

TOLL PROGRAM/DIST. 4 CONSTR.

Job Stamp:
04-SF-80-13.2/13.9 04-0120F4
SFOBB SAS
San Francisco Co. in San Francisco
Fm 0.6 km to 1.3 km East of Yerba Buena
Tunnel East Portal

Report No. _____ 7-day const. cal.: 615
Project work day: 825
Date the Shift Began: 8/19/08
 NIGHTWORK TUESDAY
Shift Hrs Start 6:30 Stop 15:00
Engineer's Hrs Start 6:45 Stop 15:15

ASSISTANT RESIDENT ENGINEER'S DAILY BRIDGE REPORT

Location: E2 Cross Beam
Remark: sandblast/tieback/tin
Weather: overcast AM/
Hi 69F/Lo 60F

Description of Operation:
ABF - begin drilling holes into east panel for tie-back rods/continue nailing tin sheets to panel edges/continue sandblasting ironwork at E2E.
RPS - unload steel from flatbed trucks to barge at Pier 7 (for names see Masoud Modanlou's diary)

2055001-10 #005802

EQUIPMENT AND/OR LABOR:			HOURS - ITEM NO.						CONTRACTORS		
			38	48	133				Prime	REMARKS	
EQPT. NO.	NO. MEN	DESCRIPTION (Of Equipment or Labor)	Structural Concrete, Bridge	Bar Reinforcing Steel (Bridge)	Construction Surveying				Name	Classification	Prime / Sub
	1	Foreman					8.00		Ben Neal	Pilebutt	P
	1	Journeyman	8.00						Allen Briney	Pilebutt	P
	1	Journeyman					8.00		Jason Matlock	Pilebutt	P
	1	Journeyman	8.00						Luke Paulk	Pilebutt	P
	1	Journeyman					8.00		Edward Mendoza	Pilebutt	P
	1	Apprentice	8.00						Garrieth Nelson	Pilebutt	P
	1		8.00						Ray Oku	Deck Engr	P
	1	Journeyman	8.00						Kevin Fitzgerald	Operator	P
	1	Apprentice	8.00						David Bian	Oiler	P
	1			8.00					Byron Contreran	Laborer	P
	1						8.00		Ignacio Gil	Laborer	P
	1			8.00					Riyoberto Cancon	Laborer	P
	1	Apprentice			8.00				Chris Roberts	Surveyor	P
	1	Journeyman			8.00				Terry Dennis	Surveyor	P
	1	Ringer Barge					8.00		Manitowac Crane		P
002062	1	Lincoln Electric					8.00		Vantage 500	6008088	P
002083	1	Lincoln Electric					8.00		Vantage 500	6008091	P
002068	1	MQ Power Generator	8.00						Whisperwatt 25	6007939	P
002069	1	MQ Power Generator	8.00						Whisperwatt 25		P
	1	Honda 115 Horse Skiff					8.00				P
002045	1	Ingersoll Rand P185R		8.00							P
	1	Clemco Classic 150		8.00					Sand Bucket		P
674015	1	Sullair SF 375H		8.00					Compressor		P
679095002	1	Ditch Witch FX60		8.00					Vacuum		P
	1	Flex Anchor					8.00		Equipment Barge		P
	1	"Westar I" barge					8.00				P

During the meeting with ABF at 1000 at the construction trailer on YBI: Pour will start at 0200; Mix will be 161145 w/6" slump; 580 m³; thermal control table 9; Max conc. temp 77 deg.; DCI will be taking samples; each truck will be 6.88 m³; 3 concrete washouts at W3; first truck arrives at 0200. Gil (CT senior) mentioned that ABF is required to revibrate the concrete per the special provisions. Marko (ABF superintendent) disagreed that the special provisions stated this as such. CT pointed the page and paragraph out to Marko. Marko continues to disagree with the need to revibrate. He says that this is only for footings. Caltrans requires revibrating to reconsolidate the concrete: Per the 1999 standard specifications, Section 51-1.09, "After placing, consolidating and initial screeding of concrete for structure footings, more than 750 mm in vertical dimension and with a top layer of reinforcement, the concrete shall be reconsolidated by the use of internal vibrators for a depth of 0.3 m from the top of the footing and finished. Reconsolidation shall be accomplished as late as the concrete will again respond to vibration, but not less than 15 minutes after the initial screeding has been completed." and in the special provisions, page 263 Concrete, "After the mass concrete pour has been topped out and finished, it shall be revibrated and refinished. Revibration shall extend below the top mat of reinforcement and shall be done as late as the

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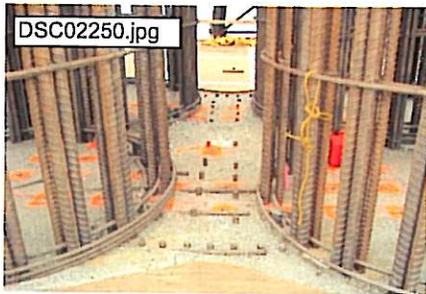
ASSISTANT RESIDENT ENGINEER'S DAILY

BRIDGE

REPORT

Location: E2 Cross Beam **Weather:** overcast AM/
cut and finished, it shall be re-vibrated and finished. Revibration shall extend below the top mat of reinforcement and shall be done as late as the concrete will again respond to vibration. For concrete pours without top reinforcement, revibration shall extend to a depth of 150 mm." The intent is to eliminate any voids under the top layer of reinforcement from self-setting of the concrete. In self-settling, the aggregate will pile up on the top of the reinforcement while the cement will continue down. There will be a small void under the steel. Revibration will help alleviate this problem.

At E2, the carpenters waited all day for ironworkers to weld #57 bars that were wrongfully cut in the column cages of E2W; ironworkers did not show up to E2 today. Ironworkers unloaded steel from arriving flatbed trucks to barges (DSC02255) and have not yet been mobilized to the Pier E2. The carpenters started drilling holes for the tie-back rods in the forms and continued to nail tin to the soffit's panel edges (DSC02253). No major work seen in the area (DSC02251) besides the sandblasting operation that continues at E2E column cages. Surveyors continue to layout shear key/bearing bolts at E2E (DSC02250).



Materials:

Insp. Hrs.	
REG: 8.0	INTERMITTENT
OT:	INSPECTION

David Chung
DAVID CHUNG

TE/CT
Title