot spots are as accessible as the George Washington Bridge, which has a pedestrian path and a low railing.

The Empire State Building has a 10-foot-high safety fence and an abundance of security guards, but more than 30 people have leaped from it to their deaths since it opened in 1931. The Brooklyn Bridge, which also has seen fatal jumps this year, has an easy-to-get-to pedestrian walkway, but it hangs over lanes of vehicle traffic rather than water.

New York City police responded to over 640 reports of people either jumping or threatening to jump from buildings or bridges as of Aug. 31, NYPD spokesman Paul Browne said — a 27 percent increase over the same period last year.

The police have officers trained to talk down and grab would-be jumpers and deploy air bags in the streets to catch people threatening to jump from buildings.

A dozen telephones are installed along the pedestrian walkways on the George Washington Bridge that patch potential jumpers through to suicide

hot lines. The phones are near signs that say, "Need help?" in both English and Spanish.

Dr. John Draper, project director of the National Suicide Prevention Hot Line in New York, says a simple concrete barrier is a much better suicide deterrent on a bridge than a telephone.

"We've seen on bridges that people don't really call hot lines in high numbers," he said.

The Port Authority of New York and New Jersey, which owns the bridge, would not release information on the number of people who have jumped from the span, saying it's impossible to determine the exact count. But Port Authority spokesman Steve Sigmund said the agency is "continuing to partner with mental health experts to further strengthen" its prevention efforts.

Psychologists who study suicide say the landmarks can become attractive ways out for emotionally disturbed people wanting to die.

"When they think about dying in this way, they may have some degree of magical thinking, knowing that it is very likely their death will get publicity and media attention," said Dr. Alan L. Berman, executive director of the Washington-based American Association of Suicidology.

Mr. Hines, now 29, who suffers from bipolar disorder, told The Associated Press that he had believed the jump

would be less painful than other forms of death, and less frightening than taking pills. He decided after doing research on the Internet that "the only option was a bridge."

By conservative estimates, 1,300 people have jumped to their deaths from the Golden Gate Bridge since it opened in 1937. At least 29 people leaped last year, and eight have committed suicide there through July this year, according to bridge officials.

Transportation authorities voted two years ago to hang stainless steel nets from the bridge to deter suicides, although funding for the \$50 million project remains elusive.

Most jumpers suffer a grisly death, with massive internal injuries, broken bones and skull fractures. Some die from internal bleeding. Others asphyxiate from drowning.

Mr. Hines said his leap was anything but painless.

"This image that you just free-fall into an abyss is just a joke," he said.

Mr. Draper, of the suicide hot line network, said that popular opinion aside, research shows that barriers making the jumps from high places impossible will prevent the public suicides.

"Many people are under the impression that if you just put up a barrier they will find another way to kill themselves. It's an argument that people will make against putting up a barrier," Mr. Draper says. "And it's myth."

SUNDAY, APRIL 11, 2010

Bridge work shows respect for life

I wish to thank Sally Hughes for her inspiring Sunday guest commentary, "Troubled Bridge." She shared the account of the suicidal loss of her much-loved 23-year-old son, Jon, in 1991 in Cupertino, when he crashed his car into a sound wall on Highway 49. Cupertino honored David by correcting the problem, for they felt one life lost is one too many.

Sally is a woman with "heartitude" and now, in the name of her beloved son, advocates for the prevention measure that will save the lives of others' loved ones.

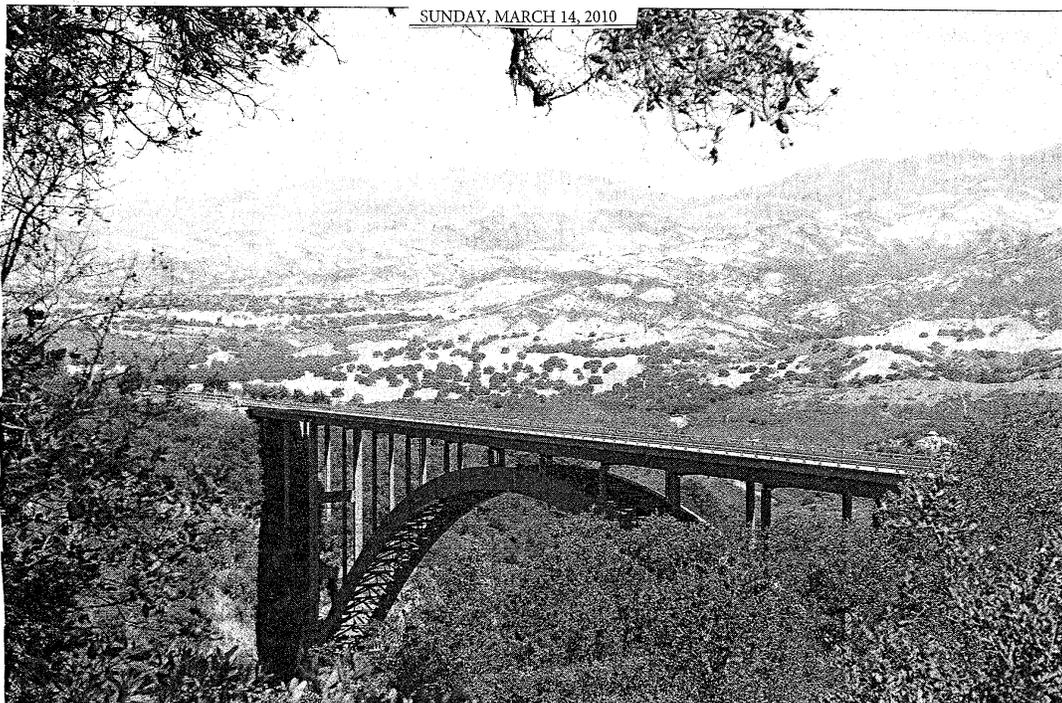
The Cold Spring Bridge spans a beautiful view, but the canyon beneath it has been the landing place for many people who fell to their deaths. We each are called to be a respecter and protector of all life. I wish to thank Caltrans for moving forward with the project to construct a barrier along this bridge.

May the words of John Wesley inspire each of us to be people with heartitude as is lived out by

Sally Hughes, who speaks out in the name of her son: "Do all the good you can, by all the means you can, in all the ways you can, in all the places you can, at all the times you can, to all the people you can, as long as you can."

Maybe we can be can-do people who use well the 86,400 seconds we are given each day to spend for humanity.

Sister Janet Corcoran, O.S.F.
Santa Maria



NEWS-PRESS FILE

TROUBLED BRIDGE

I am a Santa Barbara native and a mom. We lost our brilliant, 23-year-old son Jon in 1991, when he threaded his car into a perfect suicide slot inadvertently carved into a soundwall on Highway 49 by the Cupertino Corps of Engineers. It was the first suicide to occur here, and the last.

Cupertino earned my respect and gratitude forever when its engineers immediately paved the area over so no other in a despairing moment could take a now obvious, easy, instant and sure way out. Cupertino honored my son's death and prioritized life by correcting the problem.

It seems unbelievable that our beautiful city, my hometown, doesn't follow suit.

Malcolm Gladwell, in his acclaimed book "The Tipping Point," addresses this exact issue. Even

one instance of suicide, by any means, he writes, can function as a "virus" that infects vulnerable others. Then he discusses its "stickiness" factor: how dramatic, memorable — indelible — the means is. Both precedent and its stickiness create a tipping point, and others follow.



Sally Hughes

The author is a marriage and family therapist.

Any publicized suicide and memorable means can merge to operate as a culture, according to Mr. Gladwell, a culture that lives to infect others, be remembered and used ever after.

Yes, the Cold Spring Bridge spans a beautiful view on the horizontal plane, but below lies a tragic, gruesome and growing ad hoc cemetery of souls who chose to end life at a desperate moment because others have gone before.

It's too easy, instant and certain. Suicide is called "a permanent solution to a temporary problem" for excellent reason. I can testify

that no suicide is a solitary act. Parents, families, friends and more get "infected" as well — pain continues, afflicting survivors — never the same again. At birth, each baby lives at the center of a surrounding, growing human galaxy. Losing their center, all orbits gone, the others implode, crash or spin out.

It's no accident that we have had the most suicides in Cold Spring Bridge history during the past year. Depression, crisis and hopelessness are common feelings in a jobless economy where health care is too expensive, terror is the media's favorite watchword, and war seems an endless, miserable opportunity to increase corporate coffers.

In America, murder is illegal. My son proved that suicide is murder — the wrongful, unnatural and premature taking of a life by anyone's hand. Americans are constitutionally entitled to life,

Please see HUGHES on G

Community needs to do something to stop suicides

■ HUGHES

Continued from Page G1

liberty and the pursuit of happiness, not to aid and abet premature death, play God or treat people in pain as deserving to suffer or even die.

By doing nothing, we are culpable. Every time I drive over the bridge, I feel only pain and sorrow for what we haven't done that has by our default contributed to every life lost there. Instead of seeing a beautiful panorama, I see a hideous scene.

People assume that people committing suicide are mentally ill or lost causes. My son Jon was neither. He was a brilliant intellectual, temporarily depressed and in crisis.

Like many people in personal crises, his call for help was not heard.

At the funeral, a distant, highly successful uncle came up to me and said: "You know, I only met Jon once, but after talking with him for five minutes, I realized that here really was someone who could save the world." His words stunned me because — they were true.

Had Jon only lived past that moment, he would have contributed

to society at high levels in useful and powerful ways. Preventing just one suicide would more than pay the cost of an existing problem proven to Santa Barbara now 64 times over that is guaranteed to continue.

If Cupertino can see a problem and correct it, why can't Santa Barbara? We have the money, motivation and mandate to do so. God gave us life. It is not for us to take that away prematurely by passively standing by as another does the deed.

The "Friends of the Bridge" should err on the side of life itself. To conserve and preserve our inestimable, precious and diverse human landscape as much as those lovely vistas Santa Barbara is so famous for, I don't think it's an either/or situation. I think it's possible to do both, which Santa Barbara is known to do very well.

I implore my community to do something — something effective.

The proposed call box is good, but not enough. Even suicidal people carry cell phones these days. If they just wanted to talk, they could do so at home and save themselves the ticket, towing and impound charges.

Tragic suicides belie peaceful setting

Being a Knight of Columbus as well as a past Los Padres National Forest officer compels me to speak out on the tragic deaths that have occurred on the Cold Spring Bridge since 1964. To date, 55 people have taken their lives by jumping off the bridge, bringing it increased notoriety on the South Coast.

Dating back to 1882, some of the tenets of the Knights of Columbus, founded by Father Michael J. McGivney, are giving aid to those in times of sickness or death, strengthening families and family life, and being of service to those in need. As a retired outdoor recreation management officer, it's distressing to accept that the soothing forest setting surrounding the bridge has become a locale in which distraught individuals are taking their lives.

What a sad commentary on our society when a solution proposed to prevent these deaths is opposed. The Friends of the Bridge are opposed to the placement of suicide barriers for aesthetic reasons. It is almost as though the "Friends" have a love affair with a steel-and-concrete structure. How can one compare the adulation of a manmade structure to the loss of even one life, much less the atrocious loss of 55 lives, with eight in 2009 and one this month?

The Friends argue that suicidal individuals still will take their lives, if not at the bridge then elsewhere, if the barriers are in place. However, it is precisely the isolated location of the bridge that serves to attract those desperate enough to take their lives. At other

locales, family, friends and even good samaritans are closer and have more context and immediate opportunity to intervene.

Another real cost of the bridge as it stands is the impact of each suicide on the lives of CHP, Sheriff's, and Forest Service personnel who must respond with red lights and siren at every call for assistance.

First, the emergency call increases the hazard on Highway 154, which is always a dangerous passage. Then, rescue personnel must risk their lives as they deal with the person in crisis, and/or must climb rough terrain to remove bodies. In performing these duties, the traumatic loss of each life weighs heavily on team members as well as on others.

One outrageous assertion the Friends make is that the proposed prevention measures would "waste more taxpayer money." How can anyone equate any amount of money to human life? Are there those in our society whose mental processes have been so twisted by the dollar that all compassion for human life has been put aside?

The proposal is made by the Friends that a better prevention measure would be the use of cameras and a call box. In an imminent life crisis, seconds and minutes are critical in saving a precious life.

The past loss of lives and the potential for future deaths dictate we as a caring community must correct this untenable situation.

Sam S. Alfano lives in Santa Barbara.

**VOICE FROM
SANTA BARBARA**
Sam S. Alfano

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA 154
2011

COMMENT CARD

NAME: Sally Stewart
 ADDRESS: 139 Roma/Highway CITY: SB ZIP: 93103
 REPRESENTING: Self

Do you wish to be added to the project mailing list? YES NO already on
 Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov
 The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

The beauty of the view from the bridge pales in comparison to the beauty of a life. The view can be enjoyed from many other places, but a life can only be lived once.
Let's do everything we can do to enjoy our bridge and its view while trying to save even one life.
A suicide barrier will enable us to know and ~~remember~~ remember the Cold Spring Arch Bridge as a beautiful structure as well as knowing lives have been protected & saved due to the simple addition of a barrier.

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: email from Caltrans & Glendon

Cold Spring Canyon Bridge

SUICIDE BARRIER 154

2011

COMMENT CARD

NAME: Mary Ann Hepp
 ADDRESS: 821 Grove Lane CITY: Santa Barbara CA 93105
 REPRESENTING: myself

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

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I would like the following comments filed in the record (please print): I attended the forum at San Marcos High. How many deaths will it take before a barrier is installed along the bridge? View, View, View - Santa Barbara is full of breath-taking views - yet people are concerned that a barrier will deny them of 10 seconds of driving view when there is a pull over Vista View Stopping point just up the road - perhaps they have never lost a loved one to suicide from that bridge. Sure, people will continue to commit suicide - maybe by jumping from our beautiful Stearns ward - but it is doubtful they will die instantly and would stand a chance of being safely rescued to be offered mental counseling that they desperately need. I recall listening to a speech made by a man who did survive his suicide attempt by jumping from the Golden Gate Bridge - He said that the moment he jumped he realized he made the wrong choice - He landed in water and was able to recover and now devotes his time to helping others who are mentally ill and depressed - every time someone jumps from Cold Springs Bridge, it endangers the safety of our rescue and recovery team - A barrier will make it difficult for someone who is not thinking clearly to move forward on a quick impulse. I am convinced that it WILL SAVE LIVES and help mentally ill people with a chance at recovery - so the next time someone jumps off the bridge I hope the people who still oppose the barrier are able to live with their own consciences and will still enjoy their precious view! Thank you!

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: NAMI

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Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA 154
2011

COMMENT CARD

NAME: W. Dibble Hoy
 ADDRESS: 6773 San Julian Dr CITY: Lompoc ZIP: 93136
 REPRESENTING: SBI and v self

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

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matt_c_fowler@dot.ca.gov

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 available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

I'm in Favor of a Barricade
 I have spent 17 years as a
 Suicide Counselor both in S. and
 Marin County.
 one suicide a year is
 Too much!!!

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA 154
 2011

COMMENT CARD

NAME: Christine Milner

ADDRESS: 555 Camino Ahumada CITY: SB ZIP: 93111

REPRESENTING: SELF

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

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 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

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matt_c_fowler@dot.ca.gov

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I would like the following comments filed in the record (please print):

I AM A FRIEND OF A YOUNG MAN WHO
COMMITTED SUICIDE ON COLD SPRING BRIDGE. HE WAS
GOING THROUGH A TOUGH TIME IN HIS LIFE & MADE A
RASH DECISION WITH TRAGIC CONSEQUENCES. HAD HE
BEEN STOPPED THAT MORNING, I BELIEVE HE WOULD HAVE
RECEIVED THE HELP HE NEEDED TO STAY ALIVE. WE
MISS HIM DEARLY. IF A BARRIER COULD PREVENT
THIS KIND OF HEARTACHE, IT WOULD BE WORTH IT
BEYOND WORDS.

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA
154
 2011

COMMENT CARD

NAME: Jean Sullivan
 ADDRESS: P.O. Box 6563 CITY: Santa Barbara ZIP: 93160
 REPRESENTING: self

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

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matt_c_fowler@dot.ca.gov

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I would like the following comments filed in the record (please print): I am in favor of building.

Please build a suicide barrier. Life is precious - -
worth more than a "pretty view."

Building the barrier, even if it only gives
someone intent on killing themselves, will give them
more to think about - even if it's only another place -
that gives more time for reflection, to get out of the funk
& make a choice for life.

My concern is also for the workers who have to
risk their lives and emotional health picking up
the multi lated, splats from the rocks below. We should
be looking out for our public safety's health & welfare too!
 (Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA 154
 2011

COMMENT CARD

NAME: Shantiel Ben-Aderet
 ADDRESS: 6711 El Coleto Rd. CITY: Groleta ZIP: 93117
 REPRESENTING: Self

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

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matt_c_fowler@dot.ca.gov
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I would like the following comments filed in the record (please print):

The argument against building a suicide barrier holds no significance. Although a bridge might alter the view, that holds no weight against lost lives within the Santa Barbara county community. I would always say if asked, "would you like a nice view OR a life saved?", I would say life saved. Also, adding a barrier does not destroy the history of the area, you can still see the area.

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____



COMMENT CARD

NAME: MARK P. BRICKLEY
 ADDRESS: 3375 FOOTHILL RD. CITY: CARPINTERIA ZIP: 93013
 REPRESENTING: SELF

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

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matt_c_fowler@dot.ca.gov
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I would like the following comments filed in the record (please print):

I support the completion of the Bridge suicide project.
It may be the most important state public works
project ever undertaken in Santa Barbara County. I am
the retired Juvenile Division manager of the Santa Barbara
County Probation Dept. During my tenure several of our
probationers left from the Bridge. One of our broke
Los Pinos Boys Camp staff helped "save" an adult from
jumping off the Bridge atop San Marcos Pass, on her
way to work. If this project saves one life it
will be successful. I will support this project in any way I can.

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA 154
2011

COMMENT CARD

NAME: Anna Campbell
 ADDRESS: 735 E. Vista CITY: San Rafael ZIP: 93103
 REPRESENTING: Self

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

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matt_c_fowler@dot.ca.gov

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I would like the following comments filed in the record (please print):

Please put up the suicide barrier as soon as possible. It only takes moments to drive over the bridge, and drivers should be watching the road. There is not even a place to view the bridge as a whole where the barrier would even be plainly seen! It is a high bridge crossing a ^{great} canyon and can only be viewed at a distance. It is a documented fact that most suicides are impulsive, and can be deterred

(Please respond by 5:00 p.m. January 24, 2011)
By prevention measures such as the barrier.

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R 
 2011

COMMENT CARD

NAME: Sarah Anides
 ADDRESS: _____ CITY: Santa Barbara ZIP: 93101
 REPRESENTING: self

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov
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 available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

Build it!

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____



Cold Spring Canyon Bridge

S U I C I D E B A R R I E R

2011

COMMENT CARD

NAME: ANDY CRANMER

ADDRESS: 5662 CALLE REAL #352 CITY: GOLETA ZIP: 93117

REPRESENTING: SELF

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov

The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

I FIND THE PROPOSED VERTICAL BARRIERS TO BE REASONABLY UN-OBTUSIVE,
THE LIFE SAVING FUNCTIONALITY FAR OUTWEIGHTS ANY VISUAL
IMPAIRMENT. THERE IS A SCENE VIEWPOINT JUST A FEW HUNDRED YARDS
AWAY IF ONE WISHES TO STOP AND ENJOY THE VIEW.
A FRIEND OF MINE STOPPED HIS CAR TO SEE IF A PEDESTRIAN
ON THE BRIDGE NEEDED HELP. THAT PEDESTRIAN JUMPED TO HIS
DEATH. MY FRIEND HAD NIGHTMARES FOR MONTHS. STOP THE DELAYS
AND ELECT A BARRIER FOR THE SAFETY OF THE WOULD BE SUICIDES,
LAW ENFORCEMENT PERSONNEL, SEARCH AND RESCUE VOLUNTEERS
AND THE MOTORING PUBLIC.

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Response to comments from:

- Sam S. Alfano, Retired Outdoor Management Officer, Los Padres National Forest, and 91 signature petition
- Sally Stewart
- Mary Ann Hepp
- W. Dibbler
- Christine Milner
- Jean Sullivan
- Shaniel Ben-Aderet
- Mark P. Brickley
- Anna Campbell
- Sarah Chalder
- Andy Cranmer

Thank you for your comments. Your support for the project has been noted.

Cold Spring Canyon Bridge
 S U I C I D E B A R R I E R CALIFORNIA
154
 2011

COMMENT CARD

NAME: Hyla Fetter

ADDRESS: 302 Palisades Dr. CITY: Santa Barbara ZIP: 93109

REPRESENTING: _____

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov
 The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

I am in favor of the no-build alternative, which I believe may actually save lives. According to your statistics, it appears that many more people have come on to the bridge with the intent to commit suicide than have actually jumped off. ~~But~~ These people clearly needed help, and there would be an opportunity then to help them. Many ways that people use to commit suicide (with a gun, with pills, with a rope) are done in a more private space without any chance for someone to see them and intervene.

For myself, and for many people, the feeling of driving over that bridge with its open, panoramic views is uplifting to the spirits. I think in this modern world we need all the uplift we can get. Looking through a cage-like mesh I find very depressing. I dislike traveling across the bridge on Foothill that crosses San Roque.
 (Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA
154
2011

COMMENT CARD

NAME: Richard Kelly
 ADDRESS: 1125 Nagai CITY: SB ZIP: 93103
 REPRESENTING: SELF

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov

The Draft Supplemental Environmental Impact Report is
 available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

I recommend the NO-BUILD ALTERNATIVE.
I fail to see (where) a compelling argument
was made to construct this project.

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: web.

Response to comments from:

- Hyla Fetler
- Richard Kelty

Your opposition to the project has been noted.



Cold Spring Canyon Bridge

S U I C I D E B A R R I E R

2011

COMMENT CARD

NAME: ANDREW HANKIN
 ADDRESS: 228 WEST VICTORIA ST. APT. 6 CITY: SANTA BARBARA ZIP: CA 93101
 REPRESENTING: SELF ALREADY ON MAILING LIST

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov
 The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

MY PREFERENCE IS FOR NO BARRIER BECAUSE:
 • THE STATE \$S BROKE (\$25 BILLION BUDGET DEFICIT) AND THE COUNTY HAS NO MONEY TO SPARE EITHER
 • SUICIDE PREVENTION IS NOT CALTRANS' JOB
 • THE BARRIER WILL TURN A BEAUTIFUL, AESTHETICALLY PLEASING BRIDGE INTO A PRISON CAGE.
 * IF SOMETHING MUST BE BUILT ON 154 TO AVOID LOSING FUNDING, HOW ABOUT THE LOWER CATCH-FENCE PROPOSED BY "FRIENDS OF THE BRIDGE" OR A RUNAWAY-TRUCK RAMP AT 154 & STATE STREET IN SANTA BARBARA (WHERE THERE WAS A TRIPLE FATALITY CRASH IN 2010).

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- 4

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Response to comments from Andrew Hankin

Your opposition to the project has been noted.

Response to comment #1: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #2: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #3: The DSEIR discusses in depth several types of horizontal net barriers and their effects on the Cold Spring Canyon Bridge. Five categories of horizontal net barriers [DSEIR, page 5] were considered as potential alternatives but eliminated from further discussion. Two refined designs were developed from these five categories of ideas: the Safety Net Alternative and the Cantilever Arc Barrier Alternative.

The Safety Net Alternative is developed in the Supplemental Report, which is included in the DSEIR, Attachment 20. The Supplemental Report discusses in depth the impacts to the historic integrity of the structure as well as the evaluation of the adverse effects of the proposed Safety Net Alternative.

The Cantilever Arc Barrier Alternative is developed in the Feasibility Study included in the DSEIR, Attachment 39. This design, along with the constraints, was developed in conjunction with the State Historic Preservation Officer. The Feasibility Study discusses the impacts to the structure, including the impacts to the historic integrity of the structure.

In summary, horizontal net barriers have been rejected for the following reasons:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide

3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

Response to comment #4: California Highway Patrol, in partnership with Caltrans is working towards improving truck safety on Route 154. This effort is outside the scope of the purpose and need for this project.



Cold Spring Canyon Bridge
S U I C I D E B A R R I E R
2011

COMMENT CARD

NAME: JARRELL JACKMAN

ADDRESS: 123 E CANON PERDIDO CITY: SB ZIP: 93101

REPRESENTING: SB TRUST FOR HIST. PRESERVATION

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov
 The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

THE SBTHP (SEE ABOVE) DOES NOT
SUPPORT THE ^{VERTICAL} BARRIERS BEING PROPOSED
FOR THE HISTORIC COLD SPRING BRIDGE.
THE TRUST VOTED TO SUPPORT THE
NET ALTERNATIVE OR SIMILAR SOLUTION
THAT WOULD PRESERVE THE VIEWSHED
FROM THE BRIDGE. - 154 IS A
SCENIC HIGHWAY & THE UOLLY BARRIERS
CALTRANS IS PROPOSING SHOULD NOT

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Response to comments from Jarrell Jackman

Your opposition to the project has been noted.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. Caltrans fully identified and assessed impacts to views from the Cold Spring Canyon Bridge in its analysis of project effects to the structure as a historical resource. The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR.



Cold Spring Canyon Bridge

S U I C I D E B A R R I E R

2011

COMMENT CARD

NAME: Holly Principe Joseph
 ADDRESS: 925 Mission Ridge CITY: Santa Barbara ZIP: 93103
 REPRESENTING: SELF

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov

The Draft Supplemental Environmental Impact Report is
 available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

As a hospice medical social worker working in end-of-life care, I am always assessing for suicidality. I have found that if people want to kill themselves they will find a way and have the right to end their life.

The bridge is a beautiful piece of art - the barrier would deface it - it is not worth the effort. People need to have the choice to end their life, if

(Please respond by 5:00 p.m. January 24, 2011) desired!

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Response to comments from Holly Principe Joseph

Your opposition to the project has been noted.

As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the Cold Spring Canyon Bridge resulting from individuals jumping off the bridge. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA 154
2011

COMMENT CARD

NAME: JIM MARINO
 ADDRESS: 1026 CAMINO DEL RIO CITY: S.B. ZIP: 93110
 REPRESENTING: SELF

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov
 The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

WHERE ARE THE STUDIES OR OTHER REPORTS
INDICATING THAT THE TYPE OF BARRIER FENCE
PROPOSED HAS EFFECTIVELY STOPPED OR ELIMINATED SUICIDES
FROM BRIDGES OF THIS TYPE?

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: TELEVISION 700

©2010 12.28 Caltrans_dje_eeb

Response to comments from Jim Marino

Chapter 2 of the 2008 DEIR (pages 17-21) and 2009 FEIR (pages 18-22), which are incorporated by reference in the DSEIR, discuss many studies by experts in the field that show the effectiveness of physical barriers.

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA 154
 2011

COMMENT CARD

NAME: JAMES G. MILLS

ADDRESS: 1660 FREDENSBORG WAY CITY: SOLVANG ZIP: 93463

REPRESENTING: SELF

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov

The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

STRICTLY AS A MATTER OF PRIORITY, AND RELATED TO MAKING
HIGHWAYS SAFER, OTHER THAN AN IMPROVED BARRIER FOR SAFETY OF
EMERGENCY PERSONNEL (CHP, SHERIFFS, FIRE) ON THAT BRIDGE, MONEY
WOULD BE BETTER SPENT IN ACTUALLY MAKING HIGHWAYS SAFER. A
MEDIAN BARRIER AT MUCH LESS EXPENSE WOULD HAVE SAVED A LIFE JUST 1/4 OF
A MILE EAST OF THE BRIDGE WHEN AN IMPAIRED DRIVER CROSSED INTO ONCOMING
TRAFFIC WHILE YOU WERE HUNG UP ON A PUBLIC HEALTH MATTER - SUICIDES!
A SAFETY RAMP FOR RUNAWAY TRUCKS COULD HAVE SAVED AN 8 YEAR OLD BOY
JUST A FEW MONTHS AGO (+ 2 ADULTS)
SEVERAL YEARS AGO NEAR SANTA CLARITA IN A DARK TUNNEL A MULTI CAR
CRASH TOOK ANOTHER YOUNG BOY WITH HIS WHOLE LIFE AHEAD OF HIM.
HOW LONG WILL IT BE BEFORE THE GAVIOTA NORTHBOUND TUNNEL MIGHT
HAVE SUCH A RESULT. WIDENING CUTS ON HIGHWAY 154 WITHERG
BICYCLISTS HAVE A FOOT WIDE STRIP SHOULD TAKE PRIORITY OVER THAT
9 FOOT OVERKILL PROJECT PROPOSED. IT IS A MATTER OF PRIORITY !!

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2

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Response to comments from James G. Mills

Your opposition to the project has been noted.

Response to comment #1: The current railing along the bridge is considered standard for the construction period of the structure. The suggestion of raising the existing bridge railings in combination with constructing a horizontal net barrier would likely cause an additional adverse effect by introducing a visual element that diminishes the property's historic integrity of design, feeling, and association. Increasing the height of the bridge rail would also not comply with the Secretary of Interior's Standards for Rehabilitation, Standards 2 and 9.

Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #2: There are other projects on Route 154 designed to improve safety along the route. Caltrans is currently developing a safety project for the 154/246 intersection. The alternatives being considered for the 154/246 intersection include a roundabout and a signalized intersection. Centerline rumble strips have just been installed (January, 2011) along various segments of Route 154. The 154 Group II operational improvements project was also just completed and includes elements such as turn pockets for left turn movements from Route 154. Also, the California Highway Patrol, in partnership with Caltrans, is working towards improving truck safety on Route 154. These efforts are outside the scope of the purpose and need for this project.

Cold Spring Canyon Bridge

S U I C I D E B A R R I E R CALIFORNIA
154



COMMENT CARD

NAME: GERALD ROUNDS
 ADDRESS: 4045 E OAK TR CITY: S.Y ZIP: 93460
 REPRESENTING: SELF

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov
 The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

*FIXED
to 549-3326
on 1-24-11*

I would like the following comments filed in the record (please print):

For the record, a family member jumped off the bridge in about 1989.
I am against any modifications to the bridge. The aesthetics should be preserved.
I do not believe that the funds can not be reallocated.
I do not believe the 'below the line of sight' option can not be done.
I do not believe it is the government's business to prevent suicides in this case. Regards, Gerald Rounds

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(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: VARIOUS MEDIA

Response to comments from Gerald Rounds

Response to comment #1: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #2: Your opposition to the project has been noted.

As discussed in the 2008 DEIR, the 2009 FEIR and 2019 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Response to comment #3: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Cold Spring Canyon Bridge
 S U I C I D E B A R R I E R CALIFORNIA 154
 2011

COMMENT CARD

NAME: KAREN WATSON
 ADDRESS: 179 NOGAL DR. CITY: SR ZIP: 93110
 REPRESENTING: SEEF

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov

The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

I DO NOT BELIEVE WE NEED A SUICIDE BARRIER. BECAUSE OF ALL THE DISCUSSION, THE NUMBER OF SUICIDES ON THE BRIDGE HAS INCREASED DRAMATICALLY ^{IN THE PAST ~3 YEARS,} IF WE STOP TALKING ABOUT IT, THE NUMBER WILL SETTLE DOWN. YES, PEOPLE WILL COMMIT SUICIDE, BUT TRYING TO SOLVE A SUICIDE PROBLEM BY WASTING MONEY AND DESTROYING A BEAUTIFUL + UNIQUE HISTORIC STRUCTURE IS WRONG! PUT THE MONEY INTO SOMETHING USEFUL FOR EVERYONE, NOT JUST A FEW MENTALLY-CHALLENGED / UPSET INDIVIDUALS.

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: THE INDEPENDENT

Response to comments from Karen Watson

Your opposition to the project has been noted.

As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA 154
 2011

COMMENT CARD

NAME: Judy L. Weisman
 ADDRESS: 2909 Puestadesl CITY: SB ZIP: 93105
 REPRESENTING: me

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov

The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

If there has to be a barrier, and I am not convinced
it really will save lives, it should be horizontal like
the one on the Golden Gate Bridge (interestly not
administered by CalTrans). Apart from everything
else how much revenue is lost because car
trucks ^{comercials} will no longer be filmed there?

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(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Response to comments from Judy L Weisman

Your opposition to the project has been noted.

Response to comment #1: The proposed grid/mesh alternative meets the project's purpose, to reduce the number of suicides at the Cold Spring Canyon Bridge resulting from individuals jumping from the bridge. The purpose is to reduce the number of suicides from the bridge.

Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

Response to comment #2: The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Cold Spring Canyon Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate

Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would. As discussed above, views from the Cold Spring Canyon Bridge are not a character-defining element of the historical resource, and the impacts of the vertical barrier are therefore limited to diminishing the bridge's integrity of design, feeling, and association, as compared to impacts of a safety net barrier.

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel

4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Response to comment #3: Your comment has been noted.

Cold Spring Canyon Bridge

S U I C I D E B A R R I E R



2011

COMMENT CARD

NAME: Teresa McNeil Maclean

ADDRESS: P.O. Box 1091 CITY: Santa Ynez ZIP: 93460

REPRESENTING: -self

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the Comment Box or

Mail to: ~~Cathy Stettler~~ MATT C. FOWLER
Senior Environmental Planner
Department of Transportation
50 Higuera St.
San Luis Obispo, CA 93401

I would like the following comments filed in the record (please print):

re: Cold Springs Bridge, Hwy. 154

As Cold Springs Bridge is both an historic scenic bridge, as well as the gateway to the Santa Ynez Valley, I would like to ask that if/when a suicide prevention structure is attached to the bridge, to please use the horizontal, rather than the vertical scheme. As an artist who has lived in Santa Ynez for over twenty-five years, often driving over this breath-taking structure with the gorgeous view of our valley below I ask you to please save our beautiful bridge structure and expansive view.

(Please respond by ~~June 24, 2008~~)
JANUARY 24, 2011

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: on-line news

Response to comments from Teresa McNeil MacLean

Your opposition to the project has been noted.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. Caltrans fully identified and assessed impacts to views from the Cold Spring Canyon Bridge in its analysis of project effects to the structure as a historical resource. The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations

7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the DSEIR.

One of the purposes of the Cold Spring Canyon Bridge Suicide Barrier project is to “Reduce the exposure to risks for emergency personnel...” In his comment letter (see Section 2.0 Santa Barbara County Government and Commission), Sheriff Bill Brown, the Santa Barbara County Sheriff Department Chief Law Enforcement Officer, who is responsible for responding to emergency calls for the Cold Spring Canyon Bridge, does not support the use of the horizontal alternatives (Safety Net or Cantilever Arc Barrier Net).

The Sheriff’s office has since provided clarification that it does not support the horizontal net alternative, even if used in conjunction with increasing the existing bridge rail height.

In designing a suicide barrier, Caltrans’ efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge’s substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that “the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county’s concern for public safety and preservation of the aesthetics of the bridge.”

Cold Spring Canyon Bridge

S U I C I D E B A R R I E R



2011

COMMENT CARD

NAME: NICOLE PEÑA
ADDRESS: 2950 Old Calzada Rd CITY: SANTA YNEZ ZIP: 93460
REPRESENTING: myself

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the Comment Box or

Mail to: ~~Cathy Stettler~~ MATT C. FOWLER
Senior Environmental Planner
Department of Transportation
50 Higuera St.
San Luis Obispo, CA 93401

I would like the following comments filed in the record (please print):

I AM OPPOSED TO CALTRANS' PROPOSED "VERTICAL" BARRIER.
I SUPPORT THE ALTERNATE "HORIZONTAL NET BARRIER"
CALTRANS MAKES AN ERROR IN TRYING TO ERECT A VERTICAL
BARRIER AS IT DETRACTS FROM OUR RIGHT TO THE
HISTORIC VIEWS ON OUR SCENIC HIGHWAY 154.
SEVERAL BRIDGES IN NEW YORK STATE AS WELL AS CALIFORNIA'S
OWN GOLDEN GATE HAVE CHOSEN TO INSTALL HORIZONTAL BARRIERS.
Thank you for your consideration

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(Please respond by ~~June 24, 2008~~
JANUARY 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Response to comments from Nicole Pena

Your opposition to the project has been noted.

Response to comment #1: A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. Caltrans fully identified and assessed impacts to views from the Cold Spring Canyon Bridge in its analysis of project effects to the structure as a historical resource. The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations

7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA 154
 2011

COMMENT CARD

NAME: Gerry B. Shepherd

ADDRESS: POB 30 CITY: Santa Ynez ZIP: 93466

REPRESENTING: Self

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov
 The Draft Supplemental Environmental Impact Report is
 available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

I AM OPPOSED TO CALTRANS PROPOSED VERTICAL BARRIER.

I SUPPORT THE "NO BUILD" OPTION (PREFERRED OPTION) OR, AS LESS PREFERRED ALTERNATE OPTION, "HORIZONTAL NET BARRIER".

AS A DESIGNATED CALIFORNIA SCENIC HIGHWAY, CALTRANS ERRS IN ERECTING A VERTICAL BARRIER THAT DETRACTS FROM OUR RIGHT TO THIS HISTORIC VIEWSHED.

THE GOLDEN GATE BRIDGE AUTHORITY HAS NOW ELECTED TO NOT ERECT A VERTICAL BARRIER BUT, INSTEAD, GO WITH A HORIZONTAL NET BARRIER.

(Please respond by 5:00 p.m. January 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Response to comments from Gerry B. Shepherd

Your opposition to the project has been noted.

The No-Build Alternative does not meet the purpose of this project and therefore has not been identified as the preferred alternative.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Cold Spring Canyon Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a

component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would. As discussed above, views from the Cold Spring Canyon Bridge are not a character-defining element of the historical resource, and the impacts of the vertical barrier are therefore limited to diminishing the bridge's integrity of design, feeling, and association, as compared to impacts of a safety net barrier.

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

Cold Spring Canyon Bridge
SUICIDE BARRIER CALIFORNIA 154

2011

COMMENT CARD

NAME: Larry & Susan Musgrove
ADDRESS: 211 Valhalla Dr. CITY: Solvang, CA ZIP: 93463
REPRESENTING: Private Citizens -

Do you wish to be added to the project mailing list? YES NO

Please drop comments in the Comment Box or

Mail to: ~~Cathy Stettler~~ MATT C. FOWLER
Senior Environmental Planner
Department of Transportation
50 Higuera St.
San Luis Obispo, CA 93401

I would like the following comments filed in the record (please print):

As frequent travelers on Hwy 154 between the S.V. Valley
and Santa Barbara, we were not happy about the plan for
a vertical suicide barrier for Cold Spring Canyon Bridge. We
feel that using the new plan of a horizontal barrier
under the bridge would be a win-win situation. It would
help alleviate the suicide problem and also
preserve the view from this important structure,
that serves as a gateway to the Santa Ynez Valley.
Susan & Larry Musgrove

(Please respond by ~~June 24, 2008~~
JANUARY 24, 2011)

How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____

Response to comments from Larry and Susan Musgrove

Your opposition to the project has been noted.

As discussed in the 2008 DEIR pages 10-11, the 2009 FEIR pages 11 and 12, and DSEIR pages 5-7, the net alternative was not considered feasible. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Cold Spring Canyon Bridge
S U I C I D E B A R R I E R CALIFORNIA 154
 2011

COMMENT CARD

NAME: EDWARD STANLEY BLOMFIELD
 ADDRESS: 129 OLIVER PL CITY: SANTA BARBARA ZIP: 93109
 REPRESENTING: MYSELF

Do you wish to be added to the project mailing list? YES NO
 Please drop comments in the comment box or

Mail to: **Matt C. Fowler**
 Senior Environmental Planner
 Department of Transportation
 50 Higuera Street
 San Luis Obispo, CA 93401

Your written comments can also be e-mailed to:
matt_c_fowler@dot.ca.gov
 The Draft Supplemental Environmental Impact Report is available online:
http://www.dot.ca.gov/dist05/projects/sb_cold_springs/index.html

I would like the following comments filed in the record (please print):

THERE IS NO NEED FOR ANY KIND OF SUICIDE BARRIER FOR Cold Springs Bridge. THE COST IS ENORMOUS. MY FRIEND MARK SOUZA JUMPED TO HIS DEATH LAST YEAR. HE WOULD HAVE KILLED HIMSELF ANYWAY. HE WAS DOWN ON HIS LUCK, NO JOB, HIS WIFE LEFT HIM. HE WAS HOMELESS. 33 HOMELESS people died. LAST YEAR IN S.B. THE MONEY CAL-TRANS WANTS TO SPEND SHOULD BE DIVERTED TO HELP THE HOMELESS. IT SEEMS TO ME NO ONE CARES IF THE HOMELESS die on the

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Streets (Please respond by 5:00 p.m. January 24, 2011)
 ONLY 54 people HAVE jumped to their death
 How Did You Hear About This Meeting? newspaper newsletter someone told me about it other: _____
 From the byline.
 Don't let CAL-TRANS hood-wink AND bully SANTA BARBARA County

2011 12 28 Callans_dje_eeb

Response to comments from Edward Stanley Blomfield

Your opposition to the project has been noted.

Response to comment #1: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the Purpose and Need in the Summary and in Chapter 1 of the 2009 FEIR for a complete discussion. Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 FEIR Report (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Response to comment #2: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Section 6.0 Transcripts from Public Hearings

The following transcripts are from the public hearings held in Santa Barbara on Wednesday, January 5, 2011. Responses to comments are provided at the end of the certified transcript copies.

1 SANTA BARBARA, CALIFORNIA

2 WEDNESDAY, JANUARY 05, 2011

3 -000-

4 PUBLIC COMMENTS AT 5:20 P.M.

5 NANCY K. DUNN: About 15 years ago I was very
6 depressed and contemplated suicide, and I was at Big Sur
7 at the time, and something told me to stay in the car,
8 do not get out of the car, because I knew that I would
9 jump if I was by the cliff.

10 I am opposed to ruining this beautiful bridge
11 because there are cliffs everywhere, and if you want to
12 commit suicide, you will find a way of doing that. I
13 believe the money would be better spent on prevention
14 and counseling programs for people rather than putting
15 up a barrier. My phone number is area code 805-455-6823
16 if anyone wants to contact me. You also can contact me
17 at NancyKDunn@Yahoo.com.

18 I'm afraid that maybe big business is involved
19 in this and would like this contract because someone
20 could make a very large amount of money off of this.

21 ANDREW FIRESTONE: My name is Andrew Firestone,
22 and I had the misfortune of witnessing a suicide on the
23 San Marcos bridge. I was driving back to Santa Barbara
24 from Santa Ynez and noticed a car that was pulling
25 forward and slowing down. And the sad thing was is that

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1 I knew right away what that person was doing. And it
2 wasn't that he had a flat tire. It wasn't -- in my
3 mind, those are the things that you would normally think
4 of. Any other stretch of road in the entire world, if I
5 saw a car slowing down, I would think they were having
6 engine trouble, or a flat tire, something like that, but
7 in that instant, I knew that that person was going to
8 jump off the bridge.

9 It has the history of that, unfortunately.
10 It's becoming a stigma with that bridge. And I saw
11 firsthand how quickly a person can take their own life
12 without even having to stop and consider it for a second
13 because it's so easy to jump from. And the moment that
14 I stopped -- at the same time they stopped, before I
15 could say anything to him or run over, he took three
16 steps out of the car and just jumped right off the
17 bridge.

18 I don't think that putting up a bridge is
19 going to cure suicide. I don't think that a fence can
20 cure somebody of their grief and despair, but what I
21 think it can do is for that stretch of road, give people
22 a moment of pause to not make it quite so attractive and
23 quite so easy and quite so romantic as it has been for
24 all these other years and all these many dozens of
25 people that have decided to take their lives there.

1 Also, I don't want one of the main
2 thoroughfares in and out of Santa Barbara into Santa
3 Ynez Valley, or the hundreds of thousands of visitors
4 who come and visit Santa Ynez valley, or Santa Barbara,
5 to be going over this bridge and getting chills because
6 they know of its history. And it is becoming a very
7 attractive place for people to take their lives.

8 Now, I don't think there's an attractive place
9 for people to take their lives, whether it's in their
10 homes, or off this bridge, or anywhere else. I don't
11 know how to prevent suicide, but I think it's our
12 obligation to prevent it where we can.

13 And this bridge is somewhere where we can make
14 a difference for this one stretch of road, for this one
15 place, and maybe give people a second to consider what
16 they're doing.

17 JAMES WATSON, JR: This barrier project should
18 be stopped and its energies and funding placed
19 elsewhere. To begin with, the barrier won't stop anyone
20 resolved to the act of ending their own life. They will
21 simply find another venue, or climb up the arch from
22 underneath, which is extremely easy to do. You can
23 practically walk up the underbelly of that bridge.

24 Secondly, the budget for this fiasco, before
25 it was put on hold the first time, was touted to be in

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1 the neighborhood of \$4 million -- I'll repeat that
2 figure -- \$4 million for a fence. This is not -- if
3 this is not another attempt by Caltrans to push through
4 a project, to justify their inflated budget for another
5 year, I don't know what is.

6 Taxpayer monies are better spent elsewhere,
7 perhaps on suicide prevention hotlines and other such
8 programs. A \$4 million fence. Give a privately-owned
9 and operated local contractor a welding torch,
10 materials, and five guys, and I'll bet they'll go out
11 there and have it done for a quarter million in less
12 than a week.

13 Lastly, at what point does our nation and our
14 government stop regulating to the point of lessening our
15 quality of life. This barrier project, should it be
16 allowed to go forward, will set a precedent for every
17 other bridge in the state, if not in the country.

18 Aside from being a means for getting from one
19 point to another, bridges are works of art. They are
20 landmarks, things of beauty to be appreciated. To
21 devalue the Cold Springs Arch Bridge by decimating its
22 aesthetic value would be a travesty.

23 Do we really want to see all our bridges, which
24 have forever been photographed and immortalized on
25 posters, postcards, maps, coffee table books, tourism

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1 guides, and the like, be marred by these unsightly
2 barriers? These are constructs that our state and
3 nation take pride in. What's next? The historic Bixby
4 bridge in Big Sur? Nutshell: It's ineffective; we
5 can't afford it; and it's ugly. Quite frankly, I can't
6 believe we're even here talking about it.

7 SARA VON SCHWIND: It is 5:30 and the meeting
8 is officially opened.

9 JERRY ROUNDS: I am against the suicide
10 barrier. I travel the road every day, almost every
11 day. I am impressed by the beauty that one sees only
12 from there. It is a beautiful scene that has been
13 immortalized by Elliott Porter, a famous photographer.
14 He's right up there with Ansel Adams. Truly.

15 I have had a cousin jump off the bridge. I
16 consider that a family tragedy and I do not look to
17 government to solve those types of problems. And I
18 guess that's the crux of it.

19 STEVE FEINBERG: I drive over the bridge all
20 the time, and I don't -- it's a bridge. I'm over it a
21 lot at night and stuff like that. I don't care about
22 the view, but I do care about how many people have
23 died. I've had friends who've tried to stop people from
24 jumping, and if they had the two seconds that the
25 barrier would have allowed them, they would have

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1 prevented somebody from jumping. That's mostly it.
2 So to me it is a very personal issue that for
3 this -- I'm more than willing to give up a view,
4 which -- I have a beautiful view. I have 13 miles of
5 beautiful view for 200 yards, or whatever the bridge is,
6 to save lives. And I don't buy the thing that Friends
7 have the statistics, and all that stuff, about, well,
8 they'll just go and jump somewhere else. That's not
9 true. And even if it saved one life, it's worth it.
10 And it's not an expensive thing either to put up. It's
11 very simple and easy, and this has been dragging on.
12 And I mean, personally, I think the person who is
13 objecting is -- I don't even want to say what I would
14 want to do to them. Legally, you can't. But for him to
15 value his view more than somebody's life I think is
16 incredibly selfish and self-centered. And that's my
17 thing.
18 So I am so for the project, I can't tell you.
19 I am so for it, 100 percent for this. I wish they would
20 do it already on the Golden Gate Bridge. All lives are
21 not worth losing.
22 JEAN SULLIVAN: I am in favor of suicide
23 prevention at the Cold Spring Bridge. Whenever I drive
24 by it, I think of the death of my friend's neighbor and
25 my daughter. She tried but she was not successful. And

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1 I'm glad she wasn't. There was something that kept her
2 from doing it, just a few more minutes to think. That's
3 all a lot of them really need, is that few more minutes
4 to think. Life is very precious. And I was a law
5 enforcement officer. I would not want to be the one to
6 have to see those splattered bodies on the rocks. And,
7 you know, if we can avoid having people risk their lives
8 to go down and pick up pieces of bodies off of the
9 rocks. I know a fireman here locally who was so
10 traumatized by the fire victims he saw, I can't imagine
11 this doesn't have an effect on our other rescue
12 personnel as well. It's hard to see any kind of death
13 and destruction. I don't want people to keep seeing
14 that over and over again and making it normal.

15 But that's all I have to say. Life is precious
16 and I am for putting in a barrier. It will be a lot
17 prettier if I'm not thinking about people jumping off of
18 it.

19 JERRY NATHAN: I'm a psychologist, and I know
20 that suicide prevention is a very important focus
21 inasmuch as it's a high mortality cause in our country
22 and in the world. And bridges seem to attract a lot of
23 people who are interested in taking the dramatic way
24 out, and this bridge has had its number of people
25 already using it for that purpose. And the effort to

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1 prevent this happening, it was recommended from
2 different sources and different people who tried to do
3 something about it, like Glendon Association played a
4 big part in trying to get the safety prevention -- the
5 safety barriers up.

6 And it's a beautiful place and I can
7 understand why people have in their minds that should at
8 all cost preserve the sanctity of this bridge and the
9 scenery, but it's ridiculous to be left this way to lead
10 to so many deaths unnecessarily.

11 And research has shown that people who have
12 been stopped from jumping off a bridge have not
13 attempted suicide in other ways. I am a staunch
14 believer in the necessity of having barriers on this
15 bridge, make it as attractive as possible, and do not
16 take away from the surroundings.

17 COLLEEN KELLY: Back in 2007 when the initial
18 meeting happened, I attended as an interested citizen
19 but also I attended because I thought a barrier might be
20 needed, but I also thought it would be more than
21 appropriate to have a design competition to design the
22 barrier as is done in other places in the world when
23 they have done this to other scenic bridges.

24 I'm an artist. I have entered design
25 competitions. I also do architectural work. And I know

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1 this sort of thing gets some innovative responses when
2 they put out calls for design competitions. It gets the
3 job done. All right. It gets it done beautifully and
4 it involves the community. I was very sorry to hear
5 that there was no design competition. Caltrans kept it
6 inhouse, which really irritates me that they didn't
7 throw it open for a design competition, and they did not
8 look at what the Golden Gate Bridge people did. They
9 did exhaustive studies which was that a barrier below
10 the roadway was the most effective way of dealing with
11 things and keeping the aesthetics.

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12 The Golden Gate Bridge people did many years
13 and millions of dollars worth of studies to come to this
14 conclusion. They approved it. It will be built when
15 they get the money. So why is Caltrans re-inventing the
16 wheel just because it's on a fast track for them,
17 because it keeps the work in-house? Why?

18 SAM ALFANO: Thank you. My name is Sam Alfano,
19 past recreation officer for the Los Padres National
20 Forest. And I'm very concerned that the bridge has
21 become a notable location for people to take their
22 lives. It's a wonderful forest setting. And a setting
23 of this kind should not be deterred by people who are
24 taking their lives.

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25 There's some concern about the aesthetics of

1 the barrier that is being proposed. Traveling at 45
2 miles an hour on the bridge takes 15 seconds to cross
3 the bridge. There is a maintained scenic site just on
4 the other side of the bridge from which you can see the
5 entire valley. Along with that, there's many, many
6 miles of Highway 154 that offer scenic views. The fact
7 that people are taking their lives off the bridge not
8 only affects their families and friends because of the
9 loss, but it also affects the possibility of loss of
10 life by the CHP, the sheriff's department, forest
11 service personnel, the folks who work for Caltrans. All
12 those lives are affected by this tragedy. Barriers have
13 been proven to be effective, and to say that we are
14 spending taxpayer's money, taxpayer monies uselessly, is
15 contrary to our beliefs, society, that we all should
16 pull together to save lives. So I'm for the project.

17 MARK STOUDE: My name is Mark Stouder. I'm
18 against the barrier. I feel that at this point in time,
19 we've got a lot of problems economically in the state,
20 and I think building fences to stop people from killing
21 themselves is not going to help, because we'd have to
22 fence everything. We fence one thing, then we'll have
23 to fence another thing. We'll have to fence all the
24 train tracks, all the bridges. We live in the
25 mountains. You can jump off everywhere. This is just

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1 one place to jump. And those millions of dollars should
2 go into the school system, the public health system
3 instead of just trying to fence people in. So that's my
4 opinion.

2

5 JAMES MARINO: My name is James Marino. My
6 concern is I have not seen, and I'm told it doesn't
7 exist, that there's any before-and-after study showing
8 that the number of suicides before there was a barrier
9 like this on a similar bridge anywhere has been reduced
10 by the construction of a fencing barrier like this. And
11 I'm told they don't have such a study and it wasn't in
12 the Environmental Impact Report, so I'm wondering how
13 they can conclude -- how Caltrans can conclude that this
14 barrier is going to reduce suicides.

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15 I guess the theory is that somehow common
16 sense is going to say, if you make it more difficult to
17 get over a fence, that you're going to stop people from
18 committing suicide. I'm not sure I get the connection.
19 Maybe they will go around the fence, who knows, I don't
20 know, or over it.

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21 I mean, the point I was making a moment ago,
22 they have barriers like that over these overcrossings on
23 the freeway. It doesn't seem to stop the kids that want
24 to spray graffiti on there. They seem to be able to get
25 around it okay. I guess if they wanted to jump, they

1 could jump. I think it's a little weak. That's all I
2 have to say right now.

3 RICHARD TUBIS: I'm definitely in favor of the
4 suicide barrier. I think the grid mesh alternative is
5 the better one, but I know that the barriers on other
6 bridges have prevented suicides, and I think it's
7 important that we have it on this bridge.

8 And also, people forget, if you ever go across
9 the Golden Gate Bridge in San Francisco, it's a
10 beautiful bridge and everything, and it still has
11 barriers on it, so -- and also, I notice from the
12 pictures, if you look at the bridge from a distance, you
13 can't really even see the barriers any way. So I think
14 the whole thing about destroying the beauty of the
15 bridge is overstated. It's a bridge. Human life is
16 more important than a bridge or more important than
17 somebody's aesthetic view of the bridge.

18 So I -- I really take a dim view of the people
19 who oppose it, but I'm very much for it, and you have my
20 support. I reside at 689 Via Miguel, Santa Barbara
21 93111. Phone is 805-680-4406.

22 RICHARD TUBIS: Hi. Richard Tubis again. I
23 wanted to add this to my earlier statement. I found out
24 there's no suicide barrier on the Golden Gate Bridge, so
25 eliminate the prior statement to the contrary from my

1

1 earlier statement.

2 MAUREEN SULLIVAN: I am strongly in favor of
3 having barriers on the bridge. I think it's insane that
4 there have been these delays. And I think it's really
5 sad because, in the meantime, I don't know, two or three
6 more people have died there in the delay.

7 We know from research that not only will it
8 stop people from jumping from that site but that in
9 places where they have barriers, the suicide rate goes
10 down in that whole region. And so it's not like people
11 go, "oh, the bridge is blocked. I'll jump somewhere
12 else." And I believe that barriers doing things like
13 this communicates to your community, we care about
14 everyone's life. "You may be in a crisis, but we're not
15 going to let you jump." You know? And I believe that
16 that has an impact on people's psyche.

17 What else do I want to say? I just think it
18 would be so tragic if this were -- if these few people
19 were allowed to hijack this project longer because
20 there's such untold devastation. One person jumping.
21 There's so many people that are traumatized and
22 anguished and never get over the pain, and we really --
23 with the right help, someone who is suicidal can go on
24 with a healthy life. They can get through the crisis.
25 And so I'm just praying for sanity to prevail here. And

1

1 I appreciate all that Caltrans is doing. I really feel
2 very supported by Caltrans that they want to do this.

3 SALLY HUGHES: My name is Sally Hughes. I'm a
4 marriage and family therapist in Santa Barbara, have
5 been for almost 20 years. And back in 1992 my son
6 committed suicide. He was a genius. And I know after
7 searching everything out, it wasn't off the bridge but
8 it was a fatality off a highway in a perfect suicide
9 spot.

10 But during my research I realize that he had
11 just come into a crisis moment, he called his father,
12 his father didn't understand, basically hung up on him,
13 and that was the end of him. We lost a genius. We lost
14 someone who could have talked to Einstein and who could
15 have solved many problems of the world.

16 But I have lived and I have done a tremendous
17 amount of research writing a book about him and what he
18 taught me from living and dying, and the stories are
19 happening to young children today.

20 And so the end of every story is if this is
21 happening to you, or someone you know, these are the
22 kind of things you can do to turn a negative to a
23 positive outcome.

24 And suicidology is really rampant among our
25 youth. Our teenagers are very, very sensitized right

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1 now. There's a lot of negativity in the world. There's
2 a lot of pain. They are picking it up. And when you
3 have one boy who gets his first B and jumps off the
4 bridge, and that happened a couple of years ago, that's
5 a boy who just didn't know any better, and he took a
6 moment and he made it permanent. That's exactly what my
7 son did, too.

8 And so what we aren't teaching in school that
9 we really need to address is social-emotional skills.
10 And if we balance that with IQ, we can turn all this
11 around. But in the meantime, we have to keep people
12 alive to make that kind of difference because they are
13 here as a gift to the world. But if they aren't allowed
14 to give that gift -- because it's the bright ones that
15 are going. The ones with kind of lower IQs are not so
16 prone to suicidology, so we are losing exactly the
17 people we need.

18 I am a Santa Barbara native. I'm a mother of a
19 son who did this, and I also am a marriage and family
20 therapist, and I'm writing a book about this. It's his
21 book and his gift to help people stay alive under any
22 circumstance.

23 So we need to prioritize life, period. When I
24 talked to Mr. McGinnis at Trader Joe's, he followed me
25 in and wanted to know who I was. He found out and he

1 said, oh, we're on opposite sides of the issue. I said,
2 yes, we are. And so then he went toe-to-toe with me on
3 every single issue. And finally, he said, well, the
4 bottom line is, beauty trumps life. And I said,
5 really? And I said, that's the most amazing statement I
6 think I've ever heard. And I want to say to you, I
7 couldn't possibly agree -- I positively disagree, and I
8 said, if that's true, you don't think life is
9 beautiful? You mean life isn't beautiful? It's not
10 precious?

11 If your own mother had thought that when she
12 had seen you for the first time, you wouldn't be here,
13 Mark. And about that time, he took off out the door.

14 And I feel like, you know, this is a case of
15 displaced priorities. I feel that the Friends of the
16 Bridge have systematically kept the right thing from
17 happening for many years now due to their insistence on
18 aesthetics over life, and I am absolutely unilaterally
19 opposed to their position.

20 And I really believe that Santa Barbara should
21 stand for life, not "Here's carte blanc. When you're in
22 a crisis, here, we'll give you the spot. No problem."
23 Because that spot magnetizes people in crisis because
24 they want to end their pain.

25 But if they have time, and that's what a

1 barrier does, it gives you time. It isn't as easy.
2 Killing yourself is not an easy thing to do. And so if
3 you have some time, that's when your body calms down
4 naturally from crisis, you start to breathe, things
5 start to, you know, calm down, and then usually people
6 can get to you and intervene. You can make calls. You
7 can do these things. And then all of a sudden, the
8 crisis is over.

9 And if people can know what to do, and that's
10 what this book is going to talk about, which is how to
11 intervene in situations like this. All my ex had to do
12 when he heard the call from my son -- and he didn't know
13 any better. He's innocent. He didn't do anything
14 wrong. He did the best he could do. But all you can
15 say is when somebody is making these words that are
16 scary, you say, "Are you thinking of killing yourself?"
17 And they will tell you. No matter what they say, you'll
18 get it. And then you can say, "Okay. I'm on my way."
19 And I don't care if I'm 2,000 miles away or 200 feet,
20 "I'm on my way. Do you promise not to do anything until
21 I get there"?

22 And that shows the other person that you care
23 and that you'll go the distance to be with that person
24 and take care of them. And if you don't do that,
25 there's other things we can do, but in the meantime, it

1 just takes a little shift like that that can change that
2 whole dynamic.

3 I've worked with suicidal people in San
4 Francisco from Santa Barbara, and I am very -- that was
5 20 years ago, and they are still alive because I was
6 successful when other people would have given up.

7 So I know the truth. And I know what it is.
8 And that person's now making \$300,000 a year, and I
9 don't think he'll ever be suicidal in this life.

10 So it's not a cut-and-dried thing. People are
11 human beings. We all go into crisis. We all need human
12 compassion and someone saying, "no, you're not. We will
13 do everything we can to make sure you don't." And that
14 is what this world needs to be like more than it is
15 today. And if Santa Barbara can't do something, I am
16 very sorry. I'm a native of this community, and that's
17 the bottom line. And so there you are.

18 And also Malcolm Gladwell, in his book,
19 "Outliers: The Story of Success," he addresses the
20 issue of suicide specifically and how magnets like our
21 bridges come into being. And he said -- he makes it
22 very clear. It's a moment. All you need is a slightly
23 effective intervention to give them the time, and it
24 will stop, and they will not kill themselves anyway.
25 That's a fiction. That's a myth. It's an urban myth

1 these days. It is not true. Outliers. He talks about
2 suicide in there. Thank you.

3 ASHLEY BRONZAN: I guess mostly I just wanted
4 to come and support the building of the barrier and I
5 guess the barrier alternative they had, that Caltrans
6 has come up with. They've minimized the visual impact
7 and yet at the same time, it still does look as though
8 it will help to prevent suicide. It looks to me that
9 they have done their research and I just really feel
10 strongly that it's important to provide that barrier
11 because it can be such a split-second or split-minute
12 decision. And I believe the research really does show
13 that people's plans are deterred. They don't really go
14 find another place.

15 JARRELL JACKMAN: My name is Jarrell Jackman.
16 I'm the executive director of the Santa Barbara Trust
17 for Historic Preservation. My organization is in
18 opposition to the proposed suicide barriers that are
19 being presented here tonight by Caltrans. They strongly
20 prefer an alternative, the horizontal approach to
21 constructing barriers that would protect the public and
22 preserve the view shed from the bridge. This is an
23 extremely important historic bridge, it's the most
24 important bridge in Santa Barbara County, and the
25 proposal will permanently deface its historic character,

1 and that really shouldn't happen.

2 ANNA CAMPBELL: Please put up the suicide
3 barrier as soon as possible. It only takes moments to
4 drive over the bridge. Highway 154 is a beautiful and
5 long highway. Drivers should be watching the road. And
6 passengers can see through the barrier. There is not
7 even a place to view the bridge as a whole where the
8 barriers could even be plainly seen, exclamation mark.
9 It is a high bridge crossing a great chasm and can only
10 be viewed at a distance.

11 It is a documented fact that most suicides are
12 impulsive and can be deterred by prevention measures
13 such as the barrier. Thank you.

14 JONI KELLY: My name is Joni Kelly, and I
15 strongly support the implementation of the Cold Spring
16 barriers -- Cold Spring Bridge barriers. I believe and
17 know that it will save lives in our community and that
18 the fallacies that are being put forth by Friends of the
19 Bridge are not true; that it will not merely divert
20 people to another location. Studies have proven this.
21 And that even if we save just one life, it will be worth
22 the cost. And that I know that Caltrans is doing
23 everything within their power to mitigate any aesthetic
24 implications of the barriers. And that if it's good
25 enough for the Eiffel Tower and the Empire State

1 Building, then it's good enough for our bridge and our
2 community.

3 DAN HOAGLAND: I think the main thing that I
4 want to say, after being here for this amount of time at
5 this meeting, it wasn't a meeting. It was kind of a
6 sham. Reading the boards that they have put up, they've
7 already rejected -- they say on the boards, they've
8 already rejected some of the alternatives that were put
9 before them. They won't or can't divulge any of the
10 information as far as the engineering work that was done
11 on the bridge to reject the net idea as far as
12 construction due to the fact that it is a homeland
13 security bridge. It's on the homeland security watch.
14 So any information about the construction of the bridge
15 and the reason that the engineers feel that it can't be
16 updated or done can't even be divulged to the public.

17 But I guess the reason I'm here is this was
18 not a public forum. We didn't get a chance to talk in a
19 public setting, and people weren't able to voice their
20 opinions. And I think that was wrong. And it seems
21 they're not an unbiased group at this point. They've
22 made up their minds. They want to put up the barriers
23 the way they want to put them up. And it seems that
24 they're just not listening to the public input at this
25 point.

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1 So the reason I came was I felt I would have
2 something to say from the research and work that I've
3 done in reading, and I'm not able to say a thing. It's
4 kind of a shame. I talked to a lot of really nice
5 people on both sides, but it doesn't seem that there's
6 any way that they are going to listen to any other
7 alternatives at this point. So it's -- it was a
8 disappointing meeting -- well, not really even a
9 meeting. It was a disappointing gathering. So there I
10 am.

11 PAMELA REEVES, M.D.: Well, I came out
12 tonight. I did not know that this was going on until I
13 happened to be, just by chance, at Cadwell's and picked
14 up a Daily Sound and saw that this was happening. And
15 I've lived in Santa Barbara for -- I moved here in
16 2002 -- almost nine years. I'm a practicing
17 psychiatrist. I moved here from Boston. I was on the
18 faculty at Harvard for 20-plus years before I moved
19 here. I worked at UCSB for about a year and a half and
20 then started private practice. I am married to Dr. Paul
21 Erickson, who is the Chief of Psychiatry at Cottage
22 Hospital. And the issue of a barrier across this bridge
23 has been a point of discussion professionally and
24 personally over the years for both of us.

25 Speaking for myself, I think it's imperative

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1 that we have a barrier of some kind. And I understand
2 and appreciate the beauty. That's the reason I moved
3 here. I'm from New Mexico. It's harder to get prettier
4 than that, so I truly appreciate that aspect. On the
5 other hand, when you've had 53 suicides since 1964, and
6 it's a 1400-foot span, to not do something is a
7 travesty. And yes, there is a cost. There's a fiscal
8 cost that's real. There's also a psychological cost.
9 Not just you lose the person who dies, the impact on
10 their families, the impact on the rescuers who go down
11 there and scrape the brains and bones off the rocks, and
12 some of those are volunteers, community volunteers.
13 They aren't necessarily trained for that sort of work.

14 And I've had the experience of trying to help
15 people after dealing with things like that as well as
16 having patients who have tried other means of suicide,
17 often very impulsively, and that's part of the problem
18 with this bridge. It lends itself to a very impulsive
19 jump, who come back later and say, I'm so glad I'm
20 alive. I felt like dying at the time, but I don't any
21 more. I've gotten help. My family has been
22 supportive.

23 The problem with depression is when you're
24 down in it, that's all you know. You think you're doing
25 the world a favor to kill yourself, the family a favor,

Response to comment #1 from Nancy K Dunn

Your opposition to the project has been noted.

Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the Draft Environmental Impact Report (pages 17-23) and FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #1 from Andrew Firestone

Thank you for your comment. Your support for the project has been noted.

Response to comments from James Watson, Jr

Your opposition to the project has been noted.

Response to comment #1: As stated in the 2009 FEIR, the purpose of the project is to reduce the number of suicides at the Cold Spring Canyon Bridge resulting from individuals jumping off the bridge. Because of suicides, the Cold Spring Canyon Bridge has the highest concentration of fatalities for any spot location on the state highway system in Caltrans District 5 (Santa Barbara, San Luis Obispo, Monterey, Santa Cruz, and San Benito counties).

Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 DEIR (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

Response to comment #2: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comment #3: Prior to the project activities being suspended in August of 2010 due to the court ruling, a private contractor was on board to construct the proposed barrier at cost of \$778,900 with a construction window of 75 working days.

Response to comment #4: Comment noted.

Response to comment #5: Caltrans acknowledges the uniqueness and character of the existing Cold Spring Canyon Bridge and that the installation of the barrier will constitute an adverse effect to the historic integrity of the bridge and result in substantial adverse impacts to the visual environment.

Response to comment #1 from Jerry Rounds

Your opposition to the project has been noted.

Response to comment #1 from Steve Feinberg

Thank you for your comment. Your support for the project has been noted.

Response to comment #1 from Jean Sullivan

Thank you for your comment. Your support for the project has been noted.

Response to comment #1 from Jerry Nathan, MD

Thank you for your comment. Your support for the project has been noted.

Response to comments from Colleen Kelly

Your comments on the project have been noted.

Response to comment #1: Caltrans did consider a design competition and chose to include the public in the design process through the use of an Aesthetics Design Advisory Committee which included non-Caltrans community representatives from the Santa Barbara County Historic Landmarks Advisory commission, architects, landscape architects, and County Public Works and Planning members as well as Caltrans experts. As a group, the committee concluded that if a barrier would be installed that the Grid/Mesh Alternative is the superior alternative with the least visual impacts that meets the project's purpose.

Response to comment #2: The analysis of the adverse effect that the horizontal net barrier alternatives would cause to the historic Cold Spring Canyon Bridge is presented in the Supplemental Report and in the Feasibility Report, both of which are included in the SEIR (as Attachments 20 and 39, respectively). This adverse effect would occur whether or not substructure retrofitting (physical modifications) would be required. As presented in the Feasibility Study, the kind of retrofitting necessary to address the added loads and changes in loads on this structure caused by the installation of horizontal net barriers would diminish the bridge's historic integrity even further than simply adding structures to the sides of the bridge. Horizontal net

barriers would entail changing the dimensions of substructure components like the bridge's columns, towers, or arch ribs. Cross bracing between substructure components could also be necessary during such a retrofit and would further diminish the bridge's integrity.

The comment takes note of the safety net alternative chosen for the Golden Gate Bridge's suicide barrier. The comment, however, does not account for two aspects of the suicide barrier approved for the Golden Gate Bridge. First, the environmental document for the Golden Gate Bridge suicide barrier concluded that all build alternatives, including its "Alternative 3" (the horizontal net barrier alternative), would cause an adverse effect to the historic bridge. Thus, choosing a horizontal net barrier over a vertical barrier for the Cold Spring Canyon Bridge does not avoid or eliminate a substantial adverse change to the historic structure. Second, the comment does not address the many substantial structural differences between the Golden Gate Bridge and the Cold Spring Canyon Bridge. The Golden Gate Bridge's main structure is a suspension bridge, which is a structure that has spans supported by cables draped from towers and connected to anchorages on either end of the bridge. The bridge deck is composed of a truss structure that is supported by vertical connections to the cables. The bridge deck carries the driving surface and sidewalks. Many of the Golden Gate Bridge's prominent character-defining features are situated at or above the road deck. The Cold Spring Canyon Bridge is a steel arch that, along with the columns and towers, supports the bridge deck's roadway. As noted in the HRER and Finding of Adverse Effect, almost all of the character-defining features of the Cold Spring Canyon Bridge are situated below the roadway deck.

The structural differences between the Golden Gate Bridge and Cold Spring Canyon Bridge are relevant because they explain why a safety net alternative is feasible on the Golden Gate Bridge and not feasible for the Cold Spring Canyon Bridge. The safety net on the Golden Gate Bridge will be attached to the bottom chord of the truss that carries the bridge deck, and retrofitting the structure's character-defining features is not necessary for the safety net's installation. The safety net will diminish the Golden Gate Bridge's character-defining features less than a vertical barrier will because it will be less intrusive to views of the bridge. Also, the net reduces impact to views from the bridge, which were considered to be a significant part of the history of that structure because evidence showed that views from the Golden Gate Bridge were a component of its original design. For the Cold Spring Canyon Bridge, however, a horizontal net barrier would diminish the bridge's character-defining features at the substructure more than a vertical barrier would. As discussed above, views from the

Cold Spring Canyon Bridge are not a character-defining element of the historical resource, and the impacts of the vertical barrier are therefore limited to diminishing the bridge's integrity of design, feeling, and association, as compared to impacts of a safety net barrier.

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Response to comment #1 from Sam Alfano

Thank you for your comment. Your support for the project has been noted.

Response to comment #1 from Mark Stouder

Your opposition to the project has been noted.

Response to comment #1: The purpose of the project is to reduce the number of suicides at the bridge resulting from individuals jumping off the bridge. Please refer to the 2009 FEIR Summary and Chapter 1 for a full discussion of the project's Purpose and Need.

Studies by suicidologists showing that physical barriers are effective in reducing suicides on bridges are referenced in the 2008 Draft Environmental Impact Report (pages 17-23) and 2009 FEIR (pages 18-25). Experts in the field of suicidology have

stated that there is evidence that people often do not go to another location to commit suicide, as documented in Chapter 2 of the 2009 FEIR.

Response to comment #2: Local Recovery Act funds have been designated specifically for the construction of this project. Caltrans, as an owner-operator of the bridge facility, has an obligation to promote the safe operation of the structure.

Response to comments from James Marino

Your comments on the project have been noted.

Response to comment #1: In the Alternatives section of the 2008 DEIR (page 6 and 7) and 2009 FEIR (pages 7 and 8), it is stated that experts in the field of suicidology and mental health recommend the following configuration to be the most effective method to reduce suicides on a bridge: the height should be a minimum of 6 feet; the top of the barrier should curve inward from 6 feet or higher; the pickets or members should be difficult to climb; and the barrier should run the entire span of the bridge.

Response to comment #2: The proposed grid/mesh alternative meets the project's purpose, to reduce the number of suicides at the Cold Spring Canyon Bridge resulting from individuals jumping from the bridge. The purpose is to reduce the number of suicides, not prevent all suicides from occurring.

Response to comment #1 from Richard Tubis

Thank you for your comment. Your support for the project has been noted.

Response to comment #1 from Maureen Sullivan

Thank you for your comment. Your support for the project has been noted.

Response to comment #1 from Sally Hughes

Thank you for your comment. Your support for the project has been noted.

Response to comment #1 from Ashley Bronzan

Thank you for your comment. Your support for the project has been noted.

Response to comment #1 from Jarrell Jackman

Your opposition to the project has been noted.

A horizontal net barrier would have greater impacts on the National Register eligible Cold Spring Canyon Bridge than the preferred alternative would. Caltrans fully identified and assessed impacts to views from the Cold Spring Canyon Bridge in its

analysis of project effects to the structure as a historical resource. The Historical Resources Evaluation Report (HRER) and Finding of Adverse Effect both present evidence and analysis regarding the design of the Cold Spring Canyon Bridge and whether views from the bridge should be considered as part of the historic structure's character-defining features. Conclusions in both the HRER and Finding of Adverse Effect are supported by evidence in each report and by appropriate application of the criteria of the National Register of Historic Places. As presented in the HRER, the Cold Spring Canyon Bridge was determined eligible under National Register Criterion C for its engineering design and not for social value pertaining to its ability to afford views to travelers in vehicles traversing the bridge. Unlike some bridges, including the Golden Gate Bridge, no amenities were included in the design of the Cold Spring Canyon Bridge to provide for views from the structure. The bridge was not built with sidewalks, belvederes, viewing platforms, or in conjunction with a vista point directly adjacent to the structure.

In designing a suicide barrier, Caltrans' efforts have focused on minimizing impacts to the Cold Spring Canyon Bridge's substructure. Although the grid/mesh alternative will still result in adverse impacts to the bridge, it has been identified as the preferred alternative because it minimizes impacts to the character-defining features of the bridge. As the steward of our facilities, Caltrans has taken its historic preservation responsibilities seriously in proposing a barrier design that respects those aspects of the bridge that best express its significance and its National Register eligibility. The Advisory Council on Historic Preservation has given their opinion that "the fence-type barrier proposed by Caltrans strikes a reasonable balance between the county's concern for public safety and preservation of the aesthetics of the bridge."

It is incorrect to assume that a horizontal net would have no impacts on the Cold Spring Canyon Bridge or fewer impacts than the preferred alternative would. On the contrary, the installation of a horizontal net barrier would have adverse impacts to the bridge by obscuring portions of the substructure and by requiring a type of retrofit that would change the fundamental engineering of the structure – the engineering that makes the bridge eligible.

As discussed in the 2008 DEIR, the 2009 FEIR and 2010 DSEIR, the net alternative was not considered feasible on the Cold Spring Canyon Bridge. The horizontal safety net alternative, including the design variations of the net, was considered but rejected for the reasons summarized below:

1. Unacceptable rescue response time
2. Increased danger to individuals attempting suicide
3. Unacceptable risk to emergency response and rescue personnel
4. Increased impacts to the historic substructure
5. Liability associated with an attractive nuisance
6. Design load limitations
7. Unacceptable risks associated with safety net maintenance
8. Increased costs associated with need to replace safety net.

Additional discussion on each of these points is provided on pages 6 and 7 of the SEIR.

Response to comment #1 from Anna Campbell

Thank you for your comment. Your support for the project has been noted.

Response to comment#1 from Joni Kelly

Thank you for your comment. Your support for the project has been noted.

Response to comment #1 from Dan Hoagland

The open forum public hearing for the DSEIR was conducted in accordance with the California Environmental Quality Act (CEQA) and Caltrans policies. The open forum format provided an opportunity for attendees to meet individually with Caltrans staff members in order to ask questions and discuss the project, view exhibits and handouts, and make environmental comments and formal statements on the CEQA document to the certified court reporter, in writing on comment forms, or after the hearing by U.S. Mail, email, or personal delivery.

Response to comment #1 from Pamela Reeves, MD

Thank you for your comment. Your support for the project has been noted.