

DEPARTMENT OF TRANSPORTATION

DIVISION OF ENGINEERING SERVICES

Office of Structural Materials

Quality Assurance and Source Inspection



Bay Area Branch
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Contract #: 04-0120F4Cty: SF/ALA Rte: 80 PM: 13.2/13.9File #: 69.28**WELDING INSPECTION REPORT****Resident Engineer:** Siegenthaler, Peter**Address:** 333 Burma Road**City:** Oakland, CA 94607**Report No:** WIR-026019**Date Inspected:** 01-Aug-2011**Project Name:** SAS Superstructure**OSM Arrival Time:** 700**Prime Contractor:** American Bridge/Fluor Enterprises, a JV**OSM Departure Time:** 1900**Contractor:** Zhenhua Port Machinery Company, Ltd (ZPMC), Changxing Island **Location:** Shanghai, China

CWI Name:	N/A	CWI Present:	Yes	No
Inspected CWI report:	Yes No N/A	Rod Oven in Use:	Yes	No N/A
Electrode to specification:	Yes No N/A	Weld Procedures Followed:	Yes	No N/A
Qualified Welders:	Yes No N/A	Verified Joint Fit-up:	Yes	No N/A
Approved Drawings:	Yes No N/A	Approved WPS:	Yes	No N/A
		Delayed / Cancelled:	Yes	No N/A
Bridge No:	34-0006	Component:	OBG Trial Assembly	

Summary of Items Observed:

On this date Caltrans OSM Quality Assurance (QA) Inspector Mr. S. Manjunath Math was present during the time noted above for observations relative to the work being performed.

This QA Inspector randomly observed the following work in progress:

Orthotropic Box Girder (OBG) at Trial Assembly Areas

Segment 14 East (Anchorage Plate)

This QA Inspector witnessed final bolt tension verification on bolts connecting angle plate connecting the Anchorage plate and full height Longitudinal Diaphragm at elevations 4750mm from Bottom Panel at work point E4 (Cross Beam side) at work point E3 (Bike Path side) between Panel Points (PP) 124.5 to PP 125 for Segment 14 East. Inspected the bolt tensioning on a random basis and found the tension to be in general compliance. Inspection was performed against the Notification No. 00724 dated August 01, 2011.

The bolt sizes used were M24 x 85 RC Lot # DHGM240016 and the final torque value established was 490 N-m.

The bolt sizes used were M24 x 110 RC Lot # DHGM240023 and the final torque value established was 523 N-m.

The bolt sizes used were M27 x 95 RC Lot # DHGM270035 and the final torque value established was 647 N-m.

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The bolt sizes used were M27 x 120 RC Lot # DHGM270020 and the final torque value established was 847 N-m.

The Manual Torque wrench used was Serial No. XO2-600.

Segment 14 East (Anchorage Plate)

This QA Inspector witnessed final bolt tension verification on bolts connecting angle plate connecting the Anchorage plate and full height Longitudinal Diaphragm at elevations 4750mm from Bottom Panel at work point E4 (Cross Beam side) at work point E3 (Bike Path side) and work point between E3 and E4 between Panel Points (PP) 125 to PP 127 for Segment 14 East. Inspected the bolt tensioning on a random basis and found the tension to be in general compliance. Inspection was performed against the Notification No. 00724 dated August 01, 2011.

The bolt sizes used were M22 x 80 RC Lot # DHGM220140 and the final torque value established was 380 N-m.

The bolt sizes used were M22 x 90 RC Lot # DHGM220141 and the final torque value established was 353 N-m.

The bolt sizes used were M22 x 95 RC Lot # DHGM220142 and the final torque value established was 367 N-m.

The bolt sizes used were M22 x 100 RC Lot # DHGM220049 and the final torque value established was 493 N-m.

The Manual Torque wrench used was Serial No. XO2-600.

Please reference the pictures for more comprehensive details.

Segment 14 East (Deck Panel Diaphragm)

This QA Inspector witnessed final bolt tension verification on bolts connecting doubler plate installed at Deck Panel diaphragm total 9 numbers between work point E3 and E20. Inspected the bolt tensioning on a random basis and found the tension to be in general compliance. Inspection was performed against the Notification No. 00728 dated August 01, 2011.

The bolt sizes used were M24 x 80 RC Lot # DHGM240121 and the final torque value established was 487 N-m.

The Manual Torque wrench used was Serial No. XO2-600.

Please reference the pictures for more comprehensive details.

Unless otherwise noted, all work observed on this date appeared to generally comply with applicable contract documents.

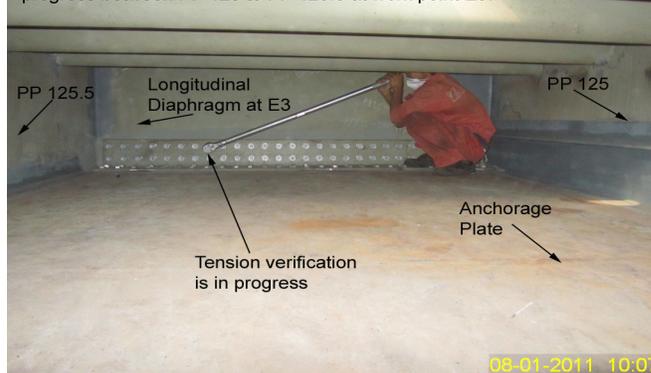
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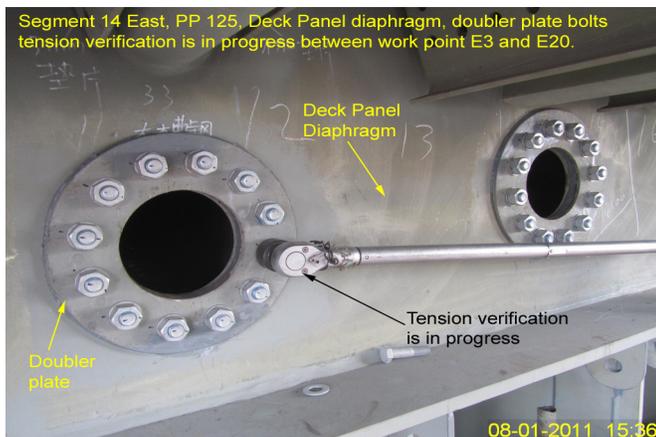
Segment 14 East, PP 125, Deck Panel diaphragm, doubler plate bolts tension verification is in progress between work point E3 and E20.



Segment 14 East, Angle splice plate connecting full Height Longitudinal Diaphragm (LD) and Anchorage plate (AP) bolts tension verification is in progress between PP 125 to PP 125.5 at work point E3.



Segment 14 East, PP 125, Deck Panel diaphragm, doubler plate bolts tension verification is in progress between work point E3 and E20.



Segment 14 East, Angle splice plate connecting full Height Longitudinal Diaphragm (LD) and Anchorage plate (AP) bolts tension verification is in progress between PP 126 to PP 126.5 at work point E3.



Summary of Conversations:

No relevant conversations were reported on this date.

Comments

This report is for the purpose of determining conformance with the contract documents and is not for the purpose of making repair or fit for purpose recommendations. Should you require recommendations concerning repairs or remedial efforts please contact Eric Tsang 15000422372, who represents the Office of Structural Materials for your project.

Inspected By: Math,Manjunath

Quality Assurance Inspector

Reviewed By: Miller,Mark

QA Reviewer