

DEPARTMENT OF TRANSPORTATION
 DIVISION OF ENGINEERING SERVICES
 Office of Structural Materials
 Quality Assurance and Source Inspection



Bay Area Branch
 690 Walnut Ave. St. 150
 Vallejo, CA 94592-1133
 (707) 649-5453
 (707) 649-5493

Contract #: 04-0120F4
 Cty: SF/ALA Rte: 80 PM: 13.2/13.9
 File #: 69.25B

QUALITY ASSURANCE -- NON-CONFORMANCE REPORT

Location: Changxing Island, Shanghai, P.R. China

Report No: NCR-000872

Prime Contractor: American Bridge/Fluor Enterprises, a JV

Date: 23-Oct-2010

Submitting Contractor: Zhenhua Port Machinery Company, Ltd (ZPMC), Changxing Island

NCR #: ZPMC-0834

Type of problem:

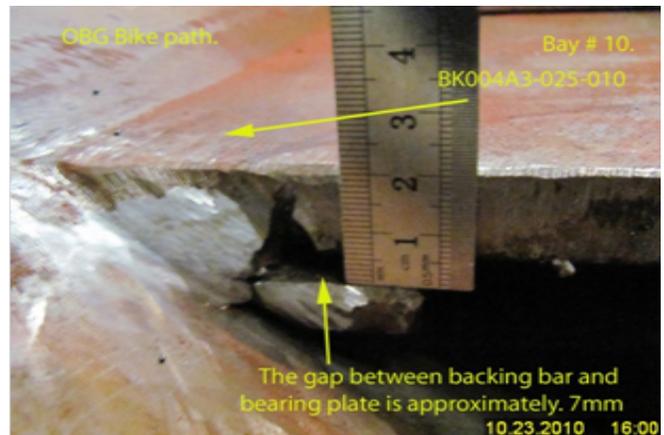
Welding	Concrete	Other	
Welding	Curing	Procedural	Bridge No: 34-0006
Joint fit-up	Coating	Other	Component: OBG Bike path. (Lift 8E)
Procedural	Procedural	Description:	

Reference Description: Steel backing was not properly placed and held after welding; ZPMC had inspected and accepted the weld subsequently

Description of Non-Conformance:

During the Caltrans Quality Assurance (QA) in-process observations of the fabrication of OBG Bike path plate BK004A-025 (Lift 8E), this QA Inspector discovered the following issue:

- Steel backing was not properly placed and held in intimate contact with the base metal.
- The gap between steel backing and bearing stiffener plate is observed approximately 7mm.
- The weld is Complete Joint Penetration (CJP) with steel backing joining the Bearing Plate and End Plate (side plate) of bike path.
- The weld is identified as: BK004A3-025-010.
- The member is located in bay #10.
- The Notice of Witness Inspection Number (NWIT) is 07078. ZPMC had inspected this weld prior to submitting the NWIT.



QUALITY ASSURANCE -- NON-CONFORMANCE REPORT

(Continued Page 2 of 2)

Applicable reference:

-AWS D1.5 2002, Section 3.13.5: "Steel backing shall be placed and held in intimate contact with the base metal. The maximum gap between steel backing and the base metal at the weld root shall be 2 mm."

-AWS D1.5 2002, Section 3.13.2: "Groove welds made with the use of steel backing shall have the weld metal thoroughly fused with the backing. Steel backing shall be continuous for the full length of each weld made with backing."

Who discovered the problem: Shailesh Gaikwad

Name of individual from Contractor notified: Liu Chen

Time and method of notification: 1645 hours, 10-23-2010, Verbal

Name of Caltrans Engineer notified: Laraine Woo

Time and method of notification: 0700 hours, 10-25-2010, Email

QC Inspector's Name: Qiu Wen

Was QC Inspector aware of the problem: Yes No

Contractor's proposal to correct the problem:

Comments:

This report is for the purpose of determining conformance with the contract documents and is not for the purpose of making repair or fit for purpose recommendations. Should you require recommendations concerning repairs or remedial efforts please contact Jim Devey, (86) 150-0002-6784, who represents the Office of Structural Materials for your project.

Inspected By:	Tsang, Eric	SMR
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Reviewed By:	Devey, Jim	SMR
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DEPARTMENT OF TRANSPORTATION - District 4 Toll Bridge

333 Burma Road
Oakland CA 94607
Tel: Fax:

NON-CONFORMANCE REPORT TRANSMITTAL

To: AMERICAN BRIDGE/FLUOR, A JV
375 BURMA ROAD
OAKLAND CA 95607

Date: 26-Oct-2010
Contract No: 04-0120F4
04-SF-80-13.2 / 13.9

Dear: Mr. Charles Kanapicki
Attention: Mr. Thomas Nilsson Project/Fabrication Manager
Subject: NCR No. ZPMC-0834

Job Name: SAS Superstructure
Document No: 05.03.06-000828

Reference Description: Steel backing was not properly placed and held after welding; ZPMC had inspected and accepted the weld subsequently

The attached Non-Conformance Report describes an occurrence where the contractor did not comply with a requirement of the contract document as indicated below:

- Material or Workmanship not in conformance with contract documents.
- Quality Control (QC) not performed in conformance with contract documents.
- Recurring QC issue that constitutes a systematic problem in quality control.
- Non-Conformance Resolved.

Material Location: Bike Path

Lift:

Remarks:

- During the Caltrans Quality Assurance (QA) in-process observations of the fabrication of OBG Bike path plate BK004A-025 (Lift 8E), this QA Inspector discovered the following issue:
- Steel backing was not properly placed and held in intimate contact with the base metal.
 - The gap between steel backing and bearing stiffener plate is observed approximately 7mm.
 - The weld is Complete Joint Penetration (CJP) with steel backing joining the Bearing Plate and End Plate (side plate) of bike path.
 - The weld is identified as: BK004A3-025-010.
 - The member is located in bay #10.
 - The Notice of Witness Inspection Number (NWIT) is 07078. ZPMC had inspected this weld prior to submitting the NWIT.

Action Required and/or Action Taken:

Propose a resolution for the identified non-conformance with revised procedures to prevent future occurrences. A response for the resolution of this issue is expected within 7 days.

Transmitted by: Laraine Woo Transportation Engineer

Attachments: ZPMC-0834

cc: Rick Morrow, Gary Pursell, Peter Siegenthaler, Stanley Ku, Brian Boal, Jason Tom, Contract Files, Ching Chao, Bill Casey

File: 05.03.06

NCR PROPOSED RESOLUTION

To: CALTRANS - SAS Superstructure
333 Burma Road
Oakland CA 94607

Attention: Siegenthaler, Peter
Resident Engineer

Ref: 05.03.06-000828

Subject: NCR No. ZPMC-0834

Dated: 11-Nov-2010

Contract No.: 04-0120F4
04-SF-80-13.2 / 13.9

Job Name: SAS Superstructure

Document No.: ABF-NPR-000834 Rev: 00

Contractor's Proposed Resolution:

Reference Resolution: NDT and VT have been performed on the weld area where the backing bar was removed and the results are acceptable

NDT and VT have been performed on the weld area where the backing bar was removed and the results are acceptable. To prevent future occurrences, the inspector fit up inspection will verify that the backing bar is tacked sufficiently to ensure that it will remain in place during welding. ZPMC has written an internal NCR to this effect to inform their inspectors.

Submitted by: Ishibashi, Joshua

Attachment(s): ABF-NPR-000834R00;

Caltrans' comments:

Status: CLO

Date: 14-Nov-2010

The proposed resolution is acceptable. This NCR is closed.

Submitted by: Woo, Laraine

Date: 14-Nov-2010

Attachment(s):



TRANSMITTAL LETTER

PROJECT: S.F.O.B.B.

DATE:2010-11-11

TO: ROSEMARY/ABF JV QA DEPARTMENT

FROM: ZPMC QA DEPARTMENT

SUBJECT: OBG NCR

SUBMITTED FOR YOUR APPROVAL AND SUBMITTAL TO CALTRANS

ENCLOSED WITH THIS TRANSMITTAL IS ONE COPY OF

(01) LR: No. B-927

(02) NCR-000872(ZPMC-0834)

MT & VT Reports (After removing backing bar)

PLEASE SIGN THIS TRANSMITTAL AND RETURN TO ME.

ACKNOWLEDGEMENT

PLAN HOLDER:

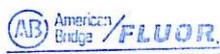
[Signature]

RECEIVED 11 NOV 2010

DATE:

16-18

COMPANY:



PHONE NO.

PLAN NUMBER:N/A

#R787-QCP-102



No. B-927

LETTER OF RESPONSE

TO: American Bridge/Flour

DATE: 2010-09-06

REGARDING: NCR-000872(ZPMC-0834)

ZPMC acknowledged this problem and has issued an internal NCR. ZPMC has removed the backing bar and checked MT. ZPMC is providing the VT and MT report for removal the backing bar area. Based on this, ZPMC is requesting closure of this NCR.

ATTACHMENT:

NCR-000872(ZPMC-0834)

MT & VT Reports (After removing backing bar)

zhuywei
11/11/10

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Location: Changxing Island, Shanghai, P.R. China

Report No: NCR-000872

Prime Contractor: American Bridge/Fluor Enterprises, a JV

Date: 23-Oct-2010

Submitting Contractor: Zhenhua Port Machinery Company, Ltd (ZPMC), Changxing Island

NCR #: ZPMC-0834

Type of problem:

Welding **Concrete** **Other**

Welding **Curing** **Procedural** **Bridge No:** 34-0006

Joint fit-up **Coating** **Other** **Component:** OBG Bike path. (Lift 8E)

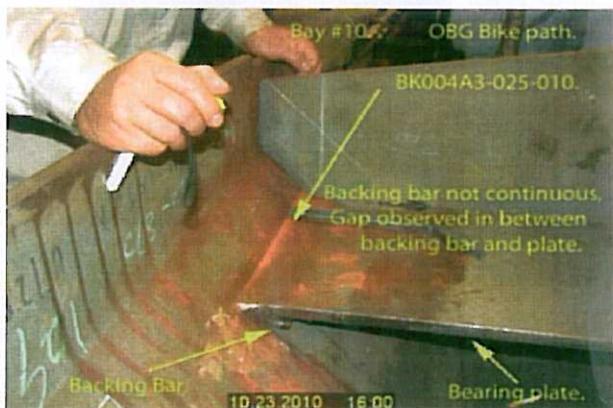
Procedural **Procedural** **Description:**

Reference Description: Steel backing was not properly placed and held after welding; ZPMC had inspected and accepted the weld subsequently

Description of Non-Conformance:

During the Caltrans Quality Assurance (QA) in-process observations of the fabrication of OBG Bike path plate BK004A-025 (Lift 8E), this QA Inspector discovered the following issue:

- Steel backing was not properly placed and held in intimate contact with the base metal.
- The gap between steel backing and bearing stiffener plate is observed approximately 7mm.
- The weld is Complete Joint Penetration (CJP) with steel backing joining the Bearing Plate and End Plate (side plate) of bike path.
- The weld is identified as: BK004A3-025-010.
- The member is located in-bay #10.
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QUALITY ASSURANCE -- NON-CONFORMANCE REPORT

(Continued Page 2 of 2)

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Who discovered the problem: Shailesh Gaikwad

Name of individual from Contractor notified: Liu Chen

Time and method of notification: 1645 hours, 10-23-2010, Verbal

Name of Caltrans Engineer notified: Laraine Woo

Time and method of notification: 0700 hours, 10-25-2010, Email

QC Inspector's Name: Qiu Wen

Was QC Inspector aware of the problem: Yes No

Contractor's proposal to correct the problem:

Comments:

This report is for the purpose of determining conformance with the contract documents and is not for the purpose of making repair or fit for purpose recommendations. Should you require recommendations concerning repairs or remedial efforts please contact Jim Devey, (86) 150-0002-6784, who represents the Office of Structural Materials for your project.

Inspected By:	Tsang, Eric	SMR
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Reviewed By:	Devey, Jim	SMR
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Contract #: 04-0120F4Cty: SF/ALA Rte: 80 PM: 13.2/13.9File #: xx.25A**QUALITY ASSURANCE -- NON-CONFORMANCE RESOLUTION****Location:** Changxing Island, Shanghai, P.R. China**Report No:** NCS-000831**Prime Contractor:** American Bridge/Fluor Enterprises, a JV**Date:** 14-Nov-2010**Submitting Contractor:** Zhenhua Port Machinery Company, Ltd (ZPMC), Changxing Island **NCR #:** ZPMC-0834**Type of problem:**

Welding	Concrete	Other	
Welding	Curing	Procedural	Bridge No: 34-0006
Joint fit-up	Coating	Other	Component:
Procedural	Procedural	Descriptor:	

Date the Non-Conformance Report was written: 23-Oct-2010**Description of Non-Conformance:**

During the Caltrans Quality Assurance (QA) in-process observations of the fabrication of OBG Bike path plate BK004A-025 (Lift 8E), this QA Inspector discovered the following issue:

- Steel backing was not properly placed and held in intimate contact with the base metal.
- The gap between steel backing and bearing stiffener plate is observed approximately 7mm.
- The weld is Complete Joint Penetration (CJP) with steel backing joining the Bearing Plate and End Plate (side plate) of bike path.
- The weld is identified as: BK004A3-025-010.
- The member is located in bay #10.
- The Notice of Witness Inspection Number (NWIT) is 07078. ZPMC had inspected this weld prior to submitting the NWIT.

Contractor's proposal to correct the problem:

Contractor will perform NDT and VT to prove the weld area where the backing bar was removed, and was not adversely affected. Contractor ensure the backing bars are tacked sufficiently in fit-up to ensure the backing bars are in tack during welding. Internal NCR will be issued to the inspectors.

Corrective action taken:

NDT and VT were performed on the weld area where the backing bar was removed, and the results are acceptable. Contractor issued the internal NCR to the inspector.

Did corrective action require Engineer's approval? Yes No**If so, name of Engineer providing approval:****Date:****Is Engineer's approval attached?** Yes No**Comments:**

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QUALITY ASSURANCE -- NON-CONFORMANCE RESOLUTION

(Continued Page 2 of 2)

of Structural Materials for your project.

Inspected By: Ng,Michael

Quality Assurance Inspector

Reviewed By: Wahbeh,Mazen

QA Reviewer
