



# Proposed Improvement Projects Along I-80



Proposition 1B - Transportation Bond

## 1 Carquinez Bridge to the Bay Bridge Toll Plaza I-80 CORRIDOR MOBILITY PROJECT FACT SHEET

### The Project

The Interstate 80 Corridor Mobility Project will install new and upgrade existing corridor management elements along the Interstate 80 (I-80) corridor, from the Carquinez Bridge (Contra Costa County) to the San Francisco Bay Bridge Toll Plaza (Alameda County). This project will allow sharing of real-time travel information among public agencies and the public. The project includes integration with the Alameda County Smart Corridors Program and Caltrans Transportation Management Center (TMC).

### The Need

The I-80 corridor is one of the most congested corridors in the Bay Area. Currently, traffic volumes in the freeway far exceed the roadway capacity, causing unreliable travel times and diversion to the local arterials. During the peak period, the majority of the corridor operates with significant congestion and delays. The congestion on the roadway network contributes to an increase in incident rates, including rear-end collisions on both the freeway and local arterials.

### Benefits

This project will enhance the current transportation management system by using state of the practice solutions to build a balanced, responsive and equitable system that will monitor and control traffic and improve the safety and mobility of the users. The solution will create a balanced network for all users with an emphasis on system reliability and efficiency.

### Partnership

This project is developed through a partnership among the Alameda County Congestion Management Agency (ACCMA), the Contra Costa Transportation Authority (CCTA), the Metropolitan Transportation Commission (MTC), and the California Department of Transportation (Caltrans). Project sponsors include local, state, and federal agencies.

### Project Status

The project is in the environmental phase; the environmental document is anticipated to be completed in 2009. The design phase is going concurrently and is expected to be completed in mid 2009.

### Project Costs

The total construction capital for this project is estimated at \$47.1 million.

### Project Schedule

Start Construction: Summer 2009

Finish Construction: Fall 2011

### Summary

The Integrated I-80 Corridor Mobility Project is needed to improve travel speeds and flow, reduce traffic incidents, and improve air quality.





# Proposed Improvement Projects Along I-80



## SHOPP-Bridge Seismic Restoration Program

### 2 Hilltop Drive Overcrossing I-80/HILLTOP DRIVE BRIDGE REPLACEMENT PROJECT

#### FACT SHEET

#### The Project

This project proposes to remove the existing bridge that carries eastbound Hilltop Drive traffic over I-80 and build a new pre-stressed concrete box girder bridge in its place on a new profile. The loop on-ramp to EB I-80 will be modified to match the new roadway profile, and its landscaping will be replaced. The bridge that carries the westbound Hilltop Dr. Traffic will remain in place.

#### The Need

I-80 is the primary route traversing geographically in the north-south direction in Eastern Contra Costa County and serves as a major commuter, commercial and recreational route between the west and east coasts. One of the existing bridges along this route, the eastbound Hilltop Drive over crossing, was found to have abutment settlement and lateral movements that have occurred since it was built in 1957. Other conditions reported in a July 29, 2003 bridge inspection report include deck cracking, diagonal cracking in abutment wing walls, nonstandard vertical clearance of 15'-5" over I-80, and no seismic gap between the pier and median barriers. Its piers and abutments also need retrofitting to meet seismic standards. A seismic event could trigger further structural damage that could force the closure of this major highway.

#### Benefits

Replacing the bridge will eliminate the structural concerns and address the non-standard clearance at this interchange allowing movement of large loads through the I-80 corridor and help improve the San Francisco Bay area economy. Additionally, the new bridge will have a span that will accommodate future additional lanes on mainline I-80.

#### Partnership

The new structure includes a bike lane and a sidewalk that is compatible with the 1994 City of Richmond General Plan. An AC Transit bus shelter will be replaced with a model that is compatible with the transit carrier's standards.

#### Project Status

Capital funding for this project was voted by CTC at its April 2008 meeting. The project was awarded on July 3, 2008.

#### Project Costs

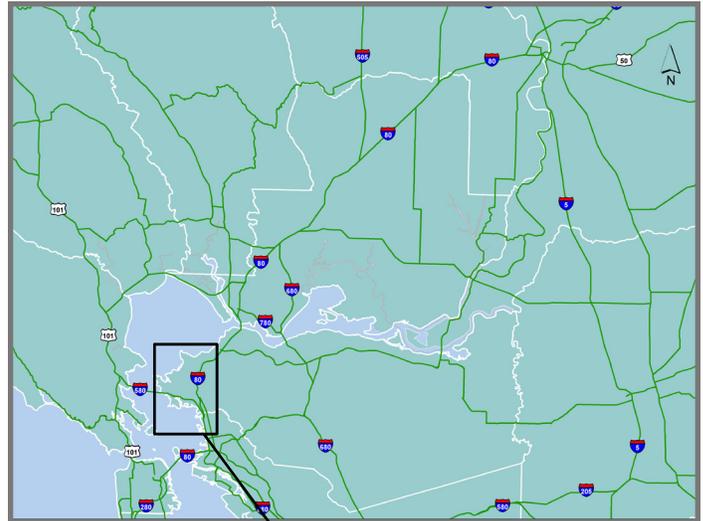
Total construction capital for this project is \$7.2 million.

#### Project Schedule

Start Construction: Fall 2008  
Open to Traffic: Fall 2009

#### Summary

Demolition will require a one time 14-hour freeway closure (Saturday, 9:00 PM to Sunday 11:00 AM). Freeway traffic will be detoured to the ramps which will be striped to a two-lane operation. A series of presentations has been made with the local entities to alert them of the project's traffic impacts.





# Proposed Improvement Projects Along I-80



100% Regional Measure (RM2) Funding

## 3 From Route 4 to the Carquinez Bridge I-80 EASTBOUND HOV LANE EXTENSION PROJECT FACT SHEET

### The Project

This project proposes to construct a 4.7 mile HOV lane in the eastbound direction of Interstate 80 between Route 4 and the Carquinez Bridge. The project will also realign the eastbound on-ramp at Route 4, widen the Willow Avenue undercrossing, construct a new structure at the Willow Avenue eastbound off-ramp, modify the eastbound off- and on-ramps at Cummings Skyway, construct retaining walls, resurface existing lanes with AC, replace PCC slabs, and construct ramp metering facilities.

### The Need

Interstate 80 is a critical east-west connector between the San Francisco Bay Area, Sacramento and the localities east of the Sierras and beyond. With the anticipated growth in both the commuter and commercial traffic along this corridor, it is expected that without improvements operational performance will continue to decline in the years to come.

### Benefits

The project will reduce travel delay, enhance intermodal transportation along the I-80 corridor in the Bay Area, and improve air quality.

### Partnership

This project is being developed in partnership between the California Department of Transportation (Caltrans) and the Metropolitan Transportation Commission (MTC).

### Project Status

The project is in the design phase and is approximately 60% complete. Ongoing work includes design and plan preparation for roadway, structures, structural section, drainage, highway lighting/traffic operations (TOS), signing, pavement delineation, erosion control, and traffic management with a target completion date of July 2009. In addition, Environmental Permits and an Environmental Revalidation are being prepared.

### Project Costs

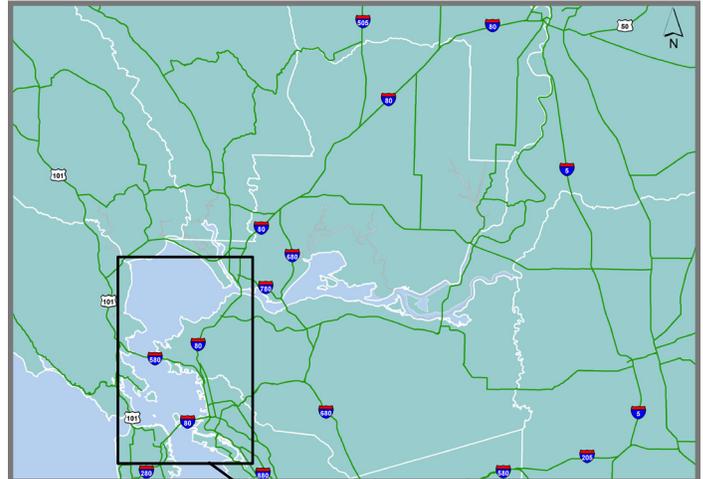
The total construction capital for this project is estimated at \$40.3 million. The project is funded entirely with Regional Measure 2 (RM2) funds.

### Project Schedule

Start Construction: Winter 2010  
Finish Construction: Winter 2011

### Summary

This congestion relief project will complete the HOV lane system for both the EB and WB directions on Interstate 80 between the San Francisco Bay Bridge and the Carquinez Bridge.





# Proposed Improvement Projects Along I-80



Proposition 1B - Transportation Bond

## 4 Various Locations TRAFFIC MONITORING STATIONS INSTALLATION PROJECT FACT SHEET

### The Project

This project is to install 84 wireless magnetometer vehicle detection stations along Interstate 80 (I-80), in Napa and Solano Counties.

### The Need

The purpose of the project is to address the need to enhance corridor management planning efforts, provide traffic control to manage the corridor, and preserve the mobility improvements associated with the Corridor Mobility Improvement Account (CMIA) investments in the corridor.

### Benefits

The project will reduce travel delay, decrease congestion, and improve mobility in the corridor.

### Partnership

The project is developed through a partnership among the Solano Transportation Authority (STA), the Metropolitan Transportation Commission (MTC), and the California Department of Transportation (Caltrans).

### Project Status

This project is currently under construction and is anticipated to be complete in October 2009.

### Project Costs

The programmed construction cost is \$3.1 million.

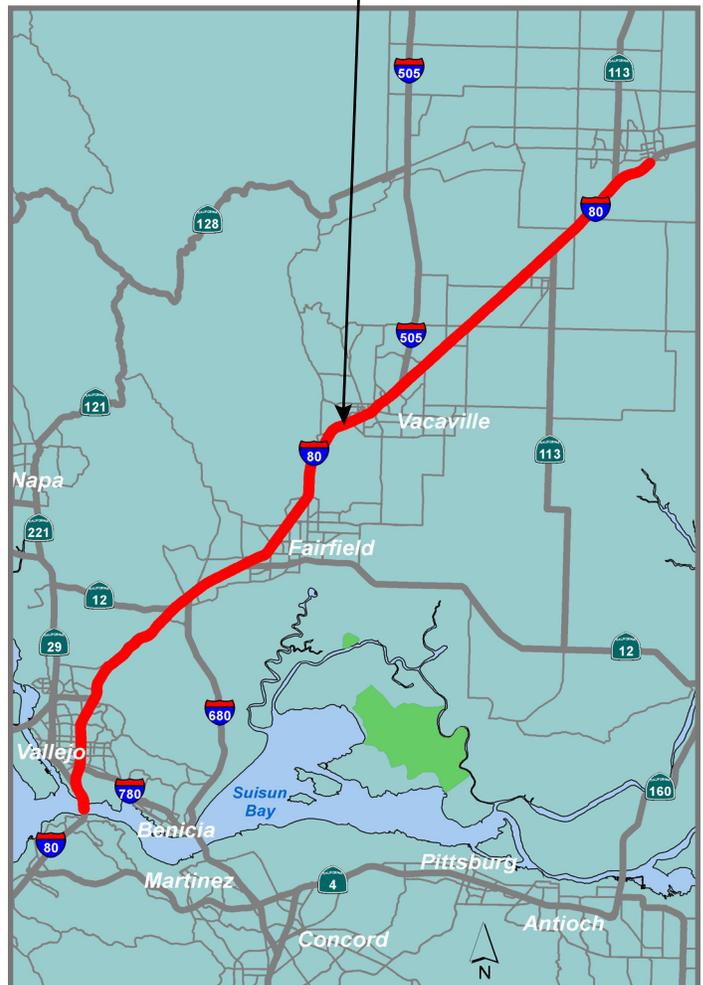
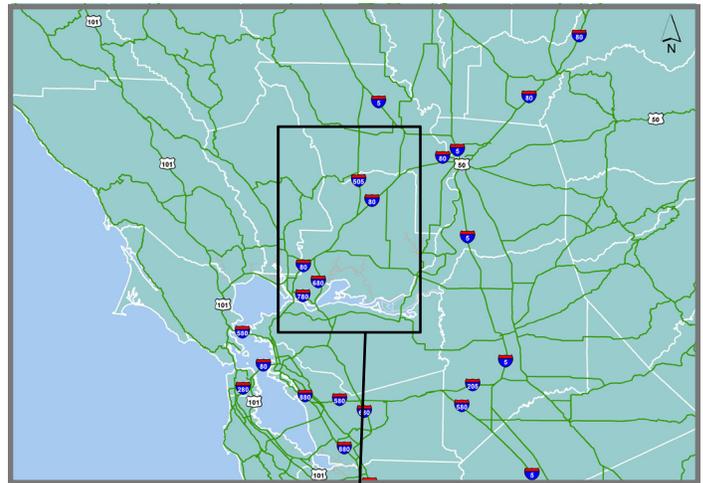
### Project Schedule

Start Construction: Summer 2008

Finish Construction: Fall 2009

### Summary

The Traffic Monitoring Stations Installation Project will fill the detection gaps along the I-80 corridor in Napa and Solano Counties.





# Proposed Improvement Projects Along I-80



SHOPP Funds

## 5 E of Tennessee St OC to Amer Canyon Rd OC PAVEMENT REHABILITATION AND METAL BEAM BARRIER UPGRADE PROJECT FACT SHEET

### The Project

This project is to rehabilitate Interstate 80 (I-80) in Napa and Solano Counties, in and near Vallejo, from 1.1 km east of Tennessee Street overcrossing to American Canyon Road overcrossing. The project will overlay asphalt concrete (AC) on existing AC pavement, and crack, seat, and overlay AC on existing Portland Concrete Cement pavement. The project will also construct a concrete barrier in the median.

### The Need

The purpose of the project is to extend the pavement service life of the existing roadway.

### Benefits

The project will reduce travel delay and improve ride quality, mobility, and safety in the corridor.

### Partnership

The project is developed by the California Department of Transportation (Caltrans).

### Project Status

The project is currently under construction and is anticipated to be complete in September 2009.

### Project Costs

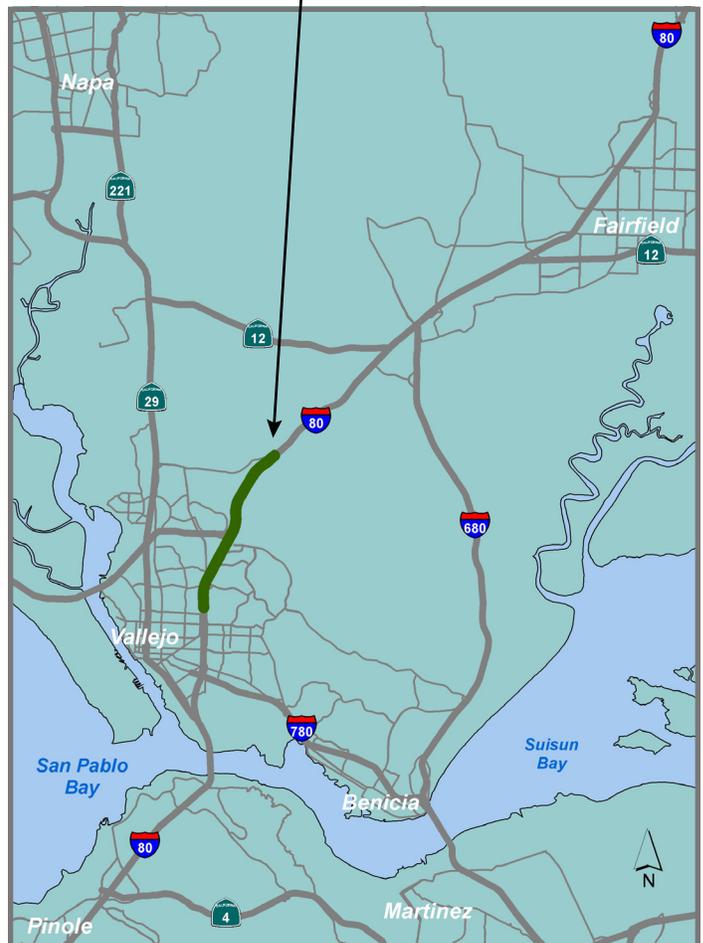
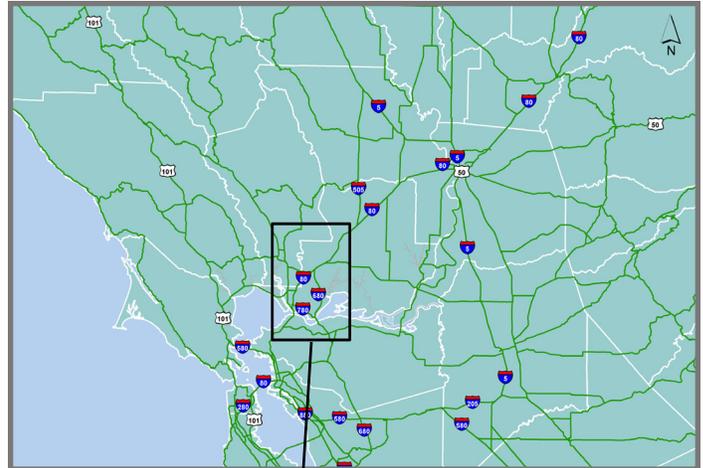
The programmed construction cost is \$35.1 million.

### Project Schedule

Start Construction: Winter 2008  
Finish Construction: Summer 2009

### Summary

This pavement rehabilitation project will provide new pavement along this stretch of I-80 in Napa and Solano Counties, improving the ride quality for motorists and extending the pavement service life in the corridor.





# Proposed Improvement Projects Along I-80



SHOPP Funds

## 6 Amer Canyon Rd OC to Green Valley Crk PAVEMENT REHABILITATION AND METAL BEAM BARRIER UPGRADE PROJECT FACT SHEET

### The Project

This project is to rehabilitate Interstate 80 (I-80), in Solano County, from American Canyon Road overcrossing in Vallejo to Green Valley Creek in Fairfield. The project will crack and seat existing portland concrete cement pavement and overlay asphalt concrete on roadway and shoulders, replace the existing metal beam guardrail with concrete barrier, and widen the inside shoulders to current standards. This project will also widen the bridge decks.

### The Need

Out of the 37.6 lane-miles (per the 2005 pavement survey), 5.8 lane-miles were identified as distressed. The purpose of the project is to extend the pavement service life of the existing roadway.

### Benefits

The project will reduce travel delay and improve ride quality, mobility, and safety in the corridor.

### Partnership

The project is developed by the California Department of Transportation (Caltrans).

### Project Status

Construction is anticipated to begin September 2009 and end October 2010.

### Project Costs

The programmed construction cost is \$37.8 million.

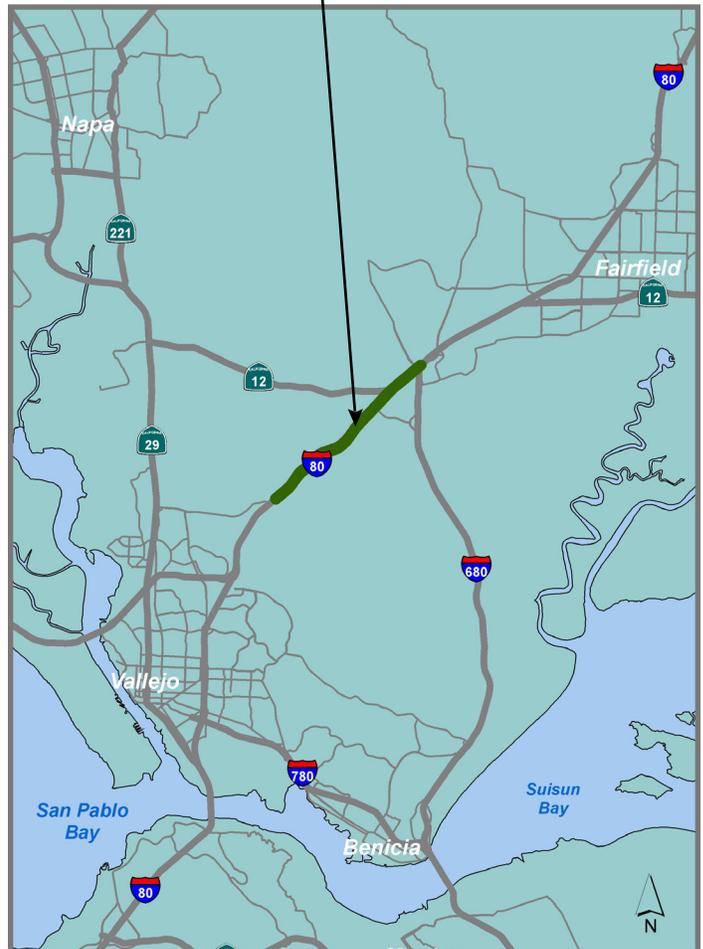
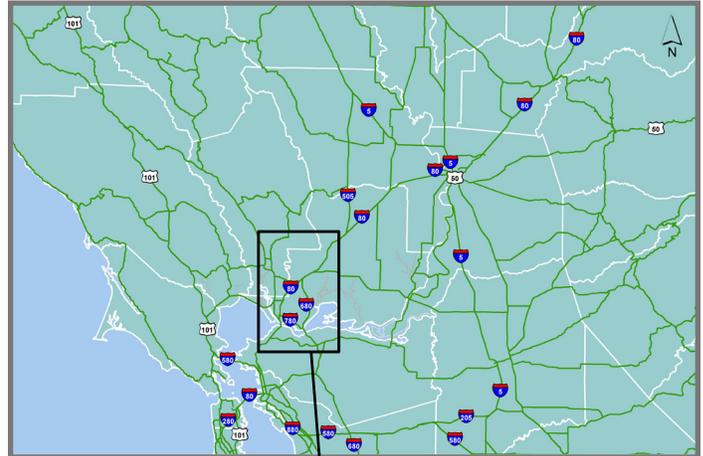
### Project Schedule

Start Construction: Summer 2009

Finish Construction: Fall 2010

### Summary

This pavement rehabilitation project will provide new pavement along this stretch of I-80 in Solano County, improving the ride quality for motorists and extending the pavement service life in the corridor.





# Proposed Improvement Projects Along I-80



SHOPP Funds

## 7 Hunter Hill Safety Roadside Rest Area HUNTER HILL SAFETY ROADSIDE REST AREA (SRRA) RESTORATION PROJECT FACT SHEET

### The Project

This project is to rehabilitate the Hunter Hill SRRA located alongside Interstate 80 in Vallejo, Solano County. The project will demolish the existing restroom facility and construct a new restroom facility, a new California Highway Patrol drop-in office, and worker breakroom.

### The Need

The purpose of the project is to provide new facilities and bring the area up to current ADA, California Occupational Safety and Health Administration, and Rest Area Master Plan standards. These standards require accessible, reliable, and safe roadside rest areas for the traveling public and those moving goods in and through California.

### Benefits

The project will give the public a better means of accessibility, safety, and comfort level. In addition, the new CHP office will add more security to the area.

### Partnership

The project is developed by the California Department of Transportation (Caltrans).

### Project Status

Construction is anticipated to begin January 2009 and end January 2012.

### Project Costs

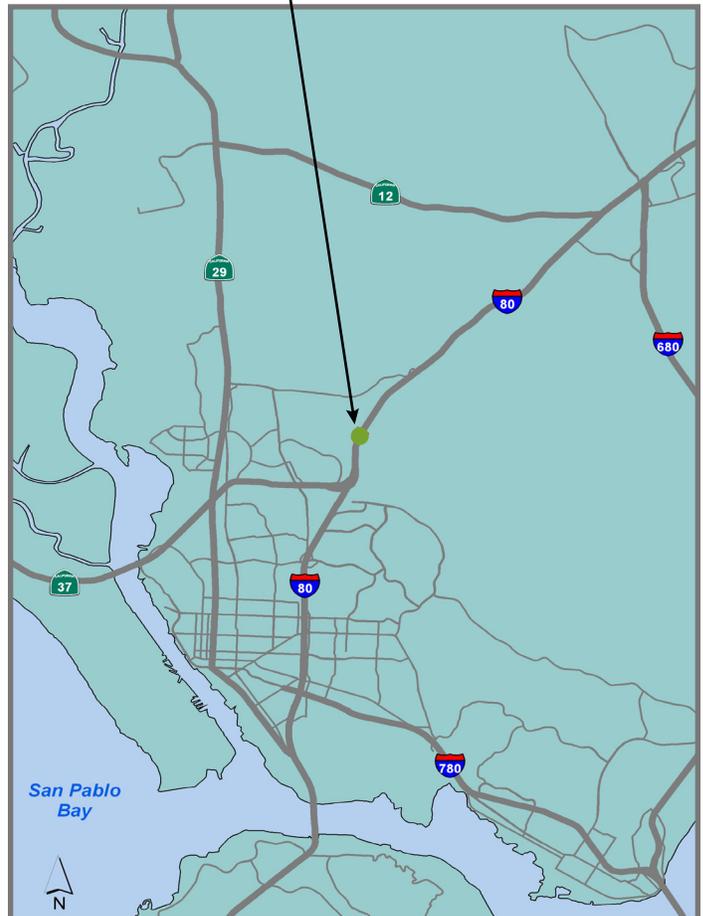
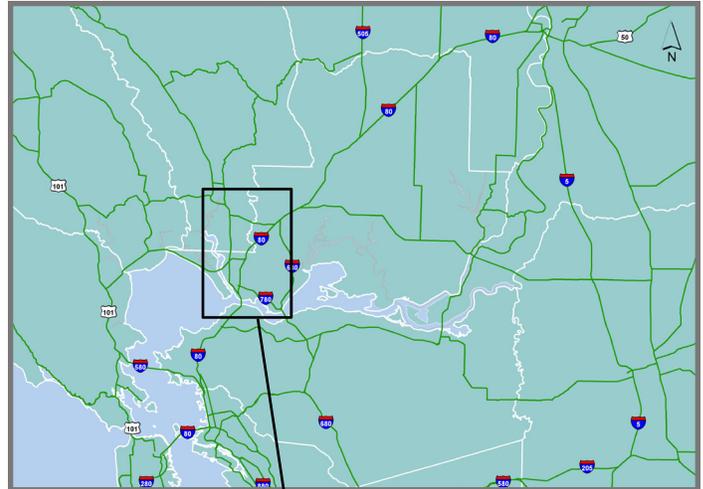
The programmed construction cost is \$8.2 million.

### Project Schedule

Start Construction: Winter 2009  
Finish Construction: Winter 2012

### Summary

The Hunter Hill SRRA Restoration project will construct new facilities and upgrade safety features, providing a more reliable and safer rest area for the public.





# Proposed Improvement Projects Along I-80



Proposition 1B - Transportation Bond

## 8 Red Top Rd to E of Air Base Pkwy HOV LANES, RED TOP ROAD TO EAST OF AIR BASE PARKWAY PROJECT FACT SHEET

### The Project

This project is to add a new 8.7-mile high occupancy vehicle (HOV) lane in each direction along Interstate 80 (I-80), in Solano County, in Fairfield, from Red Top Road to east of Air Base Parkway. The project is part of the Proposition 1B Corridor Mobility Improvement Account Program.

### The Need

I-80 is vital to interregional and regional commuting, freight movement, and recreational travel. It connects the Bay Area to Sacramento, connects Interstate 80 to the north via Interstate 505, and links the San Francisco Bay Area to the East Coast. Recent growth in Solano County has significantly increased transportation demand on the highway, necessitating the addition of an HOV lane.

### Benefits

The project will reduce travel delay, decrease congestion, and improve mobility in the corridor.

### Partnership

The project is developed through a partnership among the Solano Transportation Authority (STA), the Metropolitan Transportation Commission (MTC), the Bay Area Toll Authority (BATA), and the California Department of Transportation (Caltrans).

### Project Status

The project is currently under construction and anticipated to be complete in December 2009.

### Project Costs

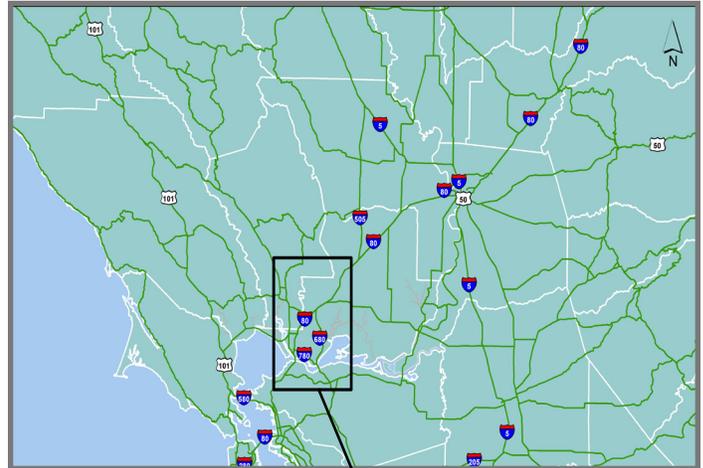
The programmed construction cost is \$53.2 million.

### Project Schedule

Start Construction: Spring 2008  
Finish Construction: Fall 2009

### Summary

The I-80 HOV Lanes Project will add capacity, thereby relieving traffic congestion and reducing delays.





# Proposed Improvement Projects Along I-80



Proposition 1B - Transportation Bond

## 9 I-80/I-680/SR-12 I/C E of Truck Scales EASTBOUND CORDELIA TRUCK SCALE RELOCATION PROJECT FACT SHEET

### The Project

This project is to rebuild and relocate the existing eastbound (EB) Cordelia Truck Scales on Interstate 80 (I-80), in Solano County, within the I-80/I-680/SR-12 interchange area. This project will also build a four-lane bridge across Suisun Creek and construct braided ramps from the new scale facility to EB I-80 and EB SR-12 ramps.

### The Need

Built in 1958, the Cordelia Truck Scales significantly contribute to the congestion on I-80 due to a large number of trucks exiting and entering I-80 and the close proximity of the scales to several interchanges. The purpose of the project is to construct new truck scales with the capacity to accommodate the anticipated 115% growth in truck traffic in the corridor by 2040; to provide traffic congestion relief in this section of I-80 by reducing truck/auto weaving and truck queuing; and to improve reliability of the system with improved capacity and up-to-date equipment.

### Benefits

The project will reduce travel delay, decrease congestion, and improve mobility in the corridor.

### Partnership

The project is developed through a partnership among the Solano Transportation Authority (STA), the Metropolitan Transportation Commission (MTC), and the California Department of Transportation (Caltrans).

### Project Status

The project is currently in the environmental phase, which is anticipated to be complete in December 2009.

### Project Costs

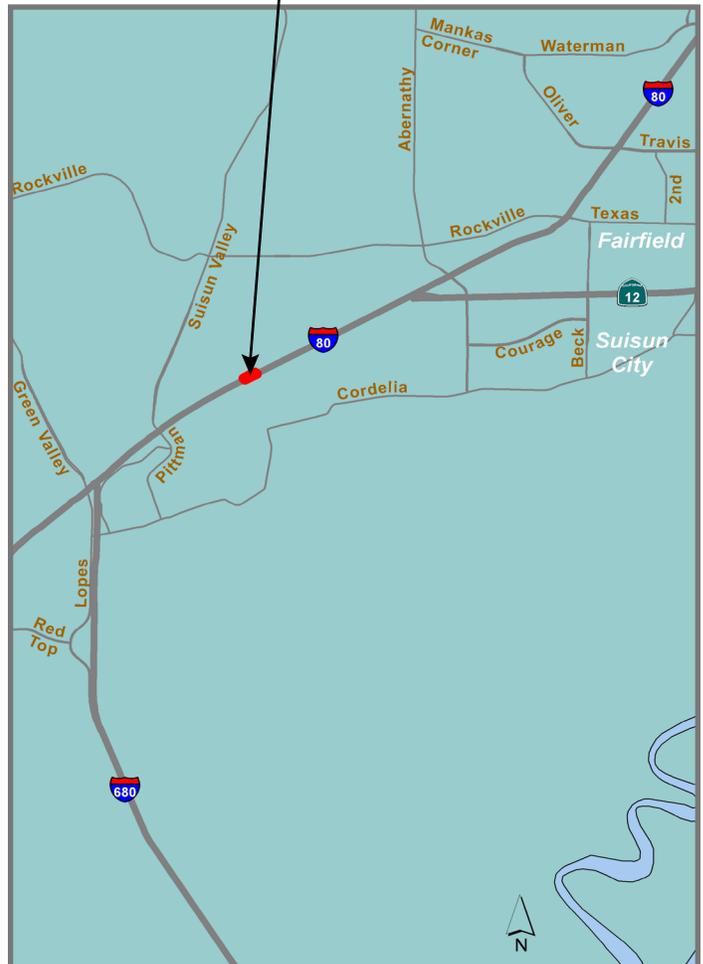
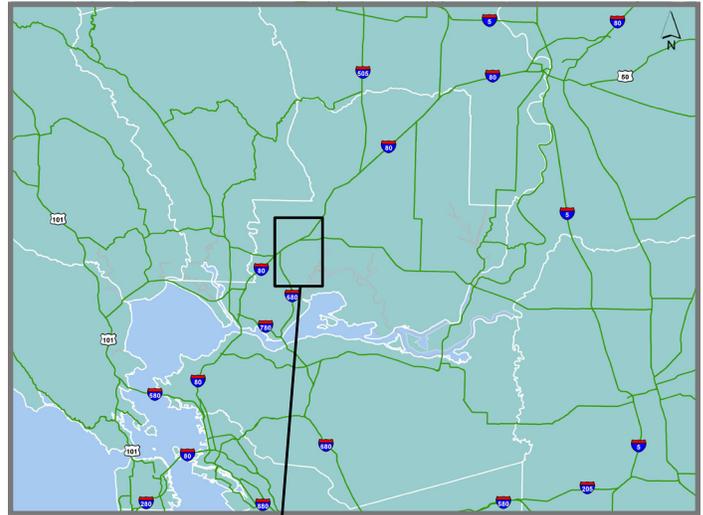
The programmed construction cost is \$64.7 million.

### Project Schedule

Start Construction: Fall 2012  
Finish Construction: Fall 2014

### Summary

The EB Cordelia Truck Relocation project will provide traffic congestion relief in this section of I-80.





# Proposed Improvement Projects Along I-80



SHOPP Funds

## 10 W of Rte12 OC to E of Air Base Pkwy OC

### CRACK, SEAT AND OVERLAY THE EXISTING PAVEMENT ON THE MAINLINE PROJECT FACT SHEET

#### The Project

This project is to resurface, restore, and rehabilitate the mainline along Interstate 80 (I-80), in Solano County, in Fairfield from 0.4 mile west of Route 12 overcrossing to 0.8 mile east of Air Base Parkway overcrossing. This project is a combination of one of three contracts split from a Proposition 1B Corridor Mobility Improvement Account Program project and a SHOPP project.

#### The Need

The purpose of the project is to extend the pavement service life of the existing roadway.

#### Benefits

The project will extend pavement service life and improve the road conditions, as well as mobility, in the corridor.

#### Partnership

The project is developed through a partnership among the Solano Transportation Authority (STA), the Metropolitan Transportation Commission (MTC), and the California Department of Transportation (Caltrans).

#### Project Status

The project is currently in the design phase, which is anticipated to be complete in August 2008.

#### Project Costs

The programmed construction cost is \$25.6 million.

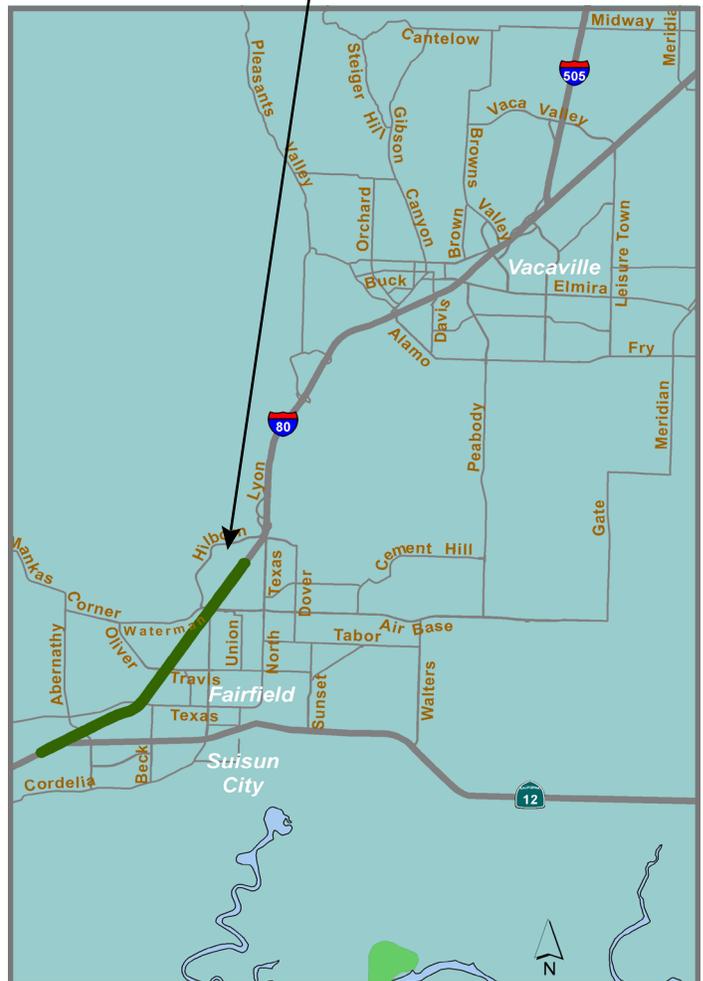
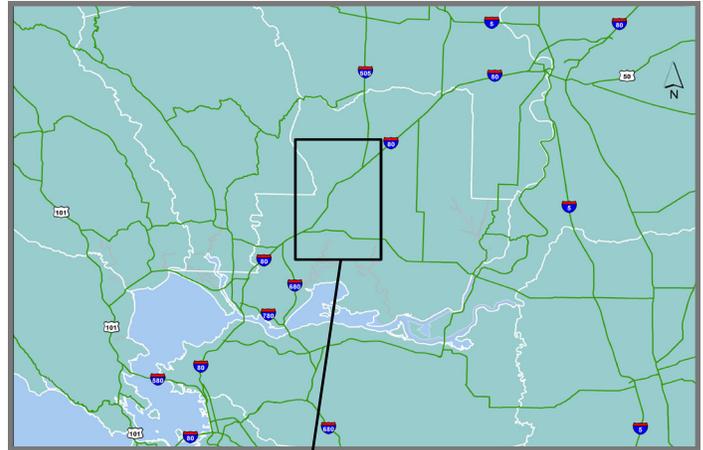
#### Project Schedule

Start Construction: Spring 2009

Finish Construction: Fall 2009

#### Summary

This pavement rehabilitation project will provide new pavement along this stretch of I-80 in Solano County, improving the ride quality for motorist and extending the pavement service life in the corridor.





# Proposed Improvement Projects Along I-80



SHOPP Funds

11

## E of Air Base Pkwy OC to E of Leisure Twn Rd OC

### CRACK, SEAT AND OVERLAY THE EXISTING PAVEMENT ON THE MAINLINE PROJECT

#### FACT SHEET

#### The Project

This project is to resurface, restore, and rehabilitate the mainline along Interstate 80, in Solano County, from 0.8 mile east of Air Base Parkway overcrossing in Fairfield to 0.7 mile east of Leisure Town Road overcrossing in Vacaville.

#### The Need

The purpose of the project is to extend the pavement service life of the existing roadway.

#### Benefits

The project will extend pavement service life and improve the road conditions, as well as mobility, in the corridor.

#### Partnership

The project is developed by the California Department of Transportation (Caltrans).

#### Project Status

This project is currently under construction and is anticipated to be complete in December 2009.

#### Project Costs

The programmed construction cost is \$43.0 million.

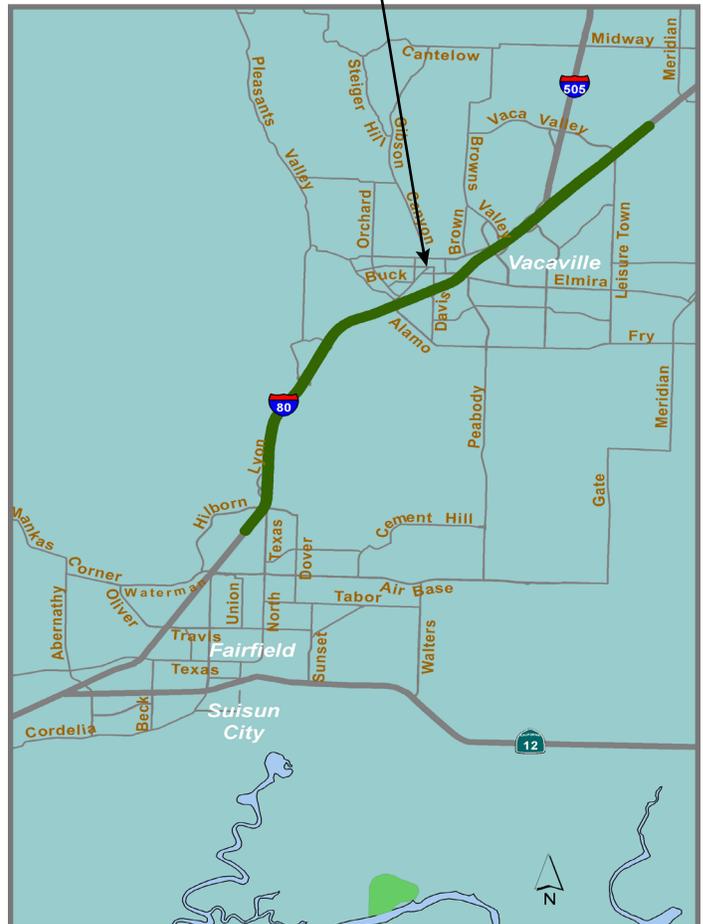
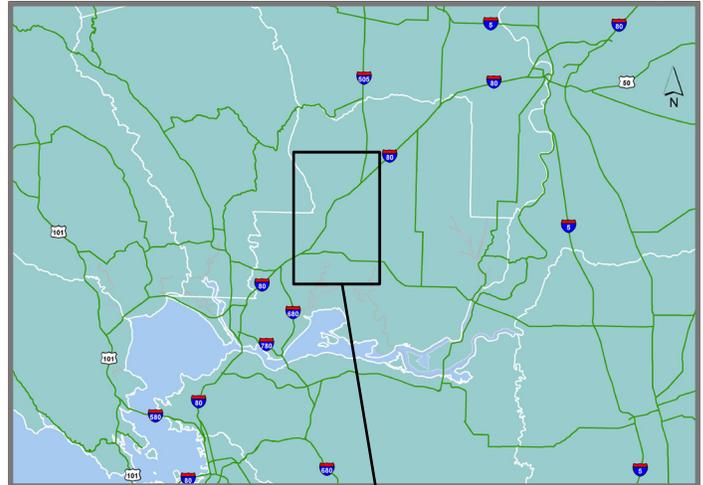
#### Project Schedule

Start Construction: Spring 2008

Finish Construction: Fall 2009

#### Summary

This pavement rehabilitation project will provide new pavement along this stretch of I-80 in Solano County, improving the ride quality for motorists and extending the pavement service life in the corridor.





# Proposed Improvement Projects Along I-80



100% Locally Funded

12

## North Texas St Interchange INTERSTATE 80 NORTH TEXAS INTERCHANGE PROJECT

### FACT SHEET

#### The Project

This project is to improve the Interstate 80 (I-80)/North Texas Street Interchange located in Fairfield, Solano County. This project will improve the I-80 eastbound ramps and three local streets (Manuel Campos Parkway (MCP), North Texas Street, and Nelson Road) within Fairfield. MCP will be extended from Dickson Hill Road to the south side of the Interchange, as an arterial road for northeast Fairfield.

#### The Need

The purpose of the project is to address improvements in land use and local access, particularly on the northeast side of Fairfield.

#### Benefits

The project will reduce travel delay, decrease congestion, and improve mobility in the corridor.

#### Partnership

The project is 100% locally funded and is developed by the City of Fairfield. The California Department of Transportation (Caltrans) will provide quality assurance as an oversight agency.

#### Project Status

The project is currently under construction and is anticipated to be complete in August 2009.

#### Project Costs

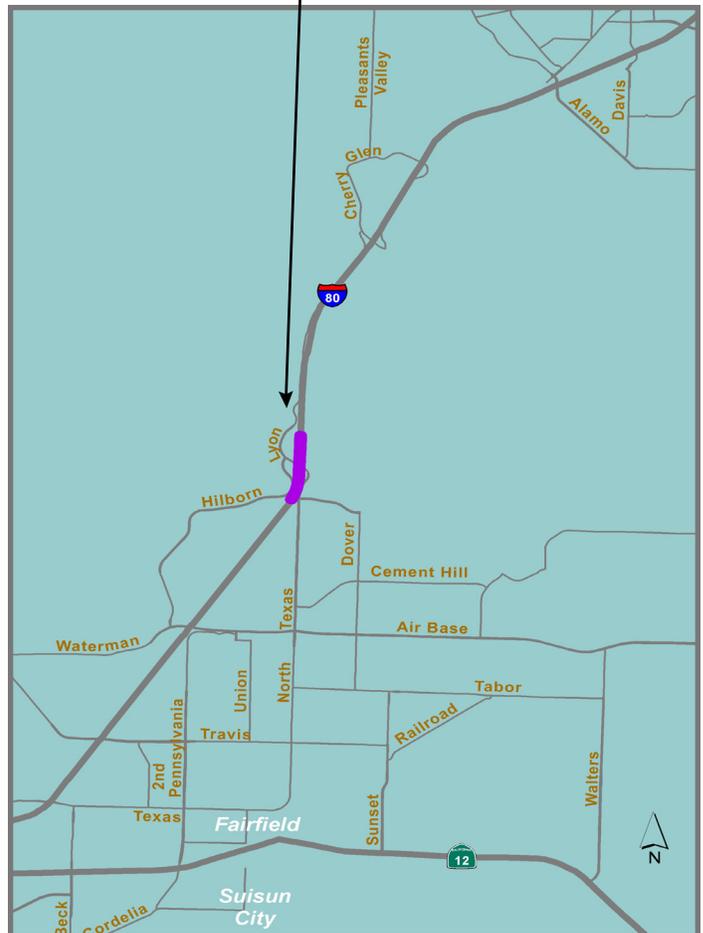
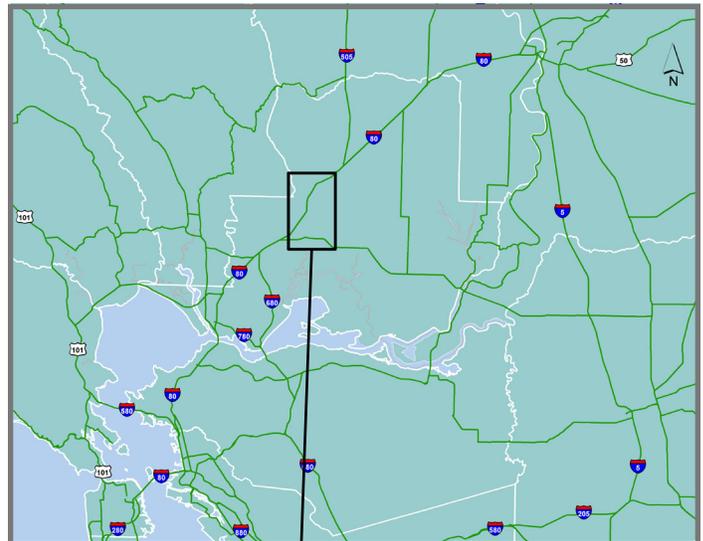
The programmed construction cost is \$16.0 million.

#### Project Schedule

Start Construction: Summer 2008  
Finish Construction: Summer 2009

#### Summary

The I-80 North Texas Interchange project will improve land use and local access in the interchange area. The MCP extension, as an arterial road, will provide needed capacity and make a safe connection to the mainline.





# Proposed Improvement Projects Along I-80



100% Locally Funded

13

## Lagoon Valley Rd Overcrossing

## LAGOON VALLEY INTERCHANGE MODIFICATION PROJECT

### FACT SHEET

#### The Project

This project is to install traffic signals, add left-turn lanes, widen a bridge, and realign on and off ramps at the Lagoon Valley interchange, in Vacaville, in Solano County.

#### The Need

Recent and planned developments have introduced additional traffic demand. The purpose of the project is to address the planned growth on the south side of the freeway in the area.

#### Benefits

The project will reduce travel delay, decrease congestion, and improve mobility in this area.

#### Partnership

The project is developed through a partnership between the City of Vacaville and the California Department of Transportation (Caltrans). Caltrans is the oversight agency.

#### Project Status

The project is currently in the planning stage, which is anticipated to be complete in November 2008.

#### Project Costs

The construction cost is estimated at \$3.0 million.

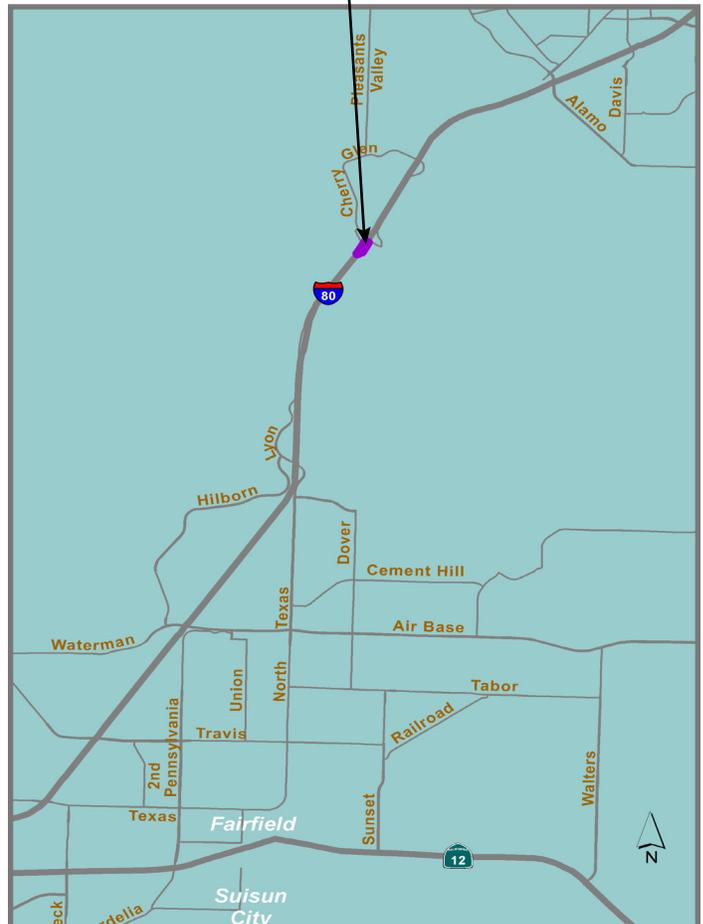
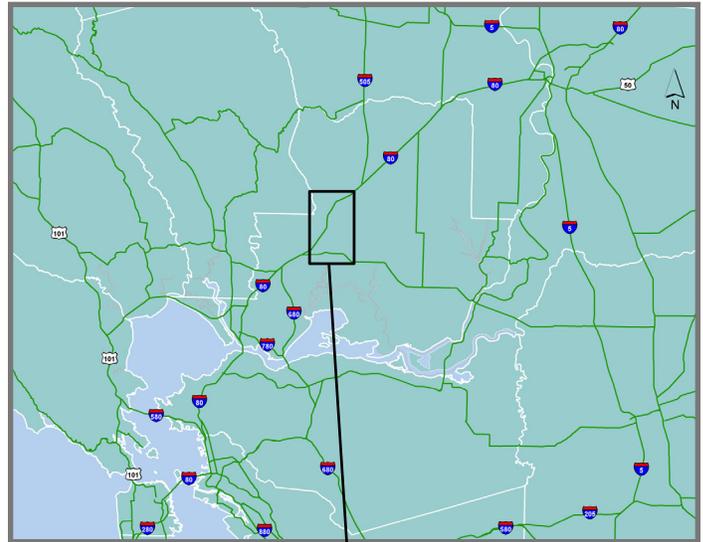
#### Project Schedule

Start Construction: Summer 2009

Finish Construction: Spring 2010

#### Summary

The Lagoon Valley Interchange Modification project will improve mobility through the interchange.





# Proposed Improvement Projects Along I-80



SHOPP Funds

## 14 W of Alamo Creek to Alamo WB Onramp ALAMO CREEK BRIDGE WIDENING AND ON-RAMP LENGTHENING PROJECT FACT SHEET

### The Project

This project is to lengthen the acceleration lane on Interstate 80 (I-80) westbound (WB) on-ramp at Alamo Drive and to widen the Alamo Creek Bridge, in Vacaville, in Solano County.

### The Need

The purpose of the project is to lengthen the on-ramp freeway entrance to current standards and widen the Alamo Creek Bridge structure.

### Benefits

The project will improve traffic safety on the westbound on-ramp and improve mobility in the corridor.

### Partnership

The project is developed by the California Department of Transportation (Caltrans).

### Project Status

The project is currently in the environmental phase, which is anticipated to be complete in September 2009.

### Project Costs

The programmed construction cost is \$4.4 million.

### Project Schedule

Start Construction: Fall 2011  
Finish Construction: Fall 2012

### Summary

The Alamo Creek Bridge Widening and On-ramp Lengthening project will improve the WB on-ramp onto I-80 and reduce accidents.

