

DEPARTMENT OF TRANSPORTATION

DIVISION OF ENGINEERING SERVICES

Office of Structural Materials

Quality Assurance and Source Inspection



Bay Area Branch
690 Walnut Ave. St. 150
Vallejo, CA 94592-1133
(707) 649-5453
(707) 649-5493

Contract #: 04-0120F4Cty: SF/ALA Rte: 80 PM: 13.2/13.9File #: 69.25B**QUALITY ASSURANCE -- NON-CONFORMANCE REPORT****Location:** Changxing Island, Shanghai, PRC**Report No:** NCR-000069**Prime Contractor:** American Bridge/Fluor Enterprises, a JV**Date:** 15-Feb-2008**Submitting Contractor:** Zhenhua Port Machinery Company, Ltd (ZPMC), Changxing Island**NCR #:** ZPMC-0067**Type of problem:**

Welding	Concrete	Other	
Welding	Curing	Procedural	Bridge No: 34-0006
Joint fit-up	Coating	Other	Component: DP-058
Procedural	Procedural	Descriptor:	OBG Deck Panel

Description of Non-Conformance:

The Contractor performed weld repairs without prior Engineer approval. The Contractor repaired closed ribs to box shell plate using by grinding that was not previously approved by the Engineer. This occurred on weld defects in multiple locations of the SAW cover pass of closed ribs U-64 (weld #1) and U-133 (weld #10) on OBG deck panel DP-058.

Applicable reference:

Special Provisions, Section 10-1.59, Steel Structures, Subsection Welding of Closed Ribs to Box Shell Plate, page 328: "Repair welding methods and procedures shall be approved by the Engineer."

Who discovered the problem: Mike Hasler**Name of individual from Contractor notified:** Ady Velasco**Time and method of notification:** Verbal notification at 19:00 on 02-15-2008**Name of Caltrans Engineer notified:** Jason Tom**Time and method of notification:** Verbal notification on 02-15-2008**QC Inspector's Name:** ZPMC QC Mr. Fu Yu Hong**Was QC Inspector aware of the problem:** Yes No**Contractor's proposal to correct the problem:****Comments:**

This report is for the purpose of determining conformance with the contract documents and is not for the purpose of making repair or fit for purpose recommendations. Should you require recommendations concerning repairs or remedial efforts please contact Patrick Lowry, 916-227-5719, who represents the Office of Structural Materials for your project.

Inspected By: Lowry,Patrick

Quality Assurance Inspector

Reviewed By: Lowry,Patrick

QA Reviewer



DEPARTMENT OF TRANSPORTATION
666 Feng Bin Road Room 708, Changxing Island
Shanghai 201913 PR China
Tel: 021-56856666 ext 207061 Fax:

NON-CONFORMANCE REPORT TRANSMITTAL

To: American Bridge/Fluor Enterprises, a JV
375 Burma Road
Oakland CA 94607

Date: 20-Feb-2008

Contract No: 04-0120F4
04-SF-80-13.2 / 13.9

Attention: Dave Williams Consultant

Job Name: SAS Superstructure

Document No: 05.03.06-000037

Subject: NCR No. ZPMC-0067

The attached Non-Conformance Report describes an occurrence where the contractor did not comply with a requirement of the contract document as indicated below:

Non-Conformance with quality control procedures. Please take appropriate corrective action. Recurrence may require further administrative action.

Non-Conformance with quality control. Records indicate that this is a recurring issue and constitutes a systematic problem in quality control. Within fourteen (14) days, please submit a plan to correct the deficiency.

Material or workmanship is not in compliance with contract requirements.

Enclosed please find the following items:

NCR No: ZPMC-0067

Remarks:

Please see attached Non-Conformance Report for details.

Transmitted by: Stanley Ku Sr. Bridge Engineer

cc: Rick Morrow, Gary Pursell, Stanley Ku, Brian Boal, Jason Tom, Dave Williams

File: 05.03.06

NCR PROPOSED RESOLUTION

To: CALTRANS - SAS Superstructure
333 Burma Road
Oakland CA 94607

Attention: Pursell, Gary
Resident Engineer

Ref: 05.03.06-000037

Subject: NCR No. ZPMC-0067

Dated: 19-Aug-2008

Contract No.: 04-0120F4
04-SF-80-13.2 / 13.9

Job Name: SAS Superstructure

Document No.: ABF-NPR-000074 **Rev:** 00

Contractor's Proposed Resolution:

Reference Resolution: Contractor repaired closed ribs to box shell plate using methods not previously approved by the engineer. Fabricator generated root pass repair procedure submitted to the State 7-15-08, SUB. 634R4

Please see attached letter.

Submitted by: Kanapicki, Charles

Attachment(s): ABF-NPR-000074R00;

Caltrans' comments:

Status: REJ

Date: 10-Sep-2008

The response is not acceptable. Submittal 634R04 referenced in this NPR has been Returned for Correction. Please provide documentation of the actual repair performed, and documentation that the required Non-Destructive Testing of the repaired area is acceptable.

Submitted by: Wright, Doug

Attachment(s):

Date: 10-Sep-2008

August 19, 2008

Reply to: SL-ABF-08-0169

Attention: Gary Pursell

Reference: San Francisco Oakland Bay SAS Bridge Superstructure
Caltrans Contract No. 04-0120F4
ABF Job No. 660110

Subject: NCR# ZPMC-0067

Mr. Pursell:

This letter is issue to provide formal response addressing the proposed resolution to NCR# ZPMC-0067.

The Contractor repaired closed ribs to box shell plate using repair methods and procedures that were not previously approved by the engineer.

The fabricator has generated a root pass repair procedure. This repair procedure was submitted to the State on 15 July 2008 submittal No. 634R4

The repairs performed on the root pass as stated above were performed per the same methods and WPS that are documented within the Root Pass repair procedure.

Please feel free to contact me directly if you require assistance or further clarification.

Sincerely,



Nate Lindell
Quality Assurance Manager
AMERICAN BRIDGE / FLUOR JV

cc: file

NCR PROPOSED RESOLUTION

To: CALTRANS - SAS Superstructure
333 Burma Road
Oakland CA 94607

Attention: Pursell, Gary
Resident Engineer

Ref: 05.03.06-000037

Subject: NCR No. ZPMC-0067

Dated: 24-Sep-2008

Contract No.: 04-0120F4
04-SF-80-13.2 / 13.9

Job Name: SAS Superstructure

Document No.: ABF-NPR-000074 **Rev:** 01

Contractor's Proposed Resolution:

Reference Resolution: ZPMC requests closure of this NCR based on the attached documentation.

See attached.

Submitted by:

Attachment(s): ABF-NPR-000074R01;

Caltrans' comments:

Status: REJ

Date: 15-Oct-2008

The response is not acceptable. Revision 0 of this NPR asked for documentation of the repair performed, and documentation that the required NDT of the repaired area is acceptable. The attachments have no documentation showing what repair procedure was used to perform the repair. Please provide the documentation of the repair performed along with the NDT of the repaired areas.

Submitted by: Wright, Doug

Date: 16-Oct-2008

Attachment(s):



No. B-295

LETTER OF RESPONSE

TO: American Bridge/Flour JV

DATE: 2008-8-28

REGARDING:

NCR-000069/000070/000078/000114/000095/000098/000097/000115/000118/000084/000123

With this letter of response, ZPMC requests closure for Caltrans NCR-000069/000070/000078/000114/000095/000098/000097/000115/000118/000084/000123 (ZPMC-0067/0068/0076/0110/0093/0096/0095/0111/0113/0082/0119). All of the non-conformance reports describe it is that zpmc repair the deck panel in the work process without received the engineer approval. As considered the welding continuity in the workshop, we directly repaired the defects which caused by the cease-arc, so that the panel fabrication can keep on performing. But not waiting for the CWR report approval for a few days. At the same time we had provided the repair procedure for welding process to government's approval. Then ZPMC can repair any defects in process following that procedure.

Furthermore we also attach the correlative VT and NDT reports which after repair and the final inspection, So that it's can prove those deck panel have reached the special requirements.

So ZPMC considers

NCR-000069/000070/000078/000114/000095/000098/000097/000115/000118/000084/000123 can be closed.

Please reference attached documentation for acceptance and closure the NCR-000069/000070/000078/000114/000095/000098/000097/000115/000118/000084/000123.

ATTACHMENT:

NCR-000069/000070/000078/000114/000095/000098/000097/000115/000118/000084/000123 (ZPMC-0067/0068/0076/0110/0093/0096/0095/0111/0113/0082/0119)

VT and MT reports which in the repair process

FVT and final MT reports for the deck panels

Zhuo Shuangbao

2008.8.28.

Thomas W Lamb
ABF QCM
9 SEPT. 08



DEPARTMENT OF TRANSPORTATION
666 Feng Bin Road Room 708, Changxing Island
Shanghai 201913 PR China
Tel: 021-56856666 ext 207061 Fax:

NON-CONFORMANCE REPORT TRANSMITTAL

To:	American Bridge/Fluor Enterprises, a JV 375 Burma Road Oakland CA 94607	Date:	20-Feb-2008
Attention:	Dave Williams Consultant	Contract No:	04-0120F4 04-SF-80-13.2 / 13.9
Subject:	NCR No. ZPMC-0067	Job Name:	SAS Superstructure
		Document No:	05.03.06-000037

The attached Non-Conformance Report describes an occurrence where the contractor did not comply with a requirement of the contract document as indicated below:

- Non-Conformance with quality control procedures. Please take appropriate corrective action. Recurrence may require further administrative action.
- Non-Conformance with quality control. Records indicate that this is a recurring issue and constitutes a systematic problem in quality control. Within fourteen (14) days, please submit a plan to correct the deficiency.
- Material or workmanship is not in compliance with contract requirements.

Enclosed please find the following items:

NCR No: ZPMC-0067

Remarks:

Please see attached Non-Conformance Report for details.

Transmitted by: Stanley Ku Sr. Bridge Engineer

cc: Rick Morrow, Gary Pursell, Stanley Ku, Brian Boal, Jason Tom, Dave Williams

File: 05.03.06

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Contract #: 04-0120F4

Cty: SF/ALA Rte: 80 PM: 13.2/13.9

File #: 69.25B

QUALITY ASSURANCE -- NON-CONFORMANCE REPORT

Location: Changxing Island, Shanghai, PRC

Report No: NCR-000069

Prime Contractor: American Bridge/Fluor Enterprises, a JV

Date: 15-Feb-2008

Submitting Contractor: Zhenhua Port Machinery Company, Ltd (ZPMC), Changxing Island

NCR #: ZPMC-006

Type of problem:

Welding Concrete Other
 Welding Curing Procedural Bridge No: 34-0006
 Joint fit-up Coating Other Component: DP-058
 Procedural Procedural Descriptor: OBG Deck Panel

Description of Non-Conformance:

The Contractor performed weld repairs without prior Engineer approval. The Contractor repaired closed ribs to box shell plate using by grinding that was not previously approved by the Engineer. This occurred on weld defects in multiple locations of the SAW cover pass of closed ribs U-64 (weld #1) and U-133 (weld #10) on OBG deck panel DP-058.

Applicable reference:

Special Provisions, Section 10-1.59, Steel Structures, Subsection Welding of Closed Ribs to Box Shell Plate, page 328: "Repair welding methods and procedures shall be approved by the Engineer."

Who discovered the problem: Mike Hasler

Name of individual from Contractor notified: Ady Velasco

Time and method of notification: Verbal notification at 19:00 on 02-15-2008

Name of Caltrans Engineer notified: Jason Tom

Time and method of notification: Verbal notification on 02-15-2008

QC Inspector's Name: ZPMC QC Mr. Fu Yu Hong

Was QC Inspector aware of the problem: Yes No

Contractor's proposal to correct the problem:

Comments:

This report is for the purpose of determining conformance with the contract documents and is not for the purpose of making repair or fit for purpose recommendations. Should you require recommendations concerning repairs or remedial efforts please contact Patrick Lowry, 916-227-5719, who represents the Office of Structural Materials for your project.

Inspected By: Lowry, Patrick

Quality Assurance Inspector

Reviewed By: Lowry, Patrick

QA Reviewer

NCR PROPOSED RESOLUTION

To: CALTRANS - SAS Superstructure
333 Burma Road
Oakland CA 94607

Attention: Pursell, Gary
Resident Engineer

Ref: 05.03.06-000037

Subject: NCR No. ZPMC-0067

Dated: 30-Oct-2008

Contract No.: 04-0120F4
04-SF-80-13.2 / 13.9

Job Name: SAS Superstructure

Document No.: ABF-NPR-000074 **Rev:** 02

Contractor's Proposed Resolution:

Reference Resolution: ZPMC performed external repairs to welds by grinding prior to submitting a Repair Procedure.

To close this NCR ZPMC is attaching the submitted Repair Procedure, final MT and VT reports for the repair performed. Repairs were performed by grinding as stated in the procedure. See attached procedure.

Submitted by:

Attachment(s): ABF-NPR-000074R02; Back up doc to close NCR ZPMC-0067

Caltrans' comments:

Status: CLO

Date: 03-Nov-2008

The proposed resolution is acceptable. The repair procedure was submitted as part of ABF-SUB-634R06, and approved as noted. Also, the welds in question were accepted as shown on the attached NDT reports. The Department concurs that Non-Conformance ZPMC-0067 is closed.

Submitted by: Wright, Doug

Date: 03-Nov-2008

Attachment(s):

DEPARTMENT OF TRANSPORTATION
 DIVISION OF ENGINEERING SERVICES
 Office of Structural Materials
 Quality Assurance and Source Inspection

Bay Area Branch
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 (707) 649-5453
 (707) 649-5493

Contract #: 04-0120F4

City: SF/ALA Rte: 80 PM: 13.2/13.9

File #: 69.25B

QUALITY ASSURANCE -- NON-CONFORMANCE REPORT

Location: Changxing Island, Shanghai, PRC

Report No: NCR-000069

Prime Contractor: American Bridge/Fluor Enterprises, a JV

Date: 15-Feb-2008

Submitting Contractor: Zhenhua Port Machinery Company, Ltd (ZPMC), Changxing Island

NCR #: ZPMC-006

Type of problem:

Welding Concrete Other
 Welding Curing Procedural Bridge No: 34-0006
 Joint fit-up Coating Other Component: DP-058
 Procedural Procedural Description: OBG Deck Panel

Description of Non-Conformance:

The Contractor performed weld repairs without prior Engineer approval. The Contractor repaired closed ribs to box shell plate using by grinding that was not previously approved by the Engineer. This occurred on weld defects in multiple locations of the SAW cover pass of closed ribs U-64 (weld #1) and U-133 (weld #10) on OBG deck panel DP-058.

Applicable reference:

Special Provisions, Section 10-1.59, Steel Structures, Subsection Welding of Closed Ribs to Box Shell Plate, page 328: "Repair welding methods and procedures shall be approved by the Engineer."

Who discovered the problem: Mike Hasler

Name of individual from Contractor notified: Ady Velasco

Time and method of notification: Verbal notification at 19:00 on 02-15-2008

Name of Caltrans Engineer notified: Jason Tom

Time and method of notification: Verbal notification on 02-15-2008

QC Inspector's Name: ZPMC QC Mr. Fu Yu Hong

Was QC Inspector aware of the problem: Yes No

Contractor's proposal to correct the problem:

Comments:

This report is for the purpose of determining conformance with the contract documents and is not for the purpose of making repair or fit for purpose recommendations. Should you require recommendations concerning repairs or remedial efforts please contact Patrick Lowry, 916-227-5719, who represents the Office of Structural Materials for your project.

Inspected By: Lowry, Patrick

Quality Assurance Inspector

Reviewed By: Lowry, Patrick

QA Reviewer

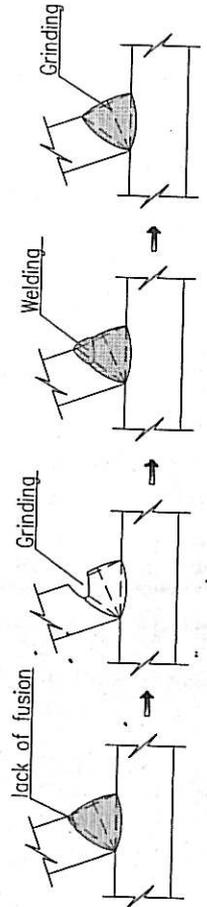
2.2.1 Prior to repair, remove all the weld slags, and clear the welds and adjacent base metal surface.

2.2.2 Lack of fusion will be removed by grinding. MT will be performed to ensure the removal of all defects.

2.2.3 Choose either WPS (WPS-345-FCAW-2G(2F)-repair-1 or WPS-345-SMAW-2G(2F)-repair) to deposit the additional weld metal. Minimum deposit weld size shall follow the corresponding WPS.

2.2.4 Grind flush with the adjacent base metal or weld surface.

2.2.5 Perform MT to the repaired area.

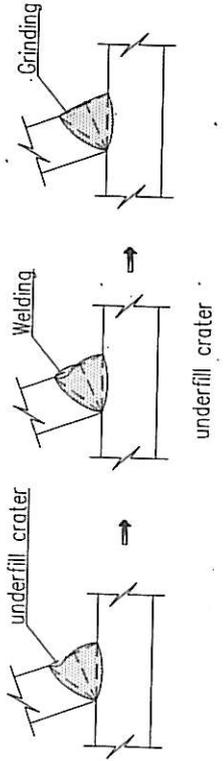


lack of fusion

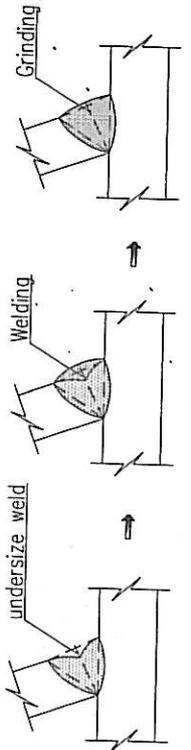
2.3 If undercut defects are found, take actions as following:

2.3.1 When undercut values ≤ 0.25 mm, no actions are needed.

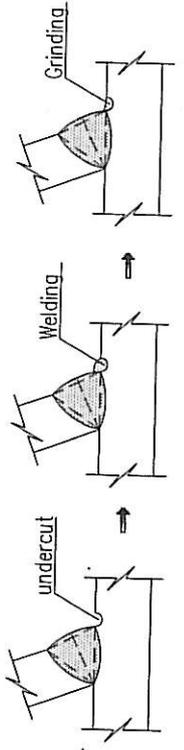
2.3.2 When undercut values > 0.25 mm, clear the weld surface to prepare for weld repair, and repair the welds according to step 2.2.



underfill crater



undersize weld



undercut



SHANGHAI ZHENHUA PORT MACHINERY CO., LTD.

SAN FRANCISCO OAKLAND BAY BRIDGE, EAST SPAN SELF-ANCHORED SUSPENSION SPAN
 ROUTE 80 EAST OF YERBA BUENA ISLAND, DISTRICT 04, SF COUNTY, CA.
 BRIDGE Nos.: 34-0006L PM 8.2, KP 13.2 & 34-0006R PM 8.7, KP 13.9
 STATE OF CALIFORNIA DEPT. OF TRANSPORTATION CONTRACT NO. 04-0120F4
 ENGINEER: CALTRANS ----- T.Y.LIN / MOFFATT & NICHOL, JV.
 CONTRACTOR: AMERICAN BRIDGE / FLUOR ENTERPRISES, JV.

NO.	DATE	REMARKS	BY
			CHECKED
		REVISIONS	

repair procedure for closed, fib welds	
DRAWN	SHEET NO.
CHECKED	RP-002
	ORDER NO.

2.4 Repair for defects occurring during GMAW root welding process, such as pores, incomplete fusion and cracks, etc.:

2.4.1 Remove all the weld slags, and clear the welds and adjacent base metal surface.

2.4.2 Remove the defects by grinding, and perform MT to ensure removal of all defects for pores or incomplete fusion, for crack locations, 50mm either side if applicable inline with WQCP.

2.4.3 Preheat and reweld the repaired area according to the approved repair procedure: WPS-B-T-2342-U1(U-rib)-4 or

WPS-345-FCAW-2G(2F)-Repair-1 (when the repair length is less than 300mm) minimum pre-heat to be 65°C.

2.4.4 Grind flush with the adjacent base metal or weld surface.

2.4.5 Perform MT to the repaired area.

2.4.6 Repaired area to be distinctly indentified on the closed rib for later NDT.

2.5 Repair of cracked tack welds prior to depositing the root run:

2.5.1 The extent of the crack to be determined by MT.

2.5.2 Carefully remove the crack by grinding in accordance with AWS D1.5 Section 3.7.2.4.

2.5.3 If the tack has to be reinstated check root gap to ensure that the gap is no greater than 0.5mm. Weld inline with 2.5.4.

2.5.4 Perform the tack weld according to the approved WPS:

WPS-B-T-2342-U2(U-rib).

2.5.5 Grind transition at each end of the tack weld.

2.5.6 VT/MT of the tack weld.

Note: If removal of crack causes root gaps exceeding 0.9 mm, refer Item 3.2 for repair.



SHANGHAI ZHENHUA PORT MACHINERY CO., LTD.

SAN FRANCISCO OAKLAND BAY BRIDGE, EAST SPAN SELF-ANCHORED SUSPENSION SPAN

ROUTE 80 EAST OF YERBA BUENA ISLAND, DISTRICT 04, SF COUNTY, CA.

BRIDGE Nos.: 34-0006L PM 8.2, KP 13.2 & 34-0006R PM 8.7, KP 13.9

STATE OF CALIFORNIA DEPT. OF TRANSPORTATION CONTRACT NO. 04-0120F4

ENGINEER: CALTRANS ----- T.Y. LIN / MOFFATT & NICHOL, JV.

CONTRACTOR: AMERICAN BRIDGE / FLUOR ENTERPRISES, JV.

repair procedure for closed Rib Welds

NO.	DATE	REMARKS	BY	REVISIONS
DRAWN				
CHECKED				
SHEET NO.			ORDER NO.	
RP-003				

3. Interior weld defects:

3.1 For all interior defects and indications, perform the following procedure.

3.1.1 Prepare excavation (defect length + 50mm each side)

- Preheat to 65 °C and carbon-arc gouge (using a 6mm diameter carbon-arc rod) or grind the weld to a depth of 5 mm thru 8 mm (if ground no. preheat required).

- Finish grind to bright metal surface to a depth of 9.6+1/-0mm. (Refer to Item 3.2 for repair of over grind areas)

- Measure excavation depth with a template.

- The contour and profile of excavation shall be suitable for welding.

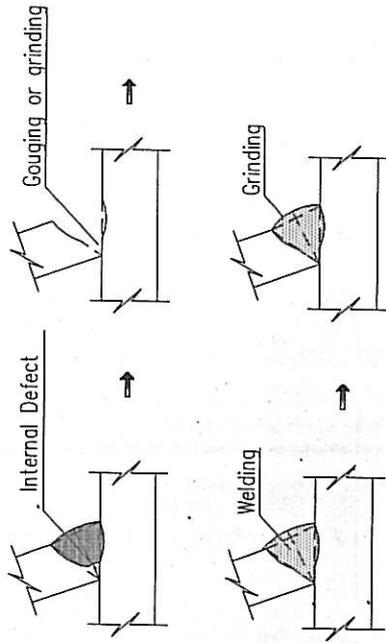
3.1.2 Weld excavation

- Preheat to minimum of 100°C
- Weld the excavation according to repair procedure WPS-345-FCAW-2G(2F)-repair-1
- MT root pass

3.1.3 Grind repair flush with base metal or

adjacent weld surface flush with base metal or no sharper than 1:10.

3.1.4 100% VT, MT and UT



All Internal Defect Repairs



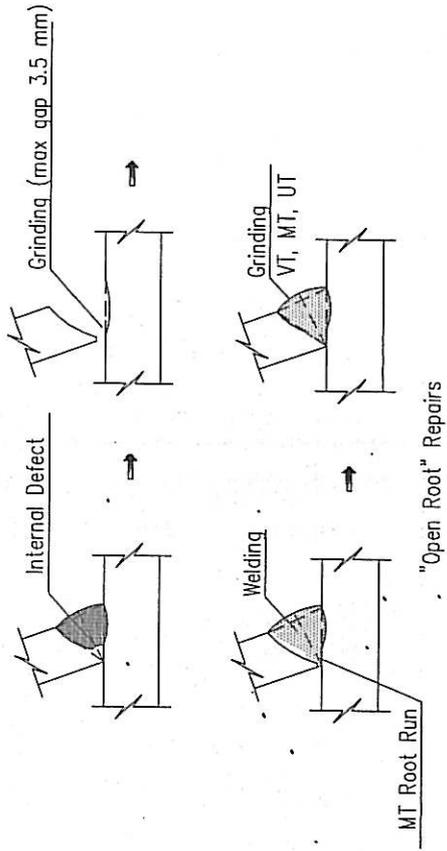
SHANGHAI ZHENHUA PORT MACHINERY CO., LTD.

SAN FRANCISCO OAKLAND BAY BRIDGE, EAST SPAN SELF-ANCHORED SUSPENSION SPAN
 ROUTE 80 EAST OF YERBA BUENA ISLAND, DISTRICT 04, SF COUNTY, CA.
 BRIDGE Nos.: 34-0006L PM 8.2, KP 13.2 & 34-0006R PM 8.7, KP 13.9
 STATE OF CALIFORNIA DEPT. OF TRANSPORTATION CONTRACT NO. 04-0120F4
 ENGINEER: CALTRANS ----- T.Y.LIN / MOFFATT & NICHOL, JV.
 CONTRACTOR: AMERICAN BRIDGE / FLUOR ENTERPRISES, JV.

repair procedure for closed rib welds

BY: DRAWN		SHEET NO. RP-004		ORDER NO.	
CHECKED					
REMARKS		REVISIONS			
NO.	DATE	Ifc HJZ			

- 3.2 Procedure for "Open Root" repair (3.5 mm gap maximum)
- 3.2.1 Prepare joint excavation according to approved SMAW WPS (WPS-345-SMAW-2G(F)-Repair-1)
- 3.2.2 Preheat to 60°C minimum.
- 3.2.3 Weld Joint according to approved SMAW WPS.
- 3.2.4 After root run, grind, and perform 100% MT.
- 3.2.5 Continue welding according to WPS.
- 3.2.6 Grind repair flush with base metal or adjacent weld surface to a smooth transition no steeper than 1:10.
- 3.2.7 Perform 100% VT, MT, UT of repair.



SHANGHAI ZHENHUA PORT MACHINERY CO., LTD.

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 BRIDGE Nos.: 34-0006L PM 8.2, KP 13.2 & 34-0006R PM 8.7, KP 13.9
 STATE OF CALIFORNIA DEPT. OF TRANSPORTATION CONTRACT NO. 04-0120F4
 ENGINEER: CALTRANS ----- T.Y.LIN / MOFFATT & NICHOL, JV.
 CONTRACTOR: AMERICAN BRIDGE / FLUOR ENTERPRISES, JV.

repair procedure for closed rib welds

NO.	DATE	REMARKS	BY	
			DRAWN	CHECKED

SHEET NO.	RP-005	ORDER NO.
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DEPARTMENT OF TRANSPORTATION

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Contract #: 04-0120F4Cty: SF/ALA Rte: 80 PM: 13.2/13.9File #: xx.25A**QUALITY ASSURANCE -- NON-CONFORMANCE RESOLUTION****Location:** Changxing Island, Shanghai, PRC**Report No:** NCS-000100**Prime Contractor:** American Bridge/Fluor Enterprises, a JV**Date:** 06-Nov-2008**Submitting Contractor:** Zhenhua Port Machinery Company, Ltd (ZPMC), Changxing Island **NCR #:** ZPMC-0067**Type of problem:**

Welding	Concrete	Other	
Welding	Curing	Procedural	Bridge No: 34-0006
Joint fit-up	Coating	Other	Component:
Procedural	Procedural	Descriptor:	

Date the Non-Conformance Report was written: 15-Feb-2008**Description of Non-Conformance:**

The Contractor performed weld repairs without prior Engineer approval. The Contractor repaired closed ribs to box shell plate using by grinding that was not previously approved by the Engineer. This occurred on weld defects in multiple locations of the SAW cover pass of closed ribs U-64 (weld #1) and U-133 (weld #10) on OBG deck panel DP-058.

Contractor's proposal to correct the problem:

The Contractor has submitted a repair procedure for closed rib welds for Engineer approval. Subsequent approval will resolve the non conformance.

Corrective action taken:

The Contractor has submitted Submittal 634R06-Repair Procedure for Closed-rib Welds to the Department to address repairs that were done without Engineer approval. The submittal has been approved as noted. Additionally, the Contractor has provided the Weld Repair Report, visual inspection report after the root repair was performed, and the magnetic particle testing report after the root repair was complete and final visual, magnetic particle and ultrasonic testing reports. All non destructive testing results confirm that the weld is acceptable.

Did corrective action require Engineer's approval? Yes No**If so, name of Engineer providing approval:** Gary Pursell **Date:** 31-Oct-2008**Is Engineer's approval attached?** Yes No See Submittal 634R06 for approval.**Comments:**

This report is for the purpose of determining conformance with the contract documents and is not for the purpose of making repair or fit for purpose recommendations. Should you require recommendations concerning repairs or remedial efforts please contact Peter Dautermann, who represents the Office of Structural Materials for your project.

Inspected By: Ishibashi, Josh

Quality Assurance Inspector

QUALITY ASSURANCE -- NON-CONFORMANCE RESOLUTION

(Continued Page 2 of 2)

Reviewed By: Smith,Ryan

QA Reviewer