

JOB STAMP

04-0120F4  
 04-SF-80-12.6/13.2  
 San Francisco Oakland Bay Bridge  
 Self Anchored Suspension Span

REPORT NO.	718	to	724	DATE	Nov 30 – Dec 6, 2008	S T W Th F S
SHIFT HOUR	START	Var.	STOP	Var.	TEMPERATURE	
WEATHER	Sunday	Monday –	Cloudy	Tuesday –	Cloudy	Wednesday – Cloudy
		Thursday –	Friday-	Cold/foggy	Saturday –	Cold/foggy

**30 November**

No work on site.

**1 December**

1. Reviewed email and PMIV Correspondence.
2. Reviewed Preliminary markups from TY-Lin for Submittal 865, 866 and 869 and generated my comments for Wednesday CRM.
3. Reviewed for completion of Submittal 915. A duplicate copy of this submittal was attached under the item detail tab. I informed John Lyons and requested him to remove these extra drawings.
4. Reviewed Submittal 759R01 and compared it with the previous redlines. Sheet CTSML1-0 4/4 should be redlined, since chart CTS5B and CTS6B are not consistent with their related sheets. Certain Part Mark, quantity and length shall be revised. On sheet CTS8A-0 1/1, it is unclear why a support for a cable tray support was removed. It was not redlined for removal in the previous revision. Also, a minor typo on the spelling of "others" is shown on the same sheet and Sheet CTS8B-0 1/1.
5. Attended E2 Concrete Pre Pour Meeting with CT and ABF representatives.
  - Two pumps will be used and estimated completion of pour sometime between 10am and 12pm.
  - 1<sup>st</sup> couple trucks will be slump flow tested at the lab and will be witnessed by CT representatives.
  - Concrete will come from both Amador and Mariposa plants.
  - There will be no washouts but bins will be available to pour concrete out.

**2 December**

1. Reviewed email and PMIV Correspondence.
2. Spent time reviewing some of Submittal 915.

**3 December**

1. Reviewed email and PMIV Correspondence.
2. Scheduled CRM was cancelled and will resume Thursday.
3. Reviewed TY-Lin Preliminary markups for several submittals prior to the CRM.
4. **Submittal 759R01:** Spoke with Abdi Sanei from Parsons Brinkerhoff and addressed that the CTS5B and CTS6B charts on Sheet CTSML1-0 4/4 should be revised and some values do not match the dimensions shown on the shop drawings. He agreed to update my findings. He also mentioned that the comment, "holes field drilled by others," should be removed on several sheets where it appears and that this comment should have never been added to the previous revision. Furthermore, he was concerned about the way in which the welding requirements (As an example, refer to Sheet CTS2A-0 1/1, Detail 2) were shown. It shows a stiffener welded to a web and flange and the welding callout states that it should be welded to "3 sides," however, the arrow does not necessarily point to 3 locations. I agreed with him, but Gary Lai met with METS representatives and it was confirmed that this statement about the welding callout should be clear to the welders about what part to weld. So this was not redlined.

RECT-09 JAN-02 #007979

Victor Altamirano SIGNATURE	<i>Victor Altamirano</i> 9-Dec-08	TITLE	
Senior SIGNATURE	<i>VA</i> 16 DEC 2008		Transportation Engineer / Assistant Struct. Rep.

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**4 December**

1. Reviewed email and PMIV Correspondence.
2. Attended Pre-Concrete Pour Meeting
  - ABF will have the form panels covered over E2 with some portions left open for inspectors to inspect the concrete.
  - 3 sets of cylinders will be made for both Amador and Mariposa plant.
  - 1-day break may happen.
  - There will still be slump flow tests for the first few trucks on site
  - Slump flow tests will be done at both plants by ABF reps. and possibly, CT reps. for every concrete truck being used to deliver concrete to E2.
  - If the concrete is rejected during the slump flow test, CT reps. are instructed to call Jim Davison and Gill to alert them of this.
3. Attended CRM and the following table were some that attended the meeting:

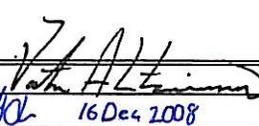
Dan Turner	Gary Lai	Sean Wichman	John Lyons	Chris Havel
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Comments for Submittal 866R0:

- Sheet SEG3012A: Contractor, Dan and CT representatives agreed to remove ratholes. The method of doing so will be to weld the five sections of the diaphragm and u-rib together and weld the entire 8590mm flange length onto the sections. This procedure will require a set of SA drawings to be submitted.

**5 December**

1. Reviewed email and PMIV Correspondence.
2. Closed out the following submittals:  
 840R04  
 582R08  
 768R01  
 759R01.
3. Prepared for pour: Visit E2 and brought some equipment needed for the sampling of cylinders.
4. E2 Cross Beam Concrete pour dated December 5, 2008 through December 6, 2008:
  - The first few Concrete trucks arrived about 9pm
  - Several slump values were conducted and averaged 26.06." (Refer to attachment) This value does follow the requirement for **Mix # 161790** of a **slump flow of 28+-2 inches**.
  - The concrete mix at the beginning of the pour appeared wet and it may have resulted by a low water to cement ratio. The Concrete mix did improve and the slump flow values increased within the required range in other loads as time went by.
  - Early into the Concrete pour, 5 gallons of water was added to one of the trucks. ABF explained how the concrete might harden since there was a back up of trucks. The Mariposa and Amador were closed temporarily due to a large amount of trucks backed up. The plants were later opened. There was also

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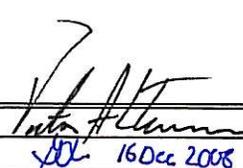
conversion from CT representatives about the slump flow being too low and that the Concrete mix weren't good. Please refer to Gill Klebanov Diary for details.

- David Chung and I did the concrete sampling, Pamela Gagnier did the 6<sup>th</sup> set. The **first full set of sample** was conducted at 11:57pm and the slump flow for this concrete mix (ticket # 82846066) was **29 inches**. The second set was carried out at 2:35 am (ticket # 82111175) and had a slump flow of **25 inches**. The **next full set** was taken at 3:34am (ticket # 82111180) with a slump flow of **28 ¾ inches**. The fourth set was taken at 5:53am (ticket # 82846121) and had a slump flow of **30.5 inches**. The fifth set was taken at 7:45am (ticket # 82111221) and had a slump flow of **29 inches**. The final set was carried out at 10am (ticket # 82111253) and had a slump of **26 ¾ inches**. See attachment for details.
- During the pour, Matt Bruce had called and reiterated that the concrete being poured on E2 looked wet. I informed Gill and he told me he was aware of this and that he has other Caltrans inspectors looking into this. It was later realized that a PVC pipe had in fact busted and water was pouring into the forms. Refer to Matt Bruce diary for details.
- Throughout most of the pour, Samon was stationed at the Amador plant and called me to be on the look out for several trucks where the concrete was questionable. In truck # 2820 around 10pm, he informed me that a half-gallon of super plasticizer was added and the slump flow was 25.5 inches before it left the plant. When this same truck arrived on site, the slump flow was taken at 11:10pm and had a value of 27.5 inches. This slump flow was well within the requirements. There were other trucks from the plant that left with low slump flow values, but the values increased by the time the concrete was pumped.

6 December

1. Please refer to December 5, 2008 for diary detail since the Concrete pour lasted through part of Saturday.

Victor Altamirano SIGNATURE



9-Dec-08

TITLE

Senior SIGNATURE

16 Dec 2008

Transportation Engineer / Assistant Struct. Rep.



San Francisco Plant # 828  
 500 Armador Street  
 San Francisco, CA 94124  
 1-800-470-2704

DATE 11/7/2008	TICKET NO. 82844671
P.O. #	PROJECT NO. 31284423

BILL TO SHIP TO

30098741 AMERICAN BRIDGE FLOUR ENT A JT VENT 375 BURMA ROAD  
 34303286 METRIC YERBA BUENA OAKLAND SAN FRANCISCO BAY BRIDGE E. SPAN SEISMIC SAFETY PROJ.

LOAD	LEAVE PLANT	ARRIVE JOB SITE	START DISCHARGE	FINISH DISCH.	LEAVE JOB SITE	ARRIVE PLANT	STAND-BY TIME
22:48:30	2305	2325					

SLUMP	PLANT NO.	TRUCK NO.	DRIVER	MAP ZONE
27.0 in. SC	828	3330	SMITH, SCOTT D	090 0649-E1
ORDER	PRODUCT CODE	REFERENCE	USAGE	
90031799 / 10	G-000-4-P-1A-98-1-Z-KND	181790	SLAB	

LOAD QTY	CUMULATIVE QUANTITY	ORDERED QUANTITY	U/M	PRODUCT CODE	PRODUCT DESCRIPTION	SLAB	PRICING UNIT	EXTENSION
6.80	129.20	1863.20	M3	181790	SCC 800LBC+F 26 1 2 P SP RET G000PIA081ZKND FUEL SURCHARGE RM			
1.00			EA	MIX DESIGN				

MATL	TARGET	ACTUAL	UNIT	TOL.	MOISTURE CONTENT
Orca Sand	6788.79	6784.81	Kgs.	IN TOL	5.05
1/2" ORCA	6258.00	6241.43	Kgs.	IN TOL	1.30
WATER	615.29	613.24	Lts.	IN TOL	
Flyash	806.49	807.39	Kgs.	IN TOL	
Type 2 cement	2419.48	2458.47	Kgs.	OUT TOL	
V-Mar 3	7361.44	7245.51	ML.	IN TOL	
Adva 100	24608.25	24575.80	ML.	IN TOL	
Recover	11830.89	11888.56	ML.	IN TOL	

1st Service Sent : 20 Bay Area San Fran Pier  
 ? #828 - Truck 2868

2nd Service Sent : 20 Bay Area San Fran Pier  
 28 - Truck 2868

DRUM REVOLUTIONS \_\_\_\_\_

ACTUAL Total Water Lts./M3 : 158.06  
 Rel. W/C : 0.33  
 Job Site Water Lts./M3 : 0.20

SPECIAL INSTRUCTIONS	SALES TAX
reset drum counters ready slump off, TAKE TREASURE ISLAND RD EXIT, R MCALLA RD	TICKET TOTAL
TAKE MCALLA TO JOBSITE AND LOOK FOR FLAGGERS.	SUB-TOTAL
	STAND-BY TIME
	TOTAL

All terms and conditions contained in the Material Quote and Credit Application and Agreement are hereby incorporated into this document and made a part hereof. This document is subject to the terms and conditions appearing on the reverse side hereof.  
 NOTICE: THE STRENGTH OF CONCRETE DESCRIBED IS BASED UPON A SLUMP OF 3 TO 5 INCHES, OR AS SPECIFIED BY THE APPROVED MIX DESIGN WHEN TESTED IN ACCORDANCE WITH ASTM C-94. THE CUSTOMER IS RESPONSIBLE FOR REQUIREMENTS OF ACI 305A AND 308.

THE UNDERSIGNED AUTHORIZED ADDITION OF 0 GALLONS OF WATER. SLUMP TAKEN:  YES  NO TEST CYL TAKEN:  YES  NO

WEIGHMASTER CERTIFICATE  
 THIS TO CERTIFY that the following described commodity was weighed, measured or counted by a weighmaster, whose signature is on this certificate, who is a recognized authority of accuracy, as prescribed by Chapter 7 (commencing with Section 12700) of Division 5 of the California Business and Professions Code, administered by the Division of Measurement Standards of the California Department of Food and Agriculture.

JAY G WREN  
 By Deputy X  
 700DT002CA

REASON FOR STANDBY: \_\_\_\_\_  
 CUSTOMER ASSUMES RESPONSIBILITY AND FOR LIABILITY FOR ANY PERSONAL OR PROPERTY DAMAGE WITHIN THE CURB AND PROPERTY LINE. ALL CLAIMS MUST BE MADE AT TIME OF DELIVERY.

RECEIVED AND ACCEPTED BY: PUMP  
 CUSTOMER 1 COPY

1902290

\* Sawon - Info from Amador

Nolan

24.5 26

E2 Cross Beam - Concrete Pour : December 5, 2008 - December 6, 2008  
Self-Consolidating Concrete: Mix 161790

Ld #	Manpoza/ Amador	Truck #	Ticket #	Temperature		Gals H <sub>2</sub> O added	Flow		By	Samples		Remarks
				Amb.	Conc.		Time	Dia./SI		Time	Cylin. (TL-502)	
	A	3499						30				29 Sawon
	A	3179						27.5				27 Sawon
	A	4864						26				BO on the hole out Sawon
1		3499						27		V.A.		
2		3279						25		V.A.		25 24.5
3		3249						25.5				24.5 26
		2132										
	A	3348						26.5				0:31 Sawon (called)
4		3279						26.5				26.5 26
		3183										
5		2128	64					22.5				23 22
		3348						26				Sawon (called)
	*	2820						23				Sawon called 1:13/24.5
		2859						23				23 24.5 25.5
		3250						25				24.5 25.5
		3246						24.25				24.5
		2949						22.5				22.5 22.5
		2976						23.5				22.5 22.5/24.5
		3206						25.25				25.5/28
		2781						18.45				29 27
		3266						25				24 26
		2750						25.5				26 25
		3251						24				23.5 24.5
		3568						23.5				23.5 23.5
		3348						27				28 26.5

E2 Cross Beam - Concrete Pour : December 5, 2008 - December 6, 2008  
 Self-Consolidating Concrete: Mix 161790

Ld #	Meriposal/ Amador	Truck #	Ticket #	Temperature		Gate H <sub>2</sub> O added	Flow		Samples		Remarks
				Amb.	Conc.		Time	Dia./NSI	Time	Cylin. (TL-502)	
OK		3326		48	59.5		11:08	25			
	*	2820		48	58.5		11:10	27.5			
		3250						31			
		3279		50	56.5		11:29	24.34			
	K	2859	82846066		55.5		11:57	29		V.A. & Lofra	11:57
											C=8.3 W=15.5
		3255			57		1:15 AM	31.5		V.A.	
	A	3255			56		1:23	28			
	M	3257			58		1:30	28.14			
	M	2821			53		1:36	24.5			
	M	502			53		1:39	28.5			
	M	3228			53		1:47	26			
	M	3509			53		2:01	24			
	A	3249	3255								
	A	3249						32			
	M	3264			53		2:12	25.9			
	M	3510	8211175		58		2:32	25			
	A	750			56		3:10	25			
	A	348			59		3:15	28.34			
	M	211	8211180		57		3:34	28.34			
	M	502			58		4:07	29.25			
	A	2859			56		4:11	27			
	A	3279	8284621		54		5:53	30.5			
	M	3506	8211121		56		7:45	29.8			
183	M	3508	8211253		57		10:00	20.5			

4.55  
10.00

46 ~ 48 loads  
Amador 92 ft

1st pour  
17.0 muck  
2 of 11

32 / 31 (samon value)  
 28 / 28  
 24 / 26 1/2  
 24.5 / 24.14  
 28.5 / 28.5 Ticket # 17  
 17 / 25  
 26 / 22  
 Samon Found in  
 Hesing truck  
 samon no Seg. (2:10)  
 25.5 / 25.5  
 25 / 25 2nd SET  
 24.5 / 26 3/4  
 28 1/2 29  
 3rd Sample 29.5 / 29  
 29.5 / 29  
 27.5 / 26.5  
 30 / 31 4th SET  
 Sample 5th SET 28 / 30  
 Sample #16 set