

Job Stamp 04-0120F4 SFOBB SAS
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Const. Calendar:	649			
Project Work Day No.:	849			
Date	09/22/2008			
Inspectors	Start	11:30	Stop	14:00
Hours				
Shift Hours		07:00		15:30

ASSISTANT RESIDENT ENGINEER'S **CONTRACTOR – ABFJV, Subs RPS**

**Weather:** Sunny with mild temperature and westerly breezes – Hi 79°F Low 56°F  
(per weather.com forecast)

**Description of Operations @ W2 Cap Beam:**

**ABF**

- Installed trumpets for continuity tendons E-1B to E-13B on the west wall.
- Continued to clean debris from stripping the pour 3 north breakout forms.
- Erected gang panel forms for the southwest vertical face near the W2E deviation saddle, and prepared gang panel forms for the northwest vertical face near the W2W deviation saddle.
- Began to place washers from the #19 formsaver at the W2E east bulkhead. ABF is assisting the ironworkers with the placement of the female end to position the male end for the closure pour between the OBG and the W2 cap beam, see photo below.
- Surveyors assisted piledrivers with the locations of the washers mentioned above. Also began to layout points for the blockouts of continuity tendons E-30B through E-44 on the west face.

Note: It appeared that more curing compound was applied to the W2E and W2W vertical bar breakout surfaces. No cracks were seen on the surface of the concrete thus far.

**RPS**

- Began to grind the mushroomheads of the #43 transverse bars on the north end of pour 3, see photos below.

**Office work:**

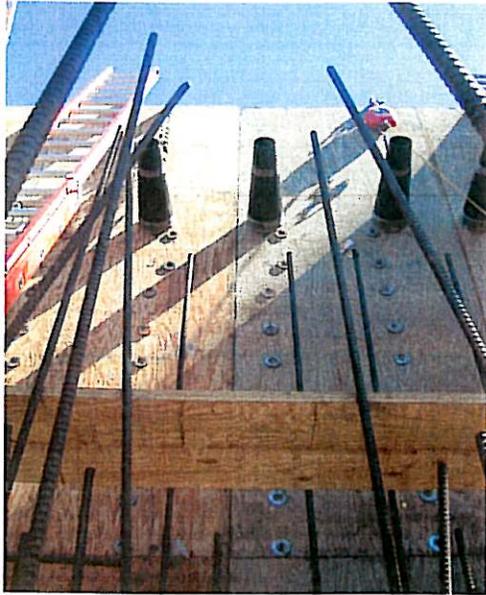
- Continued to check the specifications, tolerances, and correction procedures for checking vertical control by performing a differential leveling run.
- Wrote today's diary.

**Inspector:**

Matt Bruce     *Matt Bruce*     Transportation Engineer (D)

REC'D \*08 OCT-07 #006959

EA	04-0120F4
Co-Rte-KP (PM)	SF-080-13.2/13.9 (8.2/8.7)
Structure Rep.	Rick Morrow

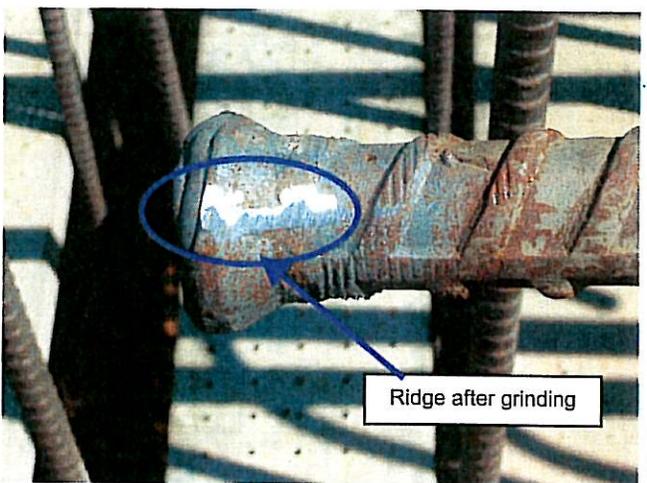
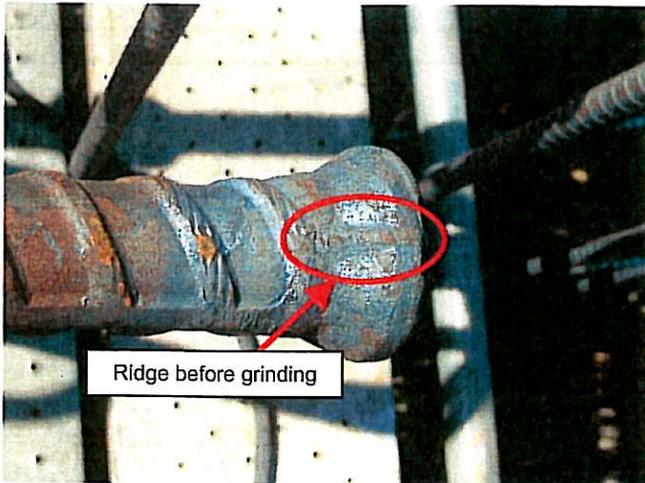


File Name:	Sept-22-2008 W2 Cap 005		
Date:	09-22-08	By Int:	M Bruce

Description: Washers from the #19 formsavers nailed to the plywood of the W2E east bulkhead forms.

File Name:	Sept-22-2008 W2 Cap 011		
Date:	09-22-08	By Int:	M Bruce

Description: Ironworkers seen propping the #43 transverse bars to facilitate grinding operations.



File Name:	Sept-22-2008 W2 Cap 008		
Date:	09-22-08	By Int:	M Bruce

Description: Photo of a #43 transverse bar before grinding operations. Per my conversation with RPS foreman Tim Greenlee he believes that the ridge seen in the photo above caused the excessive failure in slip in addition to the slag around the mushroomhead. I mentioned to RPS foreman Tim Greenlee that a few of the mushroomheads were not staggered at 600mm apart.

File Name:	Sept-22-2008 W2 Cap 007		
Date:	09-22-08	By Int:	M Bruce

Description: Photo of a #43 transverse bar after grinding operations. Many of the mushroomheads appeared to have drastic deformations even after grinding near the beveled edges and the bearing end of the bar. The vertical bars mushroomheaded in the column cages didn't have these characteristics. One theory regarding the difference in quality was the operator heating the bar.