

Job Stamp 04-0120F4 SFOBB SAS

Const. Calendar:	652			
Project Work Day No.:	852			
Date	09/25/2008			
Inspectors	Start	10:10	Stop	11:10
Hours		13:00		14:20
Shift Hours		07:00		15:30

ASSISTANT RESIDENT ENGINEER'S **CONTRACTOR – ABFJV**

Weather: Sunny with mild to warm temperature – Hi 82°F Low 57°F (per weather.com forecast)

Description of Operations @ W2 Cap Beam:

ABF

- Continued to clean debris from stripping the pour 3 south blockout forms.
- Continued to place washers from the #19 formsaver at the W2E east bulkhead. Also continued to place polystyrene shear key blockout for the east bulkhead construction joint between the OBG and the W2 cap beam, see photo below.
- Continued to construct PT blockout for E-30B through E-42B.
- Installed bike path bolts at the lower portion on the west end of the cap beam, see photo below.

Office work:

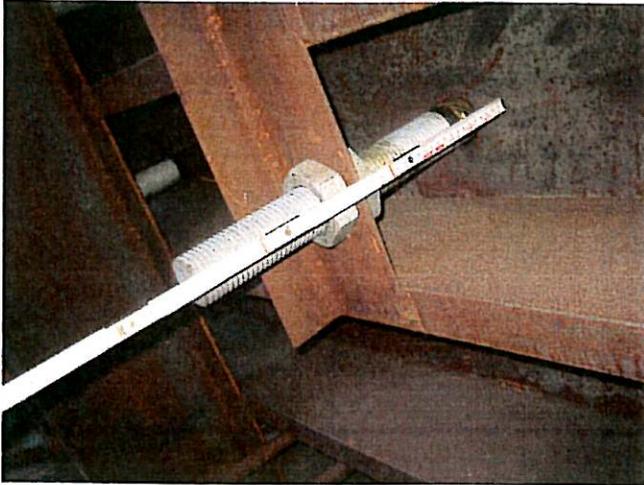
- Discussed the discrepancies in the elevation for control point TWL-270 (now SAS-270) with David Bradd and Ron Matin. Towill's published elevation of 54.129m has been approved per submittal 548R01 dated April 3rd, 2008. Section 10-1.23 of the Special Provisions states that the following "The Survey Plan, and the calculations, survey notes and other records submitted to establish the exact position of the work shall be completed under the direction of and signed by a Land Surveyor who is licensed by the State of California". To date the elevation of 54.142m set by ABF has been used to build the W2 cap beam.
- Analyzed my differential level run results of the control points on the W2 cap beam from Tuesday.
- Wrote today's diary.

Inspector:

Matt Bruce Matt Bruce Transportation Engineer (D)

REC'D *08 OCT-07 #006962

EA	04-0120F4
Co-Rte-KP (PM)	SF-080-13.2/13.9 (8.2/8.7)
Structure Rep.	Rick Morrow

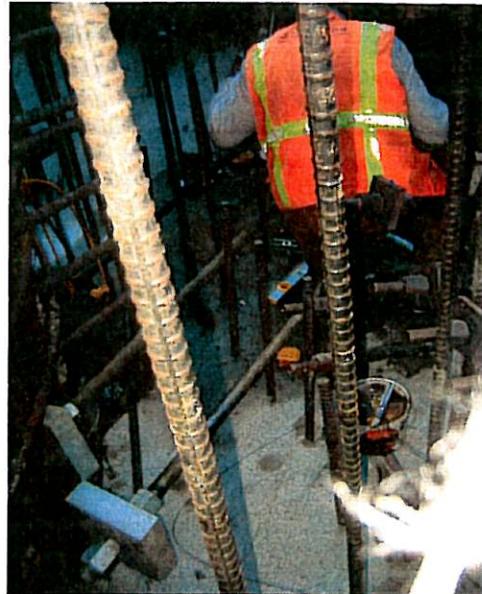
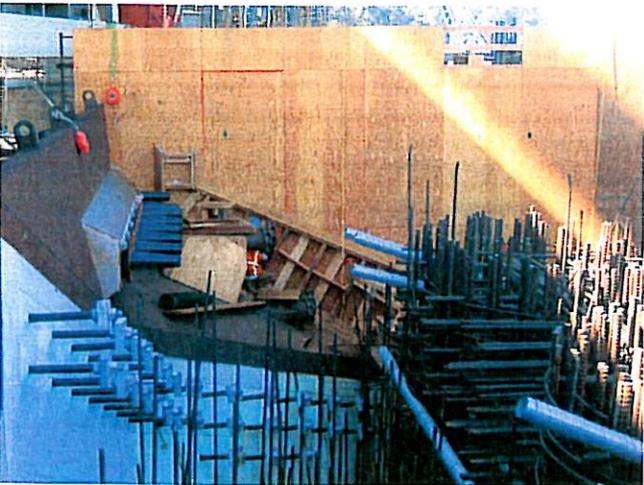


File Name:	Sept-25-2008 W2 Cap 002
Date:	09-25-08
By Int:	M Bruce

Description: Deviation saddle anchor rod at W2E measuring 390mm from the 12mm thick steel forms. A couple of anchor rods on Panel A were measured on both sides of the forms. The distance from the 50mm polystyrene grout leveling pad to bottom of the bearing plate was measured ten times between 480mm and 490mm. Similarly to the photo above the distance from the forms to the end of the rod was measured 6 times from 380mm to 390mm.

File Name:	Sept-25-2008 W2 Cap 005
Date:	09-25-08
By Int:	M Bruce

Description: Continued placement of E-1A through E-42A trumpets, #19 formsaver washers, and polystyrene shear keys at the W2E east bulkhead. Per RFI 1240R00 there are 11 rows of polystyrene shear keys, so far only 10 have been installed.



File Name:	Sept-25-2008 W2 Cap 006
Date:	09-25-08
By Int:	M Bruce

Description: Piledriver constructing the PT blockout for continuity tendons E-30B through E-42B.

File Name:	Sept-25-2008 W2 Cap 007
Date:	09-25-08
By Int:	M Bruce

Description: Three bike path bolts were installed at the lower portion on the west end of the cap beam.