



C.C. MYERS, INC.

An Equal Opportunity / Affirmative Action Employer

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LETTER OF TRANSMITTAL

Document No: 215-STT.00423
Dated Oct 12 2005 Job No.: 215
Attention: Mr. Lourdes David
Re: 04-0120R4
Temporary Bypass Structure

To: State of California
Department of Transportation
333 Burma Road
Oakland CA 94607

Subject: TBS Deck Drainage

We are sending you:

Attached Via Fax

- Drawing Plans Prog. Pmt
- Samples Certificates of compliance Calculations
- Payroll Specs Copy of Letter
- Change Order Schedule Invoice

Copies	Item	Date	Description

These are transmitted as checked below:

- For Approval For Review/comment Return For Correction
- For Your Use As Requested For Information

Remarks:

Please see attached Letter # 155 from Imbsen and Associates, dated October 5, 2005.

Copy To: Robert Coupe, Main Office

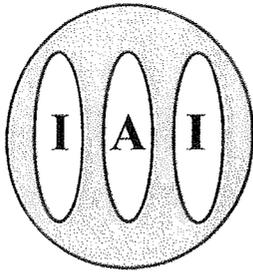
File: 215-101

Signed:

Juan C. Gray

Juan C Gray
Project Engineer





IMBSEN & ASSOCIATES, INC.
Engineering Consultants
 A **TRC** Company

RECEIVED

October 5, 2005

#1295-320

OCT 07 2005

IAI Letter #: 155

Mr. Bob Coupe
 C.C. Myers, Inc.
 3286 Fitzgerald Road
 Rancho Cordova, CA 95742

CC MYERS, INC.
 JOB 215 TEMP. BYPASS STRUCTURE

IC-02565
 215-201 215-990

Subject: TBS Deck Drainage

RC
 JG
 CT (transmittal)

Dear Mr. Coupe:

This letter is in reference to our meeting on Friday September 30, 2005 at Caltrans field office and in response to State Letter 05.03.01-000567, dated September 13, 2005. Based on our meeting, it is our understanding that we are to ignore State Letter 05.03.01-000567, and provide a drainage proposal based on the discussions in our Friday meeting.

Based on our meeting, it is our understanding that Caltrans agrees that the drainage criteria provided in the contract is not adequate for the proper bridge drainage design of the TBS structure. It is also our understanding that Caltrans would want us to provide a drainage proposal based on the following criteria:

- Provide the maximum number of Type D drainage inlets that can be incorporated on the TBS without affecting the structural integrity of the structure.
- Provide deck drainage scuppers in the bridge barrier railings. The spacing of scuppers should be dictated by the maximum number of scuppers that can be provided in the barrier without affecting the structural integrity of the barrier. The contract documents do not provide procedures and details for the design of deck drainage scuppers. We are to use Bridge Design Aids 17-1, dated January 1999 for the proposed scupper design.
- Caltrans wants us to provide depressed curb openings at each scupper location. We are to use the FHWA "Urban Drainage Design Manual-Hydraulics Engineering Circular No. 22" dated November 1996, for the design of depressed curb openings. The gutter depression (dimension a) shall be 1 ½ inches.

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- The drainage flow from the scuppers can be disposed of by free fall (Class II) directly under the drain. The contract documents state that the drainage for the TBS is Class I.
- We are to assume zero incoming flow from the existing bridge onto the TBS.
- Assume 45 degrees of rainfall from both sides of the structure onto the lower deck.
- Calculation for 10-year and 25-year rainfall intensity shall be provided.

We assume that this is the direction that we need to proceed with unless we are notified otherwise by October 12, 2005.

Please feel free to contact me at (916) 366-0632 should you have any questions.

Sincerely,

for: [Handwritten Signature]
Roy A. Imbsen, P/E., D. Engr.
Project Manager

cc: IAI File, EA, MV, RI

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